

MINUTES

GENERAL MEETING

Wednesday, 17 November 2021

The Council Chambers 91 - 93 Bloomfield Street CLEVELAND QLD

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General Meeting

HELD AT The Council Chambers, 91 - 93 Bloomfield Street, CLEVELAND QLD ON Wednesday, 17 November 2021 AT 9.30am

1 DECLARATION OF OPENING

The Mayor declared the meeting open at 9:32am and acknowledged the Quandamooka people, who are the traditional custodians of the land on which Council meets.

The Mayor also paid Council's respect to their elders, past and present, and extended that respect to other indigenous Australians who were present.



2 RECORD OF ATTENDANCE AND LEAVE OF ABSENCE

- MEMBERS PRESENT: Cr Karen Williams (Mayor), Cr Wendy Boglary (Division 1), Cr Peter Mitchell (Division 2), Cr Paul Gollè (Division 3), Cr Lance Hewlett (Division 4), Cr Mark Edwards (Division 5), Cr Julie Talty (Deputy Mayor and Division 6), Cr Rowanne McKenzie (Division 7), Cr Tracey Huges (Division 8), Cr Adelia Berridge (Division 9), Cr Paul Bishop (Division 10)
- **EXECUTIVE LEADERSHIP TEAM:** Andrew Chesterman (Chief Executive Officer), Louise Rusan (Acting General Manager Organisational Services), David Jeanes (Acting General Manager Community & Customer Services), Dr Nicole Davis (General Manager Infrastructure & Operations), Deborah Corbett-Hall (Chief Financial Officer), Andrew Ross (General Counsel)

MINUTES: Natalie Merlehan (Corporate Meetings & Registers Coordinator)

LEAVE OF ABSENCE

Nil

COUNCILLOR ABSENCES DURING THE MEETING

Cr Mark Edwards entered the meeting at 9:33am via teleconference (after item 2) Cr Wendy Boglary left the meeting at 10:04am and returned at 10:05am (during Item 7.1) Cr Lance Hewlett left the meeting at 10:32am and returned at 10:33am (during Item 13.2) Cr Wendy Boglary left the meeting at 10:32am and returned at 10:33am (during Item 13.2) Cr Rowanne McKenzie left the meeting at 11:19am and returned at 11:21am (during Item 14.1) Cr Peter Mitchell left the meeting at 11:22am and returned at 11:23am (during Item 14.1) Cr Lance Hewlett left the meeting at 11:43am and returned at 11:46am (during Item 14.2) Cr Wendy Boglary left the meeting at 11:50am and returned at 10:52am (during Item 14.2) Cr Lance Hewlett left the meeting at 12:47pm and returned at 12:49pm (during closed session) Cr Tracey Huges left the meeting at 12:47pm and returned at 12:51pm (during closed session) Cr Lance Hewlett left the meeting at 12:54pm (before Item 19.4) and returned at 12.55pm (after Item 19.4).

2.1 TELECONFERENCING

COUNCIL RESOLUTION 2021/251

Moved by:Cr Tracey HugesSeconded by:Cr Julie Talty

That Councillor Mark Edwards be permitted to participate in the meeting via teleconference.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Mark Edwards did not participate in the vote.



3 DEVOTIONAL SEGMENT

Reverend Doctor Joseph Wood of Nazarene Theological College, and a member of the Minister's Fellowship, led Council in a brief Devotional segment.

4 RECOGNITION OF ACHIEVEMENT

4.1 LOOK, LISTEN, UNDERSTAND PROGRAM

Mayor Karen Williams recognised Council's Look, Listen, Understand education program:

I am very proud to highlight that Redland City Council is once again shining on the National Stage. This week, Council's commitment to creating positive and lasting relationships with First Nations communities has been recognised through a prestigious national excellence award.

Council's Look, Listen, Understand education program has been awarded Best Diversity and Inclusion Program in the Australian Institute of Training and Development Excellence Awards.

Our Redlands Coast is built on 40,000 years of Indigenous culture and this award is testament to our commitment as an organisation to understanding and celebrating this history.

So far, more than 528 employees have participated in the program, which includes a two-hour Indigenous information session as well as a full day of immersive training on-country on Minjerribah (North Stradbroke Island) at the invitation of the Minjerribah Moorgumpin Elders-in-Council (MMEIC).

The MMEIC, and Traditional Owner Matt Burns, have been gracious and generous in helping Council deliver this program which was developed by Council's Learning and Development Adviser Merrin McCulloch and Indigenous Partnerships and Programs Coordinator Brett Nutley.

We were up against strong competition to take out the award, with other finalists including large companies such as Origin Energy and Engineers Australia.

This award is a wonderful affirmation of Council's commitment to being a culturally safe, inclusive and resilient workplace. I would like to thank and congratulate all those involved in the design and delivery of this program; along with all of those who have participated. I am sure that they are reaping the rewards as well.

The award winning program is just one of the many initiatives in our internal Reconciliation Action Plan, Kanara Malara – One People 2019-2021.

Well done everyone.



4.2 MAYOR KAREN WILLIAMS

Deputy Mayor Julie Talty recognised Mayor Karen Williams:

I wanted to take a moment to recognise our Mayor, who on Friday was chosen by her fellow South East Queensland Mayors as the Lord Mayor's nomination on the Organising Committee of the Olympic Games. This is a substantial achievement which will benefit our City.

The organisation is tasked with organising the Brisbane 2032 Games.

Securing the Olympics in South East Queensland (SEQ) is a huge coup, and having a venue here in the Redlands is an even bigger coup; but having our Mayor sitting on the committee that will organise the Games takes it to a whole other level in terms of putting the Redlands Coast on the global radar.

Mayor Williams was at the table in March 2015 when the region's mayors started investigating SEQ Games and she has stayed the course for more than six years.

She has also represented the Lord Mayor of Brisbane at a number of important meetings with key government and Olympic representatives; flying the flag for not only the Redlands but the wider Queensland and SEQ region.

Mayor Williams was one of only three Mayors asked to attend the International Olympic Committee future host commission delegation in Brisbane earlier this year, to present the final pitch for the 2032 Games. The fact we were awarded the right to host the 2032 Games shows what a great job she did.

I have no doubt Mayor Williams will do not only the Redlands proud, but also the whole of SEQ and greater Queensland.

Congratulations Madam Mayor, well done and were very proud of you.

4.3 BUSHCARE GROUPS

Councillor Paul Bishop recognised the many Bushcare groups across Redland City:

I wanted to give a brief acknowledgement of the many Bushcare group members, many of whom gathered together on Saturday to receive thanks and acknowledgement for the years they have spent volunteering.

The volunteer contribution of those who roll up their sleeves every month for hours in our city and do a huge amount of work, not only supporting the management of the environmental estate but also the sharing their skills and knowledge, help save Council money and add diversity to the flora and fauna; regenerating landscapes in ways that is very impressive.

My thanks also goes to the officers at IndigiScapes and to all of those Bushcare groups across the City, of which we all have many.

These people really value add to our city and I think it is worth acknowledging these activities. The cultural collaboration of this day also included a presentation by Matt Burns who gave a personal account of his connection to country and his historic line which goes back thousands of generations.

For the Bushcare volunteers to hear that story really did reinforce the significance of this place and they are great contributors who are worth acknowledging.



5 RECEIPT AND CONFIRMATION OF MINUTES

COUNCIL RESOLUTION 2021/252

Moved by:Cr Peter MitchellSeconded by:Cr Paul Bishop

That the minutes of the General Meeting held on 20 October 2021 be confirmed.

CARRIED 11/0



6 DECLARATION OF PRESCRIBED CONFLICT OF INTERESTS AND DECLARABLE CONFLICT OF INTERESTS

6.1 DECLARABLE CONFLICT OF INTEREST – CR PETER MITCHELL

Councillor Peter Mitchell declared a Declarable Conflict of Interest in relation to Item 14.3 *Late Report – Submission to Draft Minjerribah Public Transport,* stating that he made a separate submission as the Divisional Councillor to the State Department, which was in response to a broad general stakeholders request from the Department.

Councillor Mitchell considered his position and was firmly of the opinion that he could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/253

Moved by:Cr Julie TaltySeconded by:Cr Paul Bishop

That Councillor Peter Mitchell may participate in future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Item 14.3 Submission to Draft Minjerribah Public Transport.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Peter Mitchell did not participate in the vote.

The motion was CARRIED as Council was of the opinion that Councillor Peter Mitchell had no greater interest in the matter than that of other people in the local government area.

Cr Mitchell voted in the affirmative for motion in Item 14.3.



6.2 DECLARABLE CONFLICT OF INTEREST – CR WENDY BOGLARY

Councillor Wendy Boglary declared a Declarable Conflict of Interest in relation to *Southern Thornlands Potential Future Growth Area* stating that she knows, and has coffee with former Councillor Murray Elliott who is a submitter in relation to this matter.

Councillor Boglary considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/254

Moved by: Cr Paul Bishop Seconded by: Cr Rowanne McKenzie

That Councillor Wendy Boglary may participate in future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 10/0

Crs Karen Williams, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Boglary did not participate in the vote.

The motion was CARRIED as Council was of the opinion that Councillor Wendy Boglary had no greater interest in the matter than that of other people in the local government area.



6.3 DECLARABLE CONFLICT OF INTEREST – CR ADELIA BERRIDGE

Councillor Adelia Berridge declared a Declarable Conflict of Interest in relation to Item 14.1 *MCU17/0057 and MCU17/0058 - Material Change of Use for a Multiple Dwelling and Reconfiguring a Lot for 3 into 2 Lots and New Road - 410-426 Boundary Road, 359-379 and 381-405 Cleveland Redland Bay Road, Thornlands,* stating that she was a submitter in relation to this item in 2018.

Councillor Berridge considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/255

Moved by:Cr Rowanne McKenzieSeconded by:Cr Wendy Boglary

That Councillor Adelia Berridge may participate in all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Item 14.1 MCU17/0057 and MCU17/0058 - Material Change of Use for a Multiple Dwelling and Reconfiguring a Lot for 3 into 2 Lots and New Road - 410-426 Boundary Road, 359-379 and 381-405 Cleveland Redland Bay Road, Thornlands.

CARRIED 6/4

Crs Karen Williams, Wendy Boglary, Paul Gollè, Lance Hewlett, Rowanne McKenzie and Paul Bishop voted FOR the motion.

Crs Peter Mitchell, Mark Edwards, Julie Talty and Tracey Huges voted AGAINST the motion.

Cr Berridge did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Adelia Berridge had no greater interest in the matter than that of other people in the local government area.

Cr Berridge voted FOR the motion in Item 14.1.

6.4 DECLARABLE CONFLICT OF INTEREST – CR JULIE TALTY

Councillor Julie Talty declared a Declarable Conflict of Interest in relation to *Southern Thornlands Potential Future Growth Area*, stating that she knows a number of people on the submitter's list for this matter, including Mr K Foxover and Ms R Young. Cr Talty stated that her relation with them is purely professional in that her interactions with them are as part of her role as Councillor, interacting with them as local business people and through the local Chamber of Commerce.

Councillor Talty considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest .

COUNCIL RESOLUTION 2021/256

Moved by:Cr Rowanne McKenzieSeconded by:Cr Wendy Boglary

Councillor Julie Talty may participate in future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Talty did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Julie Talty had no greater interest in the matter than that of other people in the local government area.



6.5 DECLARABLE CONFLICT OF INTEREST – CR WENDY BOGLARY

Councillor Wendy Boglary declared a Declarable Conflict of Interest in relation to *Medium Density Residential Amendment Package*, (as well as the Southern Thornlands Potential Future Growth Area declared previously in this meeting), stating that she has a personal friendship with former Councillor Murray Elliott who is a submitter in relation to these matters.

Councillor Boglary considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/257

Moved by: Cr Julie Talty Seconded by: Cr Paul Bishop

That Councillor Wendy Boglary may participate in future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 10/0

Crs Karen Williams, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Wendy Boglary did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Wendy Boglary had no greater interest in the matter than that of other people in the local government area.



6.6 DECLARABLE CONFLICT OF INTEREST – MAYOR KAREN WILLIAMS

Mayor Karen Williams declared a Declarable Conflict of Interest in relation to the *Medium Density Residential Amendment Package*, stating she knows a number of the submitters in relation to this matter as follows:

Ms Jacky Burkett who made a submission on the proposed amendment and is a friend and member, with Mayor Williams on the committee who help plan the annual Diner en Rouge fundraiser against domestic and family violence.

Mr Foxover who Mayor Williams frequently purchases produce from.

Ms Rebecca Young who provided a submission on this matter as president of the Chamber of Commerce and is a casual friend who Mayor Williams has had social interactions with through the Chamber.

Ms Kirsty Chesser Brown, Chief Executive Officer of Urban Development Institute of Australia (UDIA), who made a submission to the proposed amendment; Mayor Williams sits on the Redlands UDIA Advisory Panel in a voluntary capacity.

Mr Tom Taranto, Mr Steve McDonald and Mr Chris Walker who are coordinators of Redlands 2030 and made submissions to the proposed amendment, to which Mayor Williams has previously declared a conflict of interest due to a confidential legal agreement, the details of which are confidential and follows a Concerns Notice being issued in response to public comments made on the 2030 website and social media platforms.

Mr Vaughan Bowden, who, on behalf of Fitini Homes and Sutgold Pty Ltd, made a submission to the proposed amendment; Sutgold's associated business purchased Mayor Williams' mother's and brother's property many years ago with settlement occurring post Mayor William's mother's death, Mayor Williams being one of the executors of her estate.

Mayor Karen Williams considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/258

Moved by:Cr Rowanne McKenzieSeconded by:Cr Tracey Huges

That Mayor Karen Williams may participate future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to the Medium Density Residential Amendment Package.

CARRIED 10/0

Crs Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Mayor Karen Williams did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Mayor Karen Williams had no greater interest in the matter than that of other people in the local government area.

DECLARABLE CONFLICT OF INTEREST – MAYOR KAREN WILLIAMS 6.7

Mayor Karen Williams declared a Declarable Conflict of Interest in relation to the Southern Thornlands Potential Future Growth Area, stating that a submitter in this matter, Casey Jackson, appears on her register in relation to an election event from May 2012.

Mayor Williams considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/259

Moved by: **Cr Peter Mitchell Cr Rowanne McKenzie** Seconded by:

That Mayor Karen Williams may participate in all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands **Potential Future Growth Area.**

CARRIED 10/0

Crs Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion

Mayor Karen Williams did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Mayor Karen Williams had no greater interest in the matter than that of other people in the local government area.



6.8 DECLARABLE CONFLICT OF INTEREST – CR WENDY BOGLARY

Councillor Wendy Boglary declared a Declarable Conflict of Interest in relation to *Medium Density Residential Amendment Package*, stating that she sits on the Redlands Urban Development Institute of Australia (UDIA) Advisory Panel in a voluntary capacity and Ms Kirsty Chesser Brown, Chief Executive Officer of UDIA, has made a submission to the proposed amendment.

Councillor Boglary considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/260

Moved by:Cr Rowanne McKenzieSeconded by:Cr Paul Bishop

That Councillor Wendy Boglary may participate in all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Medium Density Residential Amendment Package.

CARRIED 10/0

Crs Karen Williams, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Wendy Boglary did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Wendy Boglary had no greater interest in the matter than that of other people in the local government area.



6.9 DECLARABLE CONFLICT OF INTEREST – CR ADELIA BERRIDGE

Councillor Adelia Berridge declared a Declarable Conflict of Interest in relation to the *Southern Thornlands Potential Future Growth Area*, stating that she holds relationships with the following submitters:

Lynn Roberts, Livina Woods, Chris Walker and Tom Taranto of Redlands 2030.

Pam Spence of Birkdale Progress Association and Thorne Communicators.

Rebecca Young of the Redlands Chamber of Commerce, Redlands softball and Sharks softball clubs.

Councillor Berridge considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/261

Moved by: Cr Paul Bishop Seconded by: Cr Lance Hewlett

That Councillor Adelia Berridge may participate in all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, and Paul Bishop voted FOR the motion.

Cr Adelia Berridge did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Adelia Berridge had no greater interest in the matter than that of other people in the local government area.



6.10 DECLARABLE CONFLICT OF INTEREST – CR LANCE HEWLETT

Councillor Lance Hewlett declared a Declarable Conflict of Interest in relation to *Southern Thornlands Potential Future Growth Area*, stating that he has a personal friendship with former Councillor Murray Elliott who is a submitter in relation to this matter.

Councillor Hewlett considered his position and was firmly of the opinion that he could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/262

Moved by: Cr Julie Talty Seconded by: Cr Tracey Huges

That Councillor Lance Hewlett may participate all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 9/0

Crs Karen Williams, Peter Mitchell, Paul Gollè, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Wendy Boglary was not present when the motion was put.

Cr Lance Hewlett did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Lance Hewlett had no greater interest in the matter than that of other people in the local government area.



6.11 DECLARABLE CONFLICT OF INTEREST – CR ADELIA BERRIDGE

Councillor Adelia Berridge declared a Declarable Conflict of Interest in relation to the *Southern Thornlands Potential Future Growth Area*, stating that she has relationships with submitters Amy Glade, who was an authority on her election campaign, and also Debbie Pointing from the Koala Action Group, to which she is a member of.

Councillor Berridge considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest at both future Statutory and Non-Statutory meetings of Council.

COUNCIL RESOLUTION 2021/263

Moved by:Cr Wendy BoglarySeconded by:Cr Paul Bishop

That Councillor Adelia Berridge may participate all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges and Paul Bishop voted FOR the motion

Cr Adelia Berridge did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Adelia Berridge had no greater interest in the matter than that of other people in the local government area.



6.12 DECLARABLE CONFLICT OF INTEREST – CR WENDY BOGLARY

Councillor Wendy Boglary declared a Declarable Conflict of Interest in relation to *Southern Thornlands Potential Future Growth Area*, stating that a submitter on this matter, Toni Bowler is known to her and they occasionally go to coffee.

Councillor Boglary considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/264

Moved by: Cr Paul Bishop Seconded by: Cr Rowanne McKenzie

That Councillor Wendy Boglary may participate all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Potential Future Growth Area.

CARRIED 10/0

Crs Karen Williams, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Wendy Boglary did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Wendy Boglary had no greater interest in the matter than that of other people in the local government area.



6.13 DECLARABLE CONFLICT OF INTEREST – CR ADELIA BERRIDGE

Councillor Adelia Berridge declared a Declarable Conflict of Interest in relation to the *Southern Thornlands Potential Future Growth Area*, stating that a submitter on this matter, Toni Bowler is known to her and they occasionally go to coffee.

Councillor Berridge considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest.

COUNCIL RESOLUTION 2021/265

Moved by:Cr Rowanne McKenzieSeconded by:Cr Tracey Huges

That Councillor Adelia Berridge may participate all future Statutory Meetings (including voting on the matter), Non-Statutory and Informal Meetings of Council in relation to Southern Thornlands Growth Area.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges and Paul Bishop voted FOR the motion.

Cr Adelia Berridge did not participate in the vote on this matter.

The motion was CARRIED as Council was of the opinion that Councillor Adelia Berridge had no greater interest in the matter than that of other people in the local government area.



6.14 DECLARABLE CONFLICT OF INTEREST – CR LANCE HEWLETT

Councillor Lance Hewlett declared a Declarable Conflict of Interest in relation to Item 19.4 2021/22 Sponsorship Round 1 Report - Applications over \$15,000, stating that his sister Suzie Tafolo is on the Committee of the Redlands Coast Chamber of Commerce.

Councillor Lance Hewlett considered his position and chose to exclude himself from the meeting while this matter was being discussed and the vote taken.

7 MATTERS OUTSTANDING FROM PREVIOUS COUNCIL MEETINGS

7.1 EXPRESSIONS OF INTEREST CAMPAIGN - REDLANDS COAST TOURIST AND COMMUNITY DESTINATION, MACARTHUR ST, ALEXANDRA HILLS

At the General Meeting 2 December 2020 (Item 19.2 refers), Council resolved as follows:

That Council resolves as follows:

- 1. To note the outcomes of the Expressions of Interest Campaign for a Tourist Park and associated community uses that has now finished, and that no tourism-related proposals were received.
- 2. To hold discussions with proponents of non-tourism related purposes to understand how other proposals may fit into the planning for development of the land that align with Council's policies and plans.
- 3. To workshop with Councillors, the outcome of these discussions.
- 4. To provide a further report to Council in regards to the site upon completion of item 3 above.
- 5. That this report and attachments remain confidential to ensure proposed commercial arrangements and details pertaining to individuals are kept private, subject to maintaining the confidentiality of legally privileged and commercial in confidence information.

7.2 INVESTIGATIONS TO POTENTIALLY ACQUIRE ADDITIONAL LAND FOR SPORT AND RECREATION PURPOSES

At the General Meeting 18 December 2019 (Item 19.3 refers), Council resolved as follows:

That the petition be received and referred to the Chief Executive Officer for consideration and a report to the local government.

7.3 NOTICE OF MOTION - MACLEAY ISLAND CAMP GROUND

At the General Meeting 18 August 2021 (Item 17.1 refers), Council resolved as follows:

That Council resolves as follows:

- 1. That the Chief Executive Officer prepare a report to come back to Council by 15 December 2021 on the options for a proposed 12 month camping ground trial at 79 to 87 Coast Road, Sandpiper Beach, Macleay Island.
- 2. That the proposal is for a basic facility only catering for kayakers, mountain bikers and boaties with a maximum 3 day stay. The intention would be for a local community group to run the camping ground on behalf of Council.
- 3. That the report should consider planning scheme, local laws, risks and budget considerations.

A report will be brought to Council by 15 December 2021.



7.4 MAYORAL MINUTE - STATE GOVERNMENT BULK WATER REBATE

At the General Meeting 15 September 2021 (Mayoral Minute Item 8.1 refers), Council resolved as follows:

That Council resolves as follows:

- 1. To write to the State Government and Seqwater and request that they support Council's existing concealed leaks policy by implementing a concealed leaks policy and associated processes to cover the State Government's bulk water component of water consumption in Redland City.
- 2. To seek support for the policy change from Redlands Coast Members of Parliament. Through a petition seeking public support to State Parliament to be published on Council's website and shared through media.
- 3. To request that any decision by the Government to provide a concealed leaks rebate be conveyed to Council by February 2022, to allow time for Council 2022-23 Budget deliberations.
- 4. Subject to the State Government implementing a bulk water rebate, Council considers any policy change to complement the State's bulk water rebate to further assist ratepayers.

A report will be brought to a future meeting of Council by February 2022.



7.5 MAYORAL MINUTE - OLYMPIC LEGACY WORKING GROUP

At the General Meeting 20 October 2021 (Mayoral Minute Item 8.1 refers), Council resolved as follows:

- 1. Council requests officers investigate options to establish a Redlands Coast Olympic and Paralympic Legacy Working Group.
- 2. In undertaking these investigations, Officers are asked to consider:
 - a. Objectives: Identify objectives for the Redlands Coast Olympic and Paralympic Games Legacy Working Group that help identify and advise on community legacy opportunities created from the Brisbane 2032 Olympic and Paralympic Games.
 - b. Governance: Investigate a governance model that supports the overall objectives of the Working Group while also providing the necessary flexibility to ensure it remains dynamic and contemporary throughout the Olympic and Paralympic Games development.
 - c. Membership: Investigate membership composition that supports the Legacy Working Group's objectives. This should include external representatives in disciplines including but not limited to; transport, sport and recreation, community and cultural development, education and economic development.

It is envisaged that membership will be honorary, unremunerated and appointed for a fixed term, with the flexibility to appoint new members as agreed by the Legacy Working Group.

d. Reporting: Provide options for the frequency and format of meetings that supports the Working Group's objectives and provides regional legacy opportunities for the Redlands Coast community. This should include options for the Legacy Working Group to communicate with other regional Olympic and Paralympic working groups and authorities.

The Mayor, as Council's appointed Council of Mayors (SEQ) representative, will utilise the Legacy Working Group's advice and recommendations to advocate for regional legacy opportunities through the Council of Mayors (SEQ) and other stakeholders.



7.6 NOTICE OF MOTION - INVESTIGATION AND CONSIDERATION AROUND NEW COMMERCIAL DEVELOPMENTS LOCATED WITHIN EXISTING DISTRICT CENTRES

At the General Meeting 20 October 2021 (Item 17.1 refers), Council resolved as follows:

That Council resolves as follows:

- 1. To investigate and consider the issues related to the new commercial development which is located within existing district centres to include, but not limited to:
 - a) Vehicular and pedestrian access, safety and lighting
 - b) Potential reduction in car parking, traffic and amenity impacts on the surrounding road network
 - c) Neighbourhood and operating characteristics of the use
 - d) Relevant benchmark provisions of other local government authorities in South East Queensland
 - e) Options and recommendations relating to the appropriateness of existing assessment benchmarks, including the scale and intensity of the proposed use i.e. number of customer visits and gross floor area, as well as the levels of assessment.
- Prepare a major amendment as part of the next general amendment package, pursuant to Part 4 of the Minister's Guidelines and Rules under the Planning Act 2016, if required, incorporating the proposed changes to City Plan as supported by Council.
- 3. That officers prepare a report to Council addressing the above by the end of January 2022.



7.7 OUTCOMES OF ECONOMIC NEEDS ASSESSMENT – SHORT STAY FACILITIES FOR SELF-CONTAINED RECREATIONAL VEHICLES AND CARAVANS ON THE REDLANDS COAST

At the General Meeting 19 May 2021 (Item 15.2 refers), Council resolved as follows:

- 1. To note the contents of the report including the executive summary of the Economic Needs Assessment at Attachment 1.
- 2. To endorse Council's role as facilitator or advocate for the establishment of *a* short stay facilities for self-contained recreational vehicles and caravans.
- 3. To invite Expressions of Interest in accordance with s.228 of the Local Government Regulation 2012 from community or not-for-profit organisations to operate and manage a short stay facility for self-contained recreational vehicles and caravans for Council owned or managed land identified as preferred sites within the report.
- 4. That a report be brought back to Council outlining the outcomes of the Expressions of Interest process.
- 5. To continue to support and work with existing commercial campground and caravan park operators.



8 MAYORAL MINUTE

Nil



9 PUBLIC PARTICIPATION

MOTION TO ADJOURN MEETING AT 10:05AM

COUNCIL RESOLUTION 2021/266

Moved by: Cr Wendy Boglary Seconded by: Cr Peter Mitchell

To adjourn the meeting for a 15 minute public participation segment.

CARRIED 11/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

1. Ms Gayle Dean of, a resident of Macleay Island, addressed Council regarding the camping trial on Sandpiper Bay, Macleay Island.

MOTION TO RESUME MEETING AT 10:15AM

COUNCIL RESOLUTION 2021/267

Moved by: Cr Paul Bishop Seconded by: Cr Julie Talty

That the meeting proceedings resume.

CARRIED 11/0



10 PETITIONS AND PRESENTATIONS

10.1 PETITION CR JULIE TALTY – TEMPORARY MACLEAY ISLAND CAMPGROUND

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/268

Moved by: Cr Julie Talty Seconded by: Cr Paul Bishop

That the petition is of an operational nature and be received and referred to the Chief Executive Officer for consideration.

CARRIED 11/0



10.2 PETITION CR ADELIA BERRIDGE - PROPOSED DEVELOPMENT MCU21/0109

COUNCIL RESOLUTION 2021/269

Moved by:Cr Adelia BerridgeSeconded by:Cr Wendy Boglary

That the petition be received.

CARRIED 11/0



11 MOTION TO ALTER THE ORDER OF BUSINESS

Nil

12 REPORTS FROM THE OFFICE OF THE CEO

Nil



13 REPORTS FROM ORGANISATIONAL SERVICES

| 13.1 OCTOBER 202 | 21 MONTHLY FINANCIAL REPORT |
|-----------------------------|---|
| Objective Reference: | A5995235 |
| Authorising Officer: | Deborah Corbett-Hall, Chief Financial Officer |
| Responsible Officer: | Deborah Corbett-Hall, Chief Financial Officer |
| Report Author: | Udaya Panambala Arachchilage, Corporate Financial Reporting Manager |
| Attachments: | 1. Monthly Financial Report RCC Oct 21 🗓 |

PURPOSE

To note the year to date financial results as at 31 October 2021.

BACKGROUND

Council adopts an annual budget and then reports on performance against the budget on a monthly basis. This is not only a legislative requirement but enables the organisation to periodically review its financial performance and position and respond to changes in community requirements, market forces or other outside influences.

ISSUES

Capital carryover budget 2020-21

Council adopted a carryover budget on 18 August 2021 to accommodate capital works straddling two financial years. The attached monthly financial report for October includes the carryover budget adopted by Council. The differences between the carryover budget figures contained in the attached report and those published on 18 August 2021 are due to the actual opening balances on 1 July 2021. The final audited opening balances, together with other revisions to the budget, will be adopted as part of the revised budget in early 2022, and will reconcile to the financial management system and end of year accounts finalisation process.

Monitoring of the capital program progress

As mentioned in the risk management section below, the Executive Leadership Team reviews the progress of the capital program on a regular basis. Over the last nineteen months, the global pandemic has played a role in the procurement lead time, availability of contractors and price of materials. Constant focus, review and mitigation where possible is occurring by the organisation's senior leaders and these factors are considerations when management reviews the organisation risk registers.

2021-22 Budget review

Council officers are currently compiling submissions for a budget review. The monthly analysis will be consolidated to update Council's budget for the 2021-22 financial year. Officers are planning to table a revised budget for Council's consideration in February 2022.



STRATEGIC IMPLICATIONS

Council has either achieved or favourably exceeded the following key financial stability and sustainability ratios as at the end of October 2021.

- Operating surplus ratio
- Net financial liabilities
- Level of dependence on general rate revenue
- Ability to pay our bills current ratio
- Ability to repay our debt debt servicing ratio
- Cash balance
- Cash balances cash capacity in months
- Longer term financial stability debt to asset ratio
- Interest coverage ratio
- Operating performance

The following ratios did not meet the target at the end of October 2021:

• Asset sustainability ratio

The asset sustainability ratio did not meet the target at the end of October 2021 and continues to be a stretch target for Council with renewal spends of \$6.55M and depreciation expense of \$19.52M year to date on infrastructure assets. This ratio is an indication of how Council currently maintains, replaces and renews its existing infrastructure assets as they reach the end of their useful lives. Capital spend on non-renewal projects increases the asset base and therefore increases depreciation expense, resulting in a lower asset sustainability ratio.

Council's Capital Portfolio Prioritisation Administrative Directive demonstrates its commitment to maintaining existing infrastructure and the adoption of a renewal strategy for its existing assets ahead of 'upgrade' and/or 'new' works.

Legislative Requirements

The October 2021 financial report is presented in accordance with the legislative requirement of section 204(2) of the *Local Government Regulation 2012,* requiring the Chief Executive Officer to present the financial report to a monthly Council meeting.

Risk Management

The October 2021 financial report has been noted by the Executive Leadership Team and relevant officers who can provide further clarification and advice around actual to budget variances.

Financial

There is no direct financial impact to Council as a result of this report; however it provides an indication of financial outcomes at the end of October 2021.

People

Nil impact expected as the purpose of the attached report is to provide financial information to Council based upon actual versus budgeted financial activity.



Environmental

Nil impact expected as the purpose of the attached report is to provide financial information to Council based upon actual versus budgeted financial activity.

Social

Nil impact expected as the purpose of the attached report is to provide financial information to Council based upon actual versus budgeted financial activity.

Human Rights

There are no human rights implications for this report as the purpose of the attached report is to provide financial information to Council based upon actual versus budgeted financial activity.

Alignment with Council's Policy and Plans

This report has a relationship with the following items of Council's Our Future Redlands – A Corporate Plan to 2026 and Beyond:

Efficient and effective organisation objectives

- 7.1 Improve the efficiency and effectiveness of Council's service delivery to decrease costs, and enhance customer experience and community outcomes.
- 7.4 Demonstrate good governance through transparent, accountable processes and sustainable practices and asset management.

CONSULTATION

| Consulted | Date | Comment |
|---|---------------------------|--|
| Council departmental officers | Year to date October 2021 | Consulted on financial results and outcomes |
| Financial Services Group officers | Year to date October 2021 | Consulted on financial results and outcomes |
| Executive Leadership Team and Senior Leadership Team | Year to date October 2021 | Recipients of variance analysis between actual and budget. Consulted as required |

OPTIONS

Option One

That Council resolves to note the financial position, results and ratios for October 2021 as presented in the attached Monthly Financial Report.

Option Two

That Council resolves to request additional information.

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/270

Moved by: Cr Julie Talty Seconded by: Cr Paul Bishop

That Council resolves to note the financial position, results and ratios for October 2021 as presented in the attached Monthly Financial Report.

CARRIED 11/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.







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1. EXECUTIVE SUMMARY

This monthly report illustrates the financial performance and position of Redland City Council compared to its adopted budget at an organisational level for the period ended 31 October 2021. The year to date annual revised budget referred to in this report incorporates the changes from budget capital carryovers adopted by Council on 18 August 2021.

| Key Financial Highlights and Overview | | | | | | |
|---------------------------------------|-----------------------------|---------------|---------------|-----------------|-------------------|---|
| Key Financial Results (\$000) | Annual Revised Budget | YTD Budget | YTD Actual | YTD Variance | YTD Variance % | Status Favourable ✓ Unfavourable ■ |
| Operating Surplus / (Deficit) | 43 | 35,571 | 40,194 | 4,623 | 13% | ✓ |
| Recurrent Revenue | 310,942 | 135,714 | 137,236 | 1,522 | 1% | ✓ |
| Recurrent Expenditure | 310,899 | 100,143 | 97,042 | (3,101) | -3% | Image: A set of the set of the |
| Capital Works Expenditure | 102,732 | 20,146 | 17,201 | (2,945) | -15% | ✓ |
| Closing Cash & Cash Equivalents | 196,457 | 221,816 | 189,084 | (32,732) | -15% | * |

Council reported a year to date operating surplus of \$40.19M which is favourable to budget by \$4.62M mainly on account of lower than budgeted expenditure on materials and services and higher fees income. The second quarter rate notices were issued in October 2021.

The Infrastructure and Operations (I&O) Department presented a briefing to Council that addressed the supply chain issues that are contributing to forecast underspends to the FY2021-22 capital works portfolio. This briefing included mitigation strategies that addressed the risks to delivering the capital works which was agreed to and resolved by Council at the General Meeting held on 20 October 2021.

Council's cash balance is behind budget mainly due to higher than anticipated payments to suppliers and lower than expected receipts from customers. Constrained cash reserves represent 60% of the cash balance.





2. KEY PERFORMANCE INDICATORS

* The net financial liabilities ratio exceeds the target range when current assets are greater than total liabilities (and the ratio is negative) ** The interest coverage ratio exceeds the target range when interest revenue is greater than interest expense (and the ratio is negative)



3. STATEMENT OF COMPREHENSIVE INCOME

| STATEME | STATEMENT OF COMPREHENSIVE INCOME | | | | |
|---|-----------------------------------|----------------------------|-----------------|-----------------|-------------------|
| For the | period ending | 31 October 2 | 021 | | |
| | Annual | Annual | YTD | YTD | YTD |
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual \$000 | Variance \$000 |
| Recurrent revenue | | | | | |
| Rates charges | 111,574 | 111,650 | 55,439 | 55,571 | 132 |
| Levies and utility charges | 170,378 | 170,378 | 69,625 | 69,612 | (13) |
| Less: Pensioner remissions and rebates | (3,486) | (3,486) | (1,668) | (1,735) | (67) |
| Fees | 15,337 | 15,337 | 5,523 | 6,455 | 932 |
| Rental income | 1,067 | 1,067 | 360 | 401 | 41 |
| Interest received | 2,037 | 2,037 | 634 | 546 | (88) |
| Sales revenue | 3,682 | 3,682 | 1,395 | 1,800 | 405 |
| Other income | 469 | 469 | 299 | 504 | 205 |
| Grants, subsidies and contributions | 9,496 | 9,808 | 4,107 | 4,082 | (25) |
| Total recurrent revenue | 310,554 | 310,942 | 135,714 | 137,236 | 1,522 |
| | | | | , | |
| Recurrent expenses | | | | | |
| Employee benefits | 97,172 | 97,295 | 32,210 | 32,477 | 267 |
| Materials and services | 145,459 | 145,725 | 45,351 | 42,855 | (2,496) |
| Finance costs | 2,007 | 2,007 | 656 | 643 | (13) |
| Depreciation and amortisation | 67,563 | 67,563 | 22,522 | 21,725 | (797) |
| Other expenditure | 522 | 522 | 128 | 109 | (19) |
| Net internal costs | (2,213) | (2,213) | (724) | (767) | (43) |
| Total recurrent expenses | 310,511 | 310,899 | 100,143 | 97,042 | (3,101) |
| OPERATING SURPLUS / (DEFICIT) | 43 | 43 | 35,571 | 40,194 | 4,623 |
| Capital revenue | | | | | |
| Grants, subsidies and contributions | 22,133 | 28,638 | 4,384 | 4,351 | (33) |
| Non-cash contributions | 2,461 | 2,461 | 793 | - | (793) |
| Total capital revenue | 24,594 | 31,099 | 5,177 | 4,351 | (826) |
| Capital expenses | | | | | |
| (Gain) / loss on disposal of non-current assets | 289 | 289 | 42 | (295) | (337) |
| Total capital expenses | 289 | 289 | 42 | (295) | (337) |
| TOTAL INCOME | 335,148 | 342,041 | 140,891 | 141,587 | 696 |
| TOTAL EXPENSES | 310,799 | 311,188 | 100,185 | 96,747 | (3,438) |
| NET RESULT | 24,349 | 30,853 | 40,706 | 44,840 | 4,134 |
| Other comprehensive income / (loss) | | | | | |
| Items that will not be reclassified to a net result | | | | | |
| Revaluation of property, plant and equipment | - | - | - | - | - |
| TOTAL COMPREHENSIVE INCOME | 24,349 | 30,853 | 40,706 | 44,840 | 4,134 |
| TOTAL COMPREHENSIVE INCOME | 24,545 | 00,000 | 40,706 | 44,040 | 4,154 |

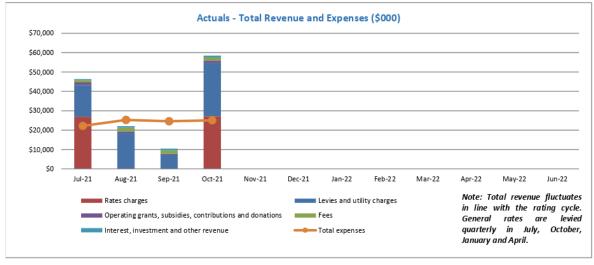


3. STATEMENT OF COMPREHENSIVE INCOME - CONTINUED

| LEVIES AN | D UTILITY CHAR | | YSIS | | |
|--|--|---|---|---|----------------------------------|
| For the | period ending 31 | October 20 | 21 | | |
| | Annual | Annual | YTD | YTD | YTD |
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual \$000 | Variance \$000 |
| Levies and utility charges | | | | | |
| Refuse collection rate charge | 30,931 | 30,931 | 10,220 | 10,282 | 62 |
| SES separate charge | 514 | 514 | 256 | 255 | (1) |
| Environment separate charge | 10,802 | 10,802 | 5,300 | 5,355 | 55 |
| Separate charge landfill remediation | 3,473 | 3,473 | 1,853 | 1,722 | (131) |
| Wastewater charges | 50,354 | 50,354 | 24,863 | 24,911 | 48 |
| Water access charges | 20,949 | 20,949 | 10,442 | 10,389 | (53) |
| Water consumption charges | 53,355 | 53,355 | 16,691 | 16,698 | 7 |
| Total levies and utility charges | 170,378 | 170,378 | 69,625 | 69,612 | (13) |
| | ALS AND SERVIC period ending 31 Annual | | | YTD | YTD |
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual \$000 | Variance \$000 |
| Materials and services | | | | | |
| Contractors | 37,447 | 38,123 | 9,034 | 8,669 | (365) |
| Consultants | 2,775 | 3,030 | 853 | 491 | (362) |
| Other Council outsourcing costs* | 26,444 | 24,926 | 8,275 | 7,215 | (1,060) |
| outer obtainer outooutering costs | | | | | |
| | 54,490 | 55,245 | 17,487 | 17,134 | (353) |
| Purchase of materials Office administration costs | 54,490 7,194 | 55,245 7,195 | 17,487 4,053 | 17,134 3,829 | (353) (224) |
| Purchase of materials Office administration costs | | · · · | · · · | , | |
| Purchase of materials Office administration costs Electricity charges | 7,194 | 7,195 | 4,053 | 3,829 | (224) |
| Purchase of materials Office administration costs Electricity charges | 7,194 5,723 | 7,195 5,723 | 4,053 1,943 | 3,829 1,812 | (224) (131) |
| Purchase of materials Office administration costs Electricity charges Plant operations | 7,194 5,723 3,458 | 7,195 5,723 3,458 | 4,053 1,943 1,007 | 3,829 1,812 1,083 | (224) (131) 76 |
| Purchase of materials Office administration costs Electricity charges Plant operations Information technology resources | 7,194 5,723 3,458 3,685 | 7,195 5,723 3,458 3,665 | 4,053 1,943 1,007 1,254 | 3,829 1,812 1,083 1,458 | (224) (131) 76 204 |
| Purchase of materials Office administration costs Electricity charges Plant operations Information technology resources General insurance | 7,194 5,723 3,458 3,685 1,467 | 7,195 5,723 3,458 3,665 1,467 | 4,053 1,943 1,007 1,254 489 | 3,829 1,812 1,083 1,458 494 | (224) (131) 76 204 5 |

* Other Council outsourcing costs are various outsourced costs including refuse collection and disposal, waste disposal, legal services, traffic control, external training, valuation fees, etc.

** Community assistance costs represent community related costs including community grants, exhibitions and awards, donations and sponsorships.





4. STATEMENT OF FINANCIAL POSITION

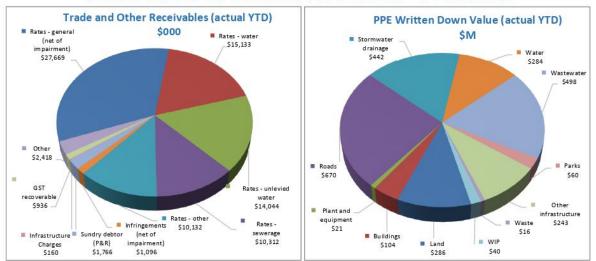
| STATEMENT OF FIN As at 31 Oct | | ION | | |
|---|-----------------------------|----------------------------|-----------------|------------------------|
| | Annual | Annual | YTD | YTD |
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual \$000 |
| CURRENT ASSETS | | | | |
| Cash and cash equivalents | 198,990 | 196,457 | 221,816 | 189,084 |
| Short-term investment - CBA | - | - | 10,000 | 10,068 |
| Trade and other receivables | 42,672 | 43,012 | 78,228 | 83,666 |
| Inventories | 916 | 1,024 | 917 | 868 |
| Other current assets | 1,810 | 4,967 | 4,967 | 4,802 |
| Total current assets | 244,389 | 245,460 | 315,928 | 288,488 |
| NON-CURRENT ASSETS | | | | |
| Investment property | 1,225 | 1,225 | 1,225 | 1,225 |
| Property, plant and equipment | 2,619,909 | 2,705,684 | 2,666,470 | 2,663,623 |
| Intangible assets | 1,135 | 1,160 | 1,522 | 1,511 |
| Right-of-use assets | 4,723 | 4,984 | 5,679 | 5,676 |
| Other financial assets | 73 | 73 | 73 | 73 |
| Investment in other entities | 12,657 | 12,657 | 12,657 | 12,657 |
| Total non-current assets | 2,639,722 | 2,725,783 | 2,687,626 | 2,684,765 |
| TOTAL ASSETS | 2,884,111 | 2,971,243 | 3,003,554 | 2,973,253 |
| CURRENT LIABILITIES | | | | |
| Trade and other payables | 37,171 | 45,927 | 50,306 | 34,109 |
| Borrowings - current | 8,326 | 8,919 | 8,919 | 8,919 |
| Lease liability - current | 1,294 | 1,130 | 1,130 | 1,130 |
| Provisions - current | 15,270 | 15,791 | 16,107 | 16,547 |
| Other current liabilities | 1,911 | 5,758 | 35,155 | 15,823 |
| Total current liabilities | 63,972 | 77,525 | 111,617 | 76,528 |
| NON-CURRENT LIABILITIES | | | | |
| Borrowings - non-current | 38,659 | 37,990 | 26,823 | 26,741 |
| Lease liability - non-current | 4,377 | 4,704 | 5,466 | 5,496 |
| Provisions - non-current | 21,539 | 22,676 | 21,446 | 22,154 |
| Total non-current liabilities | 64,576 | 65,370 | 53,735 | 54,391 |
| TOTAL LIABILITIES | 128,547 | 142,895 | 165,352 | 130,919 |
| NET COMMUNITY ASSETS | 2,755,563 | 2,828,348 | 2,838,202 | 2,842,334 |
| | 2,100,000 | 2,020,040 | 2,000,202 | 2,042,004 |
| | | | | |
| | 1.005.040 | 4 400 050 | 1 100 050 | 4 400 000 |
| Asset revaluation surplus | 1,035,840 | 1,106,353 | 1,106,353 | |
| Asset revaluation surplus Retained surplus | 1,619,513 | 1,623,315 | 1,621,431 | 1,106,353 1,622,570 |
| Asset revaluation surplus | | | | |



5,676

5,679

Monthly Financial Report



4. STATEMENT OF FINANCIAL POSITION - CONTINUED

| For | RIGHT-OF-USE ASSETS the period ending 31 October | 2021 | | |
|---------------------|--|--------------------------------------|------------------------|-----------------------------------|
| | Annual Original Budget \$000 | Annual Revised Budget \$000 | YTD Budget \$000 | YTD Actual Balance \$000 |
| Right-of-use asset | | | | |
| Buildings | 2,109 | 2,127 | 2,532 | 2,536 |
| Land | 2,435 | 2,533 | 2,794 | 2,787 |
| Plant and Equipment | 179 | 324 | 353 | 353 |

4,723

4,984

| | Annual | Annual | YTD | YTD |
|--|-----------------------------|----------------------------|-----------------|----------------------------|
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual Balance \$000 |
| PPE movement | | | | |
| Opening balance (includes WIP from previous years) | 2,614,439 | 2,667,979 | 2,667,979 | 2,667,97 |
| Acquisitions and WIP in year movement | 72,958 | 105,193 | 20,938 | 17,20 |
| Depreciation in year | (65,977) | (65,977) | (21,992) | (21,182 |
| Disposals | (1,511) | (1,511) | (455) | (380 |
| Other adjustments** | - | <u> </u> | - | |

* This table includes movement relating to property, plant and equipment only and is exclusive of intangible assets.

** Other adjustments include transfers between asset classes, revaluation adjustments, prior period adjustments and depreciation thereon.



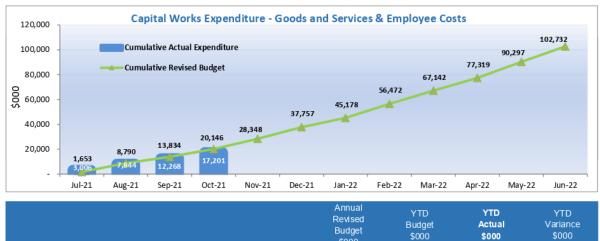
Closing balance

5. STATEMENT OF CASH FLOWS

| STATEMENT OF For the period ending | | | | |
|--|--|----------------------------|-------------------------------|----------------------------------|
| · · · | Annual | Annual | YTD | YTD |
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual \$000 |
| CASH FLOWS FROM OPERATING ACTIVITIES | | | | |
| Receipts from customers | 297,941 | 298,017 | 127,902 | 93,907 |
| Payments to suppliers and employees | (246,606) | (246,995) | (73,643) | (85,125 |
| | 51,334 | 51,022 | 54,259 | 8,78 |
| nterest received | 2,037 | 2,037 | 634 | 52 |
| Rental income | 1,067 | 1,067 | 360 | 40 |
| Non-capital grants and contributions | 14,109 | 14,421 | 4,015 | 4,17 |
| Borrowing costs | (1,763) | (1,763) | (1,736) | (1,769 |
| Right-of-use assets interest expense | (131) | (131) | (44) | (42 |
| Net cash inflow / (outflow) from operating activities | 66,654 | 66,654 | 57,488 | 12,07 |
| CASH FLOWS FROM INVESTING ACTIVITIES | 1 | | | |
| Payments for property, plant and equipment | (70,498) | (102,732) | (20,146) | (15,870 |
| Proceeds from sale of property, plant and equipment | 1,222 | 1,222 | 413 | 67 |
| Capital grants, subsidies and contributions | 22,133 | 28,638 | 4,384 | 9,05 |
| Other cash flows from investing activities* | 3,500 | 3,500 | -,004 | 3,50 |
| Net cash inflow / (outflow) from investing activities | (43,642) | (69,372) | (15,349) | (2,643 |
| | (,, | (| (| (_, |
| CASH FLOWS FROM FINANCING ACTIVITIES | | | | |
| Proceeds of borrowings | 10,323 | 10,323 | - | |
| Repayment of borrowings | (7,243) | (7,243) | (7,180) | (7,230 |
| Right-of-use lease payment | (1,145) | (1,145) | (383) | (353 |
| Net cash inflow / (outflow) from financing activities | 1,936 | 1,935 | (7,563) | (7,583 |
| Net increase / (decrease) in cash held | 24,947 | (783) | 34,576 | 1,84 |
| Cash and cash equivalents at the beginning of the year | 174,043 | 197,240 | 187,240 | 187,24 |
| Cash and cash equivalents at the end of the financial year / period | 198,990 | 196,457 | 221,816 | 189,08 |
| Cash Inflow (actual YTD) | Cas | h Outflow | (actual Y | TD) |
| 48% | | | | Materials and services 47% |
| Rates charges 29% Other cash receipts 3% contributions 3% Interest received 3% | Em ployee costs 30% Repayment of borrowings 7% | propert | nts for y, plant igment | Borrowing costs 2% |
| 370 8% 0% | | 14 | | |
| | | Arres (Astrol VITD) | | 440.00 |
| | Total Cash Expend Total Cash Expendit | . , | d Dudact | 110,38 360,00 |

* Loan drawn down by RIC from February to June 2021 has been repaid in July 2021.



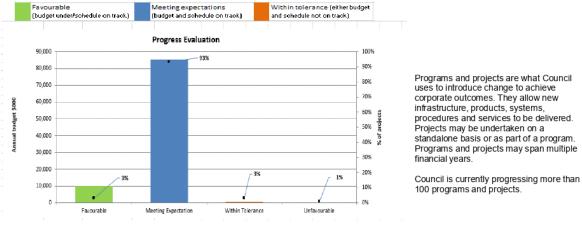


6. CAPITAL EXPENDITURE

| | Annual Revised Budget \$000 | YTD Budget \$000 | YTD Actual \$000 | YTD Variance \$000 |
|---------------------------------|--------------------------------------|------------------------|------------------------|--------------------------|
| Capitalised goods and services* | 95,085 | 18,659 | 14,571 | (4,088) |
| Capitalised employee costs | 7,647 | 1,487 | 2,630 | 1,143 |
| Total | 102,732 | 20,146 | 17,201 | (2,945) |

* Excludes capital prepayments.

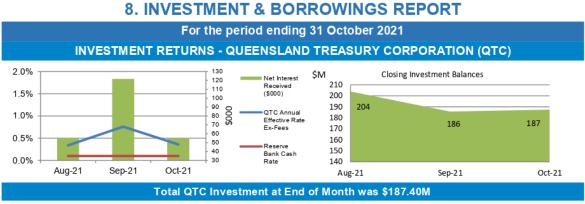
7. PROGRAM AND PROJECT UPDATE



| The status of two notable projects are as follows: | |
|--|-------------------------|
| Project description | Progress |
| Road Upgrade & Expansion Program - Gravel road resealing program for Mainland and NSI. | Meeting Expectations |
| Aquatic Paradise Navigation Beacon Renewal | Meeting Expectations |

Notable Projects



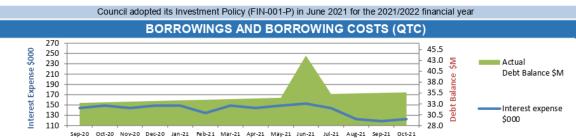


Council investments are currently held predominantly in the Capital Guaranteed Cash Fund, which is a fund operated by the Queensland Treasury Corporation (QTC). In October 2021 \$10.068M was re-invested in a term deposit of Commonwealth Bank of Australia (CBA) to maximise interest earnings.

The movement in interest earned is indicative of both the interest rate and the surplus cash balances held, the latter of which is affected by business cash flow requirements on a monthly basis as well as the rating cycle.

Note: the Reserve Bank reduced the cash rate down to 0.10% during November 2020.

On a daily basis, cash surplus to requirements is deposited with QTC to earn higher interest as QTC is offering a higher rate than what is achieved from Council's transactional bank accounts. The current annual effective interest rate paid by QTC is 0.36%. Term deposit rates are being monitored to identify investment opportunities to ensure Council maximises its interest earnings.



The existing loan accounts were converted to fixed rate loans on 1 April 2016 following a QTC restructure of loans and policies. In line with Council's debt policy, debt repayment of \$9.00M, being \$7.23M principal and \$1.77M interest has been made *annually* for 2021/2022 which will result in the loans being repaid approximately one year earlier.

The debt balance shows a decrease as the Annual Debt Service Payment (ADSP) was made during July 2021. Interest will accrue monthly on a daily balance until next ADSP in July 2022 which is reflected in the increasing debt balance.

In June 2021 borrowings of \$9.61M were undertaken as part of Council's Capital Works Plan.

| Total Borrowings at End of Month were \$35.66M | | | | | | | | |
|---|-----------------------------|----------------------------|-----------------|----------------------------|--|--|--|--|
| Council adopted its Debt Policy (FIN-009-P) in June 2021 for the 2021/2022 financial year | | | | | | | | |
| BORROWINGS | | | | | | | | |
| For the period ending 31 October 2021 | | | | | | | | |
| | Annual | Annual | YTD | YTD | | | | |
| | Original Budget \$000 | Revised Budget \$000 | Budget \$000 | Actual Balance \$000 | | | | |
| Borrowings | | | | | | | | |
| Opening balance | (44,228) | (44,153) | (44,153) | (44,153) | | | | |
| Accrued interest on borrowings | (1,439) | (1,438) | (507) | (507) | | | | |
| Interest paid on borrowings | 1,763 | 1,763 | 1,736 | 1,769 | | | | |
| Principal repaid | 7,243 | 7,243 | 7,182 | 7,231 | | | | |
| Loan drawdown | (10,324) | (10,324) | - | - | | | | |
| Closing balance | (46,985) | (46,909) | (35,742) | (35,660) | | | | |



9. CONSTRAINED CASH RESERVES

| Reserves as at 31 October 2021 | Purpose of reserve | Opening Balance \$000 | To Reserve \$000 | From Reserve \$000 | Closing Balance \$000 |
|--|--|-----------------------------|---------------------|--------------------------|-----------------------------|
| Special Projects Reserve: | | | | | |
| Aquatic Paradise Revetment Wall Reserve | To fund Aquatic Paradise revetment wall works program | - | 13 | (3) | 10 |
| Weinam Creek Reserve | Maintenance and improvements associated with Weinam Creek projects | - | 164 | (2) | 162 |
| Waste Levy Reserve | To fund Waste Levy Program | - | 2.643 | (2,041) | 602 |
| Raby Bay Revetment Wall Reserve | To fund Raby Bay revetment wall works program | 4,265 | 1,375 | (68) | 5,572 |
| Fleet Plant & Capital Equipment Reserve | To support the long term fleet replacement program | 3,716 | 902 | (314) | 4,304 |
| | | 7,981 | 5,097 | (2,428) | 10,650 |
| Constrained Works Reserve: | | | | | |
| Public Parks Trunk Infrastructure Reserve | Capital projects for public parks trunk infrastructure | 6,148 | 1,097 | (1,105) | 6,140 |
| Land for Community Facilities Trunk Infrastruture Reserve | Land for community facilities trunk infrastructure | 4,829 | 39 | - | 4,868 |
| Water Supply Trunk Infrastructure Reserve | Upgrade, expansion or new projects for water supply trunk infrastructure | 14,760 | 39 | - | 14,799 |
| Sewerage Trunk Infrastructure Reserve | Upgrade, expansion or new projects for sewerage trunk infrastructure | 11,165 | 588 | (735) | 11,018 |
| Local Roads Trunk Infrastructure Reserve | Capital projects for local roads trunk infrastructure | 36,517 | 1,726 | (410) | 37,833 |
| Cycleways Trunk Infrastructure Reserve | Capital projects for cycleways trunk infrastructure | 13,288 | 613 | (239) | 13,662 |
| Stormwater Trunk Infrastructure Reserve | Capital projects for stormwater trunk infrastructure | 9,898 | 157 | - | 10,055 |
| Tree Planting Reserve | Acquisition and planting of trees on footpaths | 169 | 41 | (7) | 203 |
| Koala Tree off-set Planting Reserve | Acquisition and planting of trees for koala habitat | 226 | 20 | (16) | 230 |
| Special Property Reserve | Acquisition of property in line with the strategic property framework | 0 | 714 | (10) | 704 |
| | | 97,000 | 5,034 | (2,522) | 99,512 |
| Separate Charge Reserve: | | | | | |
| Environment Charge Maintenance Reserve | Ongoing conservation and maintenance operations | - | 5,355 | (3,152) | 2,203 |
| SES Separate Charge Reserve | On-going costs of maintaining the Redland SES | 70 | 255 | (136) | 189 |
| Special Charge Reserve - Canals: | | 70 | 5,610 | (3,288) | 2,392 |
| Aquatic Paradise Canal Reserve* | Maintenance and repairs of Aquatic Paradise canals | 758 | | | 758 |
| Sovereign Waters Lake Reserve* | Maintenance and repairs of Aquatic Faladise canals Maintenance and repairs of Sovereign Lake | 431 | - | - | 43 |
| 1718 Raby Bay Canal Reserve | Service, facility or activity of works in respect of the canals of the Raby Bay canal estate | 219 | | | 21 |
| 1718 Aquatic Paradise Canal Reserve | Service, facility or activity of works in respect of the canals of the Aquatic Paradise canal estate | (495) | | | (495 |
| 1718 Sovereign Waters Lake Reserve | Service, facility or activity of works in respect of the lake | (156) | - | - | (450 |
| | | 857 | <u> </u> | - | 857 |
| TOTALS | | 105,908 | 15,741 | (8,238) | 113,411 |
| | | | ind cash equiva | | 189,084 |
| | | Reserves as p | ercentage of ca | sh balance | 60% |

*No interest charged for these reserves in October 2021 year to date due to low prevailing interest rate.

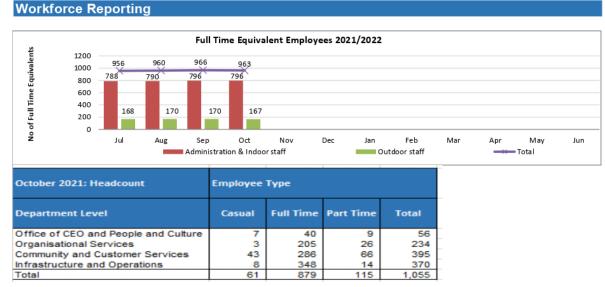


| CITY WATER S | Y WATER S | | | | |
|---|--------------------|-------------------|-----------------|-----------------|-------------------|
| | period ending | | | | |
| For the | Annual | Annual | YTD | YTD | YTD |
| | Original Budget | Revised Budget | Budget | Actual | Variance |
| | \$000 | \$000 | \$000 | \$000 | \$000 |
| Total revenue | 128,647 | 128,647 | 53,378 | 53,651 | 27 |
| Total expenses | 76,264 | 76,465 | 24,100 | 23,516 | (584 |
| Earnings before interest, tax and depreciation (EBITD) | 52,382 | 52,182 | 29,278 | 30,135 | 85 |
| External interest expense | 224 | 224 | 81 | 80 | (|
| Internal interest expense | 15,139 | 15,139 | 5,046 | 5,046 | |
| Depreciation | 24,711 | 24,711 | 8,237 | 8,230 | (|
| Operating surplus / (deficit) | 12,309 | 12,109 | 15,914 | 16,779 | 86 |
| CITY WATE | R CAPITAL FU | NDING STAT | EMENT | | |
| For the | period ending | 31 October 2 | 021 | | |
| | Annual | Annual | YTD | YTD | YTD |
| | Original | Revised | Budget | Actual | Variance |
| | Budget | Budget | \$000 | \$000 | \$000 |
| Conital contributions, denotions, grants and subsidios | \$000 | \$000 | 0.05 | | (0.5) |
| Capital contributions, donations, grants and subsidies Net transfer (to) / from constrained capital reserves | 2,956 | 2,956 | 985 (985) | 627 (598) | (35) |
| Non-cash contributions | 1,019 5,747 | 5,747 | 793 | (558) | (79 |
| Funding from utility revenue | (2,699) | 7,450 | 2,593 | 2.087 | (50) |
| Total sources of capital funding | 7,023 | 17,173 | 3,386 | 2,116 | (1,27) |
| Contributed assets | 2,379 | 2,379 | 793 | 2,110 | (79 |
| Capitalised expenditure | 3,982 | 14,132 | 2,197 | 1,682 | (51) |
| Loan redemption | 662 | 662 | 396 | 434 | 3 |
| Total application of capital funds | 7,023 | 17,173 | 3,386 | 2,116 | (1,27) |
| | Y WASTE S | | ITC | | |
| | | | | | |
| | STE OPERATI | | | | |
| For the | period ending | | | | |
| | Annual | Annual | YTD | YTD | YTD |
| | Original Budget | Revised Budget | Budget | Actual | Variance |
| | \$000 | \$000 | \$000 | \$000 | \$000 |
| Total revenue | 33,057 | 33,057 | 13,444 | 13,579 | 13 |
| Total expenses | 24,137 | 24,137 | 9,569 | 9,113 | (45 |
| Earnings before interest, tax and depreciation (EBITD) | 8,920 | 8,920 | 3,875 | 4,466 | 59 |
| External interest expense | 7 | 7 | 3 | 3 | |
| Depreciation | 423 | 423 | 141 | 116 | (2 |
| Operating surplus / (deficit) | 8,490 | 8,490 | 3,731 | 4,347 | 61 |
| CITY WAST | E CAPITAL FU | NDING STATI | EMENT | | |
| For the | period ending | 31 October 2 | 021 | | |
| | Annual | Annual | YTD | YTD | YTD |
| | Original | Revised | | | |
| | Budget | Budget | Budget \$000 | Actual \$000 | Variance \$000 |
| | \$000 | \$000 | | - | |
| Funding from utility revenue | 755 | 1,005 | 503 | 424 | (7 |
| Total sources of capital funding | 755 | 1,005 | 503 | 424 | (79 |
| Capitalised expenditure | 600 | 850 | 374 | 310 | (64 |
| Loan redemption | 155 | 155 | 129 | 114 | (15 |
| Total application of capital funds | 755 | 1,005 | 503 | 424 | (7) |





12. APPENDIX: ADDITIONAL AND NON-FINANCIAL INFORMATION



Note: Full Time Equivalent Employees includes all full time employees at a value of 1 and all other employees, at a value less than 1. The table above demonstrates the headcount by department. Following Ourspace, the table includes contract of service and temporary personnel. It includes casual staff in their non-substantive roles as at the end of the period where relevant.

| Over | Overdue Rates Debtors & Statistics | | | | | | | | | | |
|----------|---|---------|-------------|---------|--------------|----------|---|---------------|---------------|--|--|
| | Comparison October 2021 to October 2020 | | | | | | | | | | |
| Days | | % | | % | \$ | % | | | | | |
| Overdue | Oct-21 | Overdue | Oct-20 | Overdue | Variance | Variance | Rates & Charges Statistics | Oct-21 | Oct-20 | | |
| 0 - 30 | \$5,852 | 0.0% | \$220 | 0.0% | \$5,632 | 0.0% | Levied (Billed) Rates & Charges since 1 July 2021 | \$146,505,535 | \$139,614,706 | | |
| 31 - 60 | \$1,039 | 0.0% | \$711 | 0.0% | \$328 | 0.0% | Rate arrears b/fwd 1 July 2021 | \$10,693,344 | \$12,988,652 | | |
| 61 - 90 | \$2,578,362 | 1.6% | \$2,962,785 | 1.9% | -\$384,423 | -0.3% | Total | \$157,198,879 | \$152,603,358 | | |
| 91 - 180 | \$1,351,364 | 0.9% | \$1,634,744 | 1.1% | -\$283,380 | -0.2% | Balance of overdue rates & charges | \$7,237,282 | \$8,298,305 | | |
| >180 | \$3,300,665 | 2.1% | \$3,699,845 | 2.4% | -\$399,180 | -0.3% | Percentage Overdue | 4.6% | 5.4% | | |
| Total | \$7,237,282 | 4.6% | \$8,298,305 | 5.4% | -\$1,061,023 | -0.8% | | | | | |





13. GLOSSARY

Key Terms

Written Down Value:

This is the value of an asset after accounting for depreciation or amortisation, and it is also called book value or net book value

Work In Progress:

This represents an unfinished project that costs are still being added to. When a project is completed, the costs will be either capitalised (allocated to relevant asset class) or written off.

Definition of Ratios Operating Surplus Ratio*: Net Operating Surplus This is an indicator of the extent to which revenues raised cover operational Total Operating Revenue expenses only or are available for capital funding purposes Asset Sustainability Ratio*: Capital Expenditure on Replacement of Infrastructure Assets (Renewals) This ratio indicates whether Council is renewing or replacing existing non-Depreciation Expenditure on Infrastructure Assets financial assets at the same rate that its overall stock of assets is wearing out Net Financial Liabilities*: Total Liabilities - Current Assets Total Operating Revenue This is an indicator of the extent to which the net financial liabilities of Council can be serviced by operating revenues Level of Dependence on General Rate Revenue: General Rates - Pensioner Remissions This ratio measures Council's reliance on operating revenue from general rates Total Operating Revenue - Gain on Sale of Developed Land (excludes utility revenues) Current Ratio: Current Assets This measures the extent to which Council has liquid assets available to meet Current Liabilities short term financial obligations Debt Servicing Ratio: This indicates Council's ability to meet current debt instalments with recurrent Interest Expense*** + Loan Redemption^ Total Operating Revenue - Gain on Sale of Developed Land revenue Cash Balance - \$M: Cash Held at Period End Cash balance includes cash on hand, cash at bank and other short term investments Cash Held at Period End Cash Capacity in Months: This provides an indication as to the number of months cash held at period end [[Cash Operating Costs + Interest Expense] / Period in Year] would cover operating cash outflows Longer Term Financial Stability - Debt to Asset Ratio: Current and Non-current Debt** This is total debt as a percentage of total assets, i.e. to what extent will our long Total Assets term debt be covered by total assets Net Cash from Operations + Interest Revenue and Expense Cash Operating Revenue + Interest Revenue Operating Performance: This ratio provides an indication of Council's cash flow capabilities Net Interest Expense on Debt Service*** Interest Coverage Ratio Total Operating Revenue This ratio demonstrates the extent to which operating revenues are being used to meet the financing charges

These targets are set to be achieved on average over the longer term and therefore are not necessarily expected to be met on a monthly basis. ** Debt includes lease liabilities.

*** Interest expense includes interest on leases

^ Loan redemption includes lease redemption.



13.2 OPERATIONAL PLAN QUARTERLY PERFORMANCE REPORT SEPTEMBER 2021

| Objective Reference: | A59 | 94572 | | | | |
|-----------------------------|------|--|--|--|--|--|
| Authorising Officer: | Loui | Louise Rusan, Acting General Manager Organisational Services | | | | |
| Responsible Officer: | Tony | y Beynon, Group Manager Corporate Governance | | | | |
| Report Author: | Lorr | Lorraine Lee, Senior Adviser Corporate Planning and Performance | | | | |
| Attachments: | 1. | Operational Plan 2021-2022 Performance Report July to September Quarter One 2021 4 | | | | |

PURPOSE

To provide an update on the progress of the Operational Plan 2021-2022 from 1 July to 30 September 2021.

BACKGROUND

The *Local Government Act 2009* requires Council to adopt an operational plan each year. The Operational Plan 2021-2022 (the Plan) is an important planning document which sets out how Council intends to implement the Corporate Plan *Our Future Redlands – A Corporate Plan to 2026 and Beyond*.

The Plan is structured to reflect the seven goals (themes) of the Corporate Plan, and outlines 30 catalyst project activities and 37 key initiative activities that are planned to be delivered in the 2021-2022 financial year.

The *Local Government Regulation 2012* requires the Chief Executive Officer to present a written assessment of Council's progress towards implementing the annual operational plan at meetings of Council, at least quarterly.

ISSUES

The attached Operational Plan 2021-2022 Performance Report July to September Quarter One 2021 includes a progress and status update for each activity for catalyst projects and key initiatives outlined in the Plan.

The report provides commentary by exception on activity tasks that have a monitor or concern status. In addition, the report provides Key Activities and Highlights under each goal (theme) of the Corporate Plan and links to resources which provide further information and updates for the community.

The information in the report has been provided by the Council business area accountable for each activity.

The following summarises the status of the 67 activities outlined in the Plan:

| On Track | 61 |
|-----------|----|
| Monitor | 4 |
| Concern | 1 |
| Completed | 1 |
| Total | 67 |



STRATEGIC IMPLICATIONS

Legislative Requirements

This report has been prepared in compliance with section 174(3) of the Local Government Regulation 2012 which states that the "chief executive officer must present a written assessment of the local government's progress towards implementing the annual operational plan at meetings of the local government held at regular intervals of not more than 3 months".

Risk Management

The risk of not delivering the Plan is that Council does not achieve the commitments set out in the longer term Corporate Plan. Each activity has associated risks which are managed by the relevant area of Council.

Financial

The Plan is funded from the 2021-2022 Annual Budget.

People

Although delivery of the Plan itself is dependent on staff resources, there are no direct impacts on people resulting from this report.

Environmental

Some activities within the Plan directly contribute to Council's environmental commitments. However, this report does not have any direct environmental impacts.

Social

Some activities within the Plan directly contribute to Council's social commitment. However, this report does not have any direct social impacts.

Human Rights

There are no human rights implications associated with this report.

Alignment with Council's Policy and Plans

The Plan outlines activities against the seven goals (themes) in the Corporate Plan that Council plans to deliver in 2021-2022. As such, it is a key planning document and consistent with the Corporate Plan.

Council adopted its Corporate Plan on 16 December 2020, for commencement on 1 July 2021. The Corporate Plan provides Council with a framework for forward planning and future delivery of services and infrastructure.

This performance report is in response to Council's Corporate Plan.

CONSULTATION

| CONSULTED | DATE | COMMENTS |
|--|--------------|---|
| Office of CEO Head of People, Culture and Organisational Performance | October 2021 | The activity progress and comments have been provided by the officers involved in delivering the particular activities within the Operational Plan 2021-2022. |
| Organisational Services General Manager Organisational Services | October 2021 | The activity progress and comments have been provided by the officers involved in delivering the particular activities within the Operational Plan 2021-2022. |
| Community and Customer Services General Manager Community and Customer Services | October 2021 | The activity progress and comments have been provided by the officers involved in delivering the particular activities within the Operational Plan 2021-2022. |
| Infrastructure and Operations General Manager Infrastructure and Operations | October 2021 | The activity progress and comments have been provided by the officers involved in delivering the particular activities within the Operational Plan 2021-2022. |

OPTIONS

Option One

That Council resolves to note the Operational Plan 2021-2022 Performance Report July to September Quarter One 2021, as attached to this report.

Option Two

That Council resolves to note this report and requests additional information.

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/271

Moved by: Cr Julie Talty Seconded by: Cr Peter Mitchell

That Council resolves to note the Operational Plan 2021-2022 Performance Report July to September Quarter One 2021, as attached to this report.

CARRIED 11/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.





Operational Plan 2021-2022

Performance Report

July to September

Quarter One (Q1)

Operational Plan Performance Reporting

The Local Government Act 2009 requires Council to adopt an operational plan each year. The Operational Plan 2021-2022 (the Plan) is an important planning document which sets out how Council intends to implement the Corporate Plan 'Our Future Redlands – A Corporate Plan to 2026 and Beyond'.

The Plan is structured to reflect the seven goals (themes) of the Corporate Plan, and outlines 30 catalyst project activities and 37 key initiative activities that are planned to be delivered in the 2021-2022 financial year.

The *Local Government Regulation 2012* requires the Chief Executive Officer to present a written assessment of Council's progress towards implementing the annual operational plan at meetings of Council, at least quarterly.

The Operational Plan Quarterly Performance Report includes a progress and status update for each activity for catalyst projects and key initiatives outlined in the Plan. The report provides commentary by exception on activity tasks that have a monitor or concern status. In addition, the report provides Key Activities and Highlights under each goal (theme) of the Corporate Plan and links to resources which provide further information and updates for the community.

| Goals (Themes) | Catalyst Project Activities | 2021-2026 Key Initiative Activities | Status Year to Date |
|---|-----------------------------------|---|------------------------|
| | 3 | 4 | 1 |
| | 3 | 3 | 1 |
| | з | 4 | 1 |
| | 4 | 11 | 1 |
| | 4 | 4 | 1 |
| | 8 | 7 | 1 |
| EFFICIENT AND EFFECTIVE ORGANISATION | 5 | 4 | 1 |

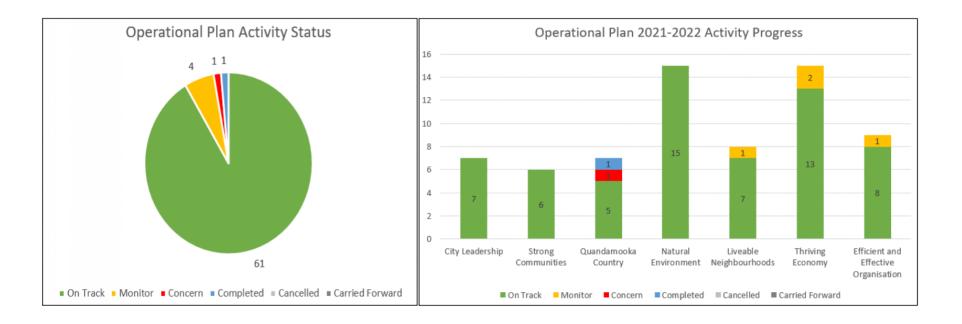
What's in the Operational Plan 2021-2022?

Operational Plan 2021-2022 Performance Report Q1

Page **2** of **61**



Operational Plan 2021-2022 Activity Progress



Operational Plan 2021-2022 Performance Report Q1

Page **3** of **61**



| Department/Key Accountable | On Track | Monitor | Concern | Completed | Cancelled | Carried Forward | Total |
|---------------------------------|----------|---------|---------|-----------|-----------|--------------------|-------|
| Office of the CEO | 2 | 1 | - | - | - | - | 3 |
| Community and Customer Services | 30 | - | - | - | - | - | 30 |
| Infrastructure and Operations | 11 | 1 | - | - | - | - | 12 |
| Organisational Services | 18 | 2 | 1 | 1 | - | - | 22 |
| Total | 61 | 4 | 1 | 1 | - | - | 67 |

Operational Plan 2021-2022 Activity Progress - Organisational Performance

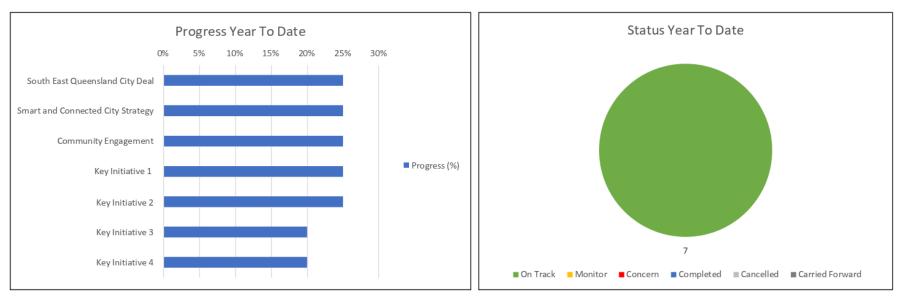
| Group/Lead | On Track | Monitor | Concern | Completed | Cancelled | Carried Forward | Total |
|---|----------|---------|---------|-----------|-----------|--------------------|-------|
| People, Culture and Organisational Performance | 2 | 1 | - | - | - | - | 3 |
| City Planning and Assessment | 3 | - | - | - | - | - | 3 |
| Community and Economic Development | 15 | - | - | - | - | - | 15 |
| Customer and Cultural Services | 2 | - | - | - | - | - | 2 |
| Environment and Regulation | 10 | - | - | - | - | - | 10 |
| City Assets | 10 | 1 | - | - | - | - | 11 |
| City Operations | 1 | - | - | - | - | - | 1 |
| Communications, Engagement and Tourism | 6 | - | - | 1 | - | - | 7 |
| Corporate Governance | 6 | - | 1 | - | - | - | 7 |
| Corporate Services | 3 | - | - | - | - | - | 3 |
| General Counsel | 1 | - | - | - | - | - | 1 |
| Office of the General Manager Organisational Services | 1 | 2 | - | - | - | - | 3 |
| Strategic Asset and Portfolio Management | 1 | - | - | - | - | - | 1 |
| Total | 61 | 4 | 1 | 1 | - | - | 67 |

Operational Plan 2021-2022 Performance Report Q1

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City Leadership



For details on City Leadership Key Initiatives 1-4 please see pages 7 to 9 of this attachment.

Operational Plan 2021-2022 Performance Report Q1

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City Leadership

Catalyst Project CP1.1 South East Queensland City Deal - Support agreement between the three levels of government to deliver investment in regionally transformative infrastructure which provides long term benefits for Redlands Coast.

| Operational Plan 2021-2022 Activity | Progress | Commentary | | | | |
|--|----------|------------|-----|------------------------------------|--|--|
| P1.1.1 Represent Redlands Coast interests in the development and nplementation of the South East Queensland City Deal. | | | | Community and Economic Development | | |
| a) Contribute through regional collaboration initiatives and targeted advocacy, including participation in Council of Mayors (SEQ) advocacy and economic development working groups. | Q1 | ٠ | 25% | | | |
| b) Promote collaborative economic outcomes through participation in local government working groups. | Q1 | ٠ | 25% | | | |
| c) Develop a Redlands Coast advocacy plan for key city building infrastructure. | Q1 | ٠ | 25% | | | |

Catalyst Project CP1.2 Smart and Connected City Strategy - Implement the Smart and Connected City Strategy to deliver smart local and regional solutions that improve the liveability, productivity and sustainability of the city.

| Operational Plan 2021-2022 Activity | Progress | Commentary | | | | |
|---|----------|------------|-----|------------------------------------|--|--|
| CP1.2.1 Enhance Redlands Coast liveability, sustainability and attractiveness as a place for business through smart and connected city initiatives. | | | | Community and Economic Development | | |
| a) Finalise and commence implementation of the Smart and Connected City Strategy. | Q1 | • | 25% | | | |
| b) Promote opportunities for driving innovation through initiatives for business and community. | Q1 | • | 25% | | | |

Operational Plan 2021-2022 Performance Report Q1

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Catalyst Project CP1.3 Community Engagement - Strengthen Council's internal capacity to undertake meaningful community engagement using a broad range of tools and deliver enhanced promotion of engagement activities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------|--------------------------------|
| CP1.3.1 Investigate Council's capacity to deliver meaningful engagement. | | | Communio | cation, Engagement and Tourism |
| a) Review internal engagement processes, resources, skills and budget requirements. | Q1 | • | 25% | |

Key Initiative KI1.1 Key Initiative 1 - Improve ways of engaging with our community to encourage diverse participation in local decision making, and co-design of community outcomes.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------|--------------------------------|
| KI1.1.1 Investigate new and improved ways of engaging with the Redlands Coast community. | | | Communio | cation, Engagement and Tourism |
| a) Review engagement activities delivered across Council and other agencies including engagement models and innovations. | Q1 | ٠ | 25% | |
| b) Implement improved engagement activities. | Q1 | • | 25% | |

Operational Plan 2021-2022 Performance Report Q1

Page **7** of **61**



Key Initiative KI1.2 Key Initiative 2 - Improve communication with our community to increase awareness and transparency around Council operations.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|----------|--------------------------------|------------|
| KI1.2.1 Investigate new and improved ways of communicating with the Redlands Coast community. | | Communio | cation, Engagement and Tourism | |
| a) Review communication processes, resources and skills including market researching best practice and innovative processes being utilised by other organisations. | Q1 | • | 25% | |
| b) Implement improved communication activities. | Q1 | • | 25% | |

Key Initiative KI1.3 Key Initiative 3 - Develop a strategic framework to guide Council's advocacy efforts with both the Commonwealth and State Governments for the betterment of Redlands Coast.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|-----------|------------|
| KI1.3.1 Develop a strategic framework to guide Council's advocacy efforts with other governments. | | | Corporate | Governance |
| a) Define the project scope, key milestones and identify resourcing required. | Q1 | • | 20% | |
| b) Develop a process and criteria for nominating key projects and initiatives to be included in Council's advocacy framework. | Q1 | • | 20% | |

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Key Initiative KI1.4 Key Initiative 4 - Advocate for all islands to be recognised as a regional status to create greater funding opportunities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-----------|------------|
| KI1.4.1 Undertake research to support advocacy efforts for all islands to be recognised as a regional status. | | | Corporate | Governance |
| a) Conduct a needs assessment for our island communities. | Q1 | | 20% | |
| b) Analyse island eligibility for State and Federal regional and remote grant allocations. | Q1 | • | 20% | |
| c) Identify specific projects and initiatives that would benefit from State and Federal regional funding. | Q1 | • | 20% | |

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City Leadership - Key Activities and Highlights

Investigate Council's capacity to deliver meaningful engagement.

Council continued to review its community engagement processes, resources, capacity and functions to identify opportunities for continuous improvement and best practice standards.

Undertake research to support advocacy efforts for all islands to be recognised as a regional status.

Brief scope drafted and implementation meeting scheduled to review existing data and identify gaps.

Investigate new and improved ways of engaging with the Redlands Coast community.

Council reviewed its engagement processes and methods for all major community engagements that occurred in the quarter and implemented formal mechanisms for capturing participant satisfaction with engagement processes and sharing project good news. Participant submission experience was improved through use of industry best practice form creation software. Council participated in the International Association of Public Participation professional development events to review engagement activities and innovations delivered across other agencies.

Investigate new and improved ways of communicating with the Redlands Coast community.

A review of communication systems and processes is currently being finalised.

Enhance Redlands Coast liveability, sustainability and attractiveness as a place for business through smart and connected city initiatives.

Work continued to finalise the draft Redlands Coast Smart and Connected City Strategy.

Develop a strategic framework to guide Council's advocacy efforts with other governments.

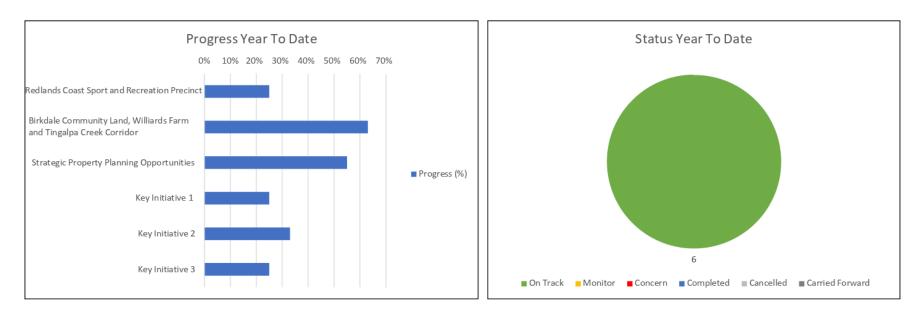
Initial scoping of advocacy needs has begun, including analysis of existing advocacy frameworks across other Councils and internal discussions to identify key advocacy projects.

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Strong Communities



For details on Strong Communities Key Initiatives 1-3 please see pages 13 to 15 of this attachment.

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Strong Communities

Catalyst Project CP2.1 Redlands Coast Sport and Recreation Precinct - Progress Council's staged master-plan including sporting fields, club buildings, car parking, cycling tracks, recreation trails and spaces to play and explore.

| Operational Plan 2021-2022 Activity | | Progress | Commentary | |
|---|----|----------|-------------|---|
| CP2.1.1 Plan for effective sport and recreation at Heinemann Road, Redland Bay. | | | City Assets | 5 |
| a) Continue to develop the Redlands Coast Regional Sport and Recreation Precinct through preliminary design and construction programming. | Q1 | • | 25% | |
| b) Commence implementing the action plan. | Q1 | | 25% | |

Catalyst Project CP2.2 Birkdale Community Land, Willards Farm and Tingalpa Creek Corridor - Create a shared vision for the end use of the land recognising the rich and diverse history of the site, building those values into creating an iconic landmark for the Redlands Coast.

| Operational Plan 2021-2022 Activity | | Progress | Commentary | |
|--|----|----------|------------|--------------------|
| CP2.2.1 Create a shared vision for the end use of Birkdale Community Land, Willards Farm and the Tingalpa Creek Corridor. | | | Environme | ent and Regulation |
| a) Develop a shared vision for the Birkdale Community Precinct, based on the previous community consultation process. | Q1 | • | 100% | |
| b) Commence development of the draft Birkdale Community Precinct master-plan and planning for future community consultation on the master-plan for the Precinct. | Q1 | • | 25% | |

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Catalyst Project CP2.3 Strategic Property Planning Opportunities - Generate enhanced community outcomes through strategic alignment and transitioning of Council's property portfolio to embrace sustainable opportunities to deliver physical and natural assets that respond to current and future needs in regards to social, environmental and economic outcomes.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------|--------------------|
| CP2.3.1 Generate enhanced community outcomes through strategic alignment and transitioning of Council's property portfolio. | | | | ent and Regulation |
| a) Complete Council's strategic property framework. | Q1 | | 65% | |
| b) Complete investigations into options for industrial land on Macleay Island. | Q1 | • | 45% | |

Key Initiative KI2.1 Key Initiative 1 - Stronger Communities Strategy – Implement the Stronger Communities Strategy and Action Plans to help ensure the city has services and social infrastructure to support the health and wellbeing of people of all ages and abilities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------|-----------------------------|
| (12.1.1 Undertake planning and implement initiatives that support community vellbeing and build community resilience, and investigate new and improved vays of engaging with the Redlands Coast community. | | | Communit | ty and Economic Development |
| a) Finalise and commence implementation of the Redlands Coast Stronger Communities Strategy 2021-2041. | | | | |
| b) Implement the Redlands Coast Age-friendly Action Plan 2021-2026. | Q1 | ٠ | 25% | |
| c) Develop the Redlands Coast Youth Action Plan 2022-2027. | Q1 | | 25% | |
| d) Develop the Redlands Coast Community Facilities Action Plan 2022-2027. | Q1 | ٠ | 25% | |

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| e) Deliver initiatives that target vulnerabilities and build community capacity and resilience. | Q1 | • | 25% | |
|---|----|---|-----|--|
| f) Deliver the Grants and Sponsorship Program to achieve strong community investment outcomes. | Q1 | • | 25% | |
| g) Deliver initiatives that aim to reduce crime and improve community safety. | Q1 | • | 25% | |

Key Initiative KI2.2 Key Initiative 2 - Develop and deliver initiatives to recognise and celebrate our local heritage and diverse cultures through the arts and events that promote connectivity.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------|-----------------------|
| KI2.2.1 Develop and deliver initiatives to recognise and celebrate our local heritage and diverse cultures through the arts and events that promote connectivity. | | | Customer | and Cultural Services |
| a) Revise Council's cultural policy to align with Our Future Redlands – A Corporate Plan to 2026 and Beyond. | Q1 | • | 25% | |
| b) Engage the Redlands Coast creative community to develop a one to five year culture plan. | Q1 | • | 25% | |
| c) Review the Redland Performing Arts Centre and Redland Art Gallery facility and site utilisation in the delivery of arts activities and events. | Q1 | • | 50% | |

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Key Initiative KI2.3 Key Initiative 3 - Advocate for lifelong learning and capacity building opportunities to enable social, cultural and economic participation and enhance productivity.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | |
|---|----|---|------------------------------------|------------|--|
| (12.3.1 Collaborate with schools, tertiary education providers, businesses, ndustries and communities to advance education opportunities in Redlands Coast. | | | Community and Economic Development | | |
| a) Continue to facilitate and develop employment and training initiatives for Indigenous and island residents. | Q1 | • | 25% | | |
| b) Advocate for education investment opportunities and initiatives from the Education and Training Network. | Q1 | ٠ | 25% | | |
| c) Continue to explore higher education models and options. | Q1 | • | 25% | | |
| d) Continue to investigate the development of a Centre of Excellence in Education for the Ageing. | Q1 | ٠ | 25% | | |
| e) Investigate opportunities and feasibility to develop an international education cluster and partnerships with international institutes. | Q1 | • | 25% | | |

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Strong Communities - Key Activities and Highlights

Undertake planning and implement initiatives that support community wellbeing and build community resilience, and investigate new and improved ways of engaging with the Redlands Coast community.

The draft Redlands Coast Stronger Communities Strategy was reviewed by Council and its structure revised to better inform development of key action plans. Implementation of Year One of the Redlands Coast Age-friendly Action Plan 2021-2026 commenced. Vulnerable groups in the local community were identified and connected to support services to improve wellbeing and help build resilience in times of crisis. The Community Grants Program was promoted, with applications assessed from a diverse range of community and recreational organisations seeking funding.

Create a shared vision for the end use of Birkdale Community Land, Willards Farm and the Tingalpa Creek Corridor.

Following community consultation, Council adopted the Birkdale Community Precinct Vision in August 2021. Council has received a regional ecosystem review report and an environmental early works and social land management plan to assist with preliminary environmental works on the site and in preparation for the master-plan.

Plan for effective sport and recreation at Heinemann Road, Redland Bay.

Developed and released tender documentation for the Preliminary and Detailed Design of the Redlands Coast Sport and Recreation Precinct. Conducted procurement evaluations and contract was awarded to the successful tenderer. Collaboration occurred with key community and primary tenant clubs. Briefs are underway for additional consultancies and peer reviews of various specialist elements of the design, such as field lighting and irrigation.

Generate enhanced community outcomes through strategic alignment and transitioning of Council's property portfolio.

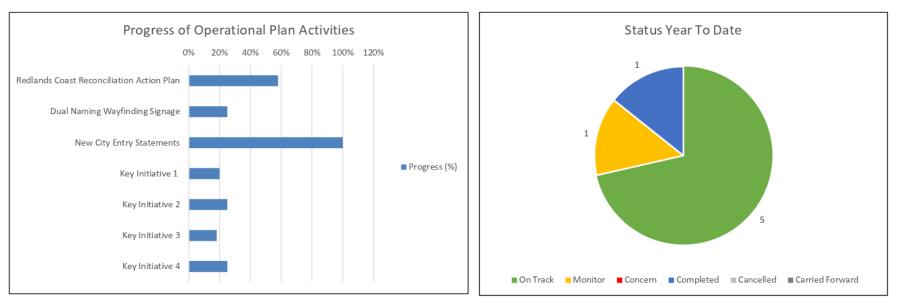
Council's Strategic Property Framework has received endorsement from internal stakeholders and Councillors. Council continued to engage with stakeholders to explore opportunities for industrial land on Macleay Island.

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Quandamooka Country



For details on Quandamooka Country Key Initiatives 1-4 please see pages 19 to 21 of this attachment.

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Quandamooka Country

Catalyst Project CP3.1 Redlands Coast Reconciliation Action Plan - Progress our reconciliation journey through the development of an externally facing Redlands Coast Reconciliation Action Plan.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|-----------|------------|
| CP3.1.1 Implement and establish reconciliation action plans. | | | Corporate | Governance |
| a) Continue to implement the Kanara Malara - One People 2019- 2021 Redland City Council Reconciliation Action Plan. | Q1 | ٠ | 90% | |
| b) Develop an externally facing Redlands Coast reconciliation action plan through Reconciliation Australia. | Q1 | • | 25% | |

Catalyst Project CP3.2 Dual Naming Wayfinding Signage - Progress roll-out of dual naming wayfinding signage across the Redlands Coast that incorporates Quandamooka Jandai language.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|-----------|------------|
| CP3.2.1 Develop a dual naming policy for Council. | | | Corporate | Governance |
| a) Investigate and research other local governments in Australia that have dual naming protocols for local Indigenous languages. | Q1 | • | 25% | |

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Catalyst Project CP3.3 New City Entry Statements - Installation of new city entry statements that include acknowledgement of Quandamooka Country.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|---------------------------------------|------------|
| CP3.3.1 Deliver City Entry Statements. | | | Communication, Engagement and Tourism | |
| a) Promote the installation of six new City Entry Statements across Redlands Coast. | Q1 | • | 100% | |

Key Initiative KI3.1 Key Initiative 1 - Work closely with the Traditional Owners of much of Redlands Coast, the Quandamooka People, and representative body Quandamooka Yoolooburrabee Aboriginal Corporation (QYAC), to foster a positive, mutually respectful working relationship.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|---|---|----------|--|
| KI3.1.1 Support delivery of Indigenous Land Use Agreement (ILUA | 1.1 Support delivery of Indigenous Land Use Agreement (ILUA) commitments. | | | Governance |
| a) Manage Council's commitments under the ILUA by coordinating with the ILUA Consultative Committee and Protecting Sea, Land and Environment Committee. | Q1 | • | 10% | Despite a number of attempts to arrange meetings of the Consultative Committee and Protecting Sea, Land & Environment Committee, this commitment has not been able to be met due to the unavailability of key QYAC personnel. Attempts by Council to meet with QYAC will continue to be made. |
| b) Monitor progress of ILUA activities delivered by key internal stakeholders and report to Council's Executive Leadership Team quarterly. | Q1 | • | 25% | |
| c) Continue to work collaboratively with Quandamooka Yoolooburrabee Aboriginal Corporation. | Q1 | • | 25% | |

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Key Initiative KI3.2 Key Initiative 2 - Continue to improve Council's capacity to deliver positive outcomes for the Traditional Owners of much of Redlands Coast, the Quandamooka People, and all Aboriginal and Torres Strait Islander people living on Redlands Coast, through policy as well as cultural awareness and heritage training for elected representatives and Council employees.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-----------|------------|
| .2.1 Implement culturally appropriate protocols and promote traditional owledge. | | | Corporate | Governance |
| a) Implement and monitor cultural heritage and cultural awareness training to employees and elected members. | Q1 | ٠ | 25% | |
| b) Promote cultural awareness to staff and celebrate Quandamooka and Aboriginal and Torres Strait Islander culture through key events such as the National Aborigines and Islanders Day Observance Committee (NAIDOC) and National Reconciliation Week. | Q1 | • | 25% | |

Key Initiative KI3.3 Key Initiative 3 - Work with the Quandamooka People to promote respect and understanding of Quandamooka culture and Native Title within the Redlands Coast community.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------------------|---|
| KI3.3.1 Engage key stakeholders for the preparation of the determination of Native Title on the Redlands Coast mainland. | | | Corporate Governance | |
| a) Work with Traditional Owners and the Queensland Government on Native Title claim requirements. | Q1 | • | 15% | Council has undertaken preparatory work in relation to the mainland Native Title Claim. This has involved the identification of over 1,100 parcels of land currently under Council's control or with Council assets built thereon. An initial meeting between Council and the claimants has taken place during the reporting period. |

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| Operational Plan 2021-2022 Activity | | | Progress | Commentary | |
|--|----|---|----------|--|--|
| KI3.3.1 Engage key stakeholders for the preparation of the determination of Native Title on the Redlands Coast mainland. | | | | Corporate Governance | |
| b) Develop a mainland Indigenous Land Use Agreement for both Traditional Owner groups. | Q1 | • | 5% | No progress on the development of the ILUA at the time of reporting due to extended Court timetable for the two Mainland Native Title Applications. | |

| KI3.3.2 Promote Quandamooka culture and information to improve understanding of Native Title. | | | Communication, Engagement and Tourism | |
|--|--|--|---------------------------------------|--|
| a) Provide information through Council's communication channels including Our Redlands magazine, social media and website. | channels including Our Redlands magazine, social media and | | | |

Key Initiative KI3.4 Key Initiative 4 - Work collaboratively with the Quandamooka People to help achieve a positive future for NorthStradbroke Island/Minjerribah post sandmining, including advocacy for local planning.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------|-----------------------------|
| KI3.4.1 Strengthen the local North Stradbroke Island (Minjerribah) economy. | | | Communit | ty and Economic Development |
| a) Engage with key stakeholders on Minjerribah to identify and deliver initiatives to strengthen the local economy. | Q1 | • | 25% | |

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Quandamooka Country - Key Activities and Highlights

Deliver City Entry Statements.

Council installed six new roadside entry signs to promote the Redlands Coast brand and our location on Quandamooka Country. The entry signs were promoted to the Redlands Coast Tourism Sub-Committee.

Promote Quandamooka culture and information to improve understanding of Native Title.

Council promoted Quandamooka culture information in the Our Redlands Coast magazine spring edition. Where known, Jandai dual names were included in fortnightly print and digital advertising and on Council's website. Quandamooka elders were invited to present Welcome to Country at Council delivered events which included information and education about native title.

Implement and establish reconciliation action plans.

Council continued to implement actions in the Kanara Malara - One People 2019-2021 Internal Redland City Council Reconciliation Action Plan. The Plan has 55 actions and 50 of these have been completed. Council has registered with Reconciliation Australia to develop an external reconciliation action plan.

Develop a dual naming policy for Council.

Research into the legalities and operation of a dual naming policy commenced in quarter one.

Implement culturally appropriate protocols and promote traditional knowledge.

Council celebrated National Aborigines and Islanders Day Observance Committee (NAIDOC) week holding many events in recognition of the week. There were 17 Council employees nominated as NAIDOC Ambassadors. Council's cultural awareness training program has been recognised as a finalist in the category of Best Diversity and Inclusion Program in the Australian Institute of Training and Development Excellence Awards 2021. Council continued to run Indigenous cultural training and information sessions within the organisation.

Support delivery of Indigenous Land Use Agreement (ILUA) commitments.

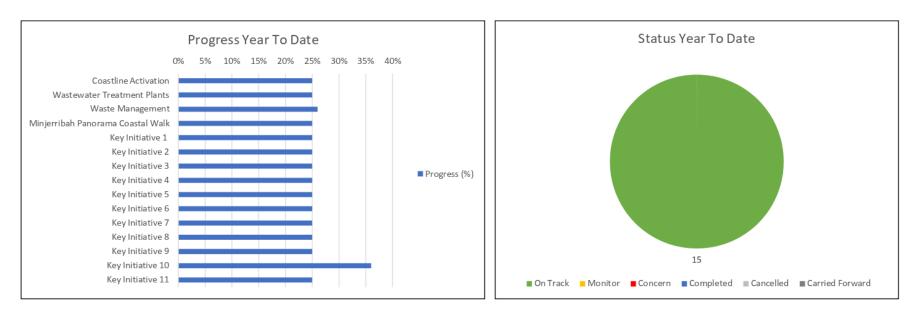
Council continued to manage the ILUA sub-committees and monitor progress of ILUA activities. The quarter four ILUA report for 2020-2021 was presented to the Executive Leadership Team in July 2021.

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Natural Environment



For details on Natural Environment Key Initiatives 1-11 please see pages 26 to 32 of this attachment.

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Natural Environment

Catalyst Project CP4.1 Coastline Activation - Finalise a strategy to activate the Redlands coastline, including Cleveland Point land access improvements and other landside improvements across the city, to encourage appreciation, recreation and utilisation of our unique natural bay and creek waterfront spaces and places.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|--|-----|-----------|--------------------|
| CP4.1.1 Commence scoping a strategy to activate the Redlands Coast coastline. | | | Environme | ent and Regulation |
| a) Establish a working group for pre-planning of the strategy. Q1 | | 25% | | |

Catalyst Project CP4.2 Wastewater Treatment Plants - Develop a strategy to review opportunities associated with Council's wastewater treatment plants including new forms of energy.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------|------------|
| CP4.2.1 Develop an overall strategy for wastewater treatment plants. | | | City Asset | S |
| a) Review release to environment requirements based on proposed strategy options for treatment plants. | Q1 | • | 25% | |
| b) Finalise the strategy based on preferred options for treatment plants. | Q1 | • | 25% | |

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Catalyst Project CP4.3 Waste Management - Participate in the development of a regional waste plan, and develop a local Waste Recycling and Reduction Plan to move towards a zero waste future.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | | |
|---|----|--|-------------|-------------|--|--|
| CP4.3.1 Participate in the development of a regional waste plan. | | | City Assets | City Assets | | |
| a) Continue to participate in regional forums. | Q1 | | 25% | | | |
| CP4.3.2 Develop a local waste recycling and reduction plan to move towards a zero waste future. | | | City Assets | S | | |
| a) Consolidate community feedback and finalise the plan. | Q1 | | 30% | | | |
| b) Seek Council endorsement of the plan. | Q1 | | 25% | | | |
| c) Implement actions from the plan. | Q1 | | 25% | | | |

Catalyst Project CP4.4 Minjerribah Panorama Coastal Walk - Progress next stages of this unique connection to enhance visitors' experiences, by harnessing key views and completing a safe pedestrian link between Point Lookout Village and Cylinder Beach.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-------------|------------|
| CP4.4.1 Plan for the Minjerribah Panorama Coastal Walk Project on North Stradbroke Island (Minjerribah). | | | City Assets | 5 |
| a) Continue to deliver stages of Minjerribah Panorama Coastal Walk works. | Q1 | • | 25% | |

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Key Initiative KI4.1 Key Initiative 1 - Continue to target rehabilitation, regeneration and habitat management works in key priority areas, based on strategic mapping and research through the delivery of the Redlands Wildlife Connections Action Plan.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------------------------|------------|
| KI4.1.1 Implement the Wildlife Connections Action Plan 2018-2023. | | | Environment and Regulation | |
| a) Improve corridor habitat by identifying strategic areas for native vegetation planting, threat management and extension programs in identified corridor gaps and pinch points. | Q1 | • | 25% | |
| b) Reduce impacts on corridors by enhancing buffers on freehold land and supporting landholders in creating buffers of native vegetation to reduce the risk of edge effects along priority corridors. | Q1 | • | 25% | |
| c) Reduce impacts on corridors by reviewing conservation land acquisition priorities. | Q1 | • | 25% | |

Key Initiative KI4.2 Key Initiative 2 - Deliver Council's Koala Conservation Strategy and review and update the Koala Conservation Action Plan to commit to proactively preserve our koala population.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-----------|--------------------|
| .2.1 Implement the Koala Conservation Strategy 2016 and review the Koala nservation Action Plan 2016-2021. | | | Environme | ent and Regulation |
| a) Continue to collaborate with research bodies, government agencies and the Redlands Coast community to develop a robust understanding of koala population, health, ecology and movement to inform and strengthen koala conservation planning. | Q1 | • | 25% | |

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| b) Continue to support the creation of a connected, high quality network of koala habitats capable of supporting a long term, viable and sustainable population of koalas. | Q1 | • | 25% | |
|--|----|---|-----|--|
| c) Manage the impacts of threatening processes on koala populations by undertaking on-ground works that reduce koala mortality. | Q1 | • | 25% | |
| d) Increase understanding, connection to and participation in koala conservation actions and behaviours with the Redlands Coast community. | Q1 | • | 25% | |
| e) Review the Koala Conservation Action Plan 2016. | Q1 | • | 25% | |

Key Initiative KI4.3 Key Initiative 3 - Partner with the community to manage fire risk through Council's fire management program.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-----------------|---|
| KI4.3.1 Implement Council's fire management program. | | | City Operations | |
| a) Plan and review key areas for mitigation of fire risk rate and carry out maintenance programs as required. | Q1 | • | 25% | Fire risk management activities have included planned burns and vegetation management in fire access trails and community engagement sessions. |
| b) Continue to deliver community education events. | Q1 | • | 25% | |

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Key Initiative KI4.4 Key Initiative 4 - Explore and implement opportunities to proactively reduce Council's carbon footprint.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------------------------|------------|
| I4.4.1 Explore and implement opportunities to proactively reduce Council's arbon footprint. | | | Environment and Regulation | |
| a) Continue to maintain and improve Council's carbon and energy dashboard. | Q1 | ٠ | 25% | |
| b) Continue to investigate renewable energy opportunities on Council buildings. | Q1 | ٠ | 25% | |
| c) Deliver information promoting sustainable living options including alternative energy usage to the Redlands Coast community. | Q1 | • | 25% | |

Key Initiative KI4.5 Key Initiative 5 - Continue to implement the Coastal Hazard Adaptation Strategy to proactively manage the impact of climate change on our foreshores.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------|------------|
| KI4.5.1 Implement the Coastal Hazards Adaptation Strategy across Redlands Coast. | | | City Asset | S |
| a) Continue to implement actions in accordance with the Strategy including coastal monitoring and protection work. | Q1 | • | 25% | |
| b) Implement knowledge sharing actions recommended in the Strategy including working with community groups, providing fact sheets and collaborating with infrastructure providers. | Q1 | • | 25% | |
| c) Embed hazard modelling in asset management practices across relevant asset classes. | Q1 | • | 25% | |

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Key Initiative KI4.6 Key Initiative 6 - Support the transition to a circular economy for waste and participate in regional collaboration and other partnership opportunities to improve resource efficiency.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-------------|--|
| KI4.6.1 Continue to implement the Sub Regional Waste Procurement Process. | | | City Assets | 5 |
| a) Continue as part of the external working group and provide status updates. | Q1 | • | 25% | Council collaboration with working group on procurement process. |

Key Initiative KI4.7 Key Initiative 7 - Partner with Seqwater and other water service providers to contribute to the development of the Water for SEQ Plan – an integrated plan for water cycle management in South East Queensland, supporting drought preparedness for the region.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-------------|------------|
| KI4.7.1 Partner with Seqwater and other water service providers. | | | City Assets | 5 |
| a) Continue participation with Seqwater and other water service providers in regular forums. | Q1 | • | 25% | |
| b) Contribute to the development of the Water for South East Queensland: Planning for our future Annual Report 2021. | Q1 | • | 25% | |

Key Initiative KI4.8 Key Initiative 8 - Deliver the Redlands Coast Biosecurity Plan to proactively manage the impacts to Redlands Coast environmental assets and community lifestyle posed by biosecurity risks.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|--|----------------------------|------------|
| KI4.8.1 Deliver the Redlands Coast Biosecurity Plan 2018-2021. | | | Environment and Regulation | |
| a) Prevent and detect invasive biosecurity matter. | Q1 | | 25% | |

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| b) Promote awareness and education of biosecurity and pest management. | Q1 | • | 25% | |
|---|----|---|-----|--|
| c) Manage systems to provide effective pest control and enforcement activities. | Q1 | • | 25% | |

Key Initiative KI4.9 Key Initiative 9 - Work with the community to provide education opportunities to support, enhance and encourage environmental understanding and grow environmental connections.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------------------------|------------|
| I4.9.1 Provide education opportunities to the Redlands Coast community to upport, enhance and encourage environmental understanding and grow nvironmental connections. | | | Environment and Regulation | |
| a) Develop and deliver a wildlife education program through scheduled workshops, events, social media and online resources which includes endangered, threatened and iconic species. | Q1 | • | 25% | |
| b) Deliver the IndigiScapes workshop and event schedule to align with environmental strategies and plans, and respond to the community. | Q1 | • | 25% | |
| c) Provide extension services with private land owners and the community in accordance with identified priority areas. | Q1 | • | 25% | |
| d) Deliver community Bushcare programs in identified priority areas. | Q1 | • | 25% | |

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Key Initiative KI4.10 Key Initiative 10 - Implement programs to address the impacts of land uses on waterways by delivering a coordinated approach to catchment management.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | | |
|---|----|---|-----------|----------------------------|--|--|
| KI4.10.1 Implement waterway catchment management programs. | | | Environme | Environment and Regulation | | |
| a) Finalise the Redlands Coast Bay and Creeks Plan. | Q1 | ٠ | 100% | | | |
| b) Contribute to the Lower Brisbane - Redlands Coastal Catchment Action Plan 2018-2021. | Q1 | ٠ | 25% | | | |
| c) Participate in relevant partnerships and networks that foster catchment management and improved water quality. | Q1 | ٠ | 25% | | | |
| d) Undertake catchment water quality monitoring. | Q1 | • | 25% | | | |
| e) Monitor recreational water quality. | Q1 | • | 25% | | | |
| f) Respond to and investigate customer service requests for erosion and sediment control matters. | Q1 | ٠ | 25% | | | |
| g) Monitor compliance with development approvals for sediment and erosion control matters. | Q1 | ٠ | 25% | | | |

Key Initiative KI4.11 Key Initiative 11 - Partner with industry leaders to investigate alternative sources of energy that provide environmental, community and economic benefits.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|------------------------------------|------------|
| KI4.11.1 Support initiatives that advance alternative sources of energy for Redlands Coast. | | | Community and Economic Development | |
| a) Investigate opportunities to advance a green hydrogen economy for Redlands Coast. | Q1 | • | 25% | |

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| b) Facilitate partnerships with industry and all levels of government to externally fund green hydrogen projects. | Q1 | • | 25% | |
|---|----|---|-----|--|
| c) Promote opportunities to partner with governments, industry and research sectors on green energy initiatives to secure economic, environmental and social opportunities for Redlands Coast. | Q1 | • | 25% | |

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Natural Environment - Key Activities and Highlights

Deliver the Redlands Coast Biosecurity Plan 2018-2021.

To prevent and detect invasive biosecurity matter, 210 properties were visited and inspected (626 hectares). As a result, 22 advisory notices were issued for declared plants, four General Biosecurity orders issued, and three Biosecurity orders relating to declared plants. Council removed seven prohibited plants from properties. To increase awareness and education of biosecurity and pest management, Council participated in two market stalls and a community education event on Karragarra Island. Information on declared weeds was provided to local produce stores throughout Redlands Coast.

Develop a local waste recycling and reduction plan to move towards a zero waste future.

Council initiated the coordination of community consultation responses and the new draft State Organics Action Plan, supporting finalisation of the Waste Reduction and Recycling Plan 2012-2025.

Plan for the Minjerribah Panorama Coastal Walk Project on North Stradbroke Island (Minjerribah).

Works on Stage 1 of the Minjerribah Panorama Coastal Walk link at Point Lookout, North Stradbroke Island/Minjerribah have progressed and are nearing completion. Stage 2 planning and procurement has commenced.

Implement the Koala Conservation Strategy 2016 and review the Koala Conservation Action Plan 2016-2021.

A review of the Koala Conservation Action Plan 2016 is underway.

Continue to implement the Sub Regional Waste Procurement Process.

Council continued to collaborate with the working group to progress the Sub Regional Waste Procurement Process.

Implement Council's fire management program.

Community engagement and communication activities such as meetings, training, and educational/information sessions were delivered at various locations. Participation in these activities has included community champions, Council, specific property owners and also the general public. Fire risk management activities undertaken have included planned burns, post-burn weed management and proactive slashing. Vegetation management in fire access trails has also occurred across the mainland and Southern Moreton Bay Islands.

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Provide education opportunities to the Redlands Coast community to support, enhance and encourage environmental understanding and grow environmental connections.

The Get Grubby program, which supports educators, families and children to learn more about sustainable living options, was launched in September 2021. Through the program, educators and families can access 10 units that are supported by a range of activity and experience kits. School holiday programs in July and September 2021 included native bee workshops, geckoes wildlife with Ranger Stacey, marine biologist for a day workshop with Ocean Life Education, big bang science, nature photography, STEAM based nature inspired creative workshops, sustainable fashion workshop, outdoor nature plan and citizen science (iNaturalist). Council's Eco Market was also held in September 2021. The re-scheduled National Tree Day planting was held at IndigiScapes. The planting attracted 68 volunteers, who successfully planted 1,700 native plants in two hours.

Implement the Wildlife Connections Action Plan 2018-2023.

Council's Community Bushcare team and volunteers promoted weed awareness at the annual Trees for Weed events, held on Saturday 18 September 2021 at the Bay Islands and North Stradbroke Island (Minjerribah), with residents invited to exchange weeds for native plants. More than 720 plants were handed out across the islands to the community in interactive community education events that highlighted two weeds; Singapore Daisy and ground Asparagus, and promoted the benefits of native plants. Council has commenced development of a Redlands Coast Flying-Fox Management Plan to manage an ecologically sustainable approach to flying-foxes across the City. Through Council's Environmental Partnerships Program, a community education workshop was held in September 2021. Officers presented information about common weeds found in Redlands Coast, and provided a field demonstration on weeding techniques.

Implement waterway catchment management programs.

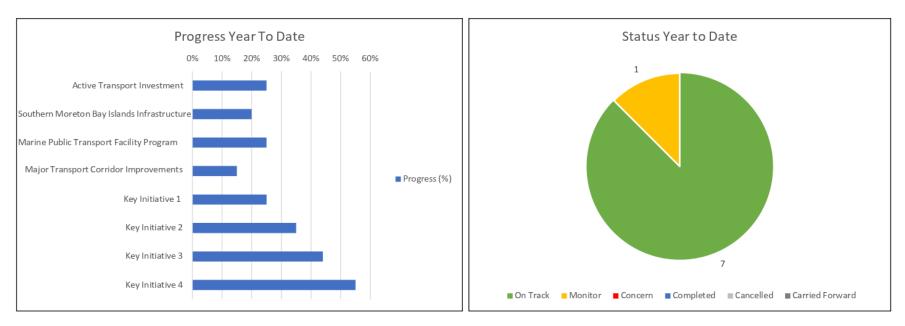
Waterway catchment management programs received 52 customer requests for investigations into potential contaminate release and dust concerns. 27 investigations were concluded, with 25 remaining open for ongoing monitoring. 15 proactive investigations were raised in the quarter, with 12 currently under routine monitoring. Council's recreational water quality monitoring is conducted monthly in winter (April – September) and fortnightly during summer months (October – March) on sites located across the mainland and islands. Since the start of the financial year five routine samples have been collected at each of the recreational water quality sites throughout Redlands Coast.

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Liveable Neighbourhoods



For details on Liveable Neighbourhoods Key Initiatives 1-4 please see pages 38 to 40 of this attachment.

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Liveable Neighbourhoods

Catalyst Project CP5.1 Active Transport Investment - Pivot Council's existing transport expenditure to deliver a step-change in active transport connectivity across the city, through improving cycling and pedestrian facilities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|-------------|------------|
| CP5.1.1 Continue to provide transport connectivity across Redlands Coast by improving cycling and pedestrian facilities. | | | City Assets | 5 |
| a) Develop capital works program planning to improve Q1 connectivity of footpaths and cycleways. | | | 25% | |
| b) Deliver the capital works program to construct footpaths and cycleways. | Q1 | • | 25% | |
| c) Continue to advocate with funding partners to align existing and future projects to targeted funding opportunities. | Q1 | • | 25% | |

Catalyst Project CP5.2 Southern Moreton Bay Islands Infrastructure - Consolidate existing plans and strategies to develop a roadmap to improve infrastructure on the islands.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------|------------|
| CP5.2.1 Consolidate existing plans and strategies for infrastructure across the Southern Moreton Bay Islands. | | | | 5 |
| a) Define the project scope and identify resourcing required to consolidate strategies and plans from a planning, social, infrastructure and environmental perspective. | Q1 | • | 20% | |

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Catalyst Project CP5.3 Marine Public Transport Facility Program - Progress the upgrade of the four Southern Moreton Bay Islands ferry terminals and review development opportunities both on adjoining land and through the repurposing of existing structures for recreational purposes.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-------------|------------|
| CP5.3.1 Deliver Southern Moreton Bay Island jetty upgrades in partnership with the Queensland Government. | | | City Assets | 5 |
| a) Collaborate with the Queensland Government to deliver stages of nominated sites. | Q1 | ٠ | 25% | |
| b) Monitor project progression including expenditure of the overall project. | Q1 | • | 25% | |

Catalyst Project CP5.4 Major Transport Corridor Improvements - Including Wellington Street and Panorama Drive Upgrade Program – Deliver Stage 1 of the multi-stage upgrade of this major transport corridor.

| Operational Plan 2021-2022 Activity | | Progress | Commentary | |
|---|----|----------|-------------|--|
| CP5.4.1 Deliver the Wellington Street and Panorama Drive Upgrade Program Stage 1 in partnership with the Federal Government. | | | City Assets | |
| a) Complete service relocations and commence civil construction works. | Q1 | • | 15% | Planning for service relocations has been completed, on-site works have not commenced and expect to release multi- year construction tender to market in late quarter two. |

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Key Initiative KI5.1 Key Initiative 1 - Undertake planning to create attractive and vibrant city centres where people can live and work locally.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | |
|--|----|---|----------|------------------------------------|--|
| KI5.1.1 Leverage the centres management approach to support mixed-use centres that provide opportunities for living and working locally. | | | | Community and Economic Development | |
| a) Identify development and investment opportunities. | Q1 | | 25% | | |
| b) Coordinate local activity to improve and promote liveability of centres. | Q1 | • | 25% | | |

Key Initiative KI5.2 Key Initiative 2 - Develop a framework for an ongoing local area planning program to provide solutions to meet the specific needs of individual local areas and support their unique character and identity.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | | |
|--|---|---|----------|------------------------------|--|--|
| KI5.2.1 Develop a framework for an ongoing local area planning p | KI5.2.1 Develop a framework for an ongoing local area planning program. | | | City Planning and Assessment | | |
| a) Define local area planning, identify key locations that require finer grade planning, establish a level of significance and priority, and prepare a work program. | Q1 | • | 25% | | | |
| b) Commence a statutory review of the Local Government Infrastructure Plan. | Q1 | ٠ | 25% | | | |
| c) Continue collaboration and input into the review of planning provisions for the City's canal and lakeside estates. | Q1 | ٠ | 25% | | | |
| d) Finalise Major Amendment 05/19 – South West Victoria Point Local Plan. | Q1 | ٠ | 50% | | | |
| e) Continue to progress land use investigations of the Southern Thornlands Potential Future Growth Area including drafting any required amendments to City Plan. | Q1 | • | 50% | | | |

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Key Initiative KI5.3 Key Initiative 3 - Engage our community to co-design the liveability of their neighbourhoods through planning, placemaking, and the management of community assets.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------|--------------------|
| KI5.3.1 Engage the Redlands Coast community to co-design the liveability of their neighbourhoods through planning, placemaking, and the management of community assets. | | | City Plann | ing and Assessment |
| a) Progress two minor/administrative amendments and Major Amendment 02/20 - General. | Q1 | • | 25% | |
| b) Commence implementation of Major Amendment 03/19 – Heritage with a supporting incentives package and implement Temporary Local Planning Instrument 01/21 – Protection of Local Heritage Places (TLPI 01/21) as an interim protection measure. | Q1 | • | 75% | |
| c) Finalise Major Amendment 04/20 - Medium Density Residential Zone Code Amendment. | Q1 | ٠ | 50% | |
| d) Finalise Major Amendment 01/21 Environmental. | Q1 | • | 25% | |

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Key Initiative KI5.4 Key Initiative 4 - Deliver the Redlands Coast Transport Strategy, to create a more efficient, accessible and safer integrated transport system.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------|--------------------|
| KI5.4.1 Deliver the Redlands Coast Transport Strategy 2041. | | | City Plann | ing and Assessment |
| a) Commence development of the Redlands Coast Active Transport Plan. | Q1 | ٠ | 25% | |
| b) Implement the Weinam Creek car share initiative. | Q1 | | 100% | |
| c) Commence an options analysis for the Northern Greenway Transport Corridor. | Q1 | • | 40% | |

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Liveable Neighbourhoods - Key Activities and Highlights

Leverage the centres management approach to support mixed-use centres that provide opportunities for living and working locally. Investment opportunities for Cleveland were promoted through development of a Cleveland Productive Precinct Prospectus, complementing Council's coordinated approach to Centres Management.

Engage the Redlands Coast community to co-design the liveability of their neighbourhoods through planning, placemaking, and the management of community assets.

At its General Meeting on 18 August 2021 Council resolved to approve the proposed Major Amendment 01/21 Environment for the purposes of State interest review. Public consultation on the proposed Major Amendment 04/20 - Medium Density Residential zone code review was undertaken from 4 August to 16 September 2021. At the close of the submission period 120 submissions had been received.

Develop a framework for an ongoing local area planning program.

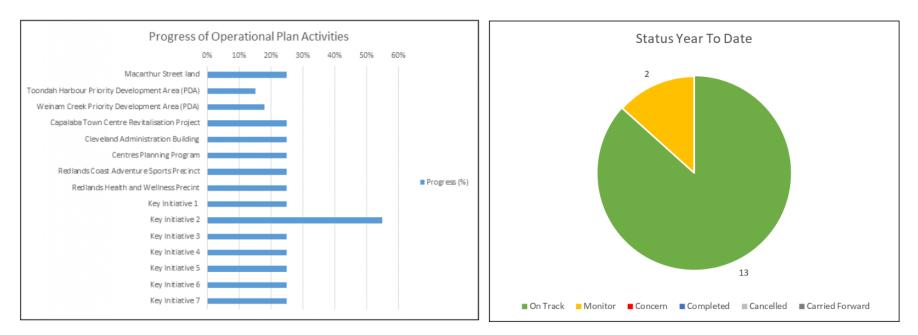
Council at its General Meeting in August 2021 approved changes to the draft South West Victoria Point Local Plan to address conditions imposed by the Planning Minister.

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Thriving Economy



For details on Thriving Economy Key Initiatives 1-7 please see pages 47 to 50 of this attachment.

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Thriving Economy

Catalyst Project CP6.1 MacArthur Street land - Seek partnerships for investment in infrastructure and facilities that balance enhanced social and economic opportunities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------|-------------------|
| CP6.1.1 Seek partnerships for investment in infrastructure and facilities for MacArthur Street Land. | | | Environmer | nt and Regulation |
| a) Finalise recommendations in relation to the social and economic opportunities for the land. | Q1 | • | 25% | |

Catalyst Project CP6.2 Toondah Harbour Priority Development Area (PDA) - Progress Council's obligations under its Development Agreement with Economic Development Queensland and Walker Corporation to support the commitment to reinforce Toondah Harbour as the regional gateway to Moreton Bay and the islands.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | |
|--|----|---|-------------|---|--|
| CP6.2.1 Progress the delivery of the Toondah Harbour PDA. | | | Office of t | Office of the General Manager Organisational Services | |
| a) Report on progress of the development of the Environmental Impact Statement (EIS) covering coastal processes, water quality, aquatic ecology, fisheries, shorebirds, koalas and cultural heritage. | Q1 | • | 15% | The draft EIS has been submitted to the Department of Agriculture, Water and the Environment and is in the adequacy review stage. | |
| b) Report on the implementation of a public consultation process on the final EIS. | Q1 | • | 15% | Public Notice period (40 business days) will commence when the Department of Agriculture, Water and the Environment approves the release of the draft EIS to the community. | |

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Catalyst Project CP6.3 Weinam Creek Priority Development Area (PDA) - Progress Council's staged master-plan that will transform the waterfront of Redland Bay, provide community infrastructure for local families, improve the usability of the transport hub, attract visitors, create jobs and drive economic development in the area.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | | |
|--|---|---|----------|---|--|--|
| CP6.3.1 Progress the delivery of the Weinam Creek PDA. | P6.3.1 Progress the delivery of the Weinam Creek PDA. | | | Office of the General Manager Organisational Services | | |
| a) Finalise the PDA master-plan with Economic Development Queensland. | Q1 | ٠ | 25% | | | |
| b) Progress Stage 3 of the Weinam Creek PDA project involving civil works on the northern side of Weinam Creek, and investigate boat ramp changes and carpark alterations. | Q1 | • | 15% | The master-plan for the Weinam Creek Priority Development area has been submitted to Economic Development Queensland for assessment. The master- plan was available for public consultation in quarter one of 2021-2022. The next step is for Economic Development Queensland to review the application and submissions made during the public consultation period and make a decision on the application. | | |

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Catalyst Project CP6.4 Capalaba Town Centre Revitalisation Project - Progress the revitalisation of Capalaba Town Centre to create a vibrant mixed-use centre anchored by community, commercial, and retail facilities, and activated public spaces with integrated transport.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------|-----------------------------|
| CP6.4.1 Progress the Capalaba Town Centre Revitalisation Project. | | | Communit | ty and Economic Development |
| a) Coordinate the project steering committee and maintain oversight over deliverables. | Q1 | • | 25% | |
| b) Leverage wider investment opportunities for the Capalaba Town Centre. | Q1 | • | 25% | |

Catalyst Project CP6.5 Cleveland Administration Building - Undertake a review of Council's Cleveland accommodation requirements.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|-------------|--|
| CP6.5.1 Commence a review of Council's Cleveland accommodation. | | | Office of t | he General Manager Organisational Services |
| a) Undertake pre-planning activities to determine options for Council's office footprint in Cleveland. | Q1 | • | 25% | |

Catalyst Project CP6.6 Centres Planning Program - Accelerate a centres planning program to enhance the city's productive precincts including the Cleveland CBD, creating opportunity for business investment, and increase in skills and capacity for employment.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------------------------------|------------|
| CP6.6.1 Implement a centre management approach to enhance Redlands Coast key economic and lifestyle precincts. | | | Community and Economic Development | |
| a) Establish a place management governance and delivery model with an initial focus on Cleveland CBD. | Q1 | • | 25% | |

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| b) Deliver priority projects focused on economic impact, activations and placemaking. | Q1 | • | 25% | |
|---|----|---|-----|--|
| c) Review progress in Cleveland and apply learnings to the implementation of other centres. | Q1 | • | 25% | |

Catalyst Project CP6.7 Redlands Coast Adventure Sports Precinct - Progress the new Redlands Coast integrated Aquatic Centre and Olympic standard Canoe-Kayak Slalom facility to provide additional sport, recreation and emergency services education and training facilities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|----------|-----------------------------|
| CP6.7.1 Undertake planning for integrated aquatic and canoe/ka facilities. | | | | ty and Economic Development |
| a) Progress detailed master-planning for an integrated aquatic Olympic standard whitewater facility. | Q1 | • | 25% | |
| b) Continue planning and advocacy for the 2032 Olympic and Paralympic Games. | Q1 | • | 25% | |
| c) Leverage opportunities connected to the broader Birkdale Community Precinct. | Q1 | • | 25% | |

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Catalyst Project CP6.8 Redlands Health and Wellness Precinct - Facilitate the delivery of the Redlands Health and Wellness Precinct as a catalyst economic opportunity to create an expanded health, social services, education and allied health cluster that can generate employment growth, and promote higher order specialist and tertiary health services.

| Operational Plan 2021-2022 Activity | | Progress | Commentary | |
|--|----|----------|------------|-----------------------------|
| CP6.8.1 Collaborate with industry and government to advance the Redlands Health and Wellness Precinct. | | | Communit | ty and Economic Development |
| a) Collaborate with project partners for delivery of the Redlands Health and Wellness Precinct and establish a Memorandum of Understanding with key stakeholders and other relevant parties. | Q1 | • | 25% | |
| b) Contribute to activating a legislative pathway for delivering the Redlands Health and Wellness Precinct Master Plan. | Q1 | • | 25% | |

Key Initiative KI6.1 Key Initiative 1 - Continue to implement the Redlands Economic Development Framework 2041 and key industry sector plans to grow the city's economy in key sectors:

- Health Care and Social Assistance
- Education and Training
- Rural Enterprise
- Manufacturing
- Tourism

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|----------|-----------------------------|
| KI6.1.1 Implement plans to grow the Redlands Coast economy in key economic industry sectors. | | | Communit | ty and Economic Development |
| a) Finalise and commence implementation of the Redland City Manufacturing Industry Sector Plan 2021-2026. | Q1 | • | 25% | |

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| b) Contribute to implementation of the Redlands Coast Destination Management Plan 2021-2026. | Q1 | • | 25% | |
|--|----|---|-----|--|
| c) Continue to implement industry sector plans for health care and social assistance, the education and training industry, and the rural enterprises industry. | Q1 | • | 25% | |

Key Initiative KI6.2 Key Initiative 2 - Promote Redlands Coast as a destination of choice through the delivery of the Redlands Coast Destination Management Plan.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|---------------------------------------|------------|
| KI6.2.1 Deliver the Redlands Coast Destination Management Plan 2021-2026. | | | Communication, Engagement and Tourism | |
| a) Finalise the Redlands Coast Destination Management Plan 2021-2026. | Q1 | • | 85% | |
| b) Implement actions of the Redlands Coast Destination Management Plan 2021-2026 | Q1 | • | 25% | |

Key Initiative KI6.3 Key Initiative 3 - Advocate for improved digital infrastructure to enhance the city's capacity to attract business investment and growth and enhance productivity.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|------------------------------------|------------|
| KI6.3.1 Undertake advocacy with all levels of government and industry for improved data and communications infrastructure to support Redlands Coast current and future business needs. | | | Community and Economic Development | |
| a) Advocate for expansion of the national broadband network business grade fibre zone in key business precincts across Redlands Coast. | Q1 | • | 25% | |

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| b) Support identification of funding opportunities for expansion | Q1 | 25% | |
|--|----|-----|--|
| of Council's dark fibre project and opportunities for businesses | | | |
| to connect. | | | |

Key Initiative KI6.4 Key Initiative 4 - Collaborate with community, industry, local businesses, and entrepreneurs to realise opportunities in the circular economy.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|------------------------------------|------------|
| KI6.4.1 Promote opportunities in the circular economy. | | | Community and Economic Development | |
| a) Facilitate partnerships that encourage industry and business to participate in the circular economy. | Q1 | • | 25% | |

Key Initiative KI6.5 Key Initiative 5 - Deliver initiatives to attract investment to Redlands Coast across key industry sectors, and support local businesses.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---------|-----------------------------|------------|
| KI6.5.1 Boost the Redlands Coast investment profile through targeted proactive strategy, marketing and focused implementation. | | Communi | ty and Economic Development | |
| a) Develop an investment attraction plan to support growth of the Redlands Coast economy, based on sound economic analysis. | Q1 | • | 25% | |
| b) Produce an investment prospectus that captures the current pipeline of investment, local success stories, specific opportunities and identifies new incentives. | Q1 | • | 25% | |
| c) Develop a marketing plan that drives implementation and supports a seamless customer experience. | Q1 | • | 25% | |

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Key Initiative KI6.6 Key Initiative 6 - Support the attraction and delivery of events through improved infrastructure at event locations across the city.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|---------------------------------------|------------|
| KI6.6.1 Review events infrastructure requirements at Council venues. | | | Communication, Engagement and Tourism | |
| a) Investigate event infrastructure requirements at key Council venues, such as parks and the showground, to maximise event attraction. | Q1 | • | 25% | |

Key Initiative KI6.7 Key Initiative 7 - Support key festivals that drive visitors to Redlands Coast.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|--|---------------------------------------|------------|
| KI6.7.1 Deliver actions from the Redland City Events Strategy and Action Plan 2017- 2022. | | | Communication, Engagement and Tourism | |
| a) Attract and retain key festivals and events on Redlands Coast. | Q1 | | 25% | |

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Thriving Economy - Key Activities and Highlights

Deliver the Redlands Coast Destination Management Plan 2021-2026.

The Redlands Coast Destination Management Plan is on track to be adopted by Council in late 2021.

Boost the Redlands Coast investment profile through targeted proactive strategy, marketing and focused implementation.

A Cleveland Productive Precinct Prospectus was developed that focuses on the location proposition, major project pipeline and investor/business attraction.

Progress the delivery of the Weinam Creek PDA.

Further detailed negotiations continued with Emergency Services (Australian Volunteer Coast Guard, Queensland Police Service, Queensland Fire and Emergency Service, Queensland Ambulance Service and the State Emergency Service) on the draft master-plan.

Implement plans to grow the Redlands Coast economy in key economic industry sectors.

The development of the draft Manufacturing Industry Sector Plan continued to be progressed with KPMG consultants, with the draft plan to be reviewed by Council. Work continued on strengthening the network of manufacturing businesses through a planned Business Brews networking event in October 2021. Implementation of actions under the Health Care and Social Assistance Industry Sector Plan 2018-2023 continued, while delivery of the Health Care and Social Assistance Summit was postponed from financial year 2020-2021 quarter four due to COVID-19 international and State border restrictions. Engagement with education stakeholders to identify education pathway opportunities, an initiative under the Education and Training Industry Sector Plan 2018-2023, continued to be undertaken with support provided to not-for-profit training organisations interested in expanding their services in the city.

Implement a centre management approach to enhance Redlands Coast key economic and lifestyle precincts.

Immediate issues have been identified and addressed within Cleveland centre, including signage and footpath hazards. A Cleveland investment prospectus was developed to promote and attract investment and business interest. A coordinated program of initiatives is in development, focused on economic outcomes and revitalisation.

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Undertake planning for integrated aquatic and canoe/kayak slalom facilities.

The International Olympic Committee announced Brisbane as host city for the 2032 Olympic and Paralympic Games on 21 July 2021. Redlands Coast was included as the location for a new canoe slalom venue. Birkdale Community Precinct was confirmed as the location of the Adventure Sports Precinct, which will include the Redland Whitewater Centre, as part of Council's adoption of the Birkdale Community Precinct Vision on 18 August 2021.

Progress the Capalaba Town Centre Revitalisation Project.

Redland Investment Corporation continued to work closely with preferred development partner Shayher Group to progress the draft masterplan and finalise commercial agreements.

Collaborate with industry and government to advance the Redlands Health and Wellness Precinct.

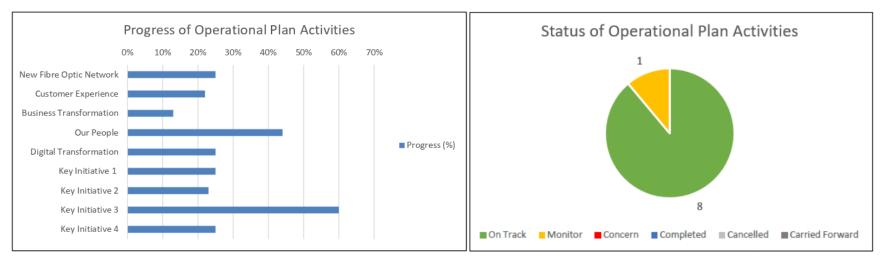
A stakeholder engagement program to advance the Redlands Health and Wellness Precinct, which will include reassessing land availability and strategic opportunities, was developed for progression in quarter two.

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Efficient and Effective Organisation



For details on Efficient and Effective Organisation Key Initiatives 1-4 please see pages 56 to 58 of this attachment.

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Efficient and Effective Organisation

Catalyst Project CP7.1 New Fibre Optic Network - Deliver Council's fast fibre optic network to provide savings for Council's own existing operating costs, improve connectivity between Council sites, while also offering capacity to improve services for local businesses.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|-----------|------------|
| CP7.1.1 Progress the Redlands Coast Fibre Optic Network. | | | Corporate | Services |
| a) Continue to build the fibre optic network across Redlands Coast including connecting Council sites to the network. | Q1 | • | 25% | |
| b) Investigate opportunities to optimise the fibre optic network for community benefit. | Q1 | • | 25% | |

Catalyst Project CP7.2 Customer Experience - Deliver Council's Customer Experience Strategy to enable customer centric service delivery which meets the community's changing needs, making it easier to work with Council and provides quality customer outcomes consistently. By engaging with our community we will work to enhance our digital first capabilities.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|--------------------------------|------------|
| CP7.2.1 Deliver Council's Customer Experience Strategy to enable customer centric service delivery. | | | Customer and Cultural Services | |
| a) Investigate options to establish a 'voice of customer' program. | Q1 | • | 20% | |
| b) Identify and review options to implement customer experience measures. | Q1 | • | 25% | |
| c) Identify customer experience training needs for employees and review training implementation options. | Q1 | • | 20% | |

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Catalyst Project CP7.3 Business Transformation - Deliver a Business Transformation Strategy to enable changes which reduce red tape, save money, and improve community outcomes.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary | |
|---|----|---|------------|--|--|
| CP7.3.1 Deliver the Business Transformation Strategy. | | | People, Cu | People, Culture and Organisational Performance | |
| a) Finalise the draft Strategy. | Q1 | • | 13% | Pre-engagement with key stakeholders across Council was undertaken to discuss the facilitation of Business Transformation Strategy development workshops. | |
| b) Implement the Strategy including engagement with key stakeholders and benchmarking with other organisations. | Q1 | • | 13% | Pre-engagement with key stakeholders across Council was undertaken to discuss the facilitation of Business Transformation Strategy development workshops. | |

Catalyst Project CP7.4 Our People - Deliver Council's People Strategy to enable a thriving organisation, supported by a healthy and inclusive work environment, organisational agility, cultural integrity and responsible leadership.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|--|------------|
| CP7.4.1 Embed and promote the Redland City Council employer brand. | | | People, Culture and Organisational Performance | |
| a) Embed the employee value proposition in all people practices within Council. | Q1 | • | 25% | |
| b) Deliver targeted and ad-hoc communications both internally and externally. | Q1 | • | 25% | |

| CP7.4.2 Embed Council's organisational values. | | | People, Culture and Organisational Performance | |
|---|----|---|--|--|
| a) Enable delivery of 'our team, our values' workshops to articulate team behaviours to values. | Q1 | • | 100% | |

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b) Promote and celebrate outcomes of 'our team, our values' Q1 **O** 25% and continue the conversation.

Catalyst Project CP7.5 Digital Transformation - Review and modernise Council's systems and implement digital processes to enable contemporary and easier ways to access and interact with Council information and services.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|--|----|---|-----------|------------|
| CP7.5.1 Develop an information management strategy for Council. | | | Corporate | Services |
| a) Identify opportunities to improve business process through automation and technology. | Q1 | • | 25% | |
| b) Identify ways to modernise Council's systems and implement digital processes. | Q1 | • | 25% | |

Key Initiative KI7.1 Key Initiative 1 - Review Council services to ensure we have the right offering, to meet our community needs and remain financially sustainable.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|--|------------|
| KI7.1.1 Review Council's services. | | | People, Culture and Organisational Performance | |
| a) Implement a self-service approach and service catalogue via Council's new intranet. | Q1 | • | 25% | |

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Key Initiative KI7.2 Key Initiative 2 - Continue our transformation toward asset management best practice by using improved data to shift from reactive to planned maintenance.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|--|------------|
| KI7.2.1 Continue to improve Council's asset management. | | | Strategic Asset and Portfolio Management | |
| a) Invest in data improvements. | Q1 | ٠ | 25% | |
| b) Strengthen the culture, increase the skills and knowledge of key stakeholders. | Q1 | • | 25% | |
| c) Develop a maintenance management strategy. | Q1 | | 20% | |

Key Initiative KI7.3 Key Initiative 3 - Improve our value for money delivery through the upgrade of our financial management system.

| Operational Plan 2021-2022 Activity | | | | Commentary |
|---|----|---|--------------------|------------|
| KI7.3.1 Improve Council's financial management system. | | | Corporate Services | |
| a) Implement Council's new financial management system. | Q1 | | 60% | |
| b) Optimise Council's financial business processes to enhance efficiency. | Q1 | • | 60% | |

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Key Initiative KI7.4 Key Initiative 4 - Strengthen internal efficiencies, external savings and Council's long-term sustainability to deliver on the Corporate Plan through implementation of strategic procurement practices and initiatives which improve opportunities for local, social, sustainable and good governance purchasing outcomes.

| Operational Plan 2021-2022 Activity | | | Progress | Commentary |
|---|----|---|------------|------------|
| KI7.4.1 Deliver the Procurement Transformation Program and continue to implement strategic contracting procedures including a forward contracting plan. | | | General Co | bunsel |
| a) Implement phase two of the MyBuy portal. | Q1 | | 25% | |
| b) Continue to develop ArcBlue dashboards to align with categories. | Q1 | • | 25% | |
| c) Continue to scope out and implement the contract management system. | Q1 | • | 25% | |
| d) Deliver category management training. | Q1 | • | 25% | |
| e) Increase local benefit and maintain a positive trend. | Q1 | • | 25% | |

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Efficient and Effective Organisation - Key Activities and Highlights

Progress the Redlands Coast Fibre Optic Network.

Tender for Stage 2A commenced in quarter one. Entered negotiations for commercial arrangements for Stage 1. This work reduced operational costs with microwave links retired and ceased fibre optic rental arrangements resulting in significant operational savings.

Embed Council's organisational values.

Council's leaders (supervisors and managers) within their teams workshopped the behaviours that align to Council's new organisational values.

Embed and promote the Redland City Council employer brand.

Continued external promotion of employer brand messages through the careers website, recruitment system, job advertisements and social media platforms such as LinkedIn. Alignment of new People Strategy with brand promise. Ongoing internal brand promotion through monthly employee letter, intranet and corporate onboarding.

Deliver the Procurement Transformation Program and continue to implement strategic contracting procedures including a forward contracting plan.

Quarter one saw a sustained level in local spend activity at approximately 26% or just over \$8m spent directly with local based suppliers. Additionally there are broader benefits with principal suppliers employing locals and working directly with local subcontractors and goods and service providers. Council has conducted two industry briefings to improve supplier awareness of procurement practices in conjunction with the Redlands Coast Chamber of Commerce and the Department of State Development, Infrastructure, Local Government and Planning. A modernised contract management system is in its final stage of implementation. Contract management training was completed with all key stakeholders and 80% of Council's procurement spend is covered by contracted performance criteria.

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Links to resources that support Operational Plan activities

City Leadership

Redlands Coast Smart and Connected City Strategy Your Say Redlands

Quandamooka Country

Calendar of Significant Events Native Title in the Redlands North Stradbroke Island Economic Transition Strategy

Liveable Neighbourhoods

Southern Moreton Bay Islands Ferry Terminals Upgrade Project Redland City Plan Redland City Plan amendments Redlands Coast Transport Strategy

Thriving Economy

Weinam Creek Priority Development Area Capalaba Town Centre Revitalisation Project Cleveland Centre Master Plan Redlands Coast Adventure Sports Precinct Future Planning and Development ASPIRE Circular Economy Business Platform

Strong Communities

Redlands Coast Regional Sport and Recreation Precinct Birkdale Community Precinct Age-friendly Action Plan 2021-2026 Grants and Sponsorship Program Community Safety Initiatives Redland City Education and Training Industry Sector Plan 2018-2023

Natural Environment

Minjerribah Panorama Coastal Walk Wildlife Connections Plan Wildlife in the Redlands - Koala Koala Conservation Program Fire Risk Management Program Coastal Hazard Adaptation Strategy Seqwater About City Water Redlands Coast Biosecurity Plan Indigiscapes Waterway management programs Recreational water quality

Efficient and Effective Organisation

Working for Redland City Council Tenders and Contracting

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Councillor Adelia Berridge declared a Declarable Conflict of Interest in the following item, stating that she was a submitter in relation to this item in 2018.

Councillor Berridge considered her position and was firmly of the opinion that she could participate in the discussion and vote on the matter in the public interest. (Refer Item 6.3 for vote)

Cr Berridge voted FOR the motion.

14 REPORTS FROM COMMUNITY & CUSTOMER SERVICES

14.1 MCU17/0057 AND MCU17/0058 - MATERIAL CHANGE OF USE FOR A MULTIPLE DWELLING AND RECONFIGURING A LOT FOR 3 INTO 2 LOTS AND NEW ROAD - 410-426 BOUNDARY ROAD, 359-379 AND 381-405 CLEVELAND REDLAND BAY ROAD, THORNLANDS

Objective Reference: A5330819

Authorising Officer: David Jeanes, Acting General Manager Community & Customer Services

Responsible Officer: Chris Vize, Acting Group Manager City Planning and Assessment

Report Author: Justin Leach, Planning Officer

- Attachments: 1. Acoustic Report <u>J</u>
 - Accustic Report <u>→</u>
 Architectural Plans ↓
 - 3. Architectural Plans U
 - 4. Engineering Plans 4
 - 5. Flooding Report <u>J</u>
 - 6. Landscape Plans
 - 7. Conditions \underline{J}
 - 8. Plan of Reconfiguration <u>J</u>
 - 9. Executed Infrastructure Agreement <u>J</u>
 - 10. Grounds for Refusal MCU17/0057 & MCU17/0058 <u>J</u>

PURPOSE

This application is referred to the General Meeting of Council for determination, due to the deviation of the request from the zoning set out by the Redlands Planning Scheme in the South East Thornlands structure plan area.

BACKGROUND

The Development Applications

Council received two applications (Council ref: MCU17/0057 and MCU17/0058) from Wolter Consulting Group Pty Ltd on behalf of the landowner Philip Usher Constructions Pty Ltd, on 10 September 2017. The applications sought development permits for:

MCU17/0057

Material change of use (MCU) for a multiple dwelling (129 units) and reconfiguring a lot for 3 into 2 lots and new road over land at 410-426 Boundary Road, 359-379 and 381-405 Cleveland-Redland

Bay Road, Thornlands, and more particularly described as Lot 18 on SP119616, Lot 10 on SP216148 and Lot 20 on SP140739.

MCU17/0058

MCU for a multiple dwelling (123 units) over part of the land (the balance) at 410-426 Boundary Road, 359-379 and 381-405 Cleveland-Redland Bay Road, Thornlands, and more particularly described as part of Lot 18 on SP119616, Lot 10 on SP216148 and Lot 20 on SP140739.

Due to the development applications being lodged on the same date, and essentially for staging purposes of the multiple dwelling development, this report relates to both applications and is structured as follows:

- Application A MCU17/0057
- Application B MCU17/0058

A red-e-map extract is provided in Figure 1 below, depicting the location of the development and the general areas subject to each development application.



Figure 1 – Development application areas in South East Thornlands (Red-e-map extract)

The key issues identified in the assessment are:

- Consistency of proposal with the Redlands Planning Scheme
- Building design/streetscape
- Access/road design
- Traffic
- Environmental values



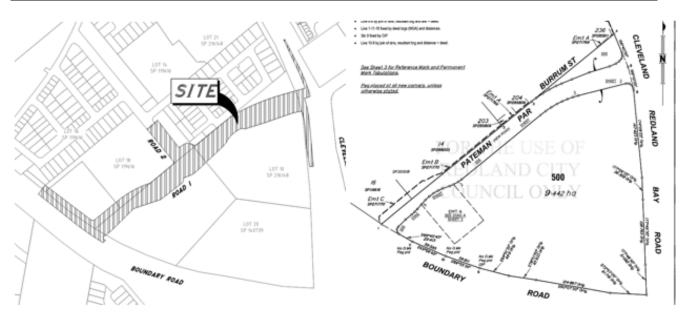
- Stormwater management
- Landscaping
- Open space design
- Earthworks
- Sewer
- Waste management
- Water
- Electricity/telecommunications and
- Road noise

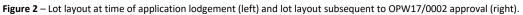
The above issues have been assessed in the report, in accordance with section 60 of the *Planning Act 2016,* and it is recommended that the applications be approved subject to conditions. **Planning history**

The following approvals are relevant to the site:

- MCU012923 Development permit for a material change of use for multiple dwellings (x103 units) was given in 2013, for the site adjoining the development to the north-west. The development permit has been changed multiple times subsequent to the initial approval, and most recently via a decision notice issued on 30 October 2018 under Council ref: MCU18/0127. The relevance of this development to the subject site is that a temporary stormwater solution was provided on the adjoining land (the subject site Figure 2 below), which will ultimately be directed to the stormwater facility proposed at the southern corner of the subject site. The development was also conditioned to provide four-way signalisation and associated works at the intersection of Pateman Parade, Boundary Road and Highgrove Street Thornlands, by both Council and SARA conditions. The landowner is the same for both sites.
- OPW17/0002 A standalone application for Operational works approval for the trunk collector extension (Pateman Parade), via negotiated decision notice approved 6 February 2018. The survey plan (Figure 2 below) approved as part of this approval (POS18/0079) created Lot 500 on SP306136 over the subject site, cancelling Lot 18 SP 119616, Lot 10 SP 216148 and Lot 20 SP 140739. It is noted that the Pateman Parade roadworks result in a left-in left-out arrangement onto Boundary Road, and terminate at the eastern extent prior to the connection with the Beveridge/Cleveland-Redland Bay Road intersection.







South East Thornlands Structure Plan

The site is within the South East Thornlands Structure Plan Overlay (SETSPO) area. The South East Thornlands structure plan was adopted by Council on 31 March 2010 and took effect as an amendment to the then Redlands Planning Scheme on 19 April 2010. A relevant extract from the SETSPO is located in Figure 3 below. The site is noted to be located within precincts 2a (attached housing), 3 (medium density housing) and 4f (flood prone area – central open space). A local park is also designated within the northern extent of the site.



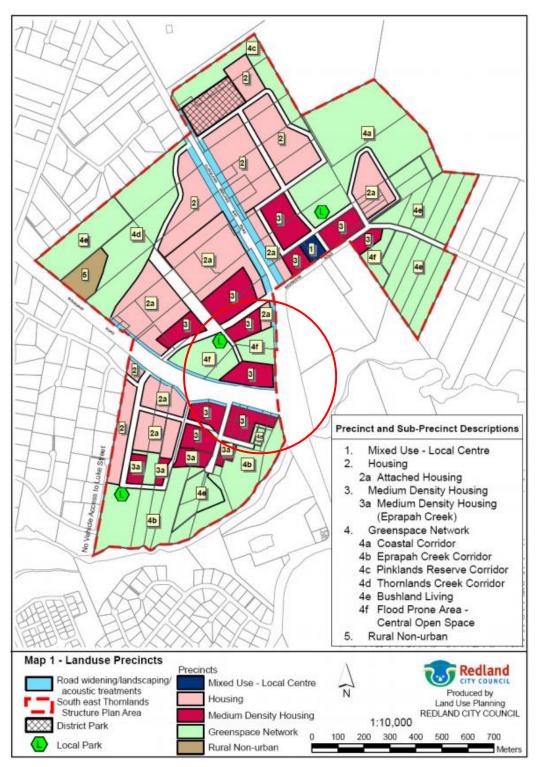


Figure 3 – South East Thornlands Structure Plan Overlay ISSUES

Proposal

Application A – MCU17/0057

The proposal includes developing land at the corner of Boundary Road and Cleveland-Redland Bay Road, Thornlands to create two (2) development lots; proposed Lot 1 (8.337 hectares) to be developed for townhouses and proposed Lot 2 (3,293m²) to remain a balance allotment to be

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developed in the future (refer Attachment 1). The lots are divided by substantial community infrastructure in the form of the new trunk connector road between Boundary Road and Cleveland-Redland Bay Road.

At initial lodgement Application A comprised 129 multiple dwelling units with 11 different unit typologies, ranging from a single-dwelling unit (manager's residence), duplexes, triplexes, quadplexes to row housing (6 attached units), within the western part of proposed Lot 1 (refer Attachment 2). Building height is limited to two storeys, with each dwelling unit provided private open space for passive recreation. A communal open space areas is located central to the site and complemented by the stormwater management wetland at the southern portion of the site. The proposal also includes dedication of a 10 metre wide strip at the Boundary Road and Cleveland-Redland Bay Road frontages for a landscape buffer area.

The development site is proposed to be accessed off of the new trunk collector (Pateman Parade), with an internal driveway arrangement providing vehicular/pedestrian movement. A total of 235 car parks are proposed as part of Application A.

Minor change

A response to the information request (IR) was initially provided on 7 September 2018 and a number of minor changes have been submitted since in response to further issues raised in the assessment. The changes are considered to collectively comprise a minor change under Schedule 2 of the *Planning Act 2016* as they do not result in substantially different development, and generally respond to matters raised in an information request. A summary of the changes and resulting proposed development is as follows:

- Reduction in proposed number of dwelling units from 129 to 119, resulting in:
 - 16 x 2 bedroom units
 - 103 x 3 bedroom units
- A low-rise two storey 'townhouse' style built form, with attached building product consisting of two, three or four contiguous units, while retaining a separate manager's residence.
- Central communal open space area with improved north/south and east/west pedestrian connections.
- Provision of 292 car parks.

An extract of the site layout (Figure 4) for Application A is extracted below:



17 NOVEMBER 2021



Figure 4 – Site layout Application A (changed plan)

Application B – MCU17/0058

Application B is limited to residential development over the eastern part of proposed Lot 1, and at initial lodgement comprised 123 multiple dwelling units over nine different typologies, ranging from duplexes, triplexes, quadplexes to row housing (six attached units) (refer Attachment 3). Building height is limited to two storeys, with each dwelling unit provided private open space for passive recreation, whilst also having access to the shared communal open space provided by Application A.

The development site is proposed to be accessed off of the new trunk collector (Pateman Parade), with a second access point to the internal driveway arrangement providing vehicular/pedestrian movement. A total of 225 car parks are proposed as part of Application B.

Minor change

A response to the IR was initially provided on 7 September 2018 and a number of minor changes have been submitted since in response to further issues raised in the assessment. The changes are considered to collectively comprise a minor change under Schedule 2 of the *Planning Act 2016* as they do not result in substantially different development, and generally respond to matters raised in an information request. A summary of the changes and resulting proposed development is as follows:

- Reduction in proposed number of dwelling units from 123 to 106, resulting in:
 - 6 x 2 bedroom units
 - 100 x 3 bedroom units



- A low-rise two storey 'townhouse' style built form, with attached building product consisting of two, three or four contiguous units, while retaining a separate manager's residence.
- Direct access to the central communal open space area (provided via Application A) with north/south and east/west pedestrian connections.
- Provision of 264 car parks, resulting in a total of 556 car parks across the site (inclusive of 58 visitor parks).

An extract of the revised site layout (Figure 5) Application B is extracted below:

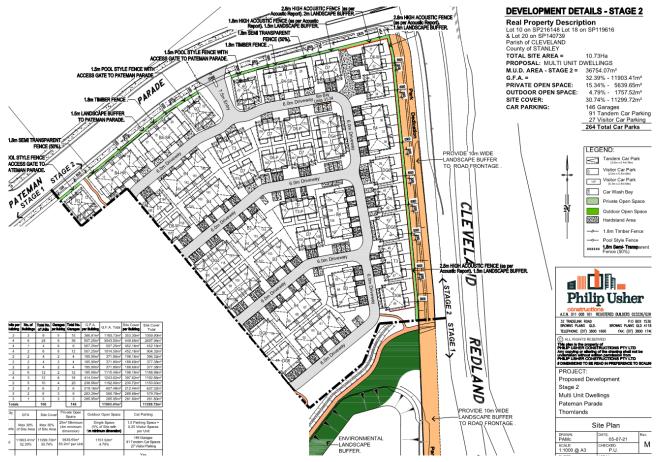


Figure 5 – Site layout Application B (changed plan)

Despite being lodged separately, both Application A and Application B are proposed to be constructed under a single works program.

Site & Locality

The site has an area of 94,420m² (excluding Pateman Parade corridor), and is currently improved by several sheds and shade structures associated with the Roundabout Nursery, located in the north-eastern corner of the site and south of Pateman Parade (refer Attachment 4). The site is predominantly clear of vegetation, however there are areas of native vegetation regrowth scattered across the site. The land slopes away from Boundary Road towards Cleveland-Redland Bay Road in an east to north east direction.

The site is triangular in shape with the apex at the southern end of the site, and formed by a roundabout, with Boundary Road comprising the south-western side boundary and Cleveland-Redland Bay Road comprising the eastern side boundary. The mostly constructed Pateman Parade



comprises the north-western side boundary, except for a smaller triangular section north of Pateman Parade that is approximately 3,450m² in area.

The site has a split zoning of medium density residential (MDR), urban residential (UR) sub-area UR1, community purpose (CP) sub-area CP7, and open space (OS) (refer Attachment 5). The site is the largest residential zoned area in the SETSPO area still to be developed, and is considered a prominent location when heading north along Cleveland-Redland Bay Road. The site adjoins UR and MDR zoned lots on all sides except for a community purposes sub-area CP3 (Faith Lutheran College) and larger conservation (CN) zoned lots to the east, bordering Eprapah Creek. The Victoria Point Shopping Centre is located within 500m of the site and the surrounding neighbourhood is a newly established residential area with a mixed density ranging from single dwelling houses to multiple dwellings.

Assessment framework

The application has been made in accordance with the *Planning Act 2016* Development Assessment Rules and constitutes an impact assessable application for a material change or use and reconfiguring a lot under the Redlands Planning Scheme V7.1.

In accordance with section 45 of the *Planning Act 2016*:

- (5) An *impact assessment* is an assessment that—
 - (a) must be carried out—
 - (i) against the assessment benchmarks in a categorising instrument for the development; and
 - (ii) having regard to any matters prescribed by regulation for this subparagraph; and
 - (b) may be carried out against, or having regard to, any other relevant matter, other than a person's personal circumstances, financial or otherwise.

Examples of another relevant matter—

- a planning need
- the current relevance of the assessment benchmarks in the light of changed circumstances
- whether assessment benchmarks or other prescribed matters were based on material errors
- (6) Subsections (7) and (8) apply if an assessment manager is, under subsection (3) or (5), assessing a development application against or having regard to—
 - (a) a statutory instrument; or
 - (b) another document applied, adopted or incorporated (with or without changes) in a statutory instrument.
- (7) The assessment manager must assess the development application against or having regard to the statutory instrument, or other document, as in effect when the development application was properly made.
- (8) However, the assessment manager may give the weight the assessment manager considers is appropriate, in the circumstances, to—



- (a) if the statutory instrument or other document is amended or replaced after the development application is properly made but before it is decided by the assessment manager—the amended or replacement instrument or document; or
- (b) another statutory instrument—
 - *(i)* that comes into effect after the development application is properly made but before it is decided by the assessment manager; and
 - (ii) that the assessment manager would have been required to assess, or could have assessed, the development application against, or having regard to, if the instrument had been in effect when the application was properly made.

Section 31 of the *Planning Regulation 2017* identifies that:

- '(1) For section 45(5)(a)(ii) of the Act, the impact assessment must be carried out having regard to—
 - (a) the matters stated in schedules 9 and 10 for the development; and
 - (c) if the prescribed assessment manager is a person other than the chief executive or the local government—the planning scheme; and
 - (d) if the prescribed assessment manager is a person other than the chief executive
 - (i) the regional plan for a region; and
 - (ii) the State Planning Policy, to the extent the State Planning Policy is not identified in the planning scheme as being appropriately integrated in the planning scheme; and
 - (iii) for designated premises—the designation for the premises; and
 - (e) any temporary State planning policy applying to the premises; and
 - (f) development approval for, and any lawful use of, the premises or adjacent premises; and
 - (g) common material.

common material, for a development application, means-

- (a) all the material about the application that the assessment manager receives before the application is decided, including—
 - (i) any material relating to a proposed development application that is substantially similar to the development application as made; and
 - (ii) any material attached to, or given with, the development application; and
 - (iii) any material relating to the application given to the assessment manager after the application is made; and
 - (iv) any referral agency's response, including any advice or comment given by a referral agency and any response given under section 57 of the Act; and
 - (v) any properly made submissions about the application, other than a submission that is withdrawn; and



- (vi) any other submission about the application that the assessment manager has accepted; and
- (vii) any other advice or comment about the application that a person gives to the assessment manager; and
- (b) if a development approval for the development is in effect—the approval; and
- (c) an infrastructure agreement applying to the premises.

Pursuant to section 45(5) of the *Planning Act 2016*, the application was assessed against the following applicable assessment benchmarks.

- Redlands Planning Scheme V7.1:
 - Medium density residential zone code
 - Urban residential zone code
 - Open space zone code
 - Community purposes zone code
 - Multiple dwelling code
 - Reconfiguration code
 - Acid sulfate soils overlay code
 - Flood prone, storm tide and drainage constrained land overlay code
 - Habitat protection overlay code
 - Road and rail noise impacts overlay code
 - South East Thornlands structure plan overlay code
 - Access and parking code
 - Erosion prevention and sediment control code
 - Excavation and fill code
 - o Landscape code
 - o Stormwater management code
- State Planning Policy 2017, Part E
- South East Queensland Regional Plan 2017
- Planning Regulation 2017, Schedule 11
- Local Government Infrastructure Plan

Pursuant to section 45(5) of the *Planning Act 2016,* Council had regard to the following matters in its assessment of the application.

- Existing development
- Current development approvals
- Submissions made during public notification
- State Assessment Referral Agency (SARA) response
- City Plan V5

Comments received

External comments received

Council has received comments that form part of the common material to the application. Council has had regard to this information in the assessment of the application, as outlined above.

State Assessment & Referral Agency (SARA)

The proposal was referred to SARA in accordance with Schedule 10, Part 9, Division 4, Subdivision 2, Table 1, Item 1 – State transport corridors and future State transport corridors; and Schedule 10, Part 9, Division 4, Subdivision 2, Table 4, Item 1 – State transport corridors and future State transport corridors. SARA provided referral agency responses dated 8 April 2021 for Application A (SARA reference: 1710-1963 SRA) and Application B (1710-1966 SRA).

SARA indicated no objection to the proposed development subject to referral agency conditions in regards to stormwater management and acoustic treatment (fences and building attenuation). The Department's referral response notices, including conditions, will be attached to Council's Decision Notice.

Public notification

The proposed development is impact assessable and required public notification. The applications were publicly notified for 16 business days from 20 September 2018 to 12 October 2018. A notice of compliance for public notification was received on 16 October 2018.

<u>Submissions</u>

There were a total of 121 properly made submissions received during the notification period (Application A – 10 submissions, Application B – 111 submissions). A further 17 submissions were received, which were not properly made but were accepted under Part 4 Section 19 of the Development Assessment Rules. The following planning matters were raised in the written submissions received.

- Zone/precinct changes inconsistent with SETSPO
- Interurban break not provided
- Filling of the flood prone area inconsistent with SETSPO and will impact on waterways
- Need for a local park in the area
- Reduction in open space and associated impacts on habitat corridor functionality and greenspace network/recreation space
- Built design out of character with surrounding area
- Density does not respond to capacity of roads, schools, health care etc
- Traffic impacts on surrounding roads network
- Insufficient infrastructure
- Stormwater management
- Pateman Parade not fauna friendly
- Koala offsets have not been considered
- Dedication of open space
- The development conflicts with desired environmental outcome (DEO) 1
- Stormwater infrastructure should not be located within the greenspace network

Internal comments received

- The application was referred to the divisional Councillor in accordance with standard procedure.
- The assessment manager has received assessment advice from the following Council teams/ officers:
 - Engineering Assessment
 - Environmental Assessment
 - Landscaping
 - Arborist
 - Infrastructure Charges Unit
 - o Health and Environment
 - Legal Services

Decision Making Rules

Section 60 of the *Planning Act 2016* states that:

- (3) To the extent the application involves development that requires impact assessment, and subject to section 62, the assessment manager, after carrying out the assessment, must decide—
 - (a) to approve all or part of the application; or
 - (b) to approve all or part of the application, but impose development conditions on the approval; or
 - (c) to refuse the application.
- (5) The assessment manager may give a preliminary approval for all or part of the development application, even though the development application sought a development permit.
- (6) If an assessment manager approves only part of a development application, the rest is taken to be refused.

Application assessment

The following section of this report provides an assessment against the relevant assessment benchmarks, relative to Application A – MCU17/0057 and Application B – MCU17/0058. As the two separate applications have been lodged to Council essentially for staging purposes the key issues raised are addressed for both applications, unless otherwise noted.

Consistency of proposal with the Redlands Planning Scheme

The development site includes a mix of zones including MDR, UR sub-area UR1, OS and CP subarea CP7. The development includes a reconfiguring a lot component, which is essentially a management subdivision that seeks to establish the multiple dwelling development on Lot 1, with Lot 2 as a balance lot on the northern side of Pateman Parade. As identified in the 'planning history' section above, a survey plan has already been registered for the road and lot alignment resulting from the operational works approval for the Pateman Parade road works under OPW17/0002. The resulting layout of the development does not align with the zone mapping in the Redlands Planning Scheme and the precincts set out by the SETSPO code. In particular the following are noted:



- Pateman Parade does not strictly follow the alignment of the CP zoned land.
- The residential component of the development is located wholly within the northern section of the site and is provided on land with a mix of zoning (MDR, UR, OS and CP).
- Communal open space and stormwater management for the development is provided at the southern extent of the site on land zoned as MDR and OS.
- A local park has not been provided on the subject site.

Accordingly, assessment of the development against the relevant provisions within relevant codes is required, which are detailed below.

SETSPO code

Specific outcome S1.3

'(2) Sub-Precinct 2a – Attached Housing is designed and located to provide an increased range of residential uses including multiple dwellings and aged persons and special needs housing.'

Specific outcome S1.4

(1) 'Precinct 3 – Medium Density Housing is designed and located to –

(a) provide for a range of medium density uses such as multiple dwellings, apartment buildings and aged persons and special needs housing;

- (b) limit overall building height to 13m and 3 storeys except in Sub-precinct 3a;
- (c) maximise views and outlook across adjoining areas of open space;

(d) facilitate convenient walking to -

(i) public open space and local and district parks; or

(ii) convenience shopping employment and community opportunities within Precinct 1 – *Mixed Use* – *Local Centre; or*

(iii) higher order facilities and bus interchange at Victoria Point Major Centre

(e) ensure that where development adjoins Precinct 4 – Greenspace network it is located and designed so as not to compromise adjoining environmental values or the hydraulic capacity and ability of the adjoining land to accommodate the one percent AEP flood;

(f) ensure a building layout and design that -

(i) contributes to the establishment of an attractive streetscape;

(ii) reduces building bulk by a combination of balconies, recesses and variations in building form and materials;

(iii) requires roofs to be pitched, articulated, gabled or other features to avoid single plane or flat rooflines;

(iv) on lot 20 on SP140739 incorporates a range of design treatments that:

• ensure the establishment of a high quality attractive streetscape on the corner of the visually prominent intersection of Cleveland-Redland Bay Road and Boundary Road;



• positively recognises the strategic position of the locality as the southern gateway to the South East Thornlands Structure Plan area.'

Overall outcome 5.14.7(2)(d)

'Sub-precinct 4f Flood Prone Area – Central Open Space protects and enhances publicly owned land that:

- maintains the hydraulic capacity, water quality and ecological values of this locally important drainage line;
- incorporates a local park;
- provides opportunity for establishing habitat and movement corridor for koalas and other fauna;
- in combination with the existing school grounds and bushland areas to the east and south east provides for a physical and visual break between the urban communities of Thornlands and Victoria Point.'

MDR zone code

S2.4

((1) Lot reconfiguration creates larger lot sizes that provide opportunities for medium density housing uses;

(2) On sub-arterial roads, consolidates lots to allow access to the development from alternative lower order roads to maximise high order road efficiency and safety;'

UR zone code

S2.4(1) 'Reconfiguration provides a mix of lot sizes to accommodate a variety of dwelling types.'

S2.8(2) 'Reconfiguration results in pleasant environments and reduced energy consumption through being climatically responsive by –

(a) lots being orientated and of a length and width to –

(i) maximise solar access to the north in winter;

(ii) minimise solar access to the east and west in summer;

(b) having regard to the topography of the land.'

Open space zone code

S1.3(1) 'Reconfiguration -

(a) facilitates the dedication of open space land to Council as non-trunk or trunk infrastructure as identified in Part 10 – Priority Infrastructure Plan;

(b) enhances social, cultural and recreational opportunities;

(c) provides linkages between existing and/or open space areas;

(d) does not prejudice the future use of this land for open space purposes.'

Strategic framework 3.2.3



(7) Recreation and Open Space -

(a) The City's key recreational resources are identified and protected for recreational purposes through complementary inclusion generally in the Open Space and Conservation Zones. Refer to Diagram 5 - Open Space and Recreation Areas and Facilities and Diagram 6 - Open Space and Recreation Areas - Local Government Control.

(b) Recreational resources and open space are provided to:

(i) ensure a diversity of recreation settings;

(ii) encourage multiple use of open space;

(iii) co-locate compatible recreation activities;

(iv) ensure a non-motorised recreation trail network is integrated into the open space and transport networks;

(v) ensure linkages are provided between open space areas;

(vi) provide a significant contribution to the greenspace character of the City and the region.'

CP zone code

S1.3 'Reconfiguration does not prejudice the intended use of this zone for its specified community purpose.'

Reconfiguration code

S1.1(1) 'The reconfiguration design –

(a) responds to the landscape setting and topography of the location;

(b) protects environmental values and functions including habitat areas, corridors and waterways;

(c) responds to potential impacts of previous land uses such as land contamination;

(d) does not result in lots that are subject to risk or hazard from flood, bushfire or landslip;

(e) maintains a high level of environmental amenity from existing or potential emissions such as noise and air quality impacts;

(f) does not compromise the ongoing operation of existing uses;

(g) integrates with surrounding neighbourhoods and areas of economic or social activity;

(h) ensures future development on adjacent and nearby lands is able to occur in an orderly, efficient and cohesive manner;

(i) complements existing attractive streetscapes and aids in establishing attractive streetscapes in newly developing areas;

(j) takes into account the location, size, accessibility and function of existing and future open space networks;



(k) enhances personal safety and perception of safety and minimises potential for crime, vandalism and fear through achievement of surveillance by drivers, pedestrians and occupants.'

The development is considered to comply with the above assessment benchmarks as follows:

- The reconfiguring a lot facilitates a suitable alignment for Pateman Parade, which has been assessed previously (under OPW17/0002) and subsequently plan sealed, dedicating the road alignment and creating the large Lot 1 for the proposed multiple dwelling development and Lot 2 as a balance lot to the north of Pateman Parade. It is considered that the shape and size of Lot 2 is suitable for providing medium density attached housing as sought by the zoning of this part of the land.
- The development facilitates the proposed medium density attached housing wholly in the northern section of the site and an open space area for stormwater management, environmental function, and communal open space at the southern part of the site. It is identified that the original zoning sought for the residential development to be at the northern and southern extents of the site separated by a central east/west open space corridor primarily for stormwater function. Accordingly, the proposal essentially swaps the zoning of the land to achieve this.
- The resulting proposal allows for an increase in amenity for future residents of the site as the development is set back considerably from the Boundary Road and Cleveland-Redland Bay Road intersection. A more effective 'break' between Victoria Point and the structure plan area is also provided, increasing the visual amenity of the location from Boundary and Cleveland-Redland Bay Roads.
- While the open space area is not dedicated to Council, and does not comply with the relevant specific outcome of the open space zone code, the proposal is considered to meet the intent of the SETSPO code and the strategic framework 3.2.3, specifically section 7(b) as the development allows for provision of open space as follows:
 - A range of recreational activities for the residents of the multiple dwelling development in the communal open space and kick and play areas.
 - Linkages are provided for pedestrian and cycle traffic primarily along Pateman Parade, but also through the site via clear pedestrian linkages.
 - The existing open space zoned land does not exhibit significant environmental values. The proposed open space area provides stormwater treatment and management, as well as revegetation to enhance habitat values.
 - A local park has been delivered to the north along Harrington Boulevard, which services the Esperance estate and the subject site.
- To enable the location of the stormwater management and communal open space areas at the southern extent of the site significant earthworks are required to be undertaken which includes reshaping of the drainage line within the site. The resulting landform will still ultimately discharge stormwater under Cleveland-Redland Bay Road to the Eprapah Wetlands to the east, as per the current scenario. On-site stormwater quantity and quality treatment, and associated revegetation works, will result in the hydraulic capacity, water quality and ecological values of this locally important drainage line being enhanced. Further



detail on this matter and impacts such as flooding is provided in the 'stormwater management' section.

- A local park has been delivered to the north of the subject site along Harrington Boulevard as part of an earlier development approval. The park is within a reasonable walking distance of the intended catchment, including the subject site, which will also be serviced by communal open space areas. The 'open space' area (recreational and stormwater) on the site will service the development and will be maintained in private ownership, as part of a community title, with the developer responsible for the maintenance of the stormwater management structures. An easement is to be secured via a condition to enable public access through this open space area via the public footpath.
- A revised vegetation management plan is a recommended condition of approval to ensure planting and revegetation works within the redefined 'open space' area meets the intent of precinct 4f of the SETSPO code.
- The development is conditioned to provide an extension to Pateman Parade and the fourth leg of the existing Beveridge/Cleveland-Redland Bay Road signalised intersection. Completion of Pateman Parade will provide a link to the surrounding neighbourhood, shops and services. The development also provides the finalisation of the east/west pedestrian cycle link along Pateman Parade joining in to the intersection at Beveridge/Cleveland-Redland Bay Roads. An internal east/west and north/south pedestrian link is provided to allow residents of the development to access the open space area at the south and external connections.
- The development facilitates suitable interaction with the streetscape in terms of built form and landscaping. Units will also overlook communal open space areas while the pedestrian paths through the site will provide good line-of-sight to ensure a safe environment for residents. Further detail on this matter is provided in the 'building design/streetscape' section.
- As demonstrated in Figure 6 below the zoning under Redlands Planning Scheme (RPS) and intent of precincts from SETSPO have been adopted into the City Plan mapping and relevant assessment benchmarks. Accordingly, the development is considered to be in keeping with the contemporary planning instrument.



Figure 6 – Comparison of zoning (RPS – left, City Plan – right)



Building design/streetscape

The following codes are relevant to the assessment of the proposed multiple dwelling design:

SETSPO code

Specific outcome S1.3

'(2) Sub-Precinct 2a – Attached Housing is designed and located to provide an increased range of residential uses including multiple dwellings and aged persons and special needs housing.'

Specific outcome S1.4

(2) 'Precinct 3 – Medium Density Housing is designed and located to –

(a) provide for a range of medium density uses such as multiple dwellings, apartment buildings and aged persons and special needs housing;

(b) limit overall building height to 13m and 3 storeys except in Sub-precinct 3a;

(c) maximise views and outlook across adjoining areas of open space;

(d) facilitate convenient walking to -

(i) public open space and local and district parks; or

(ii) convenience shopping employment and community opportunities within Precinct 1 – Mixed Use – Local Centre; or

(iii) higher order facilities and bus interchange at Victoria Point Major Centre

(e) ensure that where development adjoins Precinct 4 – Greenspace network it is located and designed so as not to compromise adjoining environmental values or the hydraulic capacity and ability of the adjoining land to accommodate the one percent AEP flood;

(f) ensure a building layout and design that -

(i) contributes to the establishment of an attractive streetscape;

(ii) reduces building bulk by a combination of balconies, recesses and variations in building form and materials;

(iii) requires roofs to be pitched, articulated, gabled or other features to avoid single plane or flat rooflines;

(iv) on lot 20 on SP140739 incorporates a range of design treatments that:

- ensure the establishment of a high quality attractive streetscape on the corner of the visually prominent intersection of Cleveland-Redland Bay Road and Boundary Road;
- positively recognises the strategic position of the locality as the southern gateway to the South East Thornlands Structure Plan area.'

Multiple dwelling code

Specific outcome S3

'(1) Layout and design enhances the built form of the surrounding streetscape by –



(a) contributing to the establishment of an attractive streetscape in new areas;

(b) ensuring the use addresses the street frontage;

(c) varying the built form appearance of each dwelling unit to provide a diversity of building styles;

(d) reducing building bulk through a combination of verandahs, recesses and variations in building form and materials;

(e) using a variety of materials, colours and/or textures between levels to create visual interest;

(f) ensuring that roof design contributes to good building form through articulation, architectural interest and attractive visual elements at the highest points of the building. The roof should be proportionate to the size, scale and bulk of the building as well as its elevation and orientation;

(g) roof forms minimize the visual intrusiveness of service elements and facilitate their use for sustainable functions;

(h) buildings on sloping sites being designed to produce a stepped pattern involving roof ridges, guttering, balustrade and floor levels;

(i) ensuring building height and site coverage is consistent with the proposed height and scale in the locality;

(j) where the built form is taller or wider than the type of building expected in the street then the use is articulated into clearly distinguishable parts, similar in scale to existing housing, so that individual dwelling units can be identified from the street;

(k) ensuring setbacks complement the existing streetscape and maximise private open space areas, privacy, solar access and provide for service areas;

(I) ensuring the streetscape is not affected by multiple access points or the dominance of garages.'

Specific outcome S6

(1) Landscape design contributes to a pleasant, safe and attractive living environment by –

(a) retaining existing mature trees;

(b) using plants that are native to the area;

(c) enhancing privacy, surveillance and amenity;

(d) providing surveillance to communal open space areas and pedestrian paths;

(e) enhancing climatic conditions;

(f) emphasising clear pedestrian entry points that offer good visibility along paths and driveways;

(g) planting being used to frame views and view corridors through the main pedestrian pathways of the site;

(h) incorporating semi-transparent fencing and planted landscaping as a buffer between communal areas and private open space areas;



(i) not blocking or interrupting overland flowpaths.'

The development is considered to comply with the above assessment benchmarks as follows:

- The development results in a two-storey 'townhouse' style attractive built form. The proposal provides a number of different attached product, which incorporate different design features through the use of a range of materials, recesses and variations in building form and articulated/gable roof forms.
- The development results in buildings addressing the Pateman Parade Street frontage, through the inclusion of built form elements such as balconies and verandahs overlooking the street.
- Direct pedestrian access is also provided to dwelling units from Pateman Parade where possible, increasing the interactivity of the streetscape.
- A landscaping strip, typically of 1.5 metres in width, is proposed along the full frontage of Pateman Parade in front of any fences. Generally, where in front of dwelling units, a combination of fencing styles will be utilised which will include either a 1.8m high timber fence, 1.8m high semi-transparent fence, or 1.5m high pool style fence with access gate. The range of fences allows for casual surveillance opportunities where possible, an aesthetically pleasing streetscape, and privacy for private open space areas for units that incorporate their primary open space adjoining Pateman Parade.
- Specifically for unit 31 a revised cladding/façade design has been conditioned where a blank wall is prominent from Pateman Parade. For units 3-4, also adjoining Pateman Parade, a revised fence design and gate are conditioned to provide interactivity of the built form with the streetscape.
- Where on the corners of Pateman Parade and Boundary Road/Cleveland-Redland Bay Road, deeper landscaping areas are provided in front of acoustic fences. This has been necessitated by SARA conditions. The landscaping areas are considered sufficient to provide vegetation that will screen the fencing at maturity.

Access/road design

The following specific outcomes of the infrastructure works code and SETSPO code are relevant to the assessment of the access to the site and road provision/design:

Infrastructure works code S7

'(1) Uses or reconfiguration that create new public roads or require the upgrading of a public road reserve –

(a) maintain or improve the safe and efficient operation of roads having regard to -

(i) the functional classification of the road from which it gains access;

(ii) the location and design of access points;

(iii) facilitating links between the use or other development and other high activity nodes such as educational facilities, communal facilities, centres and open space;

(iv) the potential for conflict between vehicles, pedestrians and cyclists;

(v) the location, construction and maintenance of utility infrastructure;



(vi) the location of activities within the site and their relationship with adjacent public roads; (vii) the nature and intensity of traffic generated by the use or other development;

(viii) the number of vehicles likely to be attracted to the site at any one time, whether due to the use or other uses;

(ix) the location, capacity and configuration of any existing or proposed car parking areas associated with the use;

(x) if located in a centre zone, the predominantly pedestrian orientated nature of public spaces in that zone;

(b) are provided with a road reserve and verge width sufficient to accommodate the -

(i) safe and efficient movement of all users, including pedestrians and cyclists;

(ii) on-street parking;

(iii) street tree planting;

(iv) utility infrastructure, including stormwater management and run-off from road surfaces;

(c) facilitate safety by providing -

(i) safe sight distances based on - a. road classification; b. target speed; c. expected access points;

(ii) pedestrian and cyclist crossings at intersections or where required to access –

- a. high activity nodes;
- *b. public transport;*
- c. centres;
- (iii) an alignment that does not result in excessive speeds;
- (iv) a combination of speed reduction techniques to achieve desired speeds including
 - a. speed platforms;
 - b. t-junction with splitter islands;

c. modified intersections;

d. roundabouts; or

e. other speed control devices.'

SETSPO code S2.1

'(1) Principal streets that include trunk collector and collector streets are provided generally in accordance with Map 2 – Road Movement Network Plan.

(2) Trunk collector and collector streets are designed to accommodate the safe and efficient movement of public transport buses.'

The development proposes to maintain the existing left-in left-out arrangement onto Boundary Road from Pateman Parade, with alternate access being provided from Harrington Boulevard to



either Boundary or Cleveland-Redland Bay Roads. The proposal and supporting traffic impact assessment has been reviewed by SARA, as a referral agency, in relation to the impact of the development on the State controlled road network in accordance with the State Development Assessment Provisions. SARA has determined that the impact of the traffic on the State controlled road network is acceptable, and no conditions have been imposed in this regard on the development.

Council has had regard to SARA's referral agency response in accordance with *Section 45(5)* of the PAct. To inform its decision, Council officers requested a copy of the assessment conducted by the State government to inform Council decision. However, the State government declined to provide this information.

Ultimately, the proposal is not considered to realise the connectivity of the movement network within the structure plan area, as the trunk collector (Pateman Parade) remains unconnected to the trunk collector (Beveridge Road) on the eastern side of Cleveland-Redland Bay Road and the four-way signalised intersection is incomplete.

The proposal is considered to be deficient in terms of providing a safe, integrated, highly accessible and interconnected road network for vehicular and pedestrian movement. There is also a noted impact on the local road network, especially for Harrington Boulevard, which would be required to act as a higher order road (and is not designed to do so) as a result of not completing Pateman Parade.

To ensure compliance with the infrastructure works and SETSPO codes, it is recommended that completion of the trunk collector road and provision of the (non-trunk) fourth leg of the signalised intersection be undertaken. A condition is also recommended for dedication of road where additional land may be required for the delivery of the intersection. It is noted that the intersection works are located within a State controlled transport corridor and as such further approval will be required by the State in this regard.

In accordance with section 45 (7) of the PAct, the assessment manager may give weight it considers appropriate to any changed planning instrument that has taken effect since the development application was properly made. City Plan continues to reflect the importance and need to provide the road connection and as the contemporary planning instrument significant weight has been afforded to it in the assessment of this development application.



PO24 of the low-medium density residential zone code requires development to facilitate the establishment of a safe, permeable, legible and functional movement network that is generally in accordance with figures 6.2.2.3.4 road movement network and 6.2.2.3.6 pedestrian/cycleway and public transport network plan (Figure 7). The purpose of the LMDR 1: South East Thornlands Precinct, requires a co-ordinated and interconnected transport network to ensure a high level of accessibility for pedestrians, cyclists, public transport and private vehicles. Further, development should not compromise or constrain the potential for well-designed future urban communities.

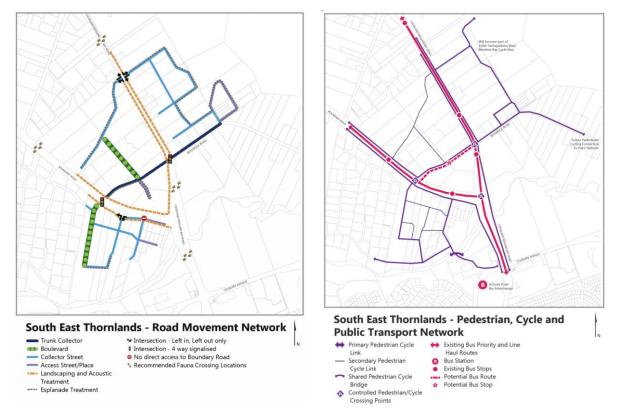


Figure 7 – Road Movement network (figure 6.6.6.3.4) and pedestrian, cycle and public transport network (figure 6.2.2.3.6) in the Low-medium density residential zone code in City Plan

Traffic and parking

Specific outcome S1 and S3.1 of the access and parking code are relevant to the assessment of the development:

Specific outcome S1

(1) Uses and other development provide off-street vehicle parking that -

(a) is clearly defined, safe and easily accessible;

(b) takes into consideration -

(i) the type and size of development;

(ii) expected resident, employee and customer movements;

(iii) the location of the use;

(iv) the capacity of the existing road network to accommodate on-street parking;

(v) access to public transport;



(c) includes dedicated parking spaces for -

(i) people with a disability;

(ii) motor cycles and bicycles;

(d) where on SMBI or NSI - incorporates the number of spaces determined by the local government on the basis of the location and nature of the use.'

Specific outcome S3.1

(1) Driveways are located having regard to the following -

(a) optimising public safety and convenience;

(b) characteristics of the frontage road including -

(i) road type;

(ii) road target speed;

(iii) traffic volumes;

(iv) vertical and horizontal geometry;

(v) queue and turn lane lengths;

(c) where the site is bounded by more than one street frontage, the secondary street provides the main vehicle entry/exit point;

(d) location of existing utility infrastructure, such as power poles, street lighting, gully pits and the like;

(e) location of existing bus stops, taxi ranks, traffic control devices;

(f) pedestrian and cycle paths and crossings;

(g) maintaining on-street parking;

(h) ensuring adequate visibility between vehicles on a driveway and pedestrians on the verge;

(i) reconfiguration, whether or not including a community management statement, allows for lots to be truncated at accessways and on corner lots and fencing and landscaping reduced in these truncated areas to ensure clear visibility between driveways and the verge;

(j) location of street trees existing in the road reserve;

(2) The maximum number of driveways accessing a lot or premises is one, unless it can be shown that multiple driveways will improve ingress/egress, internal traffic operation, and pedestrian safety.'

As identified in the 'proposal' section of the report the development includes provision of 556 car parks for a total of 225 dwelling units. The development provides car parking spaces in excess of the deemed to comply car park rate (i.e. category A development requires a minimum 282 car parks) under the access and parking code. With respect to the driveway access locations and design, it is considered that the provision of the two driveways proposed off Pateman Parade is acceptable in relation to the characteristics of the existing road network and for optimising safety

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of road users and pedestrians. The proposal is considered to comply with specific outcomes S1 and S3.1 of the access and parking code in relation to driveway access and on-site parking.

Environmental values

The subject site is mapped as being in a priority koala development assessable area and is wholly located within a koala broad-hectare area.

The following assessment benchmarks in Schedule 11 of the *Planning Regulation 2017* (current from 1 September 2017 to 5 October 2017), applicable at the time the development application was properly made, are relevant to the assessment:

Part 2, section 2, sub-section 2 states:

"The following matters are assessment benchmarks for the development—

- (a) the development provides, to the greatest extent practicable, safe koala movement measures that are appropriate for—
 - (i) the development; and
 - (ii) the habitat connectivity value of the premises;
- (b) any clearing of native vegetation complies with part 3;
- (c) measures are implemented to ensure that construction activities on the premises do not increase the risk of death or injury to koalas;
- (d) any area on the premises that is cleared of native vegetation is progressively rehabilitated, if—
 - (i) the vegetation was removed as a result of construction activities; and
 - (ii) the area is to be used to provide for safe koala movement measures, including, if appropriate, koala movement infrastructure;
- (e) if an area is to be used to provide for safe koala movement measures—the development involves landscaping that provides food, shelter and movement opportunities for koalas.
- (3) In this section—**habitat connectivity value**, of premises, means the extent to which the premises facilitate the movement of koalas across the premises, having regard to—
 - (a) the characteristics of the premises, including, for example—
 - (i) whether koalas are present on the premises; and
 - (ii) the condition of any koala habitat on the premises; and
 - (ii) whether there is a waterway or ecological corridor on the premises; and
 - *(iv)* whether the premises contain remnant vegetation or regulated regrowth vegetation; and
 - (b) whether the premises are near-



- (i) a koala habitat area, particularly a bushland habitat area, high value rehabilitation habitat area or medium value rehabilitation habitat area; or
- (ii) an area of remnant vegetation, or regulated regrowth vegetation, in which koalas are present, or which contains regional ecosystems in which koalas are known to be present (where the ecosystems are consistent with essential koala habitat, for example); or
- (iii) an area of ecological significance; or
- (iv) a waterway; or
- (v) an ecological corridor; and
- (c) factors that may reduce or adversely affect koala movement, including, for example—
 - (i) the proximity and location of roads or other infrastructure; and
 - (ii) any indirect impacts of development on the ecological features of the premises; and
 - (iii) ecological changes to koala habitat happening at the boundary of the premises."

Part 3

- "(1) For part 2, the clearing of native vegetation on premises must be carried out in a way that ensures—
 - (a) koalas have enough time to move from the area being cleared without human intervention; and
 - (b) links between koala habitats are maintained to allow koalas to move from the area being cleared; and
 - (c) a tree is not cleared if—
 - (i) a koala is present in the tree; or
 - (ii) the crown of the tree overlaps another tree in which a koala is present; and
 - (d) a non-juvenile koala habitat tree is only cleared under the guidance of a koala spotter.
- (2) Also, for premises larger than 3ha—
 - (a) the clearing must be carried out in stages, with each stage involving the clearing of—
 - (i) if the total area to be cleared is 6ha or less—no more than 50% of the area; or
 - (ii) if the total area to be cleared is larger than 6ha—no more than 3ha, or 3% of the area, whichever is greater; and



- (b) clearing must not be carried out on the premises between each stage of clearing for at least 1 period starting at 6p.m. on a day and ending at 6a.m. the following day.
- (3) In this section—koala spotter means a person who—
 - (a) has demonstrated experience in locating koalas in koala habitats or conducting fauna surveys; and
 - (b) holds a current authority under the Nature Conservation Act 1992 for the activities stated in paragraph (a)."

As the development is located within a koala broad hectare area, and according with the assessment benchmarks above there are no planting or offset requirements for the approximately 92 non-juvenile koala habitat trees for both stages that will need to be removed to facilitate the development under the Regulation. Clearing of native vegetation will be managed at construction stage as provided on the applicant's vegetation management plan.

In terms of the habitat connectivity values of the premises the following overall outcome of the SETSPO code is also relevant:

Overall outcome 5.14.7(2)(d)

'Sub-precinct 4f Flood Prone Area – Central Open Space protects and enhances publicly owned land that:

- maintains the hydraulic capacity, water quality and ecological values of this locally important drainage line;
- incorporates a local park;
- provides opportunity for establishing habitat and movement corridor for koalas and other fauna;
- in combination with the existing school grounds and bushland areas to the east and south east provides for a physical and visual break between the urban communities of Thornlands and Victoria Point.'

The site is bounded by major roads that limits habitat connectivity and safe movement opportunities for koalas, which is not encouraged for urban infill development. It is noted that the development is connected to habitat areas directly to the east via a culvert under Cleveland-Redland Bay Road. A culvert is also existing under Boundary road however links to habitat corridors are diminished in this location due to existing residential development.

Nevertheless, the SETSPO seeks that development provides opportunity for establishing habitat and movement corridors. Accordingly, revegetation is proposed within the open space portion of the site, as indicated on the concept landscape plans. A condition is imposed for a revised vegetation management plan to be provided to detail all revegetation works.

Stormwater Management

Specific outcome S1 and S2 of the stormwater management code are relevant to the assessment of the development:

Specific outcome S1



'(1) Stormwater drainage design -

(a) protects and preserves land below the 1 percent Annual Exceedance Probability (AEP) flood level;

(b) retains, enhances and incorporates natural overland drainage lines;

(c) maintains the hydraulic capacity of natural overland drainage lines within the lot or premises;

(d) maintains pre-development velocity and quantity of run-off;

(e) protects and enhances water quality of receiving waters;

(f) does not worsen or cause nuisance to adjacent, upstream and downstream land;

(g) maximises the application of water sensitive urban design principles including source, conveyance and discharge mechanisms;

(h) ensures the mechanisms incorporated are of a size and nature suited to the expected run-off;

(i) integrates with open space without adversely impacting on the core purpose of the open space;

(j) considers the full extent of maintenance requirements and costs associated with devices used within the system.'

Specific outcome S2

'(1) Stormwater drainage design -

(a) safely conveys stormwater flow resulting from the relevant AEP design storm under normal operating conditions;

(b) ensures the major system design, including overland flow paths, takes into account minor system blockage.'

The superseded *Priority Infrastructure Plan* (PIP) identifies land within the subject site as water sensitive urban design areas in the form of wetland C, which was intended to cater for stormwater quality treatment for the wider catchment. However, the upstream developed catchments have since been conditioned to treat stormwater quality and quantity on site. The only upstream development still requiring stormwater treatment on the subject site is also in the ownership of Philip Usher Constructions, therefore the proposed stormwater solution is not trunk infrastructure but development infrastructure.

Accordingly, the development manages stormwater discharge to the proposed detention basin via a treatment train within the South Eastern corner of the development site. This stormwater quality treatment solution is considered to comply with the relevant assessment benchmarks in Part E of the *State Planning Policy, July 2017* (SPP) by:

- Treating all stormwater onsite and therefore capturing nutrients and sediments which will protect environmental values external to the site
- Achieving water management design objectives outlined in tables A and B of appendix 2 of the SPP

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The stormwater treatment area is to be maintained in private ownership as part of the community title, with the responsibility for the maintenance of the stormwater management structures in private ownership. It is noted that there are also several small irrigation dams located on site that will need to be removed as part of the development. While these dams do not contain any environmental values (water storage for the nursey), the removal may present a water quality risk. A condition is recommended to provide detailed designs of the stormwater systems as part of conditioned works assessment as well as a dam dewatering report to demonstrate compliance with S1 and S2 of the stormwater management code.

Landscaping

Specific outcome S6 of the multiple dwelling code is relevant to the assessment of the development:

- '(1) Landscape design contributes to a pleasant, safe and attractive living environment by -
 - (a) retaining existing mature trees;
 - (b) using plants that are native to the area;
 - (c) enhancing privacy, surveillance and amenity;
 - (d) providing surveillance to communal open space areas and pedestrian paths;
 - (e) enhancing climatic conditions;

(f) emphasising clear pedestrian entry points that offer good visibility along paths and driveways;

(g) planting being used to frame views and view corridors through the main pedestrian pathways of the site;

(h) incorporating semi-transparent fencing and planted landscaping as a buffer between communal areas and private open space areas;

(i) not blocking or interrupting overland flowpaths.'

The development is considered to comply with the above specific outcome as follows:

- The development provides a substantial amount of landscaping along external road frontages, which has been addressed in the 'building design/streetscape' and 'road noise' sections of this report. This landscaping provides a screening function for the residential component of the development and enhances the amenity of the streetscape.
- In particular, it is noted that where the residential portion of the development adjoins Cleveland-Redland Bay Road, significant fill is required resulting in a combined retaining wall (maximum height 2.7m) and acoustic fence (height 2.8) with total height of 5.5m. Landscaping is proposed within the 'landscape buffer' between the development and the road to provide a screening function to the structure. It is noted however that this area is proposed to be combined with a swale, which limits the amount of planting provided and species selection. In this regard the fence above the retaining wall has been proposed to be set back 1.5m to allow for additional planting to screen the fence. The areas available are considered sufficient to provide a screening function for the development, noting that conditions seek that the applicant provide specific planting details as part of conditioned works assessment. The



landscape strip will be accessible from the south of unit 49, as well as via gates that will be incorporated at the rear of selected courtyards for maintenance purposes.

- Internal to the site further landscaping is provided within the 'built' portion of the site, both in front of the dwelling units and within private open space areas. Within the open space portion of the site, landscaping is proposed to complement open space activities, stormwater function, and habitat/ecological values.
- Conditions are recommended for the applicant to provide detailed landscape plans to include all planting details, such as species selection, densities and planting placements. A maintenance plan for the landscaping and details of lighting to common areas within the site will also be conditioned to be included in a revised landscape plan to ensure compliance with the code.

<u>Open space design</u>

Specific outcome S7 of the multiple dwelling code is relevant to the assessment of the development:

'(1) Open space –

(a) includes a clearly designated private open space area that provides privacy for residents and is directly accessible from the main living areas;

(b) includes sufficient communal open space areas at ground level that are usable, functional and accessible to the anticipated number of residents;

(c) have adequate dimensions to ensure spaces can be used for outdoor living and passive recreation;

(d) is situated on a suitable slope to ensure residents can easily move throughout the premise;

(e) is capable of receiving sufficient sunlight;

(f) is located behind the building frontage, and where above ground level protects the privacy of adjoining and nearby properties.'

The development is considered to comply with the above specific outcome as follows:

- Each unit is provided with a private open space area directly accessible off mainly living areas that are of suitable dimension and area, that allow for recreation, service facilities and landscaping. On average each unit is provided with 47m² of private open space.
- A dedicated communal open space area is located towards the southern end of the development. The area is largely grassed/landscaped and provides a transition between the residential use and the stormwater treatment area. The area contains a covered patio/BBQ area which also contains a gym. A kick and play area is also located in the southern part of the site, however this area does not contain specific facilities as it may be inundated during rain events.
- A central spine travelling north from the communal open space area to Pateman Parade also provides both a pedestrian link and a green space for residents to contemplate or congregate.

<u>Earthworks</u>

The following assessment benchmarks of the excavation and fill code are considered relevant to the development:

Specific outcome 1

(1) Excavation and fill -

(a) does not reduce the amenity of adjoining properties through the -

(i) loss of solar access or privacy;

(ii) intrusion of negative visual or overbearing impacts;

(iii) ensuring retaining walls or structures -

a. are constructed of materials that are of a high quality appearance;

b. incorporate landscaping or other features to assist reducing their visual bulk and length;

c. do not dominate over, and are of an appropriate scale to buildings / structures and land uses in the locality;

(b) is minimised by development being located and designed to -

- (i) prevent the unnecessary removal of native plants;
- (ii) protect existing and natural overland drainage systems;
- (iii) reduce erosion and sediment run-off;

(c) protects the safety of people and property from -

(i) drainage impacts such as the ponding or concentration of run-off or alteration of existing drainage systems;

- (ii) impacts associated with unstable fill;
- (iii) sub-standard retaining structures by ensuring
 - a. construction materials are durable;
 - b. the structural strength of the walls is sufficient to support the works.

(2) On slopes in excess of 10% excavation and fill is minimised to the extent practicable by avoiding slab on ground construction methods in preference of post supported construction methods.'

Specific outcome 2

'(1) Excavation and fill does not result in land or water contamination, or the harbourage of vermin.'

Specific outcome 3

(1) Excavation and fill does not cause environmental nuisance due to -

- (a) hours of construction;
- (b) dust emissions;
- (c) truck movements.'

A significant level of cut and fill is required across the site to facilitate building pads for the residential component of the development and to re-route stormwater/flooding risk towards the southern portion of the site where the detention basin is located. Typically the earthworks allow the development to 'tie in' with the external streetscape and avoid retaining walls fronting roads. As discussed in the 'road noise' section however there is a significant concrete sleeper retaining wall of up to 2.72m at the rear of the units fronting Cleveland-Redland Bay Road. For the reasons outlined in the 'road noise' section it is considered that the structure is adequately screened by landscape planting to reduce the visual impact of the structure on the streetscape. Conditions are recommended to ensure that the earthworks are conducted and retaining walls constructed in accordance with the relevant Australian Standards, and certified by an Registered Professional Engineer of Queensland (RPEQ) where greater than 1 metre in height. A construction management plan also ensures that impact to surrounding amenity is minimised during works associated with excavation and fill (e.g. dust management). The proposal is considered to comply with specific outcomes S1, S2 and S3 of the excavation and fill code.

<u>Sewer</u>

Specific outcome S4 of the infrastructure works code is relevant to the development:

(1) Sewerage infrastructure -

(a) is consistent with the expected capacity of the use or other development;

(b) upgrades existing networks where current capacity is insufficient for the needs of

the use or other development.'

At the time of lodgement the *Priority Infrastructure Plan* (PIP) was in effect, which has now been superseded by Council's *Local Government Infrastructure Plan* (LGIP), current from 1 July 2018. To service the structure plan area within the catchment of sewer pump station 68, Council has sought that this catchment be diverted to the Cleveland wastewater treatment plant in lieu of the existing connection to the Victoria Point wastewater treatment plant. The required works include:

- Provision of a sewer gravity main within Cleveland-Redland Bay Road between manhole 223515 and manhole 456377.
- Decommissioning of an existing sewer gravity main (and manholes) from upstream of the proposed connection point (manhole 223520) located within the Cleveland-Redland Bay Road road reserve.
- Provision of a sewer rising main within the Cleveland-Redland Bay Road road reserve from sewer pump station No.68 to the proposed discharge point at manhole 46375 and any associated works required.
- Replacement of existing sewer pump station No. 68, provision of emergency storage (wet well refurbishment and new offline emergency storage tank) and associated works.

GENERAL MEETING MINUTES

Accordingly, an infrastructure agreement has been entered in to by Council and the applicant, requiring that the works (being an 'infrastructure contribution') be completed by the applicant prior to the use commencing for stage 1 of the development (refer Attachment 6). The infrastructure agreement provides for the 'infrastructure contribution' to be subject to an offset due to the 'trunk' nature of the works. The sewer upgrade is considered to comply with specific outcome S4 of the infrastructure works code.

Waste management

Specific outcome S11 of the multiple dwelling code is relevant to the development:

'(1) Service facilities are provided to meet the needs of residents and are sited and designed in an unobtrusive and convenient manner.

(2) An on-site waste collection system has -

(a) an internal road network allowing the waste collection vehicle to service wheelie bins from independent dwelling units; or

(b) an internal bulk bin collection system to service semi-independent or dependent units.'

Each dwelling unit is proposed to be provided with individual waste and recycle wheelie bins, within the curtilage of the dwelling unit. Typically this will be at the rear of dwelling units where side access is available or otherwise at the front of a unit behind screening. Bins are generally collected from the front of each dwelling unit within the site, however there are bin bay locations identified for certain dwellings, within close proximity, for collection purposes where a waste collection vehicle cannot directly access the dwelling's frontage.

A concept waste management plan has been provided depicting suitability of the manoeuvring of the waste collection vehicle within the site. A condition is recommended to be included for a RPEQ certification of the suitability of the internal access roads design and construction. The development is considered to comply with S11 of the multiple dwelling code.

<u>Water</u>

Specific outcome S3 of the infrastructure works code is relevant to the development:

(1) Water supply infrastructure -

(a) is consistent with the expected capacity of the use or other development;

(b) upgrades existing networks where current capacity is insufficient for the needs of the use or other development.'

With the construction of Pateman Parade, a water connection and hydrant has been provided on the southern side of Pateman Parade, in the road reserve. This connection will be able to service the site and the existing water network has sufficient capacity to cater for the development. Conditions are recommended to ensure that the development complies with S3 of the infrastructure works code.

Electricity/telecommunications

Specific outcomes S2 and S5 of the infrastructure works code are relevant to the development:

Specific outcome S2



(1) Electrical infrastructure -

(a) is consistent with the expected capacity of the use or other development;

(b) upgrades existing networks where current capacity is insufficient for the needs of the use or other development;

(c) enhances opportunities for extension of below ground networks.'

Specific outcome S5

(1) Communications infrastructure -

(a) is consistent with the expected capacity of the use or other development;

(b) upgrades existing networks where current capacity is insufficient for the needs of the use or other development.'

Underground electricity infrastructure is provided to Pateman Parade. Conditions are recommended to provide an underground electricity connection and telecommunications to the multiple dwelling development to ensure that the development complies with S2 and S5 of the infrastructure works code.

Road noise

There is a road noise buffer extending 20m from the edge of Pateman Parade, and approximately 35m from both State-controlled roads, as indicated in Figure 8. The relevant assessment benchmarks relating to road noise are discussed below:





Figure 8 – Road noise buffer

SETSPO

S2.6(1)

'Road corridors under the control of the Department of Transport and Main Roads (DTMR), (Cleveland-Redland Bay Road and Boundary Road) are designed and of sufficient width, as identified on Map 2 – Road Movement Network Plan to –

- (iv) a minimum ten (10) metre wide strip of land on both sides of Cleveland-Redland Bay Road to accommodate noise attenuation treatments including fencing, landscaping and earth mounds;
- (v) a minimum ten (10) to fifteen (15) metre wide strip of land on both sides of Boundary Road to accommodate noise attenuation treatments including fencing landscaping and earth mounds.'

S2.7

'Noise attenuation measures undertaken with DTMR road corridors (Cleveland-Redland Bay Road and Boundary Road) are designed to –

(a) achieve a high quality visual appearance;



- (b) ensure any acoustic fencing is erected on property boundaries and screened from the road carriageway by landscaping and landscaped earth mounds of at least ten (10) metres in width;
- (c) incorporate physical and visual breaks to allow for pedestrian and cyclist permeability;
- (d) give consideration to innovative measures for land identified on Map 1 Land Use Precincts within Precinct 4 – Greenspace Network so as to maintain acoustic controls for adjoining properties while maintaining a high quality visual appearance, the hydraulic capacity of the land in coordination with providing opportunities for re-establishing habitat and movement corridors for koalas and other fauna.'

S3.1(1)

'Uses and other development that adjoin Boundary Road and Cleveland-Redland Bay Road incorporate noise attenuation measures that –

- (a) achieve a high quality attractive visual appearance from all public streets;
- (b) include a combination of
 - (i) vegetated earth mounds;
 - (ii) acoustic screens;
 - (iii) acoustic treatments incorporated into the building design;
- (c) incorporate landscaping to enhance visual amenity and minimise the visual impacts of noise screens;

(d) incorporate physical and visual breaks where adjoining cul de sacs and bus stops to allow for pedestrian and cyclist permeability.'

Road and rail noise impacts overlay

'S1.1(1) For reconfiguration, other than subdividing one lot into two, reduce noise nuisance through providing –

- (a) a buffer between lots and the noise source; or
- (b) service roads between lots and the noise source; or
- (c) larger lots closest to the noise source that allow dwelling units to be set back from the noise source; or
- (d) incorporating mounding and landscaping into the reconfiguration design; or (e) a mixture of (a) - (d).

S1.2(1) Uses reduce noise nuisance by –

- (a) siting to minimise the impact through distance, layout and orientation;
- (b) design and construction techniques;
- (c) incorporating soft engineering measures, such as -
 - (i) vegetated buffers;
 - (ii) vegetated earth mounds; or



- (d) fencing where it can be demonstrated that (a), (b) or (c) cannot be implemented; and
- (e) a combination of (a), (b), (c) or (d).'

S1.3(1) Noise attenuation measures utilised –

- (a) do not restrict access or movement for people or native animals;
- (b) are integrated with the streetscape and landscape setting;
- (c) are designed and constructed for longevity and a low level of maintenance.
- (d) are integrated into building design and layout;
- (e) avoid the use of acoustic fencing.

A 10 metre wide landscape buffer is proposed along both State controlled road boundaries. This area is intended to be landscaped only, except for a section along Cleveland-Redland Bay Road, with acoustic fences located on or within the property boundaries. Acoustic fences are proposed in sections on the boundaries facing both State controlled roads, and have been designed to not impact on pedestrian permeability, with linkages provided to both boundaries to the south of the acoustic fences. The acoustic fences will be screened by landscaping in the 10 metre buffer.

For a section along the Cleveland-Redland Bay Road frontage a swale is proposed for stormwater management purposes forward of a retaining wall with associated acoustic fencing above. The applicant has provided a design that addresses the performance outcomes of the SETSPO and road and rail noise impacts overlay codes, with adequate areas for landscape planting provided to minimise the visual impact of the screen and enhance the visual appearance of the development from the road. An excerpt from the landscape concept plan is provided below in Figure 9 demonstrating the outcome.

GENERAL MEETING MINUTES



Figure 9 – Landscape design for buffer area adjoining units on Cleveland-Redland Bay Road

The location of the fences in the northern part of the site will be located behind landscape buffers of varying widths, depending on the required wall height, to ensure the development integrates with the streetscape along Pateman Parade and at the entrances from the adjoining State controlled roads. Other noise mitigation including glazing, mechanical ventilation and wall, roof, floor and door construction, will be incorporated into the architectural design as part of the building approval.

Accordingly, the development is considered to comply with the relevant assessment benchmarks with respect to road noise.

Easement

An easement was created to provide a temporary drainage solution for the upstream property, which is in the same ownership as the subject site (refer Figure 10). Originally the upstream property had onsite stormwater management, but once this site was purchased by the common owner, additional units were constructed where the onsite detention was proposed, with the temporary stormwater solution relocated to the easement area. Once the subject site develops, the easement will no longer be required as the proposed stormwater management in the southern part of the site will cater for both developments.





Figure 10 – Existing stormwater discharge easement over site

Infrastructure Charges

Application A

The proposed development is subject to infrastructure charges in accordance with the Adopted Infrastructure Charges Resolution (No. 2.3) August 2016 (the Infrastructure Charge Resolution). The total charge applicable to this development is:

Total charge: \$3,428,570.60

Residential Component

| (103.00 X Multiple Dwelling - 3 or more bedroom X \$30,226.70) | \$3,113,350.10 |
|--|----------------|
| (16.00 X Multiple Dwelling - 1 or 2 bedroom X \$21,590.45) | \$345,447.20 |

Residential Demand Credit

| (1.00 X Dwelling House - 3 or more bedroom X \$30,226.70) | \$-30,226.710 | |
|---|----------------|--|
| Total Council Charge | \$3,428,570.60 | |

<u>Offsets</u>

The following offset applies under Chapter 4 Part 2 of the *Planning Act 2016*, calculated in accordance with Redland City Council's Adopted Infrastructure Charges Resolution:

Infrastructure Agreement executed between Philip Usher Constructions Pty Ltd. and Redland City Council as set out above in the sewer section of this report.

In accordance with Redland City Council's Adopted Infrastructure Charges Resolution, any offset listed in this notice only accrues when the trunk infrastructure works is accepted on maintenance by Council. Any request for an early plan sealing will require the bonding of any uncompleted trunk works.

<u>Refunds</u>

There are no refunds that apply under Chapter 4 Part 2 of the *Planning Act 2016*.

Application B

The proposed development is subject to infrastructure charges in accordance with the Infrastructure Charges Resolution. The total charge applicable to this development is:

Total charge: \$3,152,212.70

Residential Component

| (100.00 X Multiple Dwelling - 3 or more bedroom X \$30,226.70) | \$3,022,670.00 |
|--|----------------|
| (6.00 X Multiple Dwelling - 1 or 2 bedroom X \$21,590.45) | \$129,542.70 |
| Total Council Charge | \$3,152,212.70 |

<u>Offsets</u>

The following offset applies under Chapter 4 Part 2 of the *Planning Act 2016*, calculated in accordance with Redland City Council's Adopted Infrastructure Charges Resolution:

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Infrastructure Agreement executed between Philip Usher Constructions Pty Ltd. and Redland City Council.

In accordance with Redland City Council's Adopted Infrastructure Charges Resolution, any offset listed in this notice only accrues when the trunk infrastructure works is accepted on maintenance by Council. Any request for an early plan sealing will require the bonding of any uncompleted trunk works.

<u>Refunds</u>

There are no refunds that apply under Chapter 4 Part 2 of the *Planning Act 2016*.

Conclusion

The application has been assessed against the relevant planning instruments and is considered to comply. It is therefore recommended that development permits be granted subject to conditions. Strategic Implications

Legislative Requirements

In accordance with the *Planning Act 2016* this development application has been assessed against the RPS (7.1) and other relevant planning instruments. The decisions for both applications are due on 19 November 2021 and if not decided by this date the applications will be potentially deemed refused.

Risk Management

The applicant has the right to appeal a decision of Council to refuse the development applications or to not make a decision by the due date as a deemed refusal. Alternatively, the applicant also has the right to appeal conditions imposed on development permits.

Financial

Subsequent to a decision being made, and as outlined in the risk management section above, legal costs will apply if the applicant chooses to appeal Council's decision.

People

There are no implications for staff associated with this report.

Environmental

Where relevant, the environmental implications are detailed within the assessment in the 'issues' section of this report.

Social

Social implications are detailed within the assessment in the 'issues' section of this report.

Human Rights

In accordance with section 58 of the *Human Rights Act 2019*, consideration has been given to the relevant human rights in particular c.25 Privacy and Reputation, when drafting this report.

There are no known human rights implications associated with this report.



Alignment with Council's Policy and Plans

The assessment and officer's recommendation align with Council's policies and plans as described within the 'Issues' section of this report.



CONSULTATION

| Consulted | Consultation Date | Comments/Actions | |
|-----------------------|--------------------------|--|--|
| Divisional Councillor | 11 September 2017 | Sent to Councillor for comment in accordance with standard | |
| | | procedure. | |

OPTIONS

Option One

That Council resolves to grant development permits for Application A (MCU17/0057) and Application B (MCU17/0058), subject to the conditions outlined in Attachment 7.

Option Two

That Council resolves to grant development permits for Application A (MCU17/0057) and Application B (MCU17/0058), subject to alternate conditions to those outlined in Attachment 7.

Option Three

That Council resolves to grant preliminary approvals for Application A (MCU17/0057) and Application B (MCU17/0058).

Option Four

That Council resolves to refuse Application A (MCU17/0057) and Application B (MCU17/0058).

OFFICER'S RECOMMENDATION

That Council resolves to grant development permits for Application A (MCU17/0057) and Application B (MCU17/0058), subject to the conditions outlined in Attachment 7.

COUNCIL RESOLUTION 2021/272

Moved by: Cr Paul Gollè Seconded by: Cr Tracey Huges

That Council resolves to refuse Application A (MCU17/0057) and Application B (MCU17/0058) subject to Land Use, Redlands Planning Scheme version 7.1, City Plan version 5, Connectivity, Environmental Connectivity and Landscape Buffer as outlined in new Attachment 10.

CARRIED 8/3

Crs Karen Williams, Wendy Boglary, Paul Gollè, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Crs Peter Mitchell, Lance Hewlett and Mark Edwards voted AGAINST the motion.

Proposed Townhouse Development Corner Boundary Road and Cleveland-Redland Bay Road Thornlands

ACOUSTIC REPORT



Client: Philip Usher Constructions

Reference: 2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thornlands ENV RTN Date Issued: 23 November 2020



Document Information

Contact Details

Acoustic Works Unit 2/8 Castlemaine Street Coorparoo QLD 4151 (07) 3393 2222 ABN: 56 157 965 056

PO Box 1271 Coorparoo DC QLD 4151

Greg Pearce Mark Enersen Email: gpearce@acousticworks.com.au Email: menersen@acousticworks.com.au

Report Register

| Date | Revision | Author | Reviewer |
|----------|----------|----------------|-------------|
| 10/05/17 | R01A | Andrew Mackie | Greg Pearce |
| 24/07/17 | R01B | Marcus Kamppi | Greg Pearce |
| 07/08/17 | R01C | Marcus Kamppi | Greg Pearce |
| 16/07/18 | R01D | Karen McMorrow | Greg Pearce |
| 20/07/18 | R01E | Karen McMorrow | Greg Pearce |
| 25/07/18 | R01F | Karen McMorrow | Greg Pearce |
| 14/11/18 | R01G | Karen McMorrow | Greg Pearce |
| 12/06/20 | R01H | Karen McMorrow | Greg Pearce |
| 17/06/20 | R01J | Karen McMorrow | Greg Pearce |
| 23/11/20 | R01L | Mahbub Sheikh | Greg Pearce |

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2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thomlands ENV RTN $_{\odot}$ Acoustic Works 2020



. Introduction

The following revised report is in response to a request by Philip Usher Constructions for an environmental noise assessment of a proposed residential development located at 410 Boundary Road and 359-405 Cleveland-Redland Bay Road, Thornlands. The acoustic report has been revised to address changes to the proposed development layout and respond to request for further information from the State Assessment and Referral Agency (SARA) (Sara Reference:1710-1966 SRA).

To facilitate the assessment, unattended noise monitoring was conducted onsite to establish road traffic noise impacts from Cleveland-Redland Bay Road and Boundary Road to the development. Ambient noise levels were measured in the vicinity of nearby residence to determine the criteria for onsite activities.

Based on the results of the assessment, recommendations for acoustic treatments are specified in this report.

2. Site Description

2.1 Site Location

The site is described by the following:

410 Boundary Road & 359-379 Cleveland Redland Bay Road, Thornlands Lot 10 on SP216148, Lot 18 on SP119616 & Lot 20 SP140739

Refer to Figure 1 for site location.



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A comprehensive site survey was conducted on the 19th April 2017 and identified the following;

- a) The site is currently being used as a garden nursery which shall be demolished to make way for the proposed development.
- b) Cleveland-Redland Bay Road bounds the site to the east, separating the development from Faith Lutheran College and residential dwellings.
- c) Boundary Road bounds the site to the southwest, separating the site from a veterinary clinic and residential dwellings.
- d) Residential dwellings are located adjacent the north western site boundary.

2.2 Proposal

The proposal is to construct a staged townhouse development comprised of the following:

- Site area of 10.73ha.
- 225 x two storey townhouses.
- Private recreational areas are provided for each townhouse with several communal areas located throughout the site.
- Car parking: Total 499 car parking is provided including 310 Garages, 131 Tandem Car Parking, 58 Visitor Car Parking and 3 Car Wash bays.
- Site access shall be via the proposed Pateman Parade to the northwest.

Refer to the development plans in Section 11.1 of the Appendices.

2.3 Acoustic Environment

The surrounding area is primarily affected by road traffic noise from Cleveland-Redland Bay Road and Boundary Road.

3. Equipment

The following equipment was used to record noise levels:

- 3 x Rion NL 42 Environmental Noise Monitors
- BSWA Technology Co. Ltd Sound Calibrator

The Rion NL 42 Environmental Noise Monitors hold current NATA Laboratory Certification and were field calibrated before and after the monitoring period, with no significant drift from the reference signal recorded.

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4. Receivers and Noise Monitoring

4.1 Receiver Locations

Condition 17 of a previous information request from Redland City Council (Ref: *MCU17/0058*) requires an assessment of potential noise impacts from the development to sensitive uses along Beveridge Road. Based on council's request, the nearest sensitive receiver locations were identified as follows:

- 1. A two storey residential dwelling located to the northeast of the site at 2-10 Beveridge Road.
- 2. Faith Lutheran College located east of the site at 1-15 Beveridge Road.

Refer to Figure 2 for these locations.



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$$\bigcirc$$

4.2 Unattended Noise Monitoring

The Rion NL42 environmental noise monitors were set to record noise levels in "A" Weighting, Fast response. Road traffic noise monitoring was conducted generally in accordance with Australian Standard *AS2702:1984* 'Acoustics – Methods for the measurement of road traffic noise' using 1 hour statistical intervals, with ambient noise monitoring conducted generally in accordance with Australian Standard AS1055:1997 Acoustics – Description and measurement of environmental noise using 15 minute statistical intervals.

4.2.1 Cleveland-Redland Bay Road: Road Traffic and Ambient Noise Monitoring – Monitors 1a & 1b

Two Rion NL42 environmental noise monitors were placed onsite at 365 Cleveland-Redland Bay Road, 12m from the nearest lane of Cleveland-Redland Bay Road to measure road traffic and ambient noise levels. The monitors were located in a free field position with the microphone approximately 1.4 metres above ground surface level. The noise monitor was set to record noise levels between the 19th and 26th April 2017.

4.2.2 Boundary Road: Road Traffic Noise Monitoring – Monitor 2

A Rion NL42 environmental noise monitor was placed onsite at 410 Boundary Road, 20m from the nearest lane of Boundary Road to measure road traffic noise levels. The monitor was located in a free field position with the microphone approximately 1.4 metres above ground surface level. The noise monitor was set to record noise levels between the 19th and 26th April 2017.

Refer to Figure 2 for the noise monitoring locations.

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5. Measured Noise Levels

The following tables present the measured road traffic noise levels from the unattended noise surveys. Any periods of inclement weather or extraneous noise were omitted from the measured data prior to determining the results.

5.1 Road Traffic and Ambient Noise Levels - Cleveland-Redland Bay Road

The measured road traffic and ambient noise levels at the monitoring location are as follows;

Table 1: Measured road traffic noise levels - all time periods (Cleveland-Redland Bay Road), Monitor 1a

| Day | Date | L10(12h) | L10(18h) | L90(8h) | L90(18h) |
|---------------|----------|----------|----------|---------|----------|
| Thursday | 20/04/17 | 70.5 | 69.2 | 44.4 | 55.2 |
| Friday | 21/04/17 | 71.0 | 70.3 | 44.2 | 56.2 |
| Saturday | 22/04/17 | - | - | - | - |
| Sunday | 23/04/17 | - | - | - | - |
| Monday | 24/04/17 | 70.1 | 69.3 | 43.6 | 54.9 |
| Tuesday | 25/04/17 | - | - | - | - |
| Overall value | | 70.5 | 69.6 | 44.1 | 55.4 |

Table 2: Measured ambient noise levels - all time periods (Cleveland-Redland Bay Road), Monitor 1b

| Day | Date | L90 dB(A) | | | | |
|----------|----------|-----------|-----|-------|--|--|
| Day | Date | Day | Eve | Night | | |
| Thursday | 20/04/17 | 58 | 53 | 45 | | |
| Friday | 21/04/17 | 58 | 54 | 46 | | |
| Saturday | 22/04/17 | 57 | 53 | 46 | | |
| Sunday | 23/04/17 | 54 | 52 | 43 | | |
| Monday | 24/04/17 | 56 | 55 | 45 | | |
| Tuesday | 25/04/17 | 51 | 53 | 45 | | |
| Overa | l value | 56 53 45 | | 45 | | |

Road traffic data for the weekend and public holidays was not utilised as it is not considered relevant to the assessment. Refer to the appendix for graphical representation.

5.2 Road Traffic Noise Levels - Boundary Road

The measured road traffic noise levels at the monitoring location are as follows;

| Table 3: Measured road traffic noise levels - all time | e periods (Boundary Road), Monitor 2 |
|--|--------------------------------------|
|--|--------------------------------------|

| Day | Date | L10(12h) | L10(18h) | L90(8h) | L90(18h) |
|----------|----------|----------|----------|---------|----------|
| Thursday | 20/04/17 | 66.1 | 65.0 | 38.1 | 49.9 |
| Friday | 21/04/17 | 66.8 | 65.9 | 37.5 | 51.2 |
| Saturday | 22/04/17 | - | - | - | - |
| Sunday | 23/04/17 | - | - | - | - |
| Monday | 24/04/17 | 65.7 | 64.7 | 37.1 | 49.7 |
| Tuesday | 25/04/17 | - | - | - | - |
| Overal | l value | 66.2 | 65.2 | 37.6 | 50.3 |

Road traffic data for the weekend and public holidays was not utilised as it is not considered relevant to the assessment. Refer to the appendix for graphical representation.

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6. Noise Criteria

6.1 Road Traffic Noise Criteria

As the development is located near two state controlled roads (Cleveland-Redland Bay Road and Boundary Road), the SDAP criteria detailed in Section 6.1.1 applies.

6.1.1 State Development Assessment Provisions (SDAP)

The criteria applied are in accordance with the *SDAP* Version 2.6 dated 7 February 2020 by the Department of State Development, Manufacturing, Infrastructure and Planning. The SDAP *State Code 1: Development in a state-controlled road environment* sets out matters of interest for the assessment of developments near state controlled roads or multi-modal corridors. The applicable criteria for the development in the year 2030 (ten year planning horizon) requires road traffic noise to be assessed in accordance with Table 1.2.2 of the policy statement as follows;

| Table 4: | SDAP Road | Traffic Noise | Criteria |
|----------|-----------|---------------|----------|
|----------|-----------|---------------|----------|

| Performance Outcome | Acceptable Outcome |
|--|--|
| PO23 Development involving an accommodation activity or land for a future accommodation activity minimises noise intrusion from a state-controlled road or | AO23.1 A noise barrier or earth mound is provided which is designed, sited and constructed: 1. to meet the following external noise criteria at all facades of the building envelope: a. <60 dB(A) L10 (18 hour) facade corrected (measured L90 (8 hour) free field |
| type 1 multi-modal corridor in habitable rooms. | between 10pm and 6am \geq 40 dB(A)) between 10pm and 6am \geq 40 dB(A)) |
| | 2. in accordance with chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013. |
| | Note: To demonstrate compliance with the acceptable outcome, it is recommended that a RPEQ certified noise assessment report is provided, prepared in accordance with the SDAP Supporting Information: Environmental emissions in a state-controlled road environment, Department of Transport and Main Roads, 2017. |
| | If the building envelope is unknown, the deemed-to-comply setback distances for buildings stipulated by the local planning instrument or relevant building regulations should be used. |
| | In some instances, the design of noise barriers and mounds to achieve the noise criteria above the ground floor may not be reasonable or practicable. In these instances, any relaxation of the criteria is at the discretion of the Department of Transport and Main Roads. |
| | OR all of the following acceptable outcomes apply |
| | AO23.2 Buildings which include a habitable room are setback the maximum distance possible from a state-controlled road or type 1 multi-modal corridor. |
| | AO23.3 Buildings are designed and oriented so that habitable rooms are located furthest from a state-controlled road or type 1 multi-modal corridor. |

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| Performance Outcome | Acceptable Outcome |
|---|---|
| | AO23.4 Buildings (other than a relevant residential building or relocated building) are designed and constructed using materials which ensure that habitable rooms meet the following internal noise criteria: |
| | 1. \leq 35 dB(A) Leq (1 hour) (maximum hour over 24 hours). |
| | Note: Noise levels from a state-controlled road or type 1 multi-modal corridor are to be measured in accordance with AS1055.1–1997 Acoustics –Description and measurement of environmental noise. |
| | To demonstrate compliance with the acceptable outcome, it is recommended that a RPEQ certified noise assessment report is provided, prepared in accordance with the SDAP Supporting Information: Environmental emissions in a state-controlled road environment, Department of Transport and Main Roads 2017. |
| | Habitable rooms of relevant residential buildings located within a transport noise corridor must comply with the Queensland Development Code MP4.4 Buildings in a transport noise corridor, Queensland Government, 2015. Transport noise corridors are mapped on the State Planning Policy interactive mapping system. |
| PO24 Development involving an accommodation activity or land for a future accommodation activity minimises noise intrusion from a state-controlled road or type 1 multi-modal corridor in outdoor spaces for passive recreation. | AO24.1 A noise barrier or earth mound is provided which is designed, sited and constructed: 1. to meet the following external noise criteria in outdoor spaces for passive recreation: a. ≤57 dB(A) L10 (18 hour) free field (measured L90 (18 hour) free field between 6am and 12 midnight ≤45 dB(A)) b. ≤60 dB(A) L10 (18 hour) free field (measured L90 (18 hour) free field between 6am and 12 midnight >45 dB(A)) 2. in accordance with chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice – Volume 1 Road Traffic Noise, Department of Transport and Main Roads, 2013. Note: To demonstrate compliance with the acceptable outcome, it is recommended that a RPEQ certified noise assessment report is provided, prepared in accordance with the SDAP Supporting Information: Environmental emissions in a state-controlled road environment, Department of Transport and Main Roads, 2017. |
| | OR all of the following acceptable outcomes apply A024.2 Each dwelling has access to an outdoor space for passive recreation which |
| | is shielded from a state-controlled road or type 1 multi-modal corridor by a building, solid gap-free fence, or other solid gap-free structure. |
| | AO24.3 Each dwelling with a balcony directly exposed to noise from a state- controlled road or type 1 multi-modal corridor has a continuous solid gap-free balustrade (other than gaps required for drainage purposes to comply with the Building Code of Australia). |

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6.1.2 Queensland Development Code MP4.4

To determine the minimum design requirements for any new buildings located in a SDAP transport corridor, the Queensland Development Code Mandatory Part 4.4 may be applied if no alternative solutions are provided. In accordance with MP4.4, the noise categories are stated in Table 5.

| Noise level L10(18hr) dB(A) (induding façade correction) |
|---|
| ≥ 73 |
| 68-72 |
| 63-67 |
| 58-62 |
| ≤ 57 |
| |

| Table 5: Queensland Development Code Mandatory Part 4.4 Criteria | Table 5: (| Queensland | Developmen | t Code Mandator | y Part 4.4 Criteria |
|--|------------|------------|------------|-----------------|---------------------|
|--|------------|------------|------------|-----------------|---------------------|

The building treatment for any future development onsite shall be determined at Building Approval stage, in general accordance with the Queensland Development Code. Alternative solution may be provided on request.

6.2 Environmental Noise Criteria

The criteria for environmental noise as applied under the *Redland City Plan Version 4* is outlined in *Planning Scheme Policy 6 – Environmental Emissions.*

6.2.1 Planning Scheme Policy 5 – Environmental Emissions

Section 6.3.2 of the RCC *Planning Scheme Policy 5 – Environmental Emissions* requires environmental noise to be assessed in accordance with the *Environmental Protection (Noise) Policy 2008* as follows;

6.2.1.1 Acoustic Quality Objectives

Table 6 below presents the acoustic quality objectives at noise sensitive receptors as detailed in Schedule 1 of the Environmental Protection (Noise) Policy 2008.

| | Time of Day | Acoustic Quality Objectives, dB(A) | | |
|------------------------|---------------------------------------|------------------------------------|--------------|-------------|
| Sensitive Receptor | | L _{Aeq,adj,1hr} | LA10,adj,1hr | LA1,adj,1hr |
| Dwelling (outdoors) | Day and Evening (7am – 10pm) | 50 | 55 | 65 |
| Dwelling (Indoors) | Day and Evening (7am – 10pm) | 35 | 40 | 45 |
| (Indoors) | Night (10pm – 7am) | 30 | 35 | 40 |
| School (indoors) | When classes are being offered | 35 | - | - |
| School (outdoor) | When children usually play outside | 55 | - | - |

Table 6: Acoustic Quality Objectives at Noise Sensitive Properties

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6.2.1.2 Background Creep

The Background Creep criteria are as follows;

Time-varying noise:

 $L_{Aeq,adj,T_{i}} \leq Ambient L_{A90,T} + 5dB(A)$

Steady-state noise:

 $L_{A90,T} \leq Ambient L_{A90,T}$

The time period (T) is a time interval of at least 15 minutes, or if the noise continues for less than 15 minutes, the duration of the noise source.

Based on the results of ambient noise monitoring, the project specific background creep noise limits are shown in Table 7.

| Tuble 7. Buckground a cep hobe limits | | | |
|---------------------------------------|------------------------------|--------|--|
| Time Period | Noise Level Limits SPL dB(A) | | |
| Time Fenou | LAeq,Adj,T | La90,t | |
| Day 7am – 6pm | 61 | 56 | |
| Evening 6pm – 10pm | 58 | 53 | |
| Night 10pm – 7am | 50 | 45 | |

| Table 7: Background cre | ep noise limits |
|-------------------------|-----------------|
|-------------------------|-----------------|

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Road Traffic Assessment

Road traffic noise associated with Cleveland-Redland Bay Road and Boundary Road for a ten year planning horizon was assessed at the development to determine compliance with SDAP criteria (refer to Section 6.1) including any requirements for acoustic treatments.

7.1 Traffic Volumes

Traffic data for the Cleveland-Redland Bay Road and Boundary Road was obtained from TMR via the *Traffic Analysis and Reporting System (TARS)*. The traffic data includes percentage of heavy vehicles and growth rates as nominated below, used to calculate future volumes. Traffic data for the future section of Beveridge Road (Bateman Parade) was based on the traffic impact assessment for the development (by TTM, reference: 16BRT0391).

| Location | 2017 AADT | 2030 Predicted AADT | Percentage of Heavy Vehicles |
|----------------------------|-----------|---------------------------|------------------------------------|
| Cleveland-Redland Bay Road | 15,816 | 18,000 | 5.13% |
| Boundary Road | 25,313 | 40,391 | 8.65% |
| Pateman Parade | - | 3,375 | 2.00% |

7.2 Road Traffic Noise Verification

To ensure the CoRTN noise model is accurate, a verification model of the predicted $L_{A10(1Bhr)}$ was created and compared to the measured noise level based on the data provided in Section 7.1. The CoRTN method allows a 2dB(A) variation from the predicted and measured level, if the variation exceeds 2dB(A) a correction to the predicted level is required.

| Location | Measured La10(18hr) dB(A) | Predicted La10(18hr) dB(A) | Correction |
|----------------------------|------------------------------|-------------------------------|------------|
| Cleveland-Redland Bay Road | 69.6 | 69.7 | 0 |
| Boundary Road | 65.2 | 65.3 | 0 |

| Figure 3: SoundPLAN | verification of noise | e monitoring locations | |
|---------------------|-----------------------|------------------------|--|
|---------------------|-----------------------|------------------------|--|

| No. | Receiver name | Building side | Floor | Limit L10(18h) dB(A) | Level w/o NP L10(18h) dB(A) |
|-----|---------------------------------|------------------|-------|----------------------------|-----------------------------------|
| 1 | Boundary Road logger | | GF | - | 65.3 |
| 2 | Cleveland Redland Bay Rd Logger | | GF | - | 69.7 |

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7.3 Predicted Road Traffic Noise Levels – 2030

Road traffic noise modelling for the proposed development was based on the following information:

- Site layout, plans and elevations provided by Philip Usher Constructions, Job No. 676, Drawings DA-A3-00 to DA-A3-05 (Stage 1 & Stage 2), Revision J, dated 16 November 2020.
- Cleveland-Redland Bay Road speed limit of 70km/hr.
- Boundary Road speed limit of 60km/hr and 80km/hr.
- Future section of Beveridge Road (Pateman Parade) speed limit of 50km/hr.
- A Bituminous Dense Graded Asphalt Seal (DGA) surface on both roads, resulting in a 0 dB(A) correction in accordance with TMR requirements.
- Receiver heights were based on 1.8m and 4.6m above finished pad level for ground and first floor levels respectively. Receiver heights were based on 1.5m above finished pad level for outdoor recreation areas.
- SoundPLAN grid noise maps at 2.5m x 2.5m.
- Calculations include the acoustic barriers as detailed in Section 9.1.

The 3D SoundPLAN CoRTN noise model predicted $L_{A10,1Bhour}$ levels with Figure 4 to Figure 7 showing the predicted road traffic noise contours for the development with and without acoustic barriers. Table 10 presents the predicted road traffic noise impacts for each room of the development with the inclusion of the acoustic barriers detailed in Section 9.1.

| RL | Location | | | Predicted Road Traffic Noise 2030 (façade corrected) With acoustic barriers | |
|------|-----------|--------|----------------|--|--------------------------|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category |
| | 1 | Ground | Living | 62 | 1 |
| | | | Kitchen/Dining | 58 | 1 |
| 12.6 | | First | Bed 1 | 64 | 2 |
| | | | Bed 2 | 61 | 1 |
| | | | Bed 3 | 54 | 0 |
| | 2 | Ground | Living | 61 | 1 |
| | | | Kitchen/Dining | 54 | 0 |
| 12.6 | | First | Bed 1 | 63 | 2 |
| | | | Bed 2 | 57 | 0 |
| | | | Bed 3 | 57 | 0 |
| | 3-4 | Ground | Kitchen | 53 | 0 |
| 12.5 | | | Living/Dining | 62 | 1 |
| 12.5 | | First | Bed 1 & Bed 4 | 57 | 0 |
| | | | Bed 2 & Bed 3 | 64 | 2 |
| | 5-6 | Ground | Living | 61 | 1 |
| | | | Kitchen | 56 | 0 |
| 12.4 | | | Dining/Stair | 53 | 0 |
| | | First | Bed 1 & Bed 4 | 63 | 2 |
| | | | Bed 2 & Bed 3 | 56 | 0 |
| | 7-8 | Ground | Living | 53 | 0 |
| 12.2 | | | Kitchen/Family | 61 | 1 |
| 12.2 | | First | Bed 1& Bed 4 | 55 | 0 |
| | | | Bed 2 & Bed 3 | 63 | 2 |
| 12.2 | 9-10 | Ground | Living | 53 | 0 |

Table 10: Predicted traffic noise levels 2030

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| RL | Location | | | Predicted Road Traffic Nois 2030 (façade corrected) With acoustic barriers | | |
|------|-----------|--------|----------------|---|--------------------------|--|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category | |
| | | | Kitchen/Family | 61 | 1 | |
| | | First | Bed 1 | 63 | 2 | |
| | | | Bed 2 & Bed 3 | 55 | 0 | |
| | 11-12 | Ground | Living | 61 | 1 | |
| | | | Kitchen | 55 | 0 | |
| 12.0 | | | Dining | 53 | 0 | |
| | | First | Bed 1 | 63 | 2 | |
| | | | Bed 2 & Bed 3 | 56 | 0 | |
| | 13/16 | Ground | Living | 55 | 0 | |
| 11.7 | | | Kitchen/Family | 62 | 1 | |
| | | First | Bed 1 & Bed 4 | 57 | 0 | |
| | | | Bed 2 & Bed 3 | 63 | 2 | |
| | 14/15 | Ground | Living | 55 | 0 | |
| 11.7 | | | Kitchen/Family | 62 | 1 | |
| 11.7 | | First | Bed 1 | 63 | 2 | |
| | | | Bed 2 & Bed 3 | 57 | 0 | |
| | 17/20 | Ground | Living | 52 | 0 | |
| 11.3 | | | Kitchen/Family | 61 | 1 | |
| 11.5 | | First | Bed 1 & Bed 4 | 55 | 0 | |
| | | | Bed 2 & Bed 3 | 63 | 2 | |
| | 18/19 | Ground | Living | 52 | 0 | |
| 11.3 | | | Kitchen/Family | 61 | 1 | |
| 11.5 | | First | Bed 1 | 63 | 2 | |
| | | | Bed 2 & Bed 3 | 55 | 0 | |
| | 21/22 | Ground | Living | 61 | 1 | |
| 11.0 | | | Kitchen/Dining | 58 | 1 | |
| 11.0 | | First | Bed 1 | 63 | 2 | |
| | | | Bed 2 & Bed 3 | 55 | 0 | |
| | 23/24 | Ground | Living | 61 | 1 | |
| | | | Kitchen/Dining | 57 | 0 | |
| 10.7 | | First | Bed 1 | 63 | 2 | |
| | | | Bed 2 | 59 | 1 | |
| | | | Bed 3 | 55 | 0 | |
| | 25/28 | Ground | Living | 53 | 0 | |
| | | | Kitchen/Family | 62 | 1 | |
| 10.3 | | | Dining/Stair | 58 | 1 | |
| | | First | Bed 1 & Bed 4 | 56 | 0 | |
| | | | Bed 2 & Bed 3 | 64 | 2 | |
| | 26-27 | Ground | Living | 53 | 0 | |
| 10.3 | | | Kitchen/Family | 62 | 1 | |
| | | First | Bed 1 | 64 | 2 | |
| | | | Bed 2 & Bed 3 | 56 | 0 | |
| | 29 | Ground | Living | 59 | 1 | |
| 10.0 | | | Kitchen/Dining | 61 | 1 | |
| | | First | Bed 1 | 64 | 2 | |
| | | | Bed 2 & Bed 3 | 65 | 2 | |

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| RL | Location | | | Predicted Road Traffic No 2030 (façade corrected) With acoustic barriers | |
|------|-----------|--------|----------------|---|--------------------------|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category |
| | 30 | Ground | Living | 59 | 1 |
| 10.0 | | | Kitchen/Dining | 61 | 1 |
| 10.0 | | First | Bed 1 | 64 | 2 |
| | | | Bed 2 | 65 | 2 |
| | 31/32 | Ground | Living | 53 | 0 |
| 9.5 | | | Kitchen/Dining | 63 | 2 |
| 5.5 | | First | Bed 1 | 59 | 1 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 33-34 | Ground | Living | 52 | 0 |
| 9.2 | | | Kitchen/Dining | 62 | 1 |
| J.2 | | First | Bed 1 | 56 | 0 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 35/38 | Ground | Living | 53 | 0 |
| 9.0 | | | Kitchen/Family | 62 | 1 |
| 5.0 | | First | Bed 1 & Bed 4 | 56 | 0 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 36-37 | Ground | Living | 53 | 0 |
| 9.0 | | | Kitchen/Family | 62 | 1 |
| 5.0 | | First | Bed 1 | 70 | 3 |
| | | | Bed 2 & Bed 3 | 56 | 0 |
| | 39-40 | Ground | Kitchen | 56 | 0 |
| 8.8 | | | Living/Dining | 62 | 1 |
| 0.0 | | First | Bed 1 & Bed 4 | 56 | 0 |
| | | | Bed 2 & Bed 3 | 69 | 3 |
| | 41-42 | Ground | Living | 55 | 0 |
| 8.7 | | | Kitchen/Dining | 62 | 1 |
| 0.7 | | First | Bed 1 | 61 | 1 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 43-44 | Ground | Kitchen | 54 | 0 |
| 8.5 | | | Living/Dining | 62 | 1 |
| | | First | Bed 1 & Bed 4 | 57 | 0 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 45-47 | Ground | Living | 55 | 0 |
| 8.4 | | | Kitchen/Dining | 62 | 1 |
| | | First | Bed 1 | 57 | 0 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 48/49 | Ground | Kitchen | 58 | 1 |
| 8.3 | | | Living/Dining | 61 | 1 |
| 0.5 | | First | Bed 1 & Bed 4 | 59 | 1 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 50-52 | Ground | Living | 62 | 1 |
| 9.8 | | | Kitchen/Dining | 50 | 0 |
| | | First | Bed 1 | 65 | 2 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| 9.8 | 53/56 | Ground | Living | 51 | 0 |
| | | | Kitchen | 58 | 1 |

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| RL | | Predicted Road Traffic Nois 2030 (façade corrected) With acoustic barriers | | | |
|----------|-----------|---|-----------------------|--------------------|--------------------------|
| | Townhouse | Floor | Room | La10 18hr dB(A) | QDC Noise Category |
| | | First | Bed 1 | 54 | 0 |
| | | | Bed 2 & Bed 3 | 64 | 2 |
| | 54/55 | Ground | Living | 51 | 0 |
| 9.8 | | | Kitchen/Family | 58 | 1 |
| 9.0 | | First | Bed 1 | 64 | 2 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| | 57/58 | Ground | Living | 61 | 1 |
| 9.5 | | | Kitchen/Family | 51 | 0 |
| 9.5 | | First | Bed 1 | 63 | 2 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| | 59/62 | Ground | Living | 62 | 1 |
| 0.2 | | | Kitchen/Family | 51 | 0 |
| 9.3 | | First | Bed 1 & Bed 4 | 64 | 2 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| | 60/61 | Ground | Living | 62 | 1 |
| 0.2 | | | Kitchen/Family | 51 | 0 |
| 9.3 | | First | Bed 1 | 54 | 0 |
| | | | Bed 2 | 64 | 2 |
| | 63 | Ground | Living | 60 | 1 |
| | | | Kitchen/Dining | 51 | 0 |
| | | First | Bed 1 & Bed 4 | 62 | 1 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| | 64 | Ground | Family/Kitchen/Dining | 51 | 0 |
| | | | Living/Dining | 58 | 1 |
| | | First | Bed 1 | 54 | 0 |
| | | | Bed 2 & Bed 3 | 61 | 1 |
| | 65 | Ground | Living | 57 | 0 |
| | | | Kitchen/Family | 51 | 0 |
| 8.8 | | First | Bed 1 & Bed 4 | 59 | 1 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| | 67/68 | Ground | Living | 52 | 0 |
| <u>.</u> | | | Kitchen/Family | 53 | 0 |
| 9.3 | | First | Bed 1 | 56 | 0 |
| | | | Bed 2 & Bed 3 | 54 | 0 |
| | 66/69 | Ground | Living | 52 | 0 |
| | | | Kitchen/Family | 55 | 0 |
| 9.3 | | First | Bed 1 & Bed 4 | 54 | 0 |
| | | | Bed 2 & Bed 3 | 56 | 0 |
| | 70/71 | Ground | Kitchen | 58 | 1 |
| | | | Living/Dining | 58 | 1 |
| 9.5 | | First | Bed 1 & Bed 4 | 54 | 0 |
| | | | Bed 2 & Bed 3 | 59 | 1 |
| | 72/75 | Ground | Living/Dining | 54 | 0 |
| 9.8 | ,,,, | First | Bed 1 | 57 | 0 |
| | | | Bed 2, Bed 3 & Bed 4 | ≤57 | 0 |
| 9.8 | 73/74 | Ground | Living/Dining | 54 | 0 |

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| RL | Location | | | Predicted Road Traffic Noi 2030 (façade corrected) With acoustic barriers | |
|------|-----------|-----------|-----------------|--|--------------------------|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category |
| | | First | Bed 2 | 57 | 0 |
| | | | Bed 1 | ≤57 | 0 |
| | 76-78 | Ground | Kitchen | ≤57 | 0 |
| 10.0 | | First | Bed 1 & Bed 2 | 60 | 1 |
| | | | Bed 3 | 59 | 1 |
| | 79/82 | Ground | Living/Family | 63 | 2 |
| 10.3 | | First | Bed 1 & Bed 4 | 59 | 1 |
| | | | Bed 2 | 65 | 2 |
| | 80/81 | Ground | Living/Dining | ≤57 | 0 |
| 10.3 | | | Family/Kitchen | 62 | 1 |
| | | First | Bed 1 | 65 | 2 |
| | | | Bed 2 & Bed 3 | 58 | 1 |
| | 83 | Ground | Living/Dining | 59 | 1 |
| 10.5 | | | Family/Kitchen | 63 | 2 |
| 10.0 | | First | Bed 1 & Bed 4 | 59 | 1 |
| | | | Bed 2 & Bed 3 | 66 | 2 |
| | 84-85 | Ground | Family/ Kitchen | 64 | 2 |
| 10.5 | | | Living/Dining | 57 | 0 |
| 10.5 | | First | Bed 1 | 66 | 2 |
| | | | Bed 2 & Bed 3 | 60 | 1 |
| | 86 | Ground | Living | 58 | 1 |
| | | | Dining / Family | 63 | 2 |
| 10.5 | | First | Bed 4 | 61 | 1 |
| | | | Bed 1 & Bed | 69 | 3 |
| | | | Bed 2 | 67 | 2 |
| | 87 | Ground | Family/Living | 61 | 1 |
| 10.8 | | | Dining | 64 | 2 |
| | | First | Bed 1 & Bed 2 | ≤57 | 0 |
| | 88/90 | Ground | Family | 58 | 1 |
| 10.6 | | First | Bed 1 & Bed 4 | 64 | 2 |
| | | | Bed 2 & Bed 3 | 57 | 0 |
| | 89 | Ground | Living | ≤57 | 0 |
| | | | Kitchen/Family | 58 | 1 |
| 10.6 | | | Dining/Stair | ≤57 | 0 |
| | | First | Bed 1 | 57 | 0 |
| | | | Bed 2 & Bed 3 | 64 | 2 |
| | 91-93 | Ground | Living | ≤57 | 0 |
| 10.5 | | | Kitchen/Family | ≤57 | 0 |
| 10.5 | | First | Bed 1 | 60 | 1 |
| | | | Bed 2 & Bed 3 | 57 | 0 |
| 10.3 | 94-97 | All Level | All Area | 58 | 1 |
| 10.0 | 98-101 | All Level | All Area | ≤57 | 0 |
| 10.0 | 102-104 | All Level | All Area | ≤57 | 0 |
| 9.5 | 105/106 | All Level | All Area | ≤57 | 0 |
| 9.0 | 107-110 | All Level | All Area | ≤57 | 0 |
| 9.5 | 111-113 | All Level | All Area | ≤57 | 0 |

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$$\bigcirc$$

| RL | | Locati | ion | 2 (façade | ad Traffic Noi: 030 corrected) ıstic barriers |
|------|-----------|-----------|----------------|--------------------------------|--|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category |
| 10.0 | 114-116 | All Level | All Area | ≤57 | 0 |
| 10.5 | 117-120 | All Level | All Area | ≤57 | 0 |
| 10.8 | 121-124 | All Level | All Area | ≤57 | 0 |
| 11.0 | 125-127 | All Level | All Area | ≤57 | 0 |
| 11.1 | 128-131 | Ground | Living | ≤57 | 0 |
| 11.1 | | First | Bed | 58 | 1 |
| | 132/133 | Ground | Living | 60 | 1 |
| 11.3 | | | Kitchen/Family | 60 | 1 |
| 11.5 | | First | Bed 1 | 62 | 1 |
| | | | Bed 2 & Bed 3 | 62 | 1 |
| | 134 | Ground | Living | 58 | 1 |
| 11.8 | | | Family | 63 | 2 |
| 11.0 | | First | Bed 1 & Bed 4 | 60 | 1 |
| | | | Bed 2 & Bed 3 | 66 | 2 |
| | 135/136 | Ground | Living | 58 | 1 |
| 11.8 | | | Family | 63 | 2 |
| 11.0 | | First | Bed 1 | 66 | 2 |
| | | | Bed 2 & Bed 3 | 60 | 1 |
| | 137 | Ground | Living | 58 | 1 |
| 11.8 | | | Kitchen/Dining | 62 | 1 |
| 11.0 | | First | Bed 1 & Bed 4 | 61 | 1 |
| | | | Bed 2 & Bed 3 | 67 | 2 |
| | 138/139 | Ground | Living | 63 | 2 |
| 12.1 | | | Kitchen/Dining | 63 | 2 |
| 12.1 | | First | Bed 1 & Bed 2 | 65 | 2 |
| | | | Bed 3 | 57 | 0 |
| | 140/143 | Ground | Living | 59 | 1 |
| 12.0 | | | Kitchen/Family | ≤57 | 0 |
| 12.0 | | First | Bed 1 & Bed 4 | 61 | 1 |
| | | | Bed 2 & Bed 3 | 60 | 1 |
| | 141/142 | Ground | Living | 59 | 1 |
| 12.0 | | | Family | ≤57 | 0 |
| 12.0 | | First | Bed 1 | ≤57 | 0 |
| | | | Bed 2 & Bed 3 | 61 | 1 |
| | 144/145 | Ground | Living | ≤57 | 0 |
| | | | Kitchen/Family | ≤57 | 0 |
| 11.8 | | | Dining/Stair | ≤57 | 0 |
| | | First | Bed 1 | 60 | 1 |
| | | | Bed 2 & Bed 3 | ≤57 | 0 |
| 11.6 | 146-149 | All Level | All Area | ≤57 | 0 |
| 11.5 | 150-152 | All Level | All Area | ≤57 | 0 |
| 11.3 | 153-156 | All Level | All Area | ≤57 | 0 |
| 10.7 | 157-158 | All Level | All Area | ≤57 | 0 |
| 10.5 | 159-162 | All Level | All Area | ≤57 | 0 |
| 10.4 | 163-164 | All Level | All Area | ≤57 | 0 |
| 10.2 | 165-166 | All Level | All Area | ≤57 | 0 |

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| RL | | Loca | ation | 2 (façade | ad Traffic Nois 030 corrected) ıstic barriers |
|------|-----------|-----------|------------------------|--------------------------------|--|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category |
| 10.0 | 167-168 | All Level | All Area | ≤57 | 0 |
| | 169-172 | Ground | Kitchen/Dining | ≤57 | 0 |
| 9.5 | | First | Bed 1 & Bed 4 | 59 | 1 |
| | | | Bed 2 & Bed 3 | ≤57 | 0 |
| 10.0 | 173-174 | All Level | All Area | ≤57 | 0 |
| 10.3 | 175-176 | All Level | All Area | ≤57 | 0 |
| 10.5 | 177-179 | All Level | All Area | ≤57 | 0 |
| 11.0 | 180-183 | All Level | All Area | ≤57 | 0 |
| 11.2 | 184-185 | All Level | All Area | ≤57 | 0 |
| 11.8 | 186-189 | All Level | All Area | ≤57 | 0 |
| 12.0 | 190-192 | All Level | All Area | ≤57 | 0 |
| 12.1 | 193-196 | All Level | All Area | ≤57 | 0 |
| 11.9 | 197-198 | All Level | All Area | ≤57 | 0 |
| 12.3 | 199-202 | All Level | All Area | ≤57 | 0 |
| | 203-204 | Ground | Living | ≤57 | 0 |
| | | | Kitchen/Dining | ≤57 | 0 |
| 12.4 | | First | Bed 1 | 58 | 1 |
| | | | Bed 2 & Bed 3 | 59 | 1 |
| | 205-207 | Ground | Living | 63 | 2 |
| | | | Kitchen/Dining | ≤57 | 0 |
| 12.4 | | First | Bed 1 | 65 | 2 |
| | | | Bed 2 & Bed 3 | ≤57 | 0 |
| | 208-209 | Ground | Living | 64 | 2 |
| | | | Kitchen/Dining | ≤57 | 0 |
| 12.1 | | First | Bed 1 | 66 | 2 |
| | | | Bed 2 & Bed 3 | ≤57 | 0 |
| | 210 | Ground | Living | 60 | 1 |
| | | | Kitchen/Dining | 63 | 2 |
| 12.5 | | First | Bed 1/4 | 63 | 2 |
| | | | Bed 2 & Bed 3 | 69 | 3 |
| | 211/212 | Ground | Living | 60 | 1 |
| 13.5 | | | Kitchen/Family | 63 | 2 |
| 12.5 | | First | Bed 1 | 69 | 3 |
| | | | Bed 2 & Bed 3 | 63 | 2 |
| | 213 | Ground | Living | 63 | 2 |
| 10.5 | | | Kitchen/Family | 63 | 2 |
| 12.5 | | First | Bed 1 & Bed 4 | 70 | 3 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 214 | Ground | Living | 63 | 2 |
| 10 - | | | Kitchen/Dining | 63 | 2 |
| 12.7 | | First | Bed 1, Bed 2 and Bed 4 | 70 | 3 |
| | | | Bed 2 & Bed 3 | 70 | 3 |
| | 215 | Ground | Living | 61 | 1 |
| 12.7 | | First | Bed 1 & 4 | 66 | 2 |
| | | | Bed 2 & Bed 3 | 66 | 2 |
| 12.7 | 216/218 | Ground | Living | 60 | 1 |

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| RL | | 2 (façade | Predicted Road Traffic Noise 2030 (façade corrected) With acoustic barriers | | | |
|------|-----------|--------------|--|--------------------------------|--------------------------|--|
| | Townhouse | Floor | Room | L _{A10 18hr} dB(A) | QDC Noise Category | |
| | | | Kitchen/Family | 60 | 1 | |
| | | First | Bed 1 & Bed 4 | 63 | 2 | |
| | | | Bed 2 & Bed 3 | 65 | 2 | |
| | 217 | Ground | Living | 60 | 1 | |
| 12.7 | | | Family | 60 | 1 | |
| 12.7 | | First | Bed 1 | 65 | 2 | |
| | | | Bed 2 & Bed 3 | 63 | 2 | |
| | 219/221 | Ground | Living | 59 | 1 | |
| 12.6 | | | Family | 63 | 2 | |
| 12.0 | | First | Bed 1 & Bed 4 | 61 | 1 | |
| | | | Bed 2 & Bed 3 | 65 | 2 | |
| | 220 | Ground | Living | 59 | 1 | |
| 12.6 | | | Kitchen/Family | 63 | 2 | |
| 12.0 | | First | Bed 1 | 65 | 2 | |
| | | | Bed 2 & Bed 3 | 61 | 1 | |
| | 222/225 | Ground | Living | 55 | 0 | |
| 12.6 | | | Kitchen/Family | 62 | 1 | |
| 12.0 | | First | Bed 1 & Bed 4 | 58 | 1 | |
| | | | Bed 2 & Bed 3 | 64 | 2 | |
| | 223/24 | Ground | Living | 55 | 0 | |
| 12.6 | | | Kitchen/Family | 62 | 1 | |
| 12.0 | | First | Bed 1 | 64 | 2 | |
| | | | Bed 2 & Bed 3 | 58 | 1 | |

Based on the predicted noise impacts, additional façade treatments are required in accordance with the QDC assessment requirements.

As per Table 1.2.2 of the SDAP policy statement an acceptable alternative solution is compliance with AO23.2 to 23.4. Refer to Section 9 for recommended additional acoustic treatments required to achieve compliance with SDAP criteria AO23.2 to 23.4.

As per Table 1.2.2 of the SDAP policy statement an acceptable alternative solution is compliance with AO24.2 and 24.3. Refer to Section 9 for recommended additional acoustic treatments required to achieve compliance with SDAP criteria AO24.2 and 24.3.

Figure 4 to Figure 7 show the predicted road traffic noise contours for the development with and without acoustic barriers.

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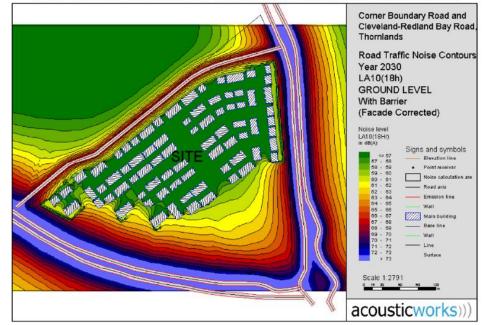
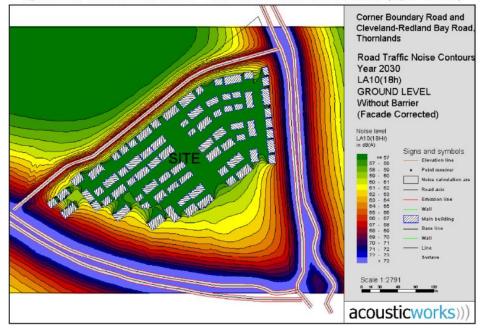


Figure 4: Road Traffic Noise Contours 2030 - Ground Floor Level with acoustic barriers (façade corrected)

Figure 5: Road Traffic Noise Contours 2030 - Ground Floor Level without acoustic barriers (façade corrected)



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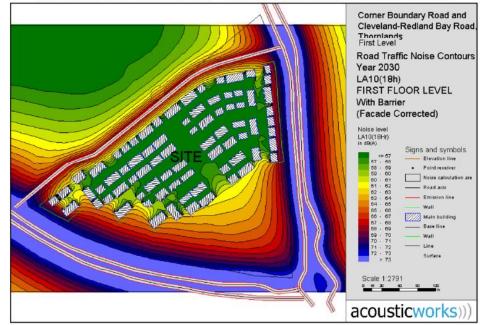
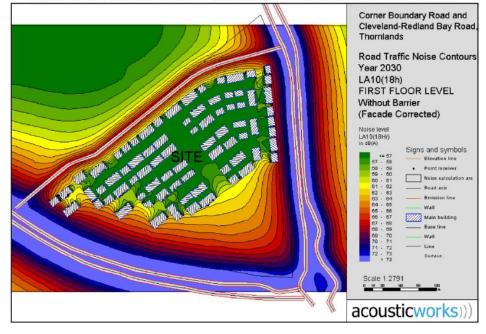


Figure 6: Road Traffic Noise Contours 2030 - First Floor Level with acoustic barriers (façade corrected)

Figure 7: Road Traffic Noise Contours 2030 - First Floor Level without acoustic barriers (façade corrected)



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An assessment of the private recreation areas was conducted with the results presented in Figure 8 (without acoustic barriers) and Figure 9 (with acoustic barriers).

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Item 14.1- Attachment 1

The following townhouses are noted to have balconies and predicted to have free field façade noise levels exceeding 60 dB(A) at the following nominated locations (room use). These balconies are required to be installed with solid gap-free balustrades (no gaps between panels, except for drainage).

| Townhouse | Floor Level | Room Use | Free Field Noise Levels LA10 18hr dB(A) |
|----------------|-------------|------------------------|--|
| 1 | First | Bed 1 | 62.5 |
| 2 | First | Bed 1 | 61.5 |
| 2 5 to 6 | First | Bed 1 & Bed 4 | 61.5 |
| | | | |
| 7 to 8 | First | Bed 2 & Bed 3 | 61.5 61.5 |
| 9 to 10 | First | Bed 1 | |
| 11 to 12 | First | Bed 1 | 61.5 |
| 14/15 17/20 | First | Bed 1 Bed 2 & Bed 3 | <u>61.5</u> 61.5 |
| | First | | |
| 18/19 | First | Bed 1 | 61.5 |
| 21/22 | First | Bed 1 | 61.5 |
| 23/24 | First | Bed 1 | 61.5 |
| 26-27 | First | Bed 1 | 62.5 |
| 29 | First | Bed 1 | 62.5 |
| 29 | First | Bed 2 & Bed 3 | 63.5 |
| 30 | First | Bed 1 | 62.5 |
| 30 | First | Bed 2 | 63.5 |
| 45-47 | First | Bed 2 & Bed 3 | 68.5 |
| 50-52 | First | Bed 1 | 63.5 |
| 53/56 | First | Bed 2 & Bed 3 | 62.5 |
| 54/55 | First | Bed 1 | 62.5 |
| 54/55 | First | Bed 2 & Bed 3 | 52.5 |
| 80/81 | First | Bed 1 | 63.5 |
| 83 | First | Bed 2 & Bed 3 | 64.5 |
| 84-85 | First | Bed 1 | 64.5 |
| 86 | First | Bed 1 & Bed | 67.5 |
| 86 | First | Bed 2 | 65.5 |
| 88/90 | First | Bed 1 & Bed 4 | 62.5 |
| 89 | First | Bed 2 & Bed 3 | 62.5 |
| 132/133 | First | Bed 1 | 60.5 |
| 132/133 | First | Bed 2 & Bed 3 | 60.5 |
| 134 | First | Bed 2 & Bed 3 | 64.5 |
| 135/136 | First | Bed 1 | 64.5 |
| 137 | First | Bed 2 & Bed 3 | 65.5 |
| 138/139 | First | Kitchen/Dining | 61.5 |
| 138/139 | First | Bed 1 & Bed 2 | 63.5 |
| 205-207 | First | Bed 1 | 63.5 |
| 208-209 | First | Bed 1 | 64.5 |
| 210 | First | Bed 1/4 | 61.5 |
| 210 | First | Bed 2 & Bed 3 | 67.5 |
| 211/212 | First | Bed 1 | 67.5 |
| 211/212 | First | Bed 2 & Bed 3 | 61.5 |
| 213 | First | Bed 1 & Bed 4 | 68.5 |
| 213 | First | Bed 2 & Bed 3 | 68.5 |

| Table 1 | 1: Townhouses Re | quiring Balcony w | with Solid Balustrade (| no gaps between panels | , except drainage) |
|---------|------------------|-------------------|-------------------------|------------------------|--------------------|
| | | | | | |

On the condition the acoustic barriers recommended in Section 9.1 are implemented, compliance is predicted with SDAP AO24.1 of L10(18h) \leq 60 dB(A) free field for all private recreation areas.

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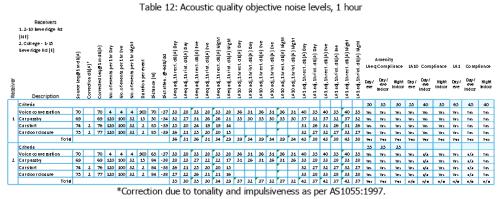
8. Environmental Noise Assessment

8.1 Onsite Activities

Noise associated with the development was assessed based on previous measurements of similar activities. The calculations assume that the nominated activities are located at the closest representative point within the development site to each receiver location. Any relevant shielding, building transmission loss or recommended acoustic screens are taken into account for these activities.

8.1.1 Acoustic Quality Objectives

The average maximum noise source levels and predicted levels at the receiver locations are shown in Table 12. Note: L_{A10} and L_{A1} results are not shown in cases where the total duration of the events is less than the minimum time required e.g. $L_{A10(1hr)}$ requires noise events to occur for at least 360 seconds of an hour long period. LAeq results are not shown where the calculated total is less than 0dBA.



Compliance is predicted with the Acoustic Quality Objectives without the need for any additional acoustic treatment.

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8.1.2 Background Creep

The noise source levels and predicted levels at the receiver locations are shown as follows;

| | Tal | ble : | 13: | Bac | kgrou | und C | reep | o noi | se le | vels | , 15 | mir | nute | | | | | | |
|----------|---|---------------------|------------------|------------------------|-----------------------------|-----------------------------|-------------------------------|----------------|--------------|---------------------|---------------------------|---------------------------|---------------------------|---------------------------|-----------------------------|------------------------|-----|------------------|-------|
| | Receivers | | | | | | | | | | | | | | | | | | |
| | 1. 2-10 Beveridge Rd (NE) 2. College - 1-15 Beveridge Rd (E) | m dB(A) | A)* | @1 m dB(A) | No. of events per 15min Day | No. of events per 15min Eve | No. of events per 15min Night | event | | 5dB/dd | dB(A) Day | IB(A) Day | dB(A) Eve | 1B(A) Eve | dB(A) Night | adj,T int. dB(A) Night | | eq 15 i mplia | |
| Receiver | Description | Source Leq@1m dB(A) | Correction dB(A) | Corrected Leq@1 m dB(A | No. of events | No. of events | No. of events | Duration per e | Distance (m) | Dist atten.@-6dB/dd | LAeq adj,T ext. dB(A) Day | LAeq adj,T int. dB(A) Day | LAeq adj,T ext. dB(A) Eve | LAeq adj,T int. dB(A) Eve | LAeq adj,T ext. dB(A) Night | LAeq adj, T int. (| Day | Eve | Night |
| | Criteria | | | | | | | | | | | | | | | | 61 | 58 | 50 |
| | Voice conversation | 70 | | 70 | 1 | 1 | 1 | 900 | 70 | -37 | 33 | 28 | 33 | 28 | 33 | 28 | Yes | Yes | Yes |
| 1 | Car passby | 69 | | 69 | 30 | 25 | 8 | 15 | 50 | - 34 | 32 | 27 | 31 | 26 | 26 | 21 | Yes | Yes | Yes |
| | Car start | 74 | 2 | 76 | 30 | 25 | 8 | 2 | 85 | - 39 | 25 | 20 | 24 | 19 | 19 | 14 | Yes | Yes | Yes |
| | Car do or closure | 75 | 2 | 77 | 30 | 25 | 8 | 2 | 85 | - 39 | 26 | 21 | 25 | 20 | 20 | 15 | Yes | Yes | Yes |
| | Total | | | | | | | | | | 36 | 31 | 36 | 31 | 34 | 29 | Yes | Yes | Yes |
| | Criteria | | | | | | | | | | | | | | | | 61 | 58 | 50 |
| | Voice conversation | 70 | | 70 | 1 | 1 | 1 | 900 | 65 | -37 | 33 | 28 | 33 | 28 | 33 | 28 | Yes | Yes | Yes |
| 2 | Car passby | 69 | | 69 | 30 | 25 | 8 | 15 | 84 | -38 | 28 | 23 | 27 | 22 | 22 | 17 | Yes | Yes | Yes |
| | Car start | 74 | 2 | 76 | 30 | 25 | 8 | 2 | 84 | -38 | 26 | 21 | 25 | 20 | 20 | 15 | Yes | Yes | Yes |
| | Car do or closure | 75 | 2 | 77 | 30 | 25 | 8 | 2 | 84 | -38 | 27 | 22 | 26 | 21 | 21 | 16 | Yes | Yes | Yes |
| | Total | | | | | | | | | | 35 | 30 | 35 | 30 | 34 | 29 | Yes | Yes | Yes |

Compliance is predicted with the Background Creep criteria without the need for any additional acoustic treatment.

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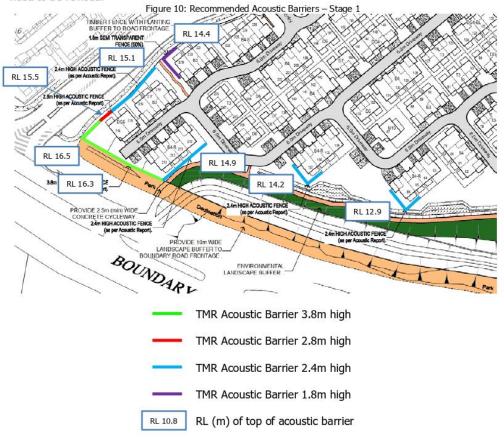
9. Recommendations

9.1 Acoustic Barriers

Acoustic barriers are recommended to be constructed as shown in Figure 10 and Figure 11 in accordance with TMR design specifications.

The TMR barriers shall be 1.8-3.8m above the finished pad level of the adjacent townhouse lot and be constructed in accordance with Main Roads Standard Specification MRTS15 "*Noise Fences*". Acoustic barriers shall be free of gaps and holes.

Figure 10 and Figure 11 show location and heights of the recommended acoustic barriers. These are calculated based on the required barrier heights and the RL of the pad level of the adjacent townhouse lot. If the RL levels of the townhouses change then the barrier recommendations may need to be revised.



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9.2 Road Traffic Noise

All building treatments for residential dwellings shall be determined at the Building Approval stage in accordance with the assessment requirements of the Queensland Development Code Part MP4.4. For reference, preliminary noise categories are nominated in Table 14.

9.2.1 Glazing & Noise Category

Calculations were conducted in accordance with Queensland Development Code Part MP4.4. The minimum glazing treatments presented in Table 14 are required to comply with the following:

- The minimum glass thickness specified shall not be reduced regardless of the R_w performance of the glazing system.
- If compliance cannot be achieved with the minimum $R_{\rm w}$ ratings, the glazing system shall be upgraded until compliance is achieved.
- Glazing specified with acoustic seals requires a Q-lon seal or an equivalent product, mohair seals are not acceptable.
- The glazier shall provide NATA test reports on request to verify compliance with the minimum $R_{\rm w}$ ratings. Generic reports are not acceptable.

| | | | | Q |)DC I | | ating | s | QDC Glazing | <u>.</u> |
|-----------|--------|----------------|--------------------------|------|-------|---------|-------|--------|-------------|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| 1 | Floor | Room | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | yes |
| | | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | yes |
| | | Bed 2 | 0 | - | - | - | - | - | | yes |
| | | Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| 2 | Ground | Living | 0 | - | - | - | - | - | | yes |
| | | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 | 0 | - | - | - | - | - | | yes |
| | | Bed 2 | 0 | - | - | - | - | - | | yes |
| | | Bed 3 | 0 | - | - | - | - | - | | yes |
| 3-4 | Ground | Kitchen | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| | | Living/Dining | 0 | - | - | - | - | - | | yes |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| 5-6 | Ground | Living | 0 | - | - | - | - | - | | yes |
| | | Kitchen | 0 | - | - | - | - | - | | yes |
| | | Dining/Stair | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 & Bed 4 | 0 | - | - | - | - | - | | yes |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | yes |
| 7-8 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | yes |
| | | Kitchen/Family | 0 | - | - | - | - | - | | yes |
| | First | Bed 1& Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | yes |
| 9-10 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | yes |
| | | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 | 0 | - | - | - | - | - | | yes |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| 11-12 | Ground | Living | 0 | - | - | - | - | - | | yes |
| | | Kitchen | 0 | - | - | - | - | - | | yes |
| | | Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 | 0 | - | - | - | - | - | | yes |

Table 14: QDC Noise Category & Glazing Treatments

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| | | | | 0 | DC I | Rw R | ating | | QDC Glazing | <u>-</u> |
|-----------|--------|----------------|--------------------------|------|------|---------|-------|--------|--|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 13/16 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ý |
| | | Kitchen/Family | 0 | - | - | - | - | - | | , ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 14/15 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ý |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | y |
| 17/20 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ý |
| | | Kitchen/Family | 0 | - | - | - | - | - | | ý |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ý |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ý |
| 18/19 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ý |
| | | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ý |
| | First | Bed 1 | 0 | - | - | - | - | - | | y y |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | y |
| 21/22 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ý |
| | | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ý |
| | First | Bed 1 | 0 | - | - | - | - | - | 10100 1411 | y |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ý |
| 23/24 | Ground | Living | 0 | - | - | - | - | - | | y |
| 23/21 | Ground | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | y y |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | y |
| | mac | Bed 2 | 0 | - | - | - | - | - | minnoac | y |
| | | Bed 3 | 0 | - | - | - | - | - | | y |
| 25/28 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | y y |
| 23/20 | oroana | Kitchen/Family | 1 | 35 | - | 27 | - | - | 4mm float | y |
| | | Dining/Stair | 0 | - | - | - | - | - | minnout | y |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | y |
| | mac | Bed 2 & Bed 3 | 0 | - | - | - | - | - | 10.00 Milli | y |
| 26-27 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | y |
| 20 27 | oround | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | y |
| | First | Bed 1 | 0 | - | - | - | - | - | 10.50 Iam | y y |
| | mac | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | y y |
| 29 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | y y |
| 25 | Ground | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | y y |
| | First | Bed 1 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | - |
| | TIISC | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | y v |
| 30 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | y v |
| 30 | Ground | Kitchen/Dining | 2 | 41 | - 38 | 35 | - | - | 10.38 lam | y v |
| | First | Bed 1 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | y v |
| | TISU | Bed 1 | 0 | - 41 | 38 | - 35 | - | - | 10.30 IgIII | y v |
| 31/32 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | y v |
| 31/32 | Ground | Kitchen/Dining | 1 | 35 | 35 | 27 | - | - | 4mm float | y y |
| | First | Bed 1 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | y y |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | y |
| 33-34 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | y y |
| JJ-JT | Ground | Kitchen/Dining | 0 | - | - | - | - | - | -min Hoat | y y |
| | E . | | | | | | | - | 12.5 Vlam or 13.52 lam or | |
| | First | Bed 1 | 3 | 47 | 41 | 38 | - | - | 14.38 lam | У |
| 0.5/5- | - · | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | y |
| 35/38 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | У |
| | | Kitchen/Family | 0 | - | - | - | - | - | | У |
| | First | Bed 1 & Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or | y |
| | | | - | | | | | | 14.38 lam | · ' |

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| | | | | | 2DC I | | ating | | QDC Glazing | Ľ. |
|-----------|--------------|---------------------------------|--------------------------|----------|-------|----------|-------|--------|--|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 36-37 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitch on /Equily | 3 | 47 | 41 | 38 | _ | - | 12.5 Vlam or 13.52 lam or | |
| | | Kitchen/Family | 3 | 47 | 41 | 38 | - | - | 14.38 lam | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 39-40 | Ground | Kitchen | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| | | Living/Dining | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 & Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 41-42 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitchen/Dining | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 43-44 | Ground | Kitchen | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| | | Living/Dining | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 & Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 45-47 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitchen/Dining | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 48/49 | Ground | Kitchen | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| | | Living/Dining | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 50-52 | Ground | Living | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 53/56 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitchen | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 54/55 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | _ | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | 4 0 . | ye |
| 57/50 | <u> </u> | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 57/58 | Ground | Living | 0 | - | - | - | - | - | 10.00 1 | ye |
| | F · · | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | 4 | ye |
| 50/62 | Creation -1 | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 59/62 | Ground | Living Kitch on (Family | 0 | - | - | - | - | - | 10.20 | ye |
| | First | Kitchen/Family Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | FIFSE | | 0 | - | - | - | - | - | 4mm float | ye |
| 60/61 | Ground | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm Tioat | ye |
| 60/61 | Ground | Living Kitchen/Family | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 0 | - | | | - | - | 10.38 lam | ye |
| | First | Bed 1 Bed 2 | 1 | 41 35 | 38 | 35 27 | - 28 | - | 4mm float | ye |
| 63 | Ground | Living | 0 | - 35 | - | - | - 20 | - | HIIII IOat | ye ve |
| 03 | Ground | Kitchen/Dining | 1 | 35 | 35 | - 27 | - | - | 4mm float | ye |
| | I | Recipiting | 1 | 55 | 55 | 21 | | | minitual | ye |

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| | | | 0.0.0 | | 2DC I | | ating | | QDC Glazing | ť |
|-------------------|------------|-------------------------|--------------------------|------|-------|----------|---------|--------|--|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| | First | Bed 1 & Bed 4 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| <i>с</i> 1 | 0 1 | Family/Kitchen | | 25 | | | 20 | | 4 6 1 | |
| 64 | Ground | /Dining | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| | | Living/Dining | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 65 | Ground | Living | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 67/68 | Ground | Living | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Family | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 66/69 | Ground | Living | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Family | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 & Bed 4 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 70/71 | Ground | Kitchen | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| · | | Living/Dining | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 & Bed 4 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 72/75 | Ground | Living/Dining | 0 | - | - | - | - | - | | ye |
| , | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2, Bed 3 & Bed 4 | 0 | - | - | - | - | - | | ye |
| 73/74 | Ground | Living/Dining | 0 | - | - | - | - | - | | ye |
| | First | Bed 2 | 0 | - | - | - | - | - | | ye |
| | | Bed 1 | 0 | - | - | - | - | - | | ye |
| 76-78 | Ground | Kitchen | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 2 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 3 | 2 | 41 | - | 35 | 33 | - | 10.38 lam | ye |
| 79/82 | Ground | Living/Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 | 0 | - | - | - | - | - | 10100 10111 | ye |
| 80/81 | Ground | Living/Dining | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 00/01 | oroana | Family/Kitchen | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 83 | Ground | Living/Dining | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| | oround | Family/Kitchen | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | TISC | Bed 2 & Bed 3 | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| 84-85 | Ground | Family/ | 0 | - | - | - | - | - | 10.50 lain | ye |
| | | Kitchen | 2 | 41 | 38 | 35 | - | - | 10.38 lam | |
| | | Living/Dining | | | | | | | | ye |
| | First | Bed 1 Red 2 % Red 2 | 1 | 35 | 35 | 27 | - 20 | - | 4mm float | ye |
| 96 | Ground | Bed 2 & Bed 3 | 1 | 35 | - | 27 35 | 28 | - | 4mm float | ye |
| 86 | Ground | Living | 2 | 41 | | | | | 10.38 lam | ye |
| | | Dining / Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 1 & Bed | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 87 | Ground | Family/Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| | | Dining | 0 | - | - | - | - | - | | ye |

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| | | | | C | QDC I | | ating | | QDC Glazing | <u>,</u> |
|-----------|-----------|--------------------------|--------------------------|------|---------|---------|-------|--------|-------------|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| | First | Bed 1 & Bed 2 | 1 | 35 | - | 27 | - | - | 4mm float | yes |
| 88/90 | Ground | Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 & Bed 4 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 89 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitchen/Family | 0 | - | - | - | - | - | | ye |
| | | Dining/Stair | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 91-93 | Ground | Living | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 94-97 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 98-101 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 102-104 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 105/106 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 107-110 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 111-113 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 114-116 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 117-120 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 121-124 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 125-127 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 128-131 | Ground | Living | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 132/133 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 102,100 | oreand | Kitchen/Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 134 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| | | Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | THE | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 135/136 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| 200,200 | Ground | Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | THOU | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 137 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| 157 | oround | Kitchen/Dining | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | _ | - | 10.38 lam | ye |
| | mac | Bed 2 & Bed 3 | 2 | 41 | - | 35 | 33 | - | 10.38 lam | ye |
| 138/139 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| 150/155 | Ground | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 & Bed 2 | 0 | - | - | - | - | - | 10:20 10111 | ye |
| | TISC | Bed 3 | 1 | 35 | - | - 27 | 28 | - | 4mm float | ye ye |
| 140/143 | Ground | Living | 0 | - | - | - | - | - | mini nuac | |
| | Ground | Kitchen/Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | HISU | Bed 2 & Bed 3 | 1 | 35 | - | 27 | - 28 | - | 4mm float | ye |
| 141/142 | Ground | Living | 0 | - 35 | - | - | - 28 | - | HIIIIIIIUal | ye |
| 141/142 | Ground | Family | 0 | - | - | - | - | - | | ye |
| | First | Bed 1 | 1 | | - 35 | | | - | 4mm float | ye |
| | First | | | 35 | 33 | 27 | - | | HIIII HOat | ye |
| 144/145 | Crowned | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 144/145 | Ground | Living Kitchon/Family | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Family | 0 | - | - 25 | - | - | - | Ame - 41+ | ye |
| | Ei+ | Dining/Stair | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |

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| | | | | | DC I | | ating | | QDC Glazing | ť |
|-----------|-----------|---------------------------|--------------------------|------|------|---------|-------|--------|--|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 146-149 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 150-152 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 153-156 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 157-158 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 159-162 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 163-164 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 165-166 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 167-168 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 169-172 | Ground | Kitchen/Dining | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 173-174 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 175-176 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 177-179 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 180-183 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 184-185 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 186-189 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 190-192 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 193-196 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 197-198 | All Level | All Area | 0 | - | - | - | - | - | | ye |
| 199-202 | All Level | All Area | 0 | - | - | - | - | - | | , ye |
| 203-204 | Ground | Living | 0 | - | - | - | - | - | | , ye |
| | | Kitchen/Dining | 1 | 35 | 35 | 27 | - | - | 4mm float | ýe |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 2 | 41 | - | 35 | 33 | - | 10.38 lam | ye |
| 205-207 | Ground | Living | 0 | - | - | - | - | - | | ye |
| | | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 2 | 41 | - | 35 | 33 | - | 10.38 lam | , ye |
| 208-209 | Ground | Living | 0 | - | - | - | - | - | | ye |
| 200 200 | Creana | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 0 | - | - | - | - | - | | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 210 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| | | Kitchen/Dining | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | | | | | | | | 12.5 Vlam or 13.52 lam or | |
| | First | Bed 1/4 | 3 | 47 | 41 | 38 | - | - | 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 211/212 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | , ye |
| · | | Kitchen/Family | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | , ye |
| | First | Bed 1 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 & Bed 3 | 2 | 41 | - | 35 | 33 | - | 10.38 lam | ye |
| 213 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| 210 | oround | Kitchen/Family | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | First | Bed 1 & Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 2 | 41 | - | 35 | 33 | - | 10.38 lam | ye |
| 214 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| | C. Curiu | Kitchen/Dining | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | First | Bed 1, Bed 2 and Bed 4 | 3 | 47 | 41 | 38 | - | - | 12.5 Vlam or 13.52 lam or 14.38 lam | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| | | | | | | | | | | 1 1 - |

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| | | | | QDC Rw Ratings | | s | QDC Glazing | <u>.</u> | | |
|-----------|--------|----------------|--------------------------|----------------|------|---------|-------------|----------|-----------|----------|
| Townhouse | Floor | Location | QDC Noise Category | Wall | Roof | Glazing | Entry | Floors | Glazing | Acoustic |
| | First | Bed 1 & 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| 216/218 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | yes |
| | | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | yes |
| 217 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | yes |
| | | Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | First | Bed 1 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | yes |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 219/221 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye |
| | | Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |
| 220 | Ground | Living | 2 | 41 | - | 35 | - | - | 10.38 lam | ye: |
| | | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 222/225 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye |
| | | Kitchen/Family | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | First | Bed 1 & Bed 4 | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | | Bed 2 & Bed 3 | 0 | - | - | - | - | - | | ye |
| 223/24 | Ground | Living | 1 | 35 | - | 27 | - | - | 4mm float | ye: |
| | | Kitchen/Family | 2 | 41 | 38 | 35 | - | - | 10.38 lam | ye |
| | First | Bed 1 | 1 | 35 | 35 | 27 | - | - | 4mm float | ye |
| | | Bed 2 & Bed 3 | 1 | 35 | - | 27 | 28 | - | 4mm float | ye |

Any locations not identified in Table 14 are nominated as QDC Noise Category 0 and require standard construction for road traffic noise.

9.2.2 Solid Balustrades

As discussed in Section 7.3, Table 11 lists the balconies predicted to have free field façade noise levels exceeding 60 dB(A). These balconies are required to be installed with solid gap-free balustrades (no gaps between panels, except for drainage).

9.2.3 Alternative Ventilation

We recommend that all locations nominated in Table 14 as QDC Noise Category 1-3 have the provision for an alternative ventilation system similar to air-conditioning or mechanical ventilation to allow windows and doors to be closed.

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9.2.4 Wall Construction

The wall construction recommendations from QDC MP4.4 are included in the table below. Note that these are not the only allowable methods of construction for the development, and alternative constructions to achieve the required Rw ratings may also be provided. QDC Noise Categories are nominated in Table 14.

Table 15: QDC typical wall construction

| QDC Noise Category | Wall R _w | Minimum Wall Treatments |
|--------------------------|------------------------|--|
| 3 | 47 | Two leaves of clay brick masonry at least 110mm thick with: (i) cavity not less than 50mm between leaves; and (ii) 50mm thick mineral insulation or 50mm thick glass wool insulation with a density of 11kg/m3 or 50mm thick polyester insulation with a density of 20kg/m3 in the cavity. OR Two leaves of clay brick masonry at least 110mm thick with: (i) cavity not less than 50mm between leaves; and (ii) at least 13mm thick cement render on each face. OR Single leaf of clay brick masonry at least 110mm thick with: (i) a row of at least 70mm x 35mm timber studs or 64mm steel studs at 600mm centres, spaced at least 20mm from the masonry wall; and (ii) mineral insulation or glass wool insulation at least 50mm thick with a density of at least 11kg/m3 positioned between studs; and (iii) One layer of plasterboard at least 13mm thick fixed to outside face of studs. OR Single leaf of minimum 150mm thick masonry of hollow, dense concrete blocks, with mortar joints laid to prevent moisture bridging. |
| 2 | 41 | Two leaves of clay brick masonry at least 110mm thick with cavity not less than 50mm between leaves OR Single leaf of clay brick masonry at least 110mm thick with: (i) a row of at least 70mm x 35mm timber studs or 64mm steel studs at 600mm centres, spaced at least 20mm from the masonry wall; and (ii) mineral insulation or glass wool insulation at least 50mm thick with a density of at least 11kg/m ³ positioned between studs; and (iii) One layer of plasterboard at least 10mm thick fixed to outside face of studs OR Single leaf of brick masonry at least 110mm thick with at least 13mm thick render on each face OR Concrete brickwork at least 110mm thick OR In-situ concrete at least 100mm thick OR Precast concrete at least 100mm thick and without joints. |
| 1 | 35 | Single leaf of clay brick masonry at least 110mm thick with: (i) a row of at least 70mm x 35mm timber studs or 64mm steel studs at 600mm centres, spaced at least 20mm from the masonry wall; and (ii) One layer of plasterboard at least 10mm thick fixed to outside face of studs OR Minimum 6mm thick fibre cement sheeting, minimum 90mm deep timber stud or 92mm metal stud, standard plasterboard at least 13mm thick internally. |
| 0 | N/A | Standard Construction |

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9.2.5 Roof Construction

The roof construction recommendations from QDC MP4.4 are included in the table below. Note that these are not the only allowable methods of construction for the development, and alternative constructions to achieve the required Rw ratings may also be provided. QDC Noise Categories are nominated in Table 14.

Table 16: QDC typical roof construction

| QDC Noise Category | Roof Rw | Minimum Roof Treatments |
|--------------------------|------------|---|
| 3 | 41 | Concrete or terracotta tile or metal sheet roof with sarking, plasterboard ceiling at least 10mm thick fixed to ceiling joists, glass wool insulation at least 50mm thick with a density of at least 11kg/m3 or polyester insulation at least 50mm thick with a density of at least 20kg/m3 in the cavity. OR Concrete suspended slab at least 100mm thick. |
| 2 | 38 | Concrete or terracotta tile or metal sheet roof with sarking, plasterboard ceiling at least 10mm thick fixed to ceiling cavity, mineral insulation or glass wool insulation at least 50mm thick with a density of at least 11kg/m ³ . |
| 1 | 35 | Concrete or terracotta tile or metal sheet roof with sarking, plasterboard ceiling at least 10mm thick fixed to ceiling cavity. |
| 0 | N/A | Standard Construction |

9.2.6 Floors

The exposed floor construction recommendations from QDC MP4.4 are included in the table below. Note that these are not the only allowable methods of construction for the development, and alternative constructions to achieve the required Rw ratings may also be provided. QDC Noise Categories are nominated in Table 14.

| QDC Noise Category | Floor R _w | Minimum Floor Treatments |
|--------------------------|----------------------|---|
| 2/3 | 45 | Concrete slab at least 100mm thick OR Tongued and grooved boards at least 19mm thick with: (i) timber joists not less than 175mm x 50mm; and (ii) mineral insulation or glass wool insulation at least 75mm thick with a density of at least 11kg/m ³ positioned between joists and laid on plasterboard at least 10mm thick fixed to underside of joists; and (iii) mineral insulation or glass wool insulation at least 25mm thick with a density of at least 11kg/m ³ laid over entire floor, including tops of joists before flooring is laid; and (iv) secured to battens at least 75mm x 50mm; and (v) the assembled flooring laid over the joists, but not fixed to them, with battens lying between the joists. |
| 0/1 | N/A | Standard Construction |

Table 17: QDC typical exposed floor construction

Refers to floors of habitable rooms which are part of the external façade of the building i.e. rooms which overhang the level below.

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9.2.7 Entry Doors

The entry door construction recommendations from QDC MP4.4 are included in the table below. Note that these are not the only allowable methods of construction for the development, and alternative constructions to achieve the required Rw ratings may also be provided. QDC Noise Categories are nominated in Table 14.

Table 18: QDC typical entry door construction

| QDC Noise Category | Entry Door R _w | Minimum Entry Door Treatments |
|--------------------------|------------------------------|---|
| 1 | 28 | Fixed so as to overlap the frame or rebate of the frame, constructed of – (i) wood, particleboard or blockboard not less than 33mm thick; or (ii) compressed fibre reinforced sheeting not less than 9mm thick; or (iii) other suitable material with a mass per unit area not less than 24.4kg/m ² ; or (iv) solid core timber door not less than 35mm thick fitted with full perimeter <i>acoustically</i> <i>rated sea</i> /s. |
| 0 | N/A | Standard Construction |

9.3 Onsite Activities

Compliance is predicted with the assessment criteria for noise associated with the development without the need for any additional acoustic treatments.

10. Conclusion

An environmental noise assessment was conducted for the proposed townhouse development located at 410 Boundary Road and 359-405 Cleveland-Redland Bay Road, Thornlands. On the condition the recommendations detailed in Section 9 are implemented, compliance is predicted with Redland City Council and SDAP assessment criteria.

If you should have any queries, please do not hesitate to contact us.

Report Prepared By

MACan

Dr. Mahbub Sheikh Senior Acoustic Consultant acousticworks)))

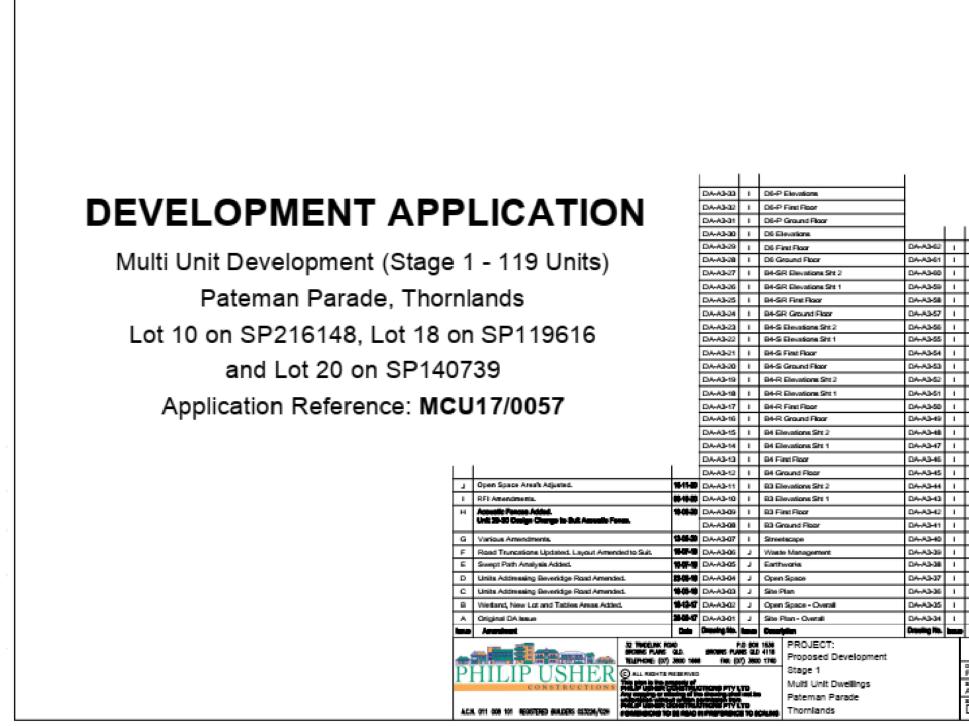
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11. Appendices

11.1 Development Plans

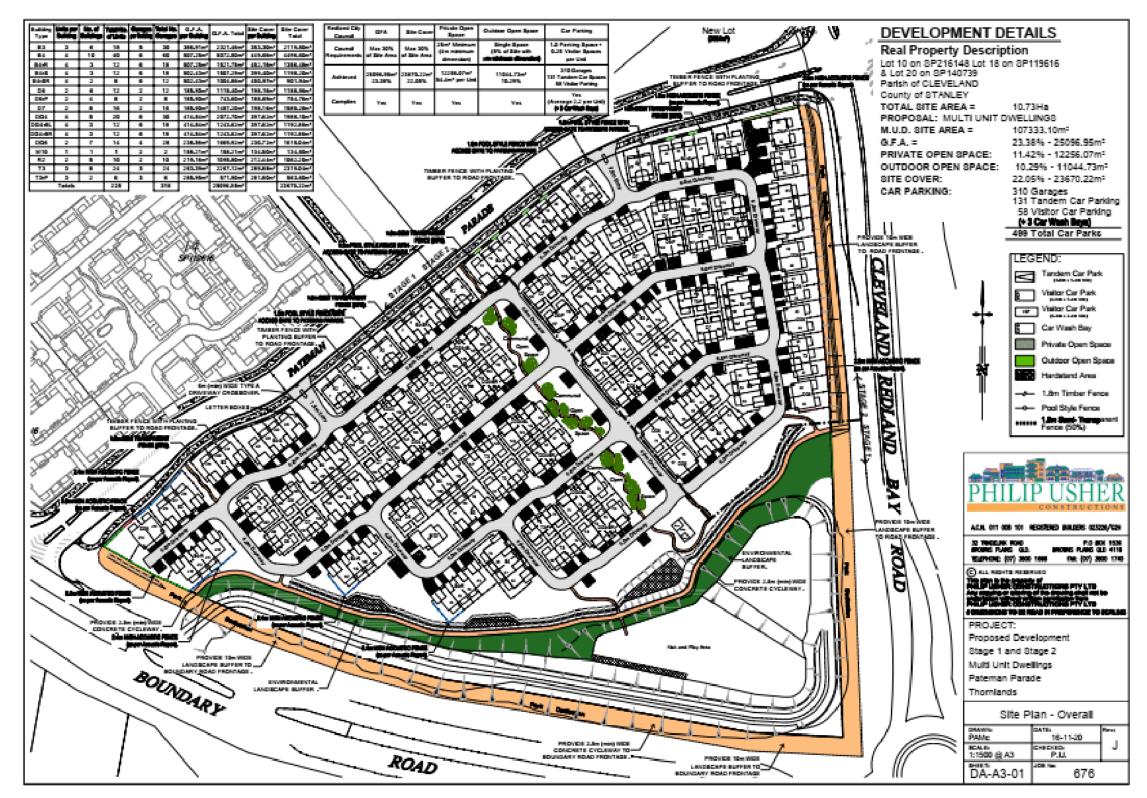
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| Gym-Patio Floor Plans and Elevations |
|--------------------------------------|
| T3-P Elevations |
| T3-P Anst Assr |
| T3-P Ground Roor |
| T3 Elevations |
| T3 Fint Roor |
| T3 Ground Floor |
| R2 Elevations |
| R2 Roor Plans |
| M10 Elevations |
| M10 Floor Flans |
| DG6 Elevations |
| DG6 First Floor |
| DG6 Ground Floar |
| DG4-BR Elevations Sht 2 |
| DG4-BR Elevations Sht 1 |
| DG4-BR First Floor |
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| DG4-BL First Floor |
| DG4-BL Ground Floor |
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| D7 Elevations |
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Cover Sheet

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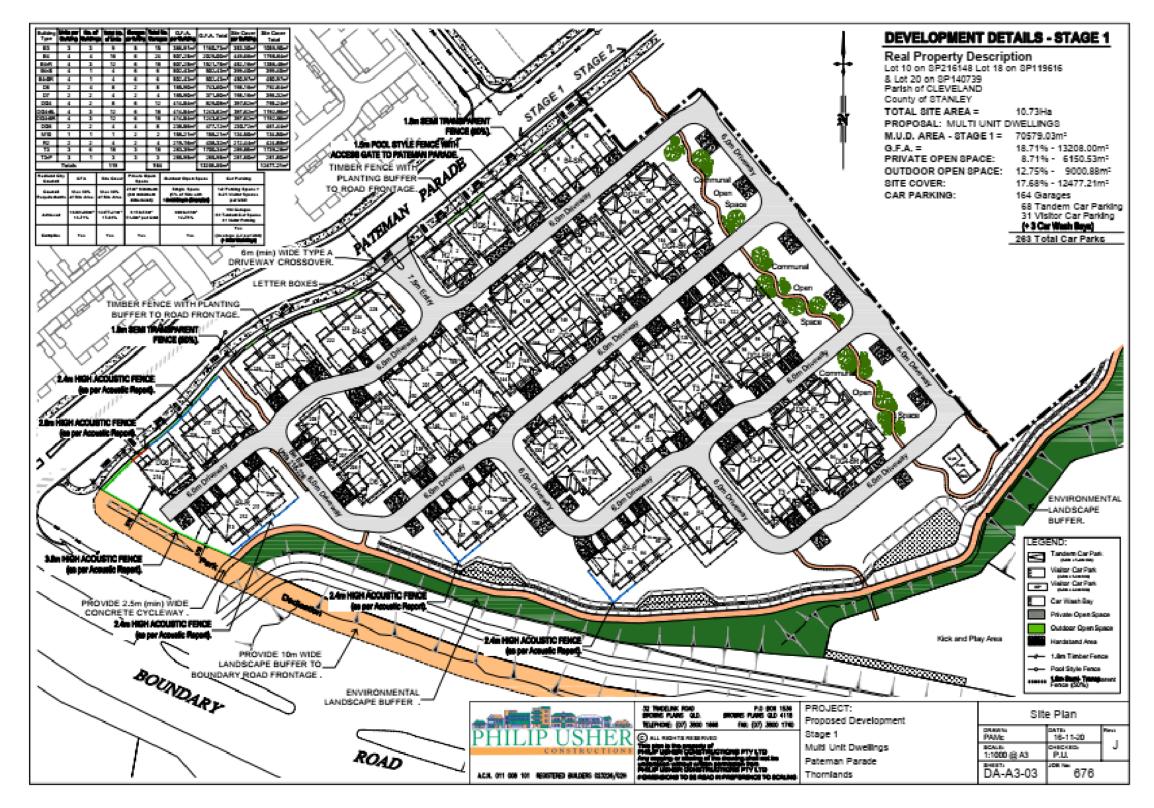
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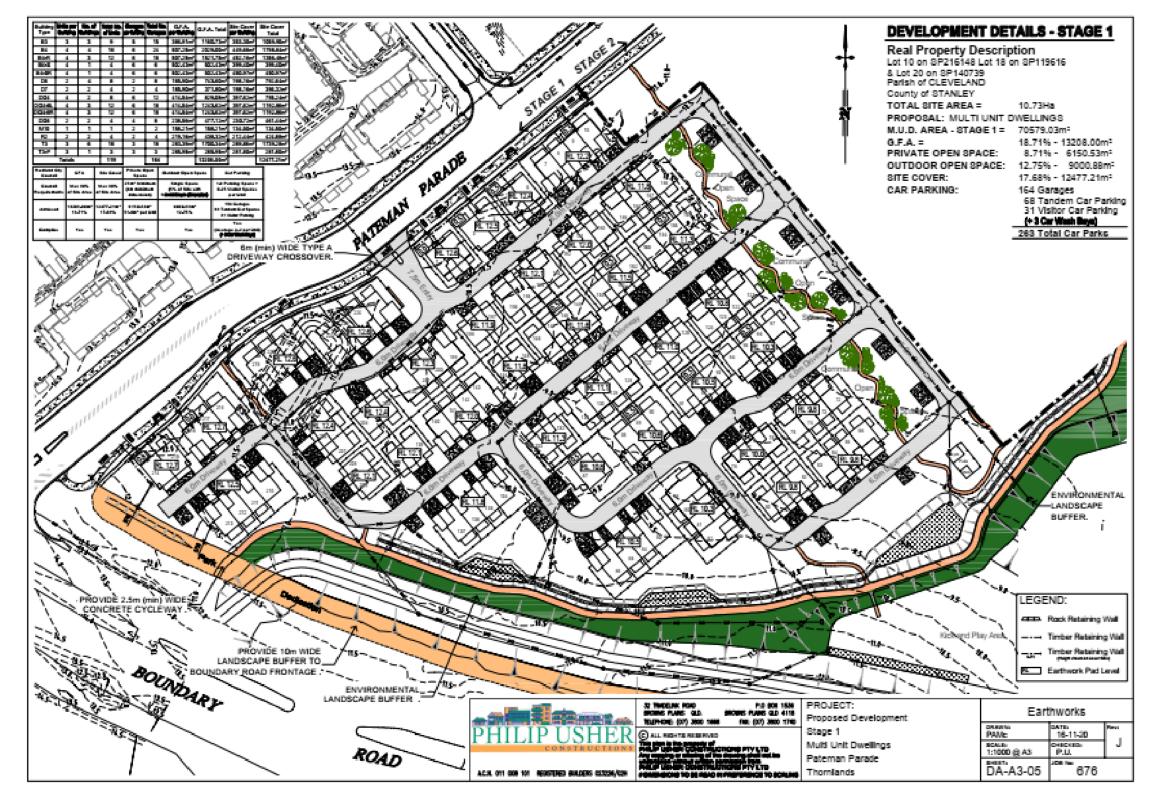


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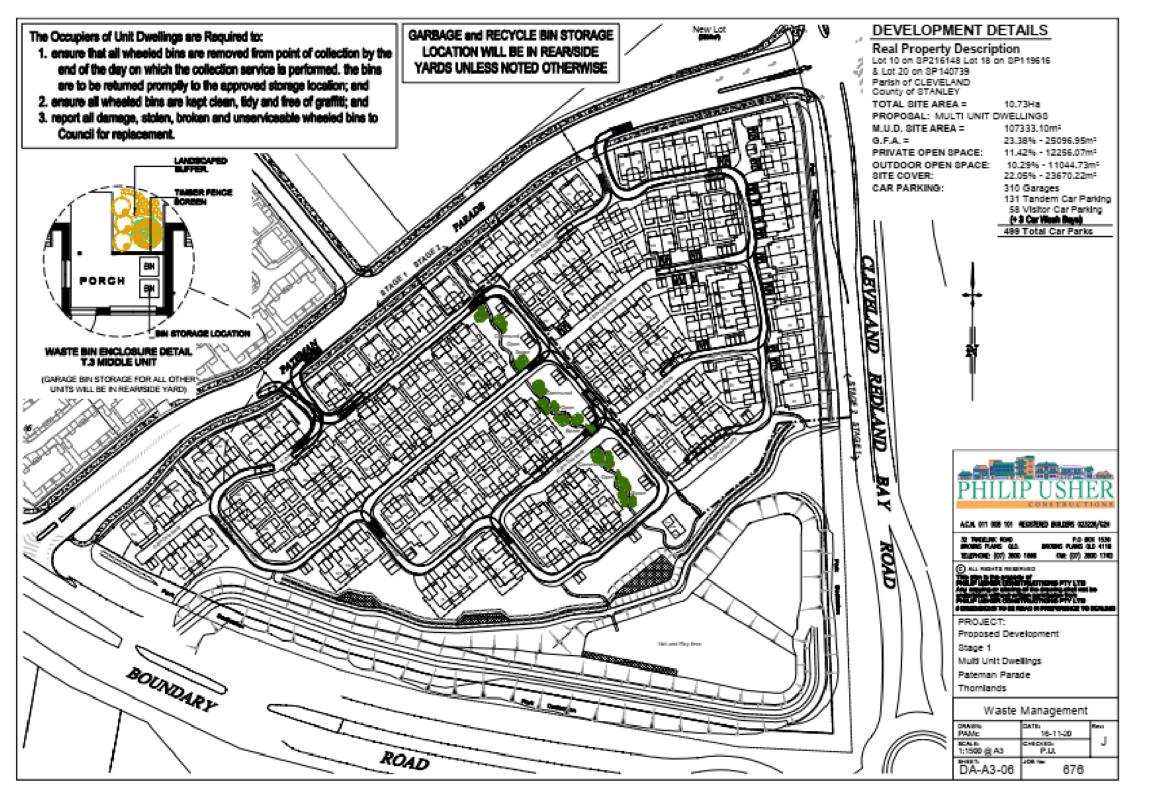
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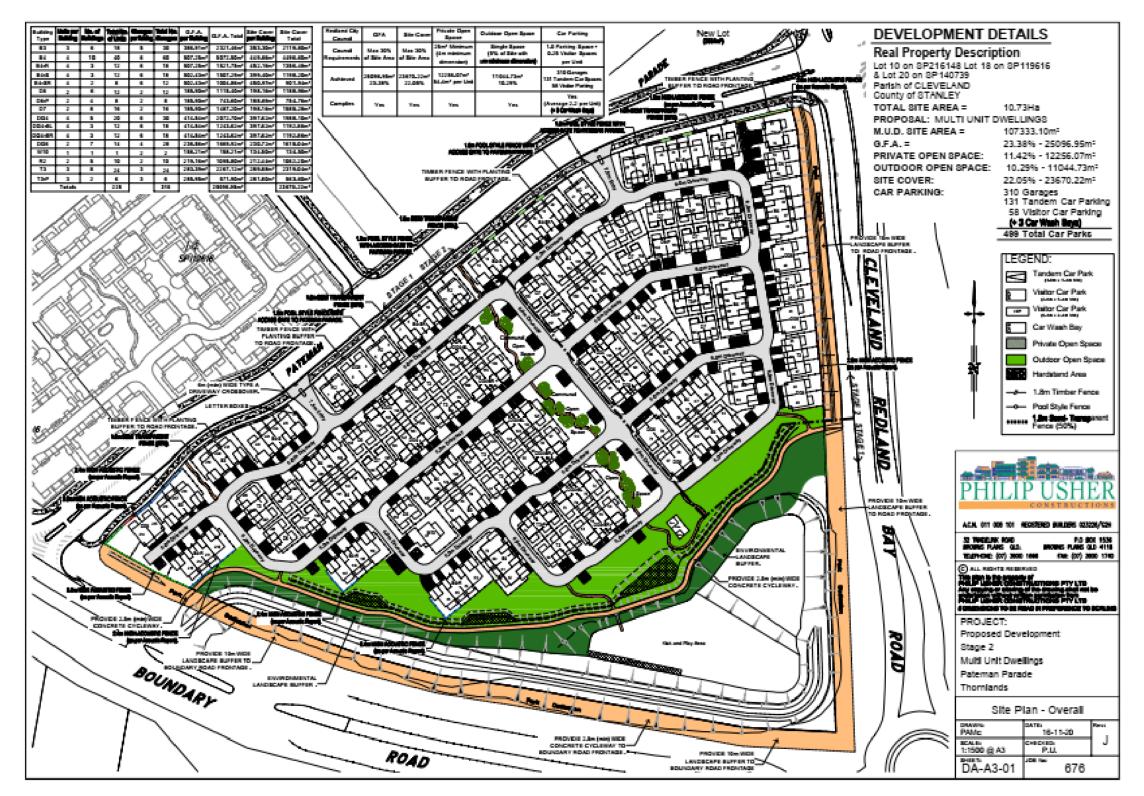
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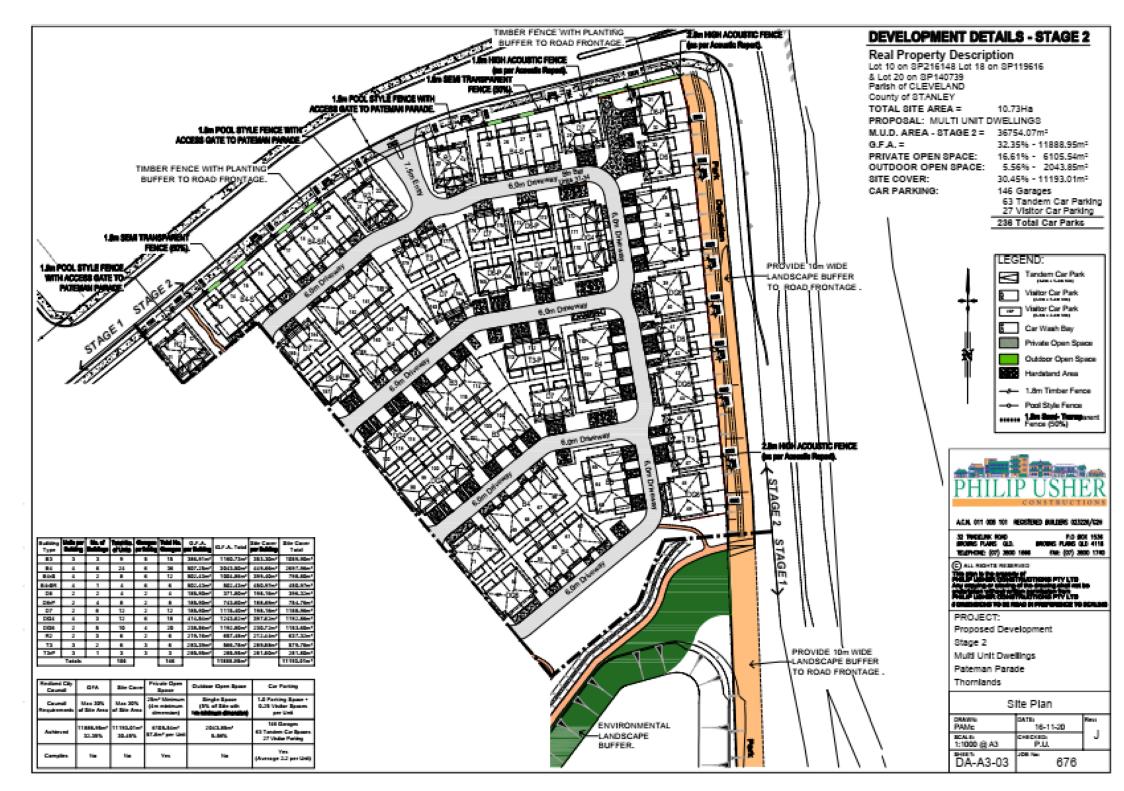
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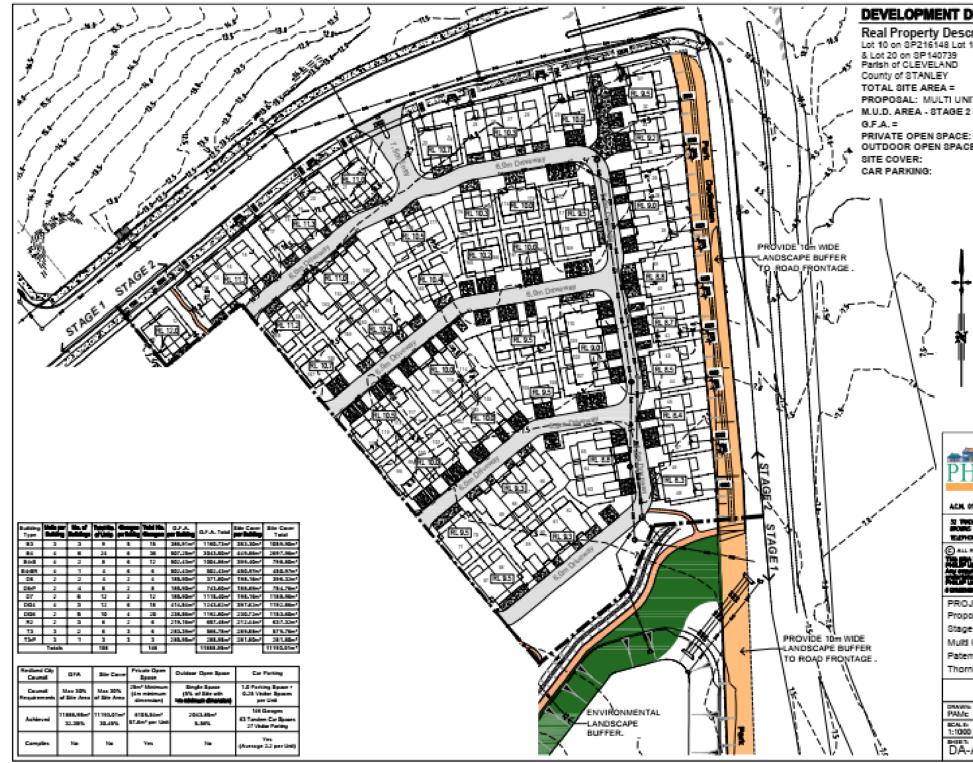


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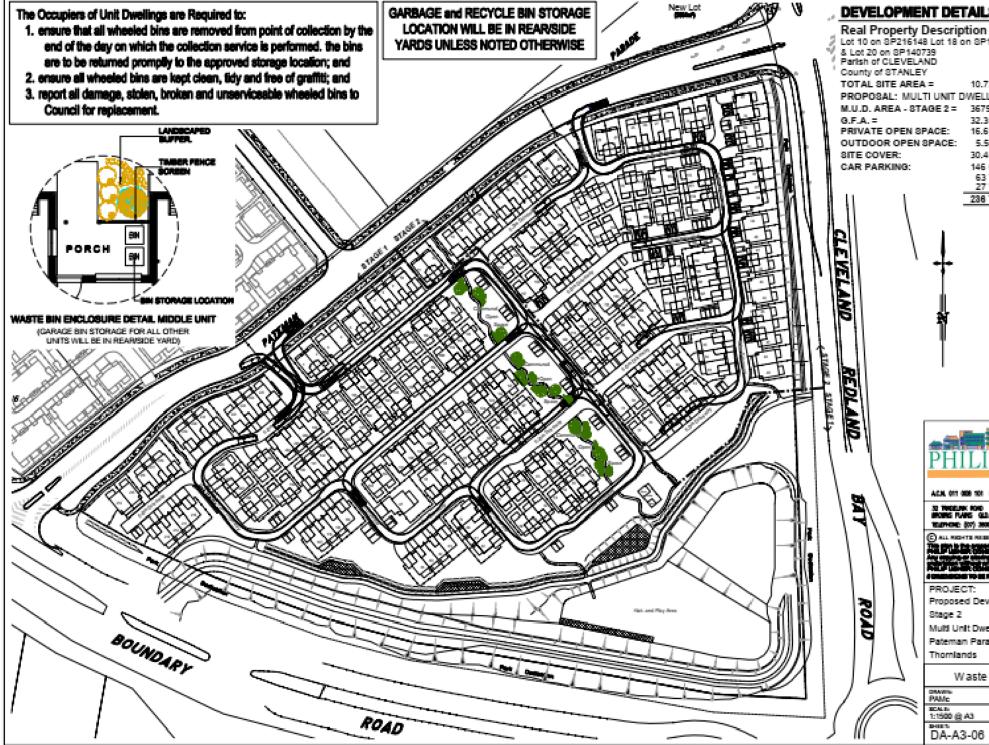
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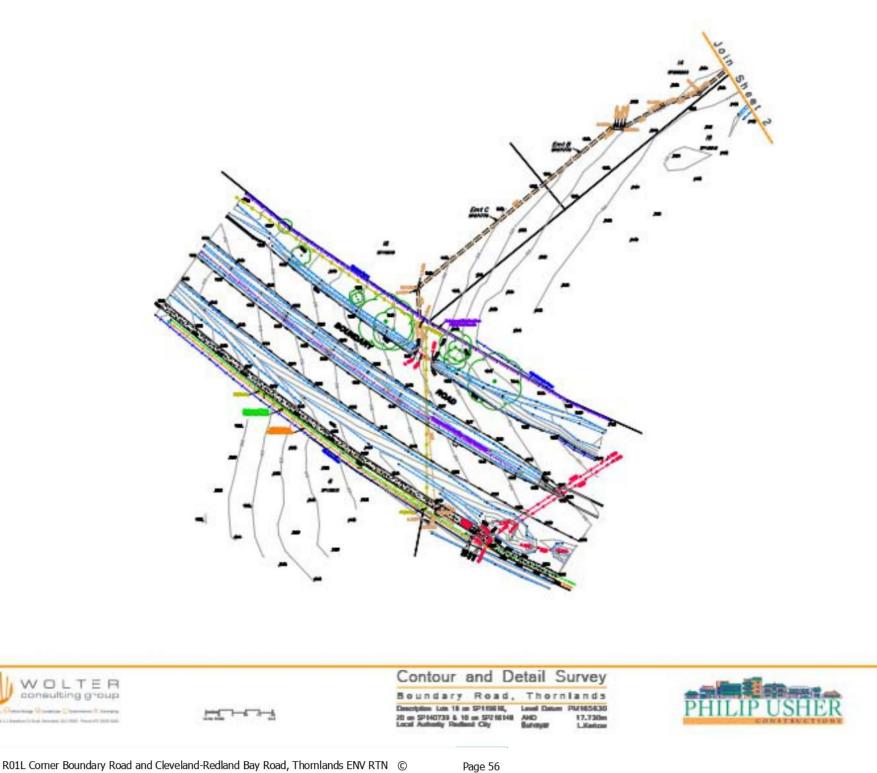
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11.3 Survey data



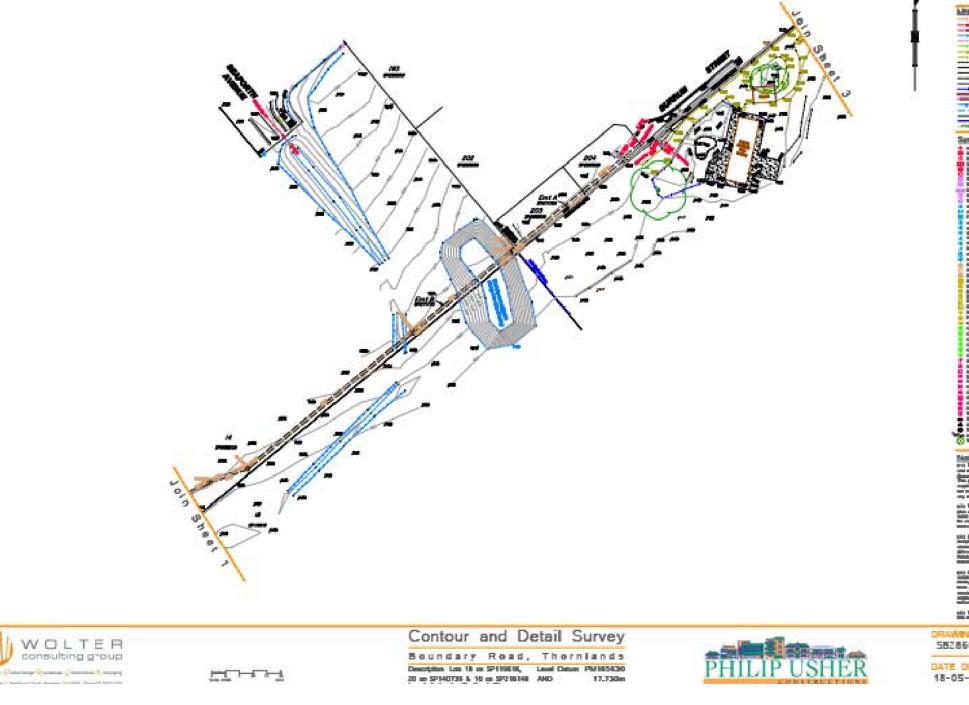
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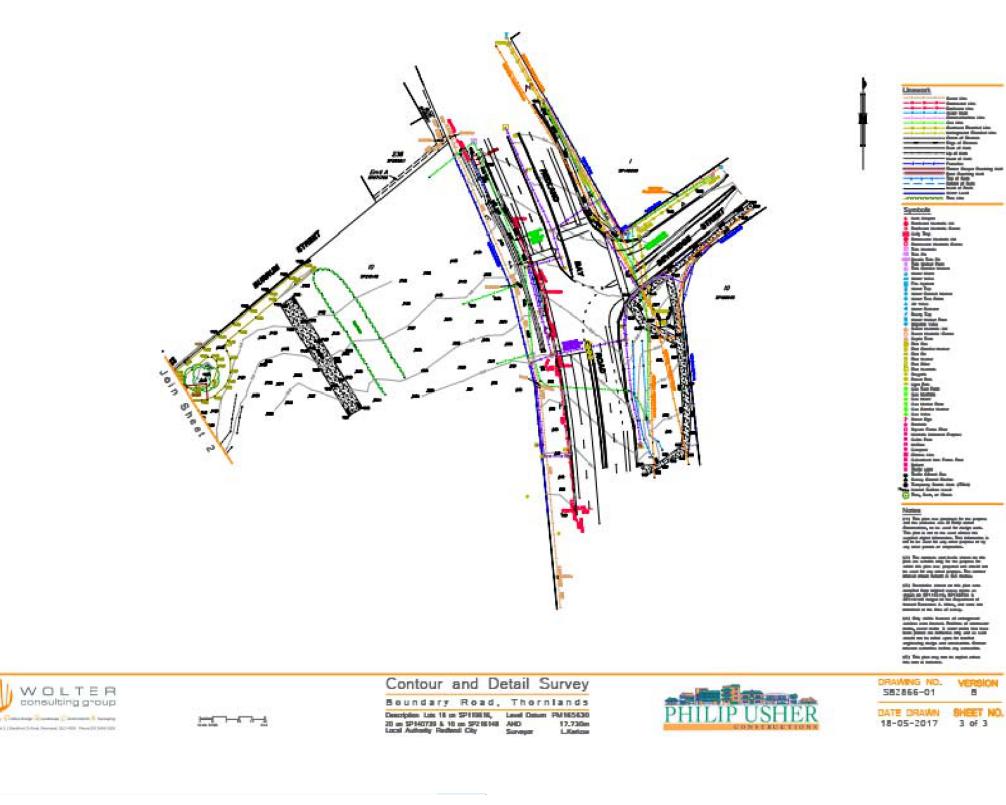
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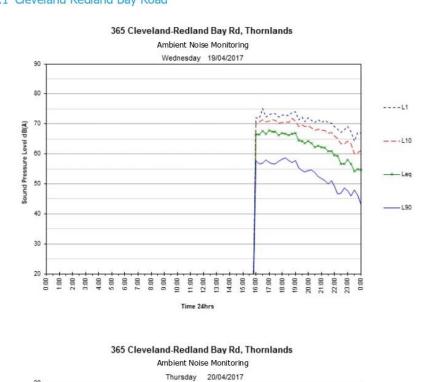
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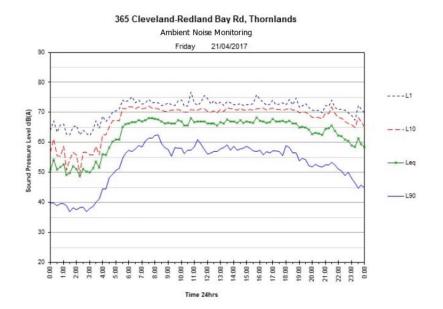
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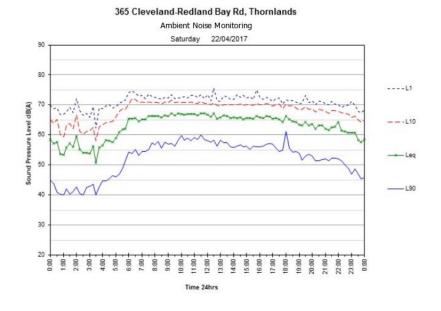
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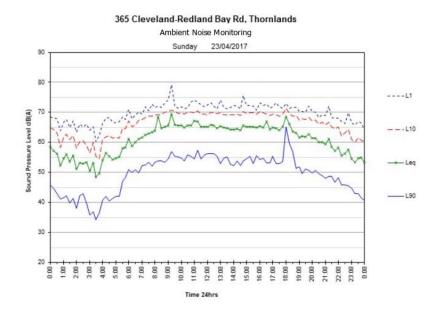


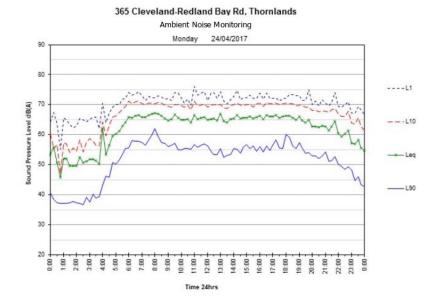


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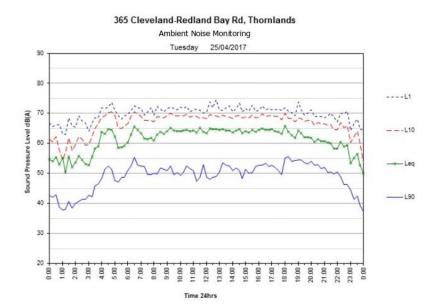


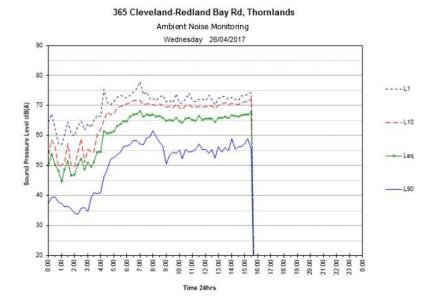




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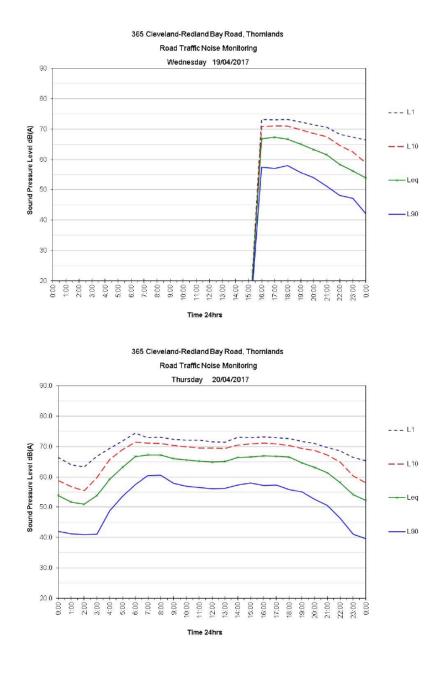




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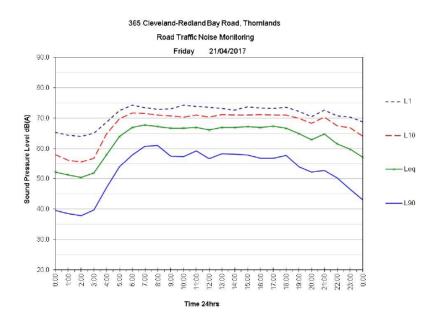


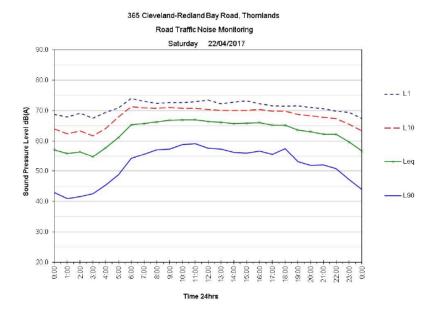




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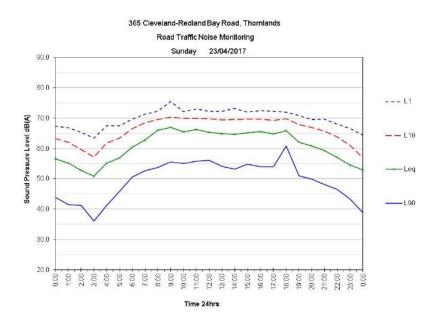






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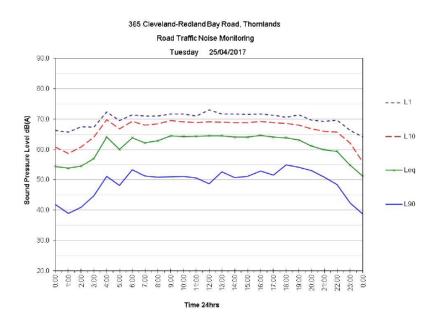




365 Cleveland-Redland Bay Road, Thornlands Road Traffic Noise Monitoring Monday 24/04/2017 90.0 80.0 --- L1 Sound Pressure Level dB(A) 70.0 - L10 60.0 Leq 50.0 -L90 40.0 30.0 20.0 0:00 5:00 6:00-23:00 8:00 9:00 13:00 15:00 16:00 17:00 18:00 19:00 20:00 0:00 1:00 12:00 14:00 21:00 22:00 Time 24hrs

2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thornlands ENV RTN © Acoustic Works 2020



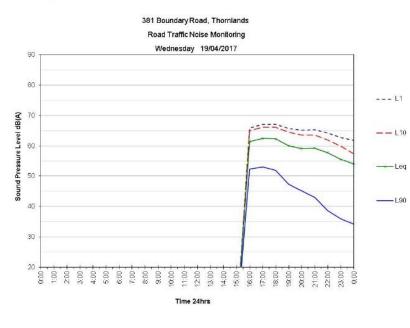


Road Traffic Noise Monitoring Wednesday 26/04/2017 90.0 80.0 ---L1 Sound Pressure Level dB(A) 70.0 — L10 60.0 Leq 50.0 -L90 40.0 30.0 20.0 17:00-18:00-0:00 1:00-2:00-3:00-5:00 6:00 -7:00 13:00 -15:00 -19:00 20:00 23:00 4:00 8:00 11:00 12:00 14:00 16:00 21:00 9:00 10:00 22:00 Time 24hrs

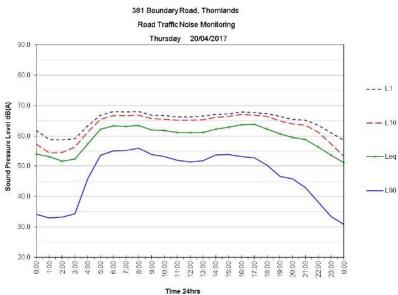
365 Cleveland-Redland Bay Road, Thornlands

2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thomlands ENV RTN © Acoustic Works 2020





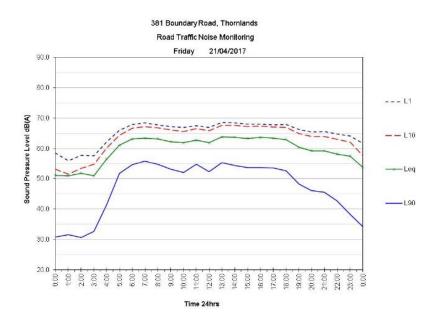
11.4.2 Boundary Road

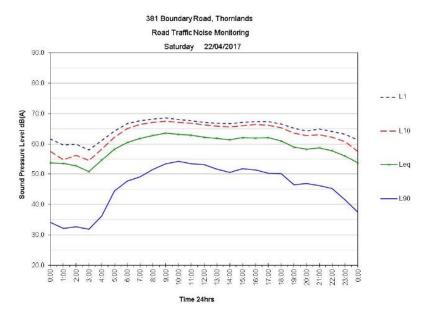


2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thornlands ENV RTN © Acoustic Works 2020





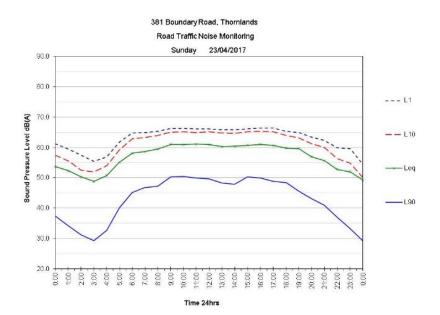


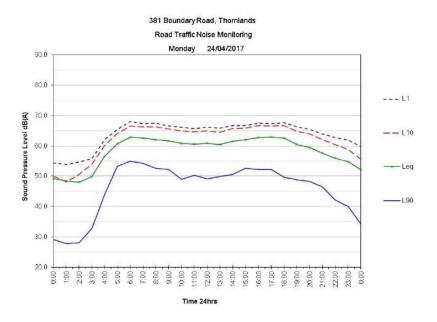


2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thornlands ENV RTN © Acoustic Works 2020





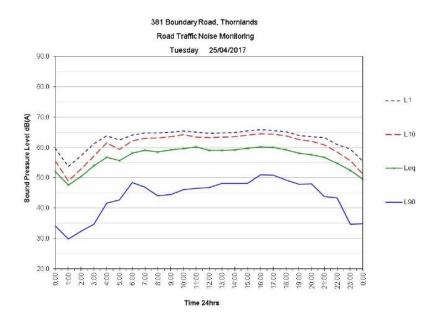




2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thornlands ENV RTN © Acoustic Works 2020







381 Boundary Road, Thornlands Road Traffic Noise Monitoring Wednesday 26/04/2017 90.0 0.08 --- L1 Sound Pressure Level dB(A) 70.0 — L10 60.0 Leq 50.0 -L90 40.0 30.0 20.0 17:00 18:00 19:00 20:00-0:00 200-3300-4:00-5:00 6:00-8:00 15:00 16:00-23:00-9:00 1:00 12:00 13:00 14:00 21:00 10:00 Time 24hrs

2017122 R01L Corner Boundary Road and Cleveland-Redland Bay Road, Thomlands ENV RTN © Acoustic Works 2020

DEVELOPMENT APPLICATION

Multi Unit Development (Stage 1 - 119 Units) Pateman Parade, Thornlands Lot 10 on SP216148, Lot 18 on SP119616 and Lot 20 on SP140739 Application Reference: **MCU17/0057**

| | | | | | | Pateman Parade Thornlands | | | |
|-------|--|---|-------------------------------|-------------------------|-----------------------|---------------------------------|---------------------------------|--|--|
| PI | HILIP USHER | RESERVED | | | | Stage 1 Multi Unit Dwellings | | | |
| | | 32 TRADELINK RO BROWNS PLAINS TELEPHONE: (07) | QLD. | BROWNS PLA | Proposed Development | | | | |
| 19976 | | | 5.000 | - | P.0 BOX | | PROJECT: | | |
| A | Original DA Issue Amendment | | 28-08-17 Date | DA-A3-01 Drawing No. | M Issue | Site Plan - Overall Description | | | |
| B | Wetland, New Lot and Tables Areas Added | d. | 18-12-17 | | M | Open Space - Overall | | | |
| С | Units Addressing Beveridge Road Amende | | 16-03-18 | 2 | M | Site Plan | | | |
| D | | | | DA-A3-04 | M | Open Space | | | |
| E | | | | | M | Earthworks | | | |
| F | Road Truncations Updated. Layout Amend | ded to Suit. | 19-07-18 10-07-18 | | м | Waste Management | | | |
| G | Various Amendments. | | | DA-A3-07 | Ν | Streetscape | | | |
| | Unit 29-30 Design Change to Suit Acoustic Fence. | | | DA-A3-08 | М | B3 G | B3 Ground Floor | | |
| Н | | | | DA-A3-09 | М | B3 First Floor | | | |
| Ι | RFI Amendments. | | | DA-A3-10 | м | B3 Elevations Sht 1 | | | |
| J | | | | DA-A3-11 | м | B3 Elevations Sht 2 | | | |
| к | | | | DA-A3-12 | м | B3-R Ground Floor | | | |
| L | RFI Amendments. | | | DA-A3-13 | N | B3-R First Floor | | | |
| М | RFI Amendments. | | 05-07-21 | DA-A3-14 | N | B3-R Elevations Sht 1 | | | |
| | Type to Rear Elevation. Streetscape Updat | | | DA-A3-15 | N | B3-R Elevations Sht 2 | | | |
| N | Building Type B3-R Amended to Include A | ditional Material | 03-08-21 | DA-A3-16 | M | B4 Ground Floor | | | |
| | | | 1 | DA-A3-18 | M | | irst Floor | | |
| | | | | DA-A3-19 DA-A3-18 | M | | levations Sht 2 | | |
| | | | | DA-A3-20 DA-A3-19 | M | | Ground Floor levations Sht 2 | | |
| U | 17/0057 | | | DA-A3-21 | M | | First Floor | | |
| | 47/0057 | | DA-A3-22 | M | B4-R Elevations Sht 1 | | | | |
| | | | | | | B4-R Elevations Sht 2 | | | |
|)7: | 39 | | DA-A3-24 M B4-SR Ground Floor | | | | | | |
| | | | | | | | | | |

DA-A3-30

DA-A3-29

DA-A3-28

DA-A3-27

DA-A3-26

DA-A3-25 M

м

М

М

М

M

D6 Elevations

D6 First Floor

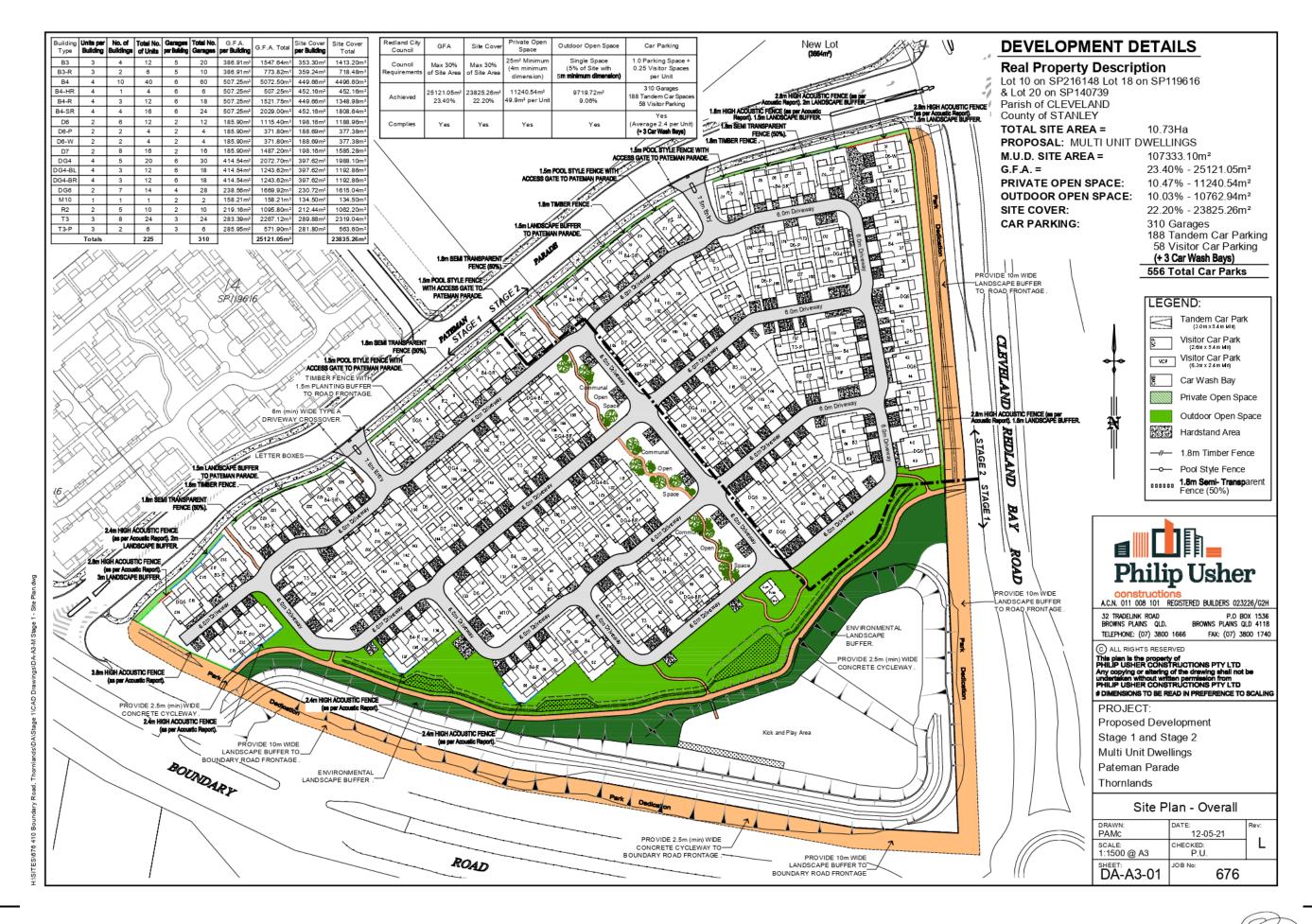
D6 Ground Floor

B4-SR First Floor

B4-SR Elevations Sht 2

B4-SR Elevations Sht 1

| 1 | | | | | | |
|-------------|-------------|----------------------------|------------------------------|------|--|--|
| DA-A3-59 | М | Gym-Patio Floor | Plans and Elevati | ions | | |
| DA-A3-58 | Μ | T3-P Elevations | | | | |
| DA-A3-57 | Μ | T3-P First Floor | | | | |
| DA-A3-56 | М | T3-P Ground Floo | pr | | | |
| DA-A3-55 | М | T3 Elevations | | | | |
| DA-A3-54 | Μ | T3 First Floor | | | | |
| DA-A3-53 | Μ | T3 Ground Floor | | | | |
| DA-A3-52 | Μ | R2 Elevations | | | | |
| DA-A3-51 | Μ | R2 Floor Plans | | | | |
| DA-A3-50 | Μ | M10 Elevations | | | | |
| DA-A3-49 | Μ | M10 Floor Plans | M10 Floor Plans | | | |
| DA-A3-48 | М | DG6 Elevations | DG6 Elevations | | | |
| DA-A3-47 | Μ | DG6 First Floor | | | | |
| DA-A3-46 | М | DG6 Ground Floo | DG6 Ground Floor | | | |
| DA-A3-45 | М | DG4-BR Elevations Sht 2 | | | | |
| DA-A3-44 | Μ | DG4-BR Elevatio | DG4-BR Elevations Sht 1 | | | |
| DA-A3-43 | Μ | DG4-BR First Flo | DG4-BR First Floor | | | |
| DA-A3-42 | М | DG4-BR Ground | DG4-BR Ground Floor | | | |
| DA-A3-41 | М | DG4-BL Elevation | DG4-BL Elevations Sht 2 | | | |
| DA-A3-40 | М | DG4-BL Elevations Sht 1 | | | | |
| DA-A3-39 | М | DG4-BL First Floo | DG4-BL First Floor | | | |
| DA-A3-38 | Μ | DG4-BL Ground I | DG4-BL Ground Floor | | | |
| DA-A3-37 | М | DG4 Elevations S | DG4 Elevations Sht 2 | | | |
| DA-A3-36 | Μ | DG4 Elevations S | DG4 Elevations Sht 1 | | | |
| DA-A3-35 | М | DG4 First Floor | DG4 First Floor | | | |
| DA-A3-34 | М | DG4 Ground Floo | DG4 Ground Floor | | | |
| DA-A3-33 | Μ | D7 Elevations | | | | |
| DA-A3-32 | Μ | D7 First Floor | | | | |
| DA-A3-31 | М | D7 Ground Floor | | | | |
| Drawing No. | Issue | Description | | | | |
| | Cover Sheet | | | | | |
| | l | DRAWN: | DATE: | Rev: | | |
| | | PAMC SCALE: As Noted | 03-08-21 CHECKED: P.U. | Ν | | |
| | ł | SHEET: | JOB No: | | | |
| | | DA-A3-00 | 676 | | | |



Item 14.1- Attachment 2

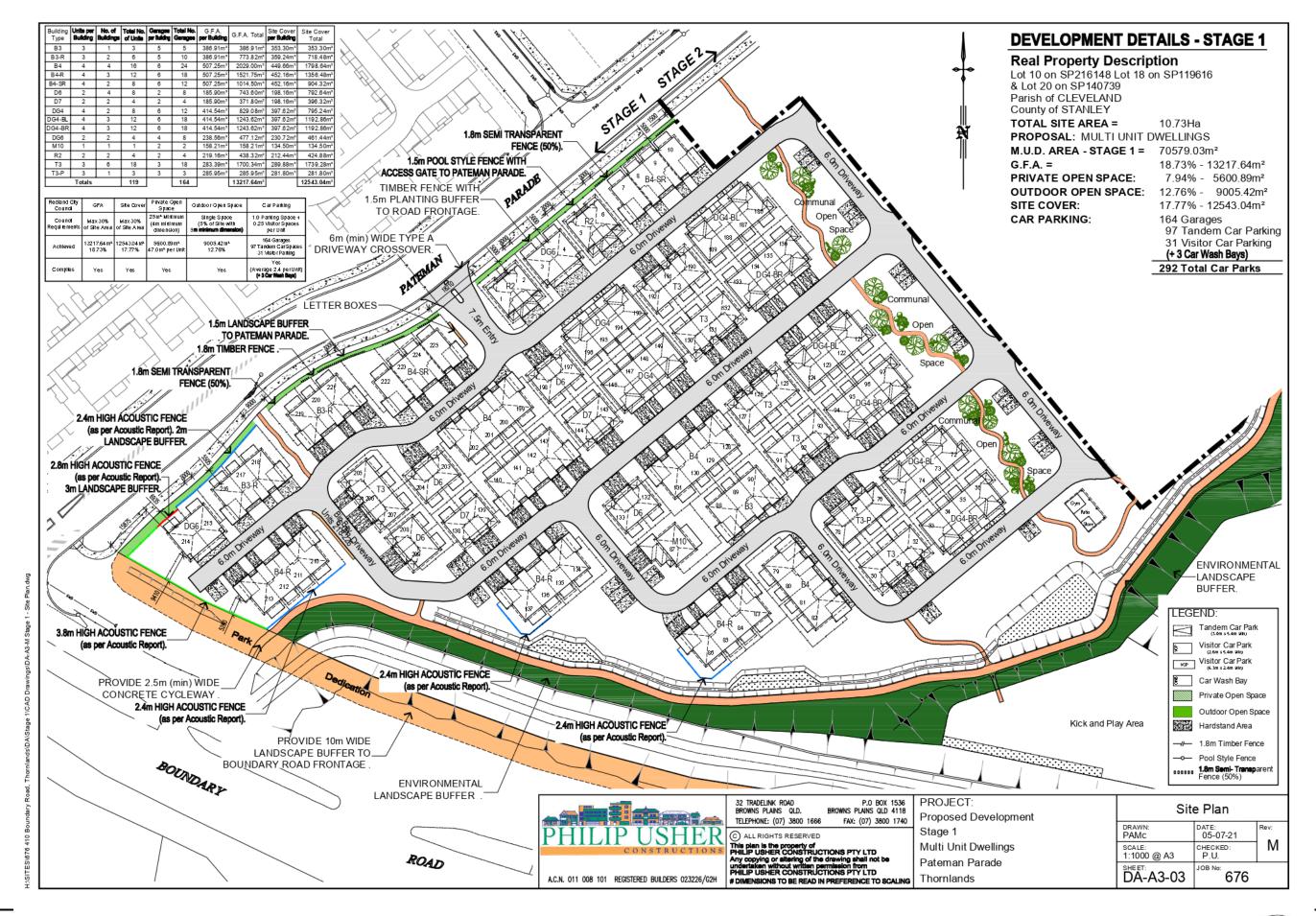
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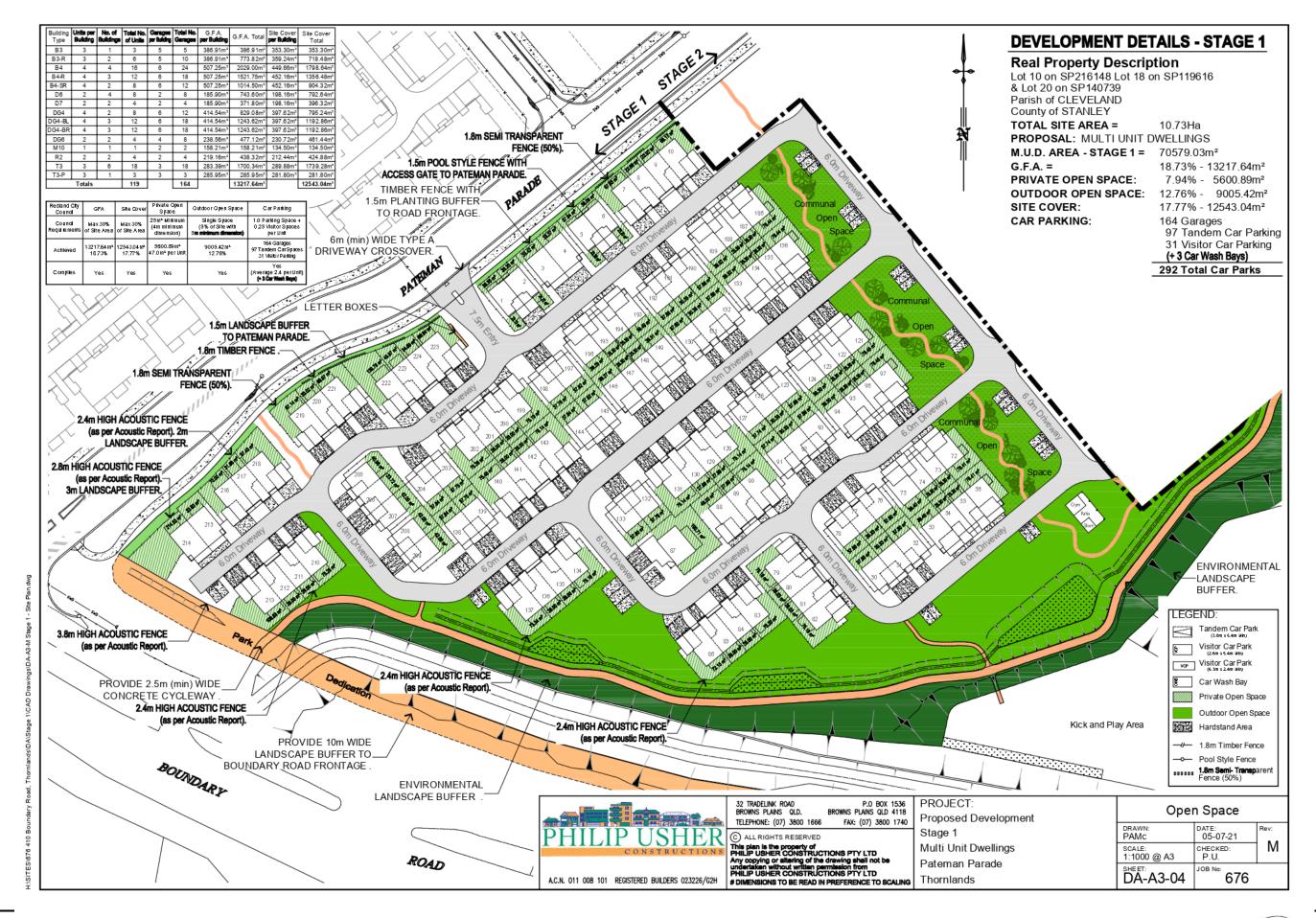


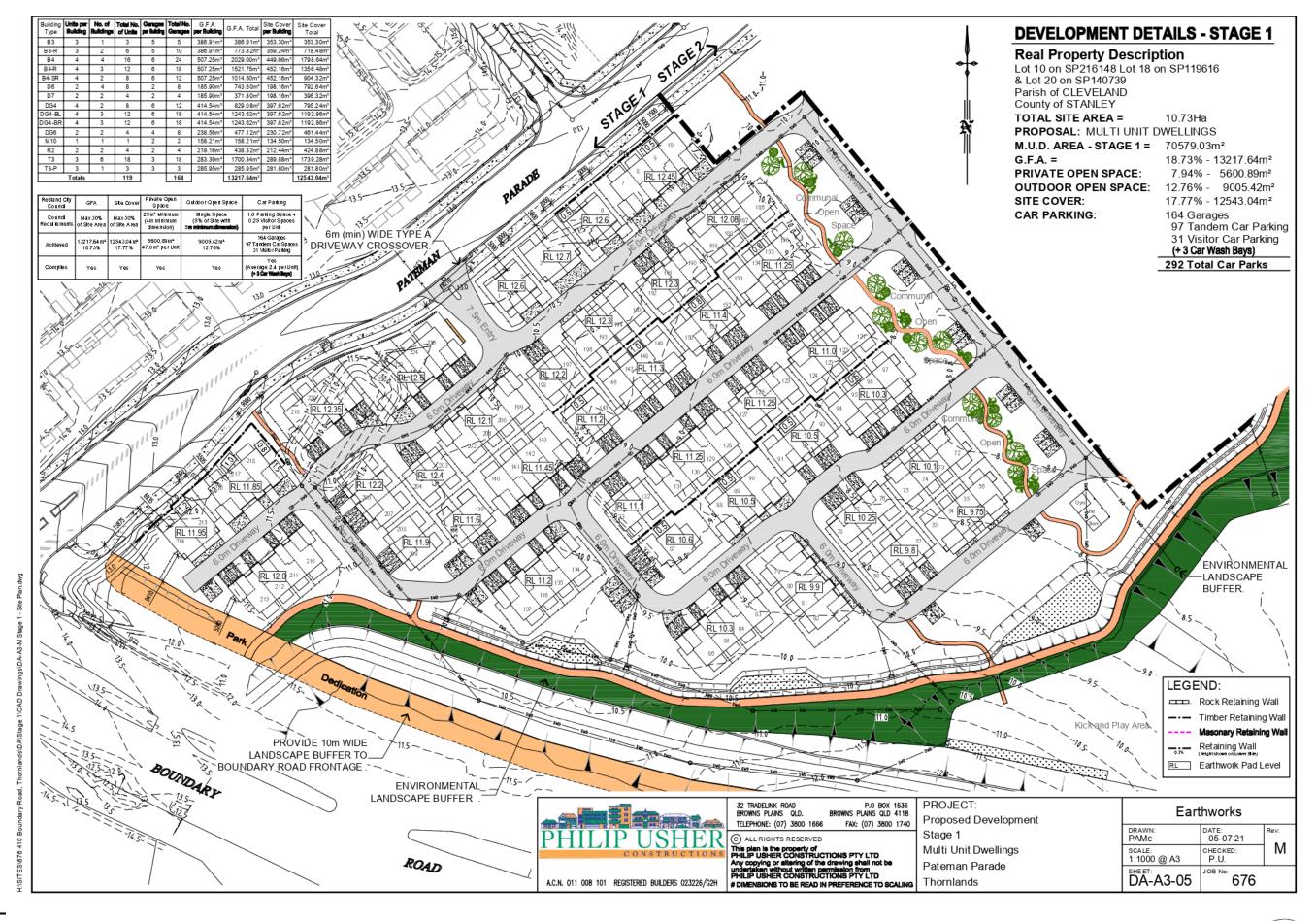


A.C.N. 011 008 101 REGISTERED BUILDERS 023226/G2H 32 TRADELINK ROAD P.O. BOX 1536 BROWNS PLAINS QLD. BROWNS PLAINS QLD 4118 TELEPHONE: (07) 3800 1666 FAX: (07) 3800 1740 This plan is the property of PHILIP USHER CONSTRUCTIONS PTY LTD Any copying or altering of the drawing shall not be undertaken without writigen permission from PHILIP USHER CONSTRUCTIONS PTY LTD # DIMENSIONS TO BE READ IN PREFERENCE TO SCALING

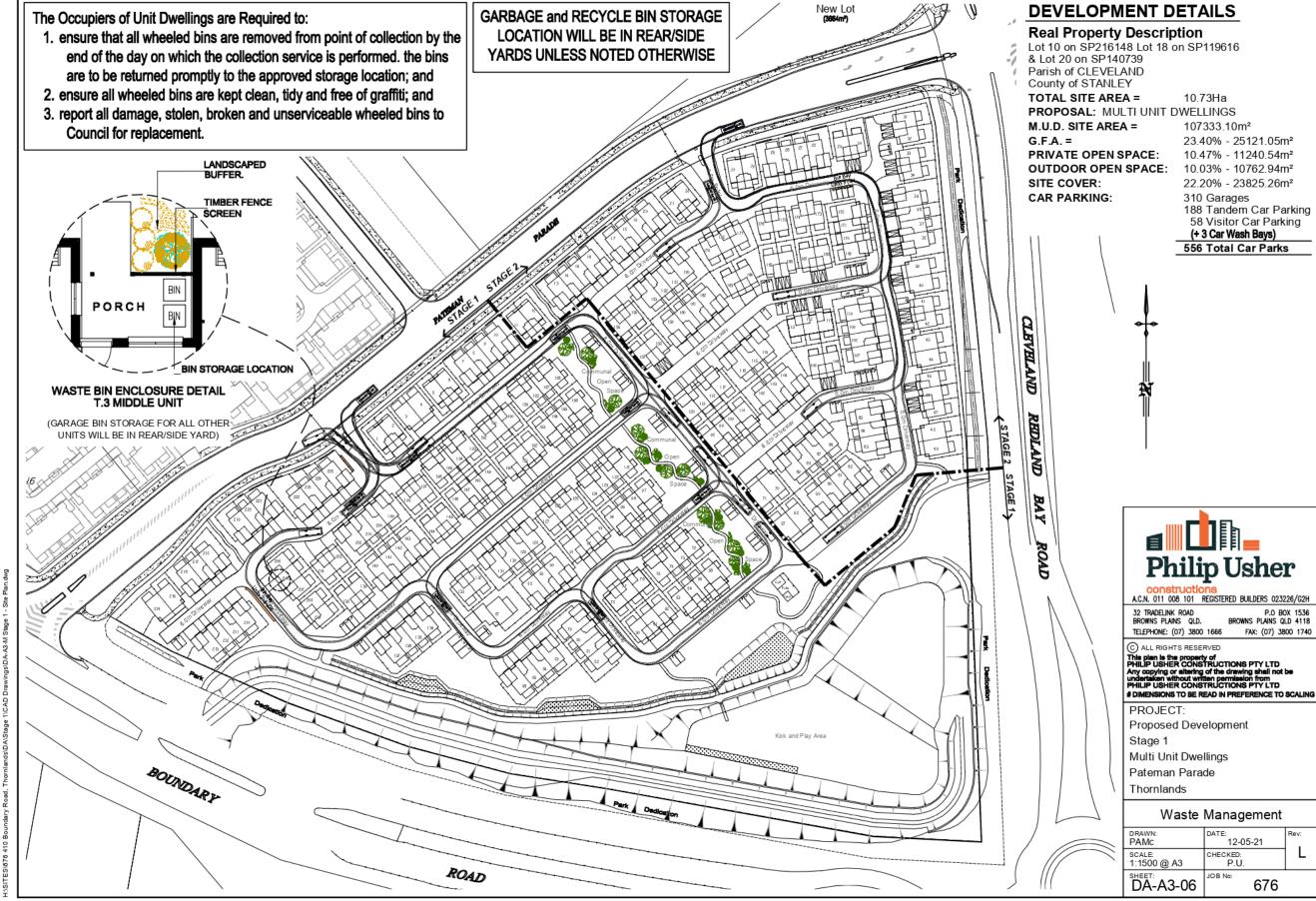
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|----------|-----------------------|-----------------------|------|
| \geq [| scale: 1:1500 @ A3 | CHECKED: P.U. | L |
| | DRAWN: PAMC | DATE: 12-05-21 | Rev: |

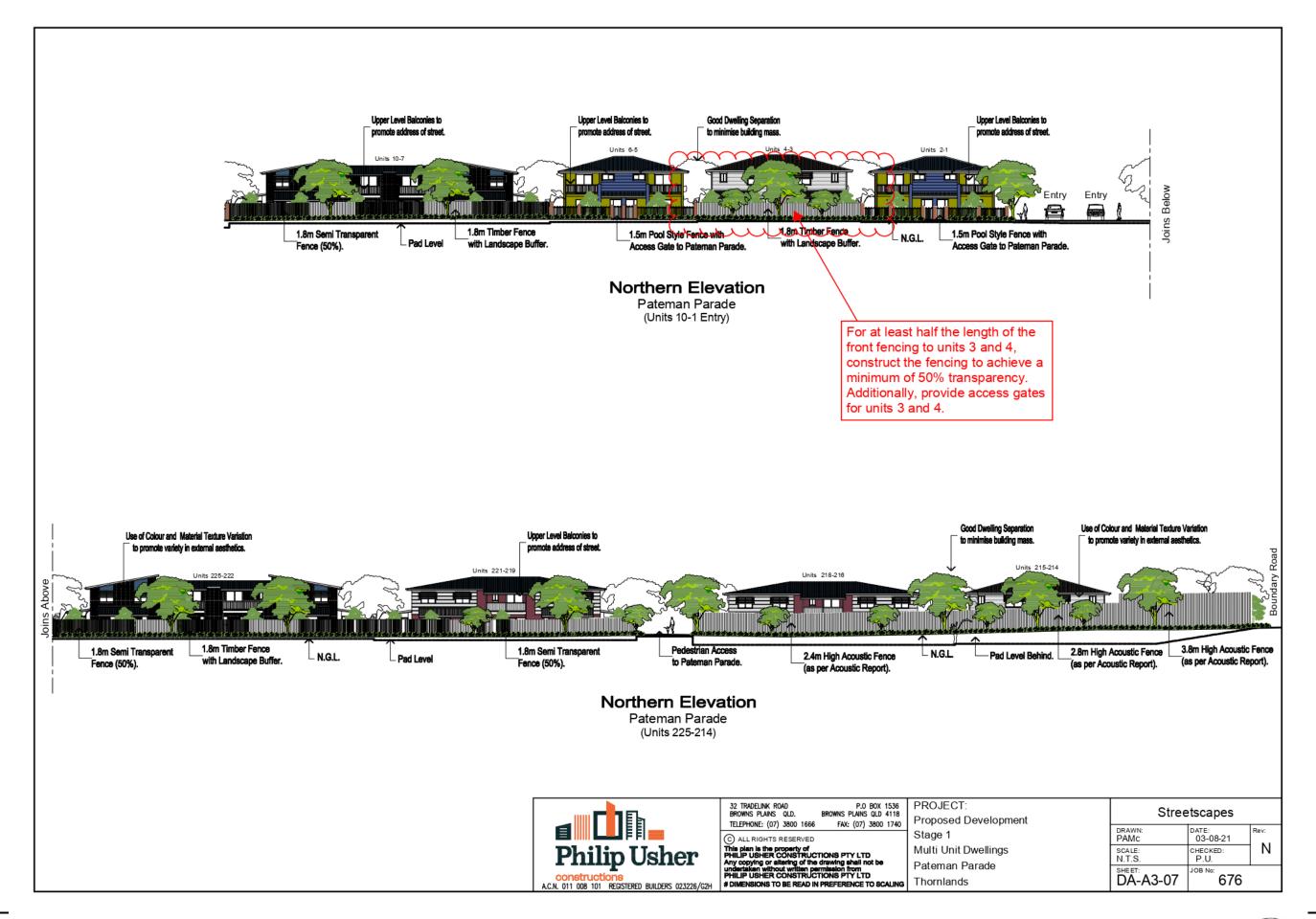


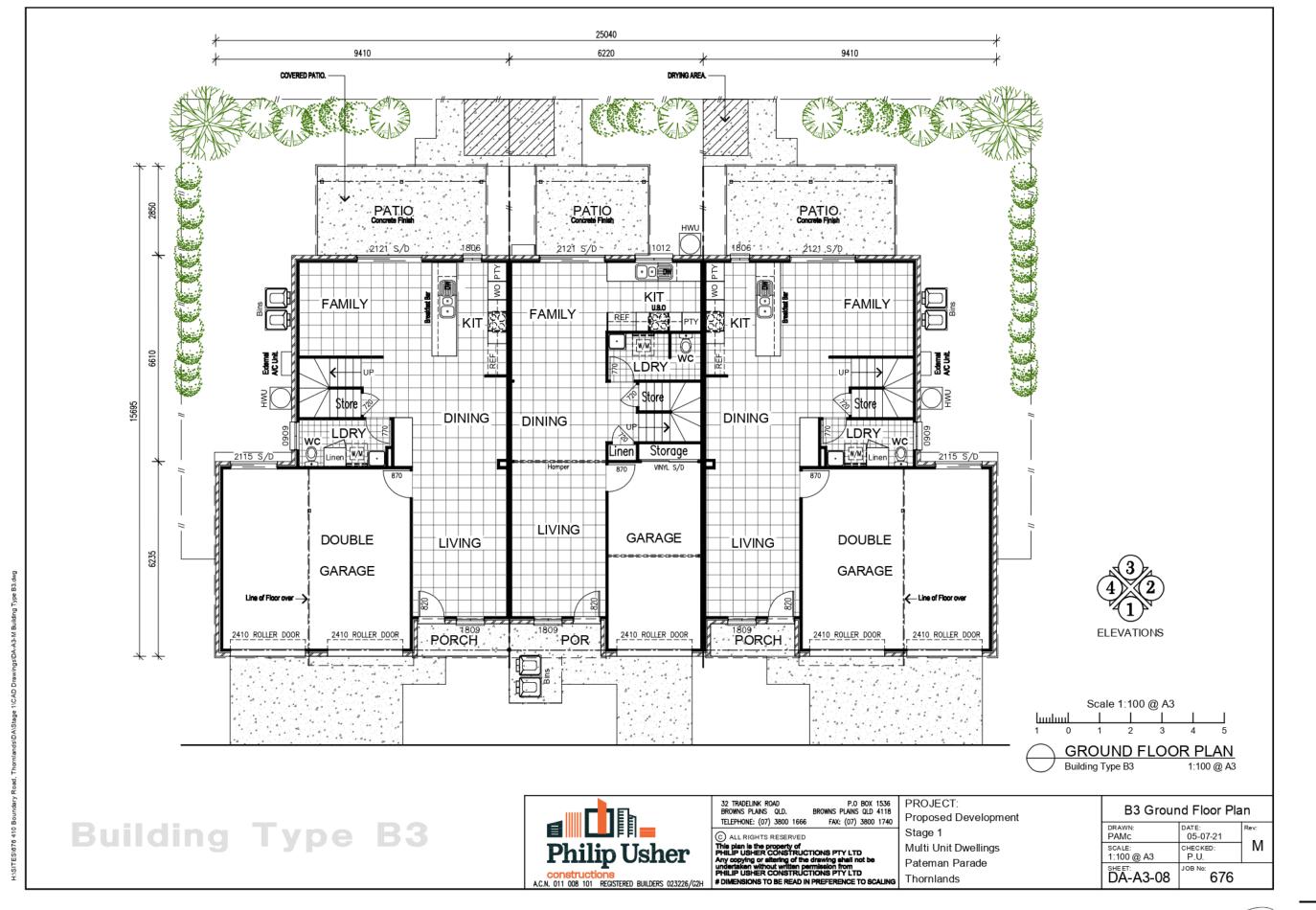




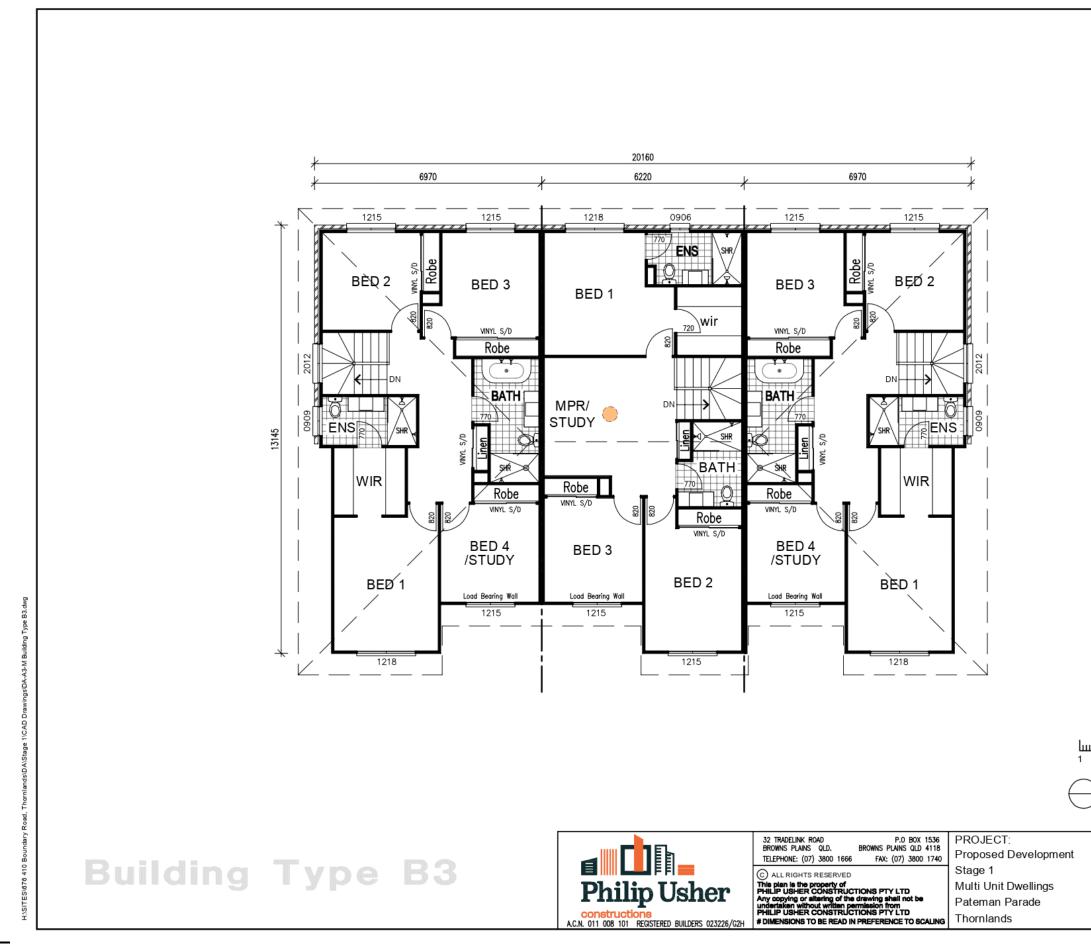
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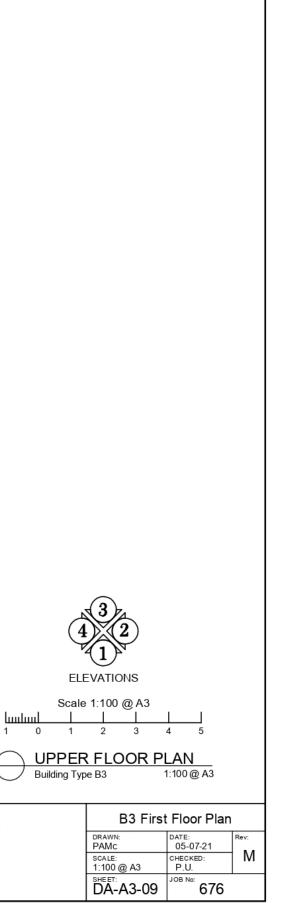


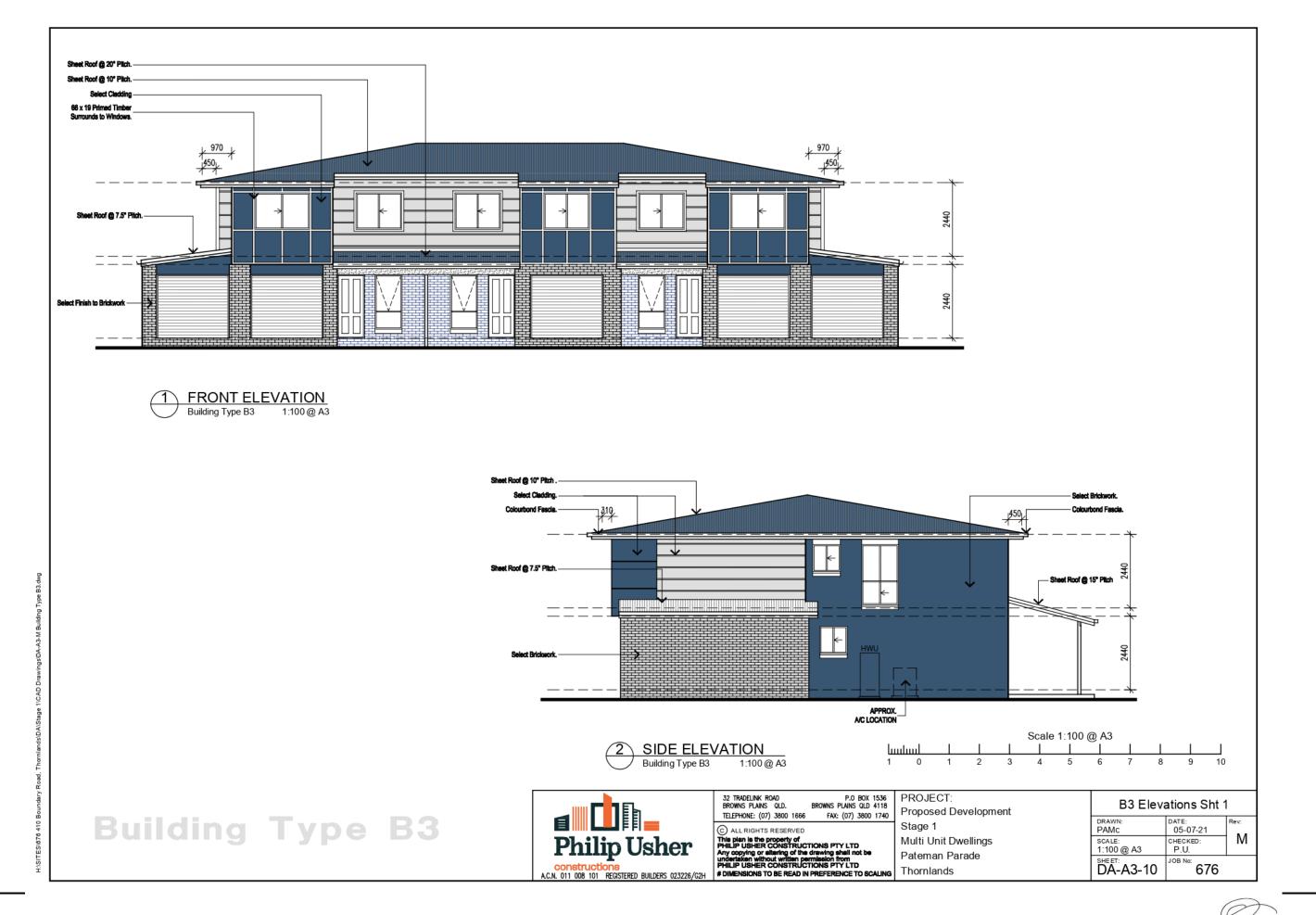




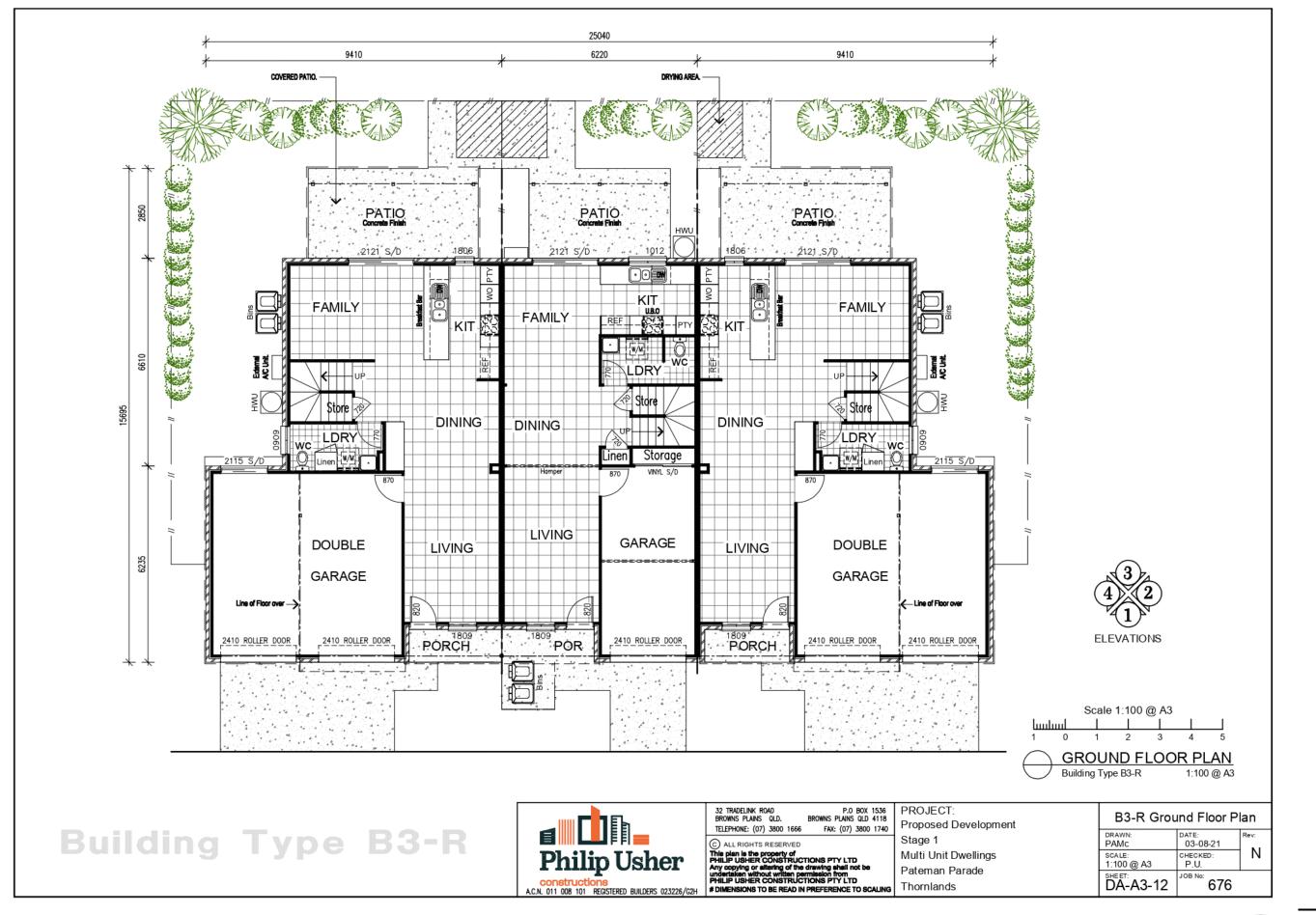
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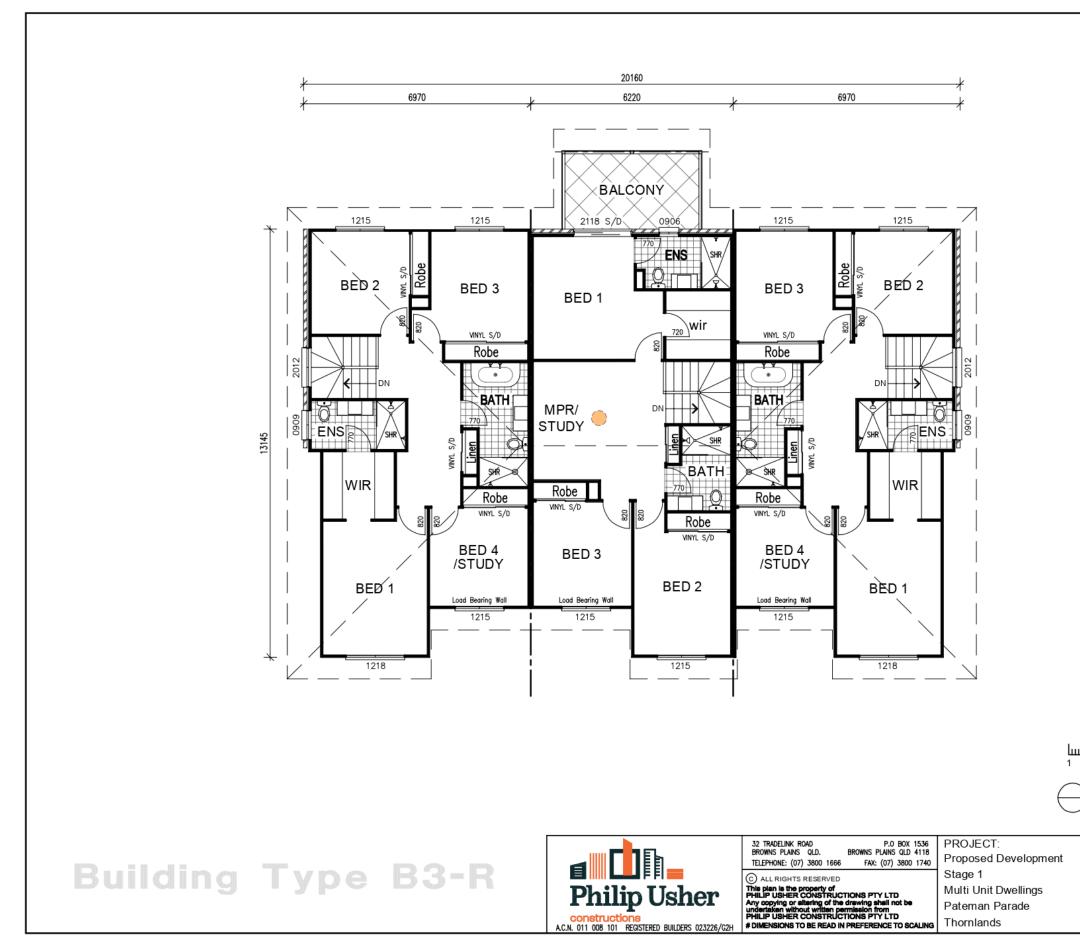


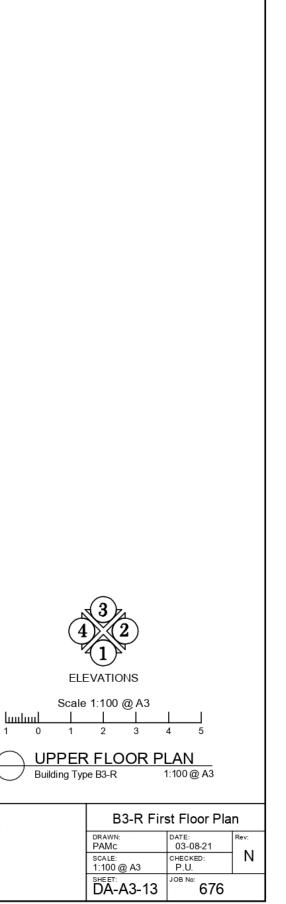


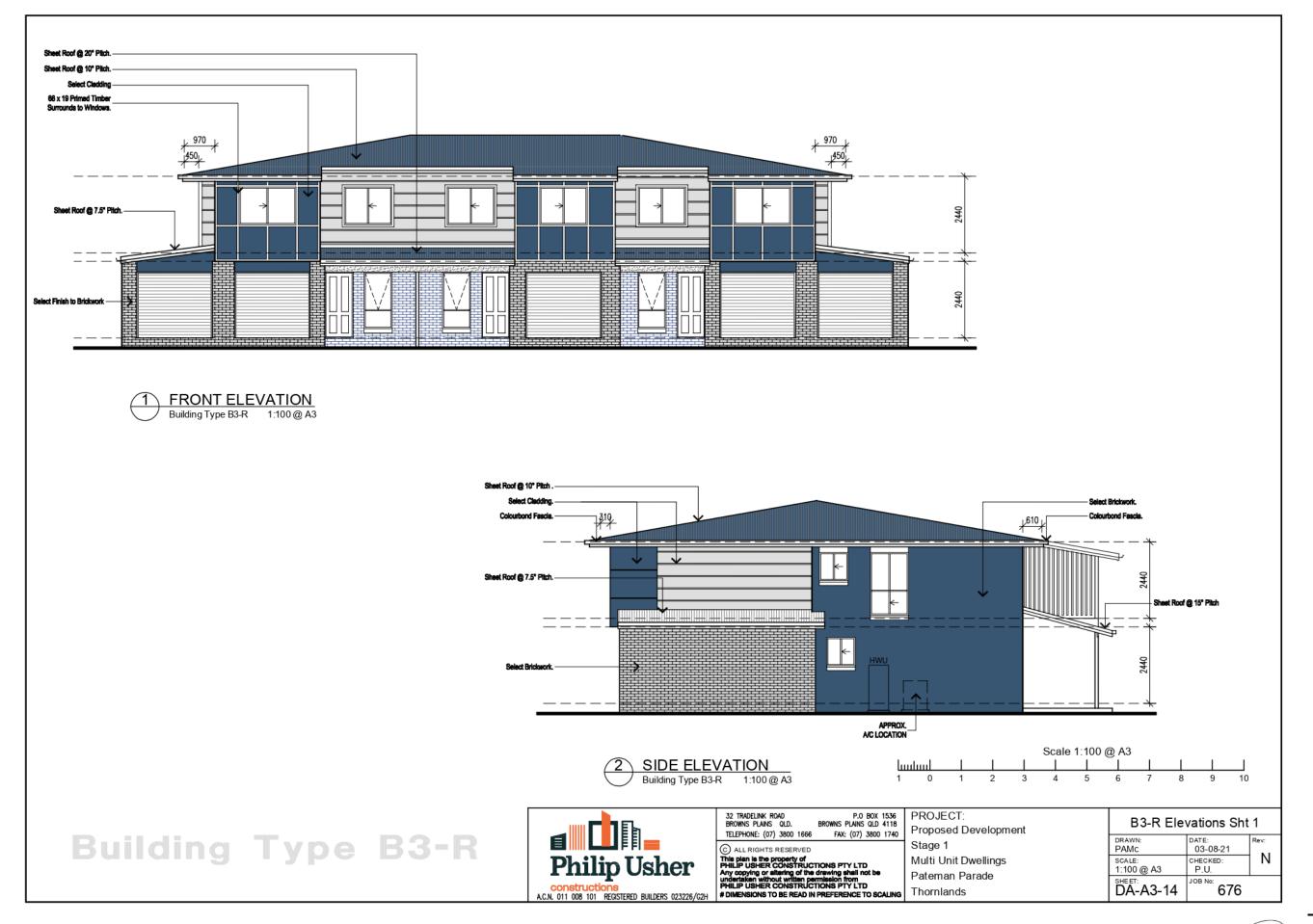




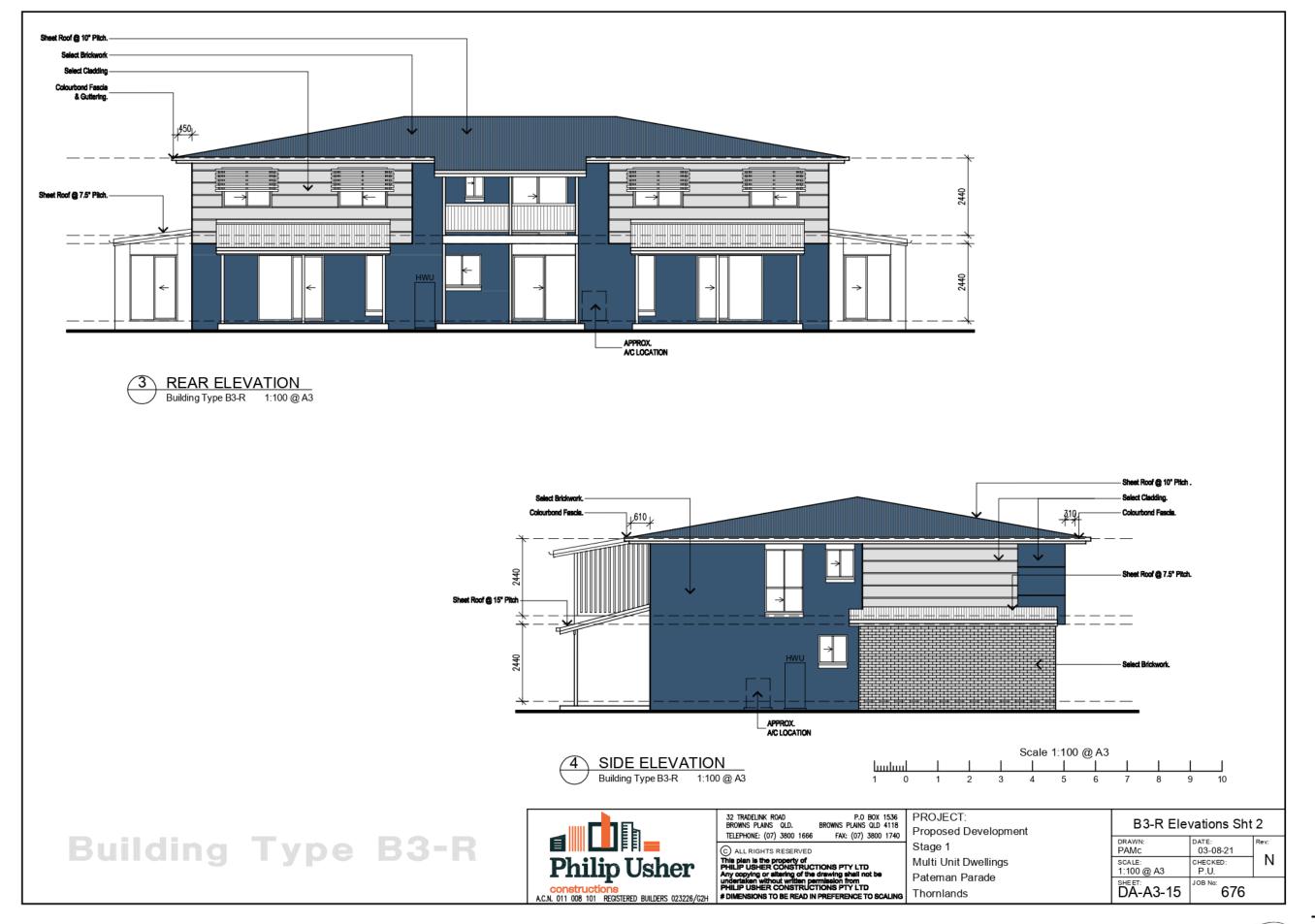


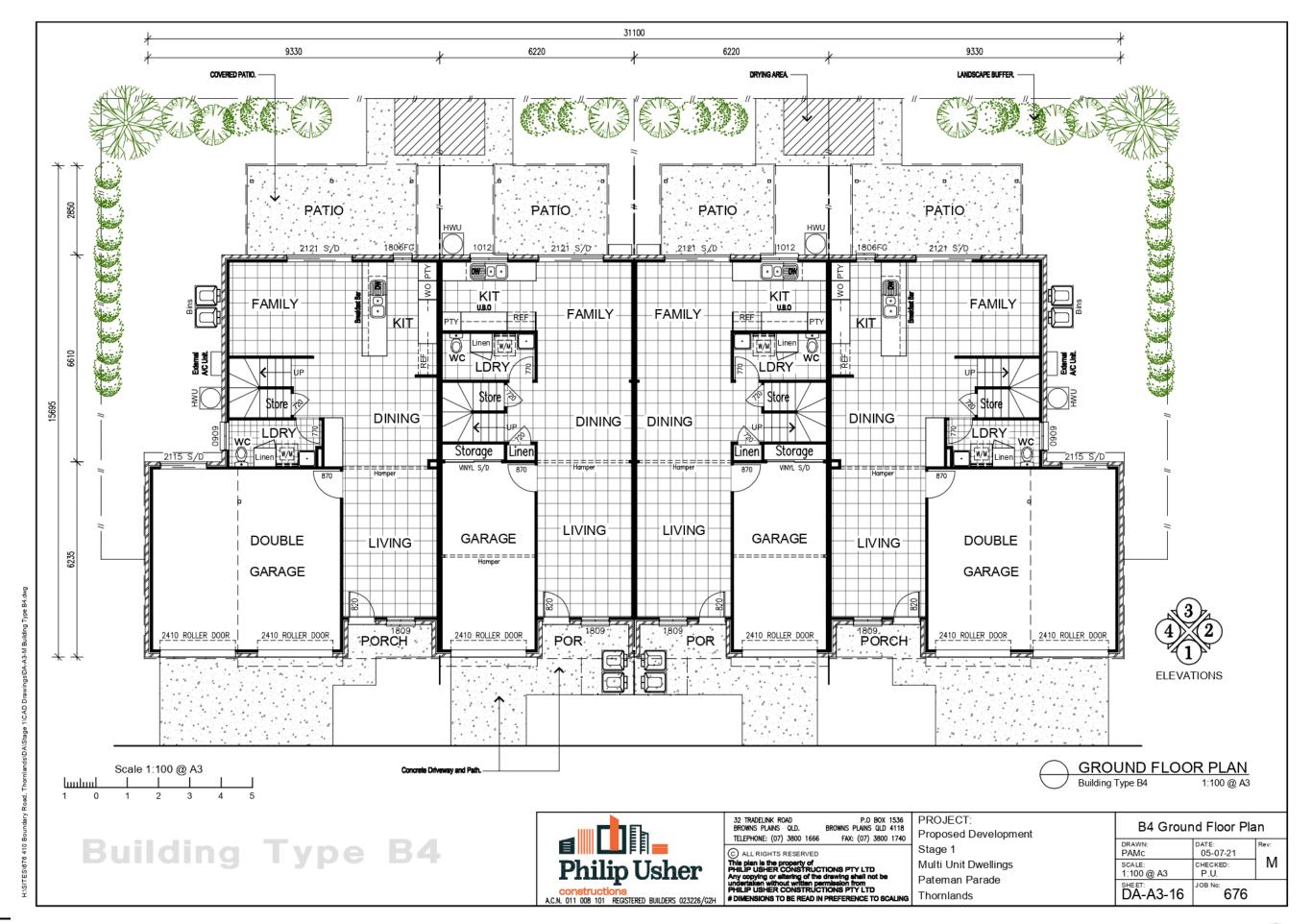


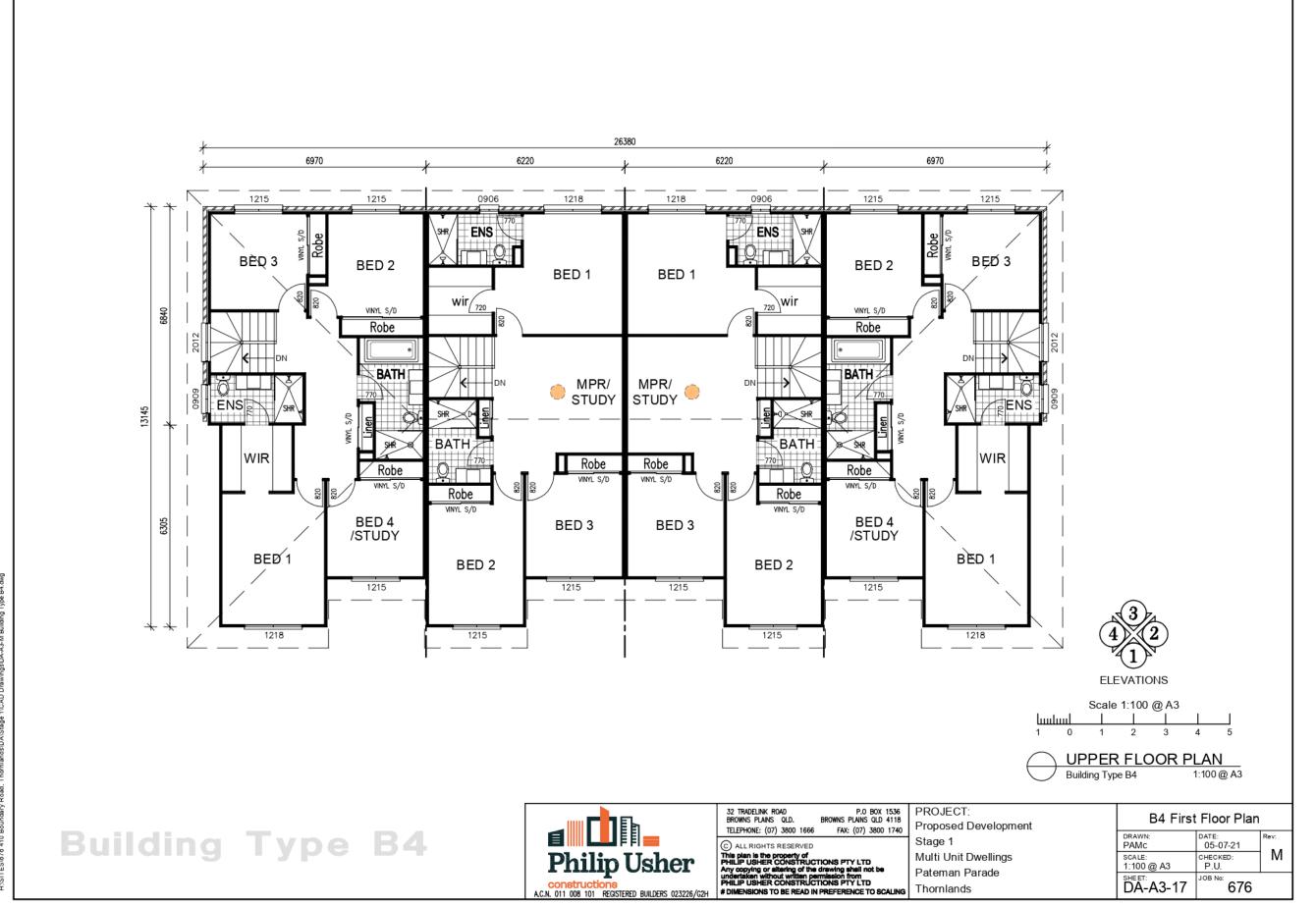


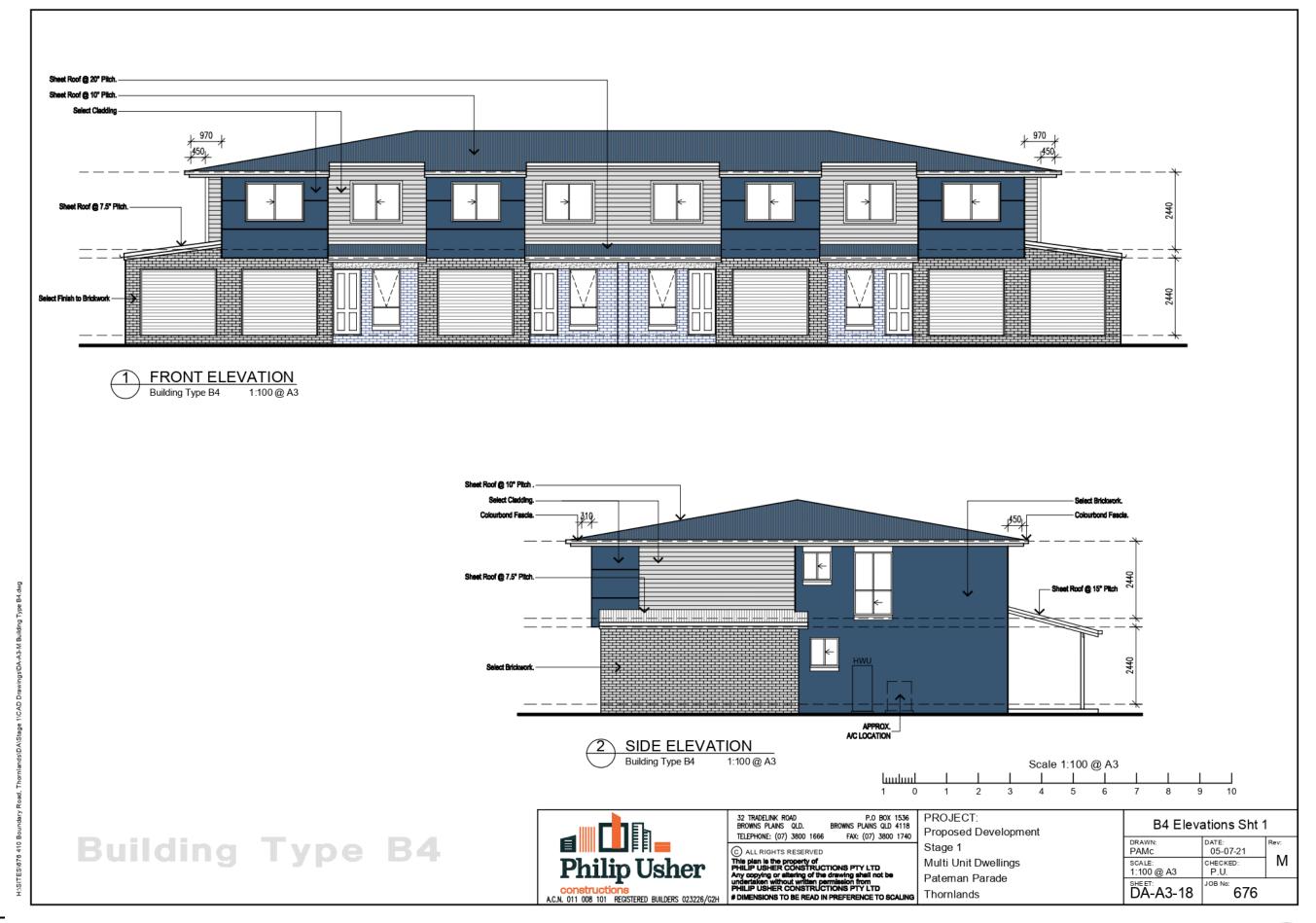


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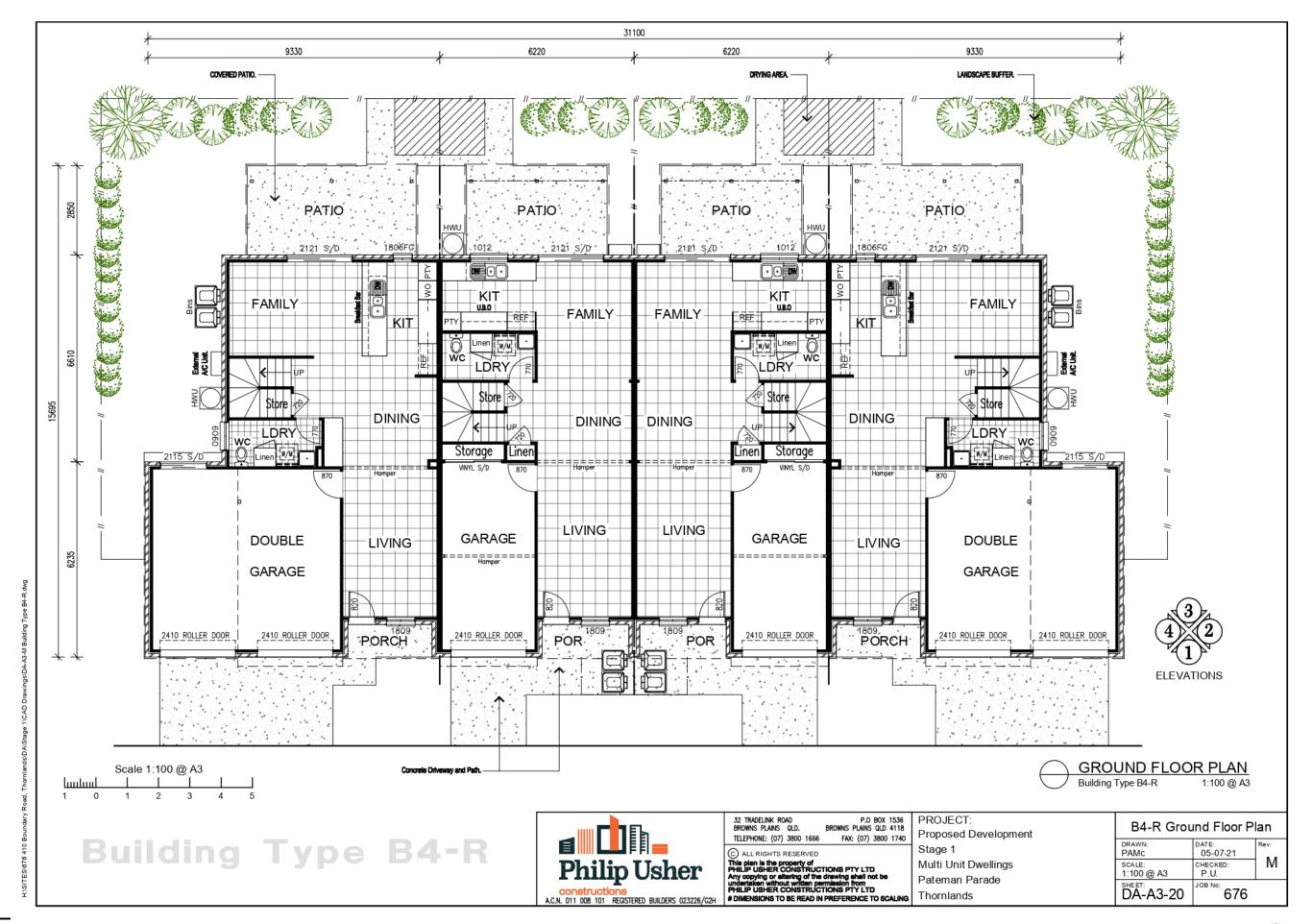


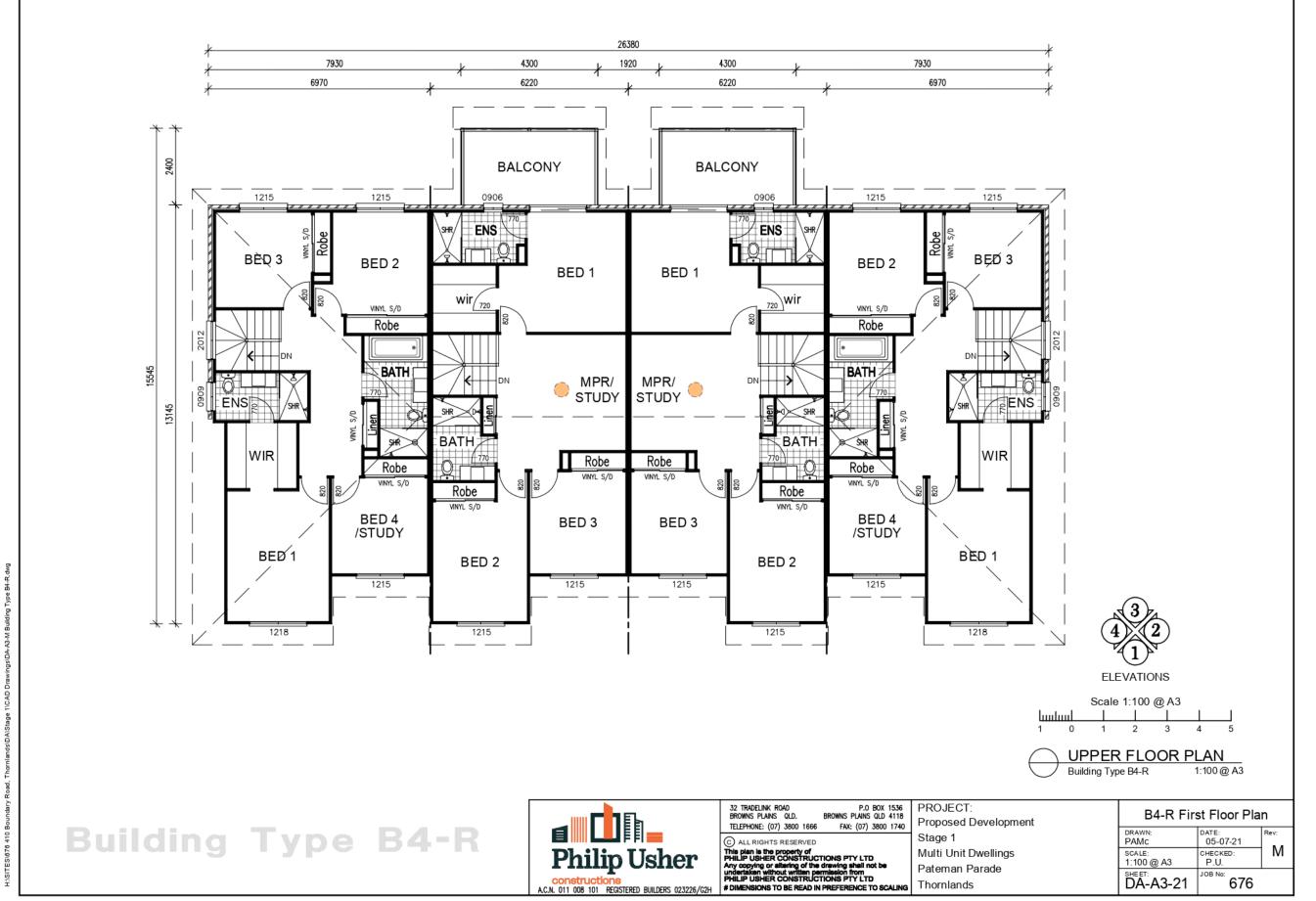




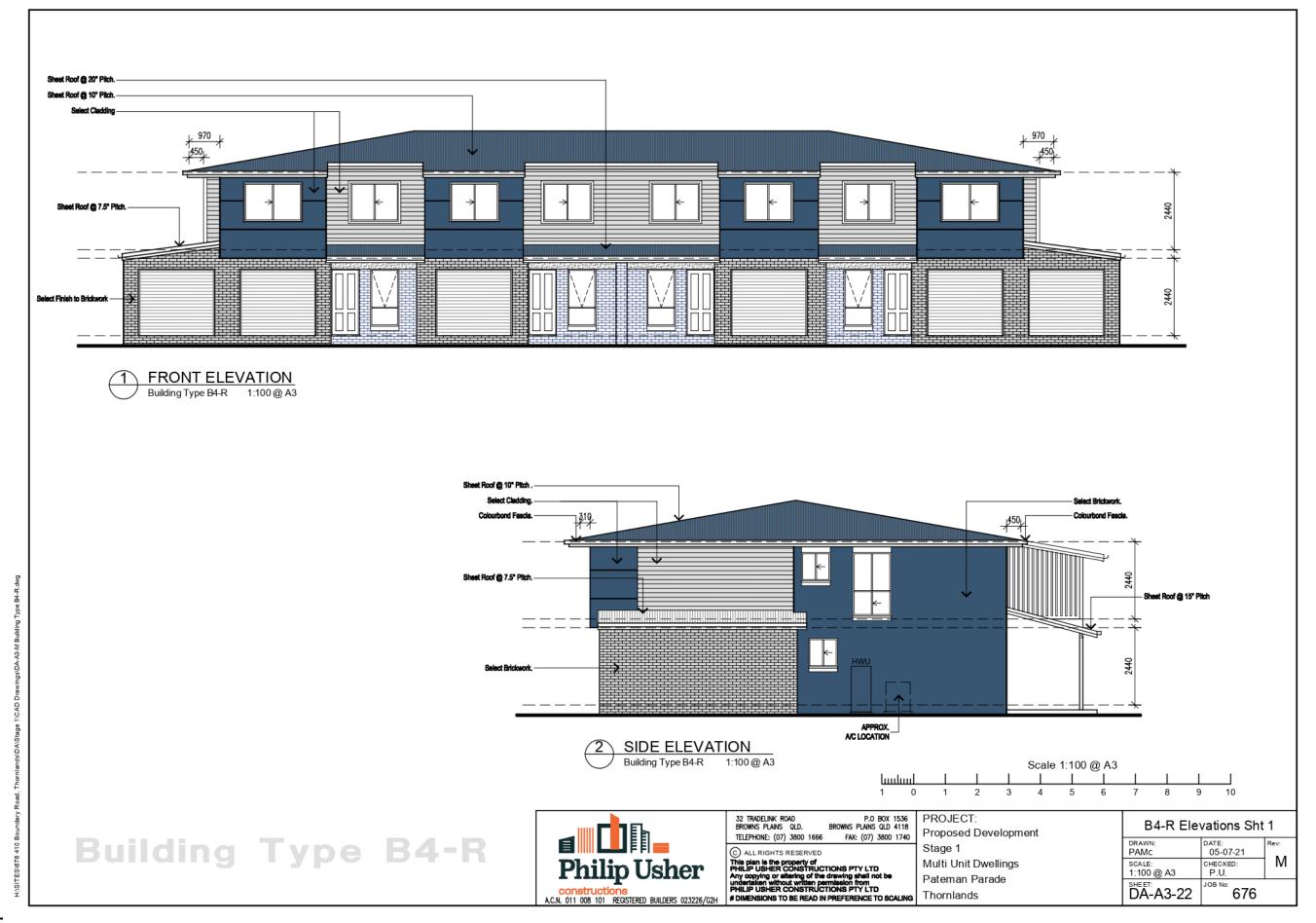




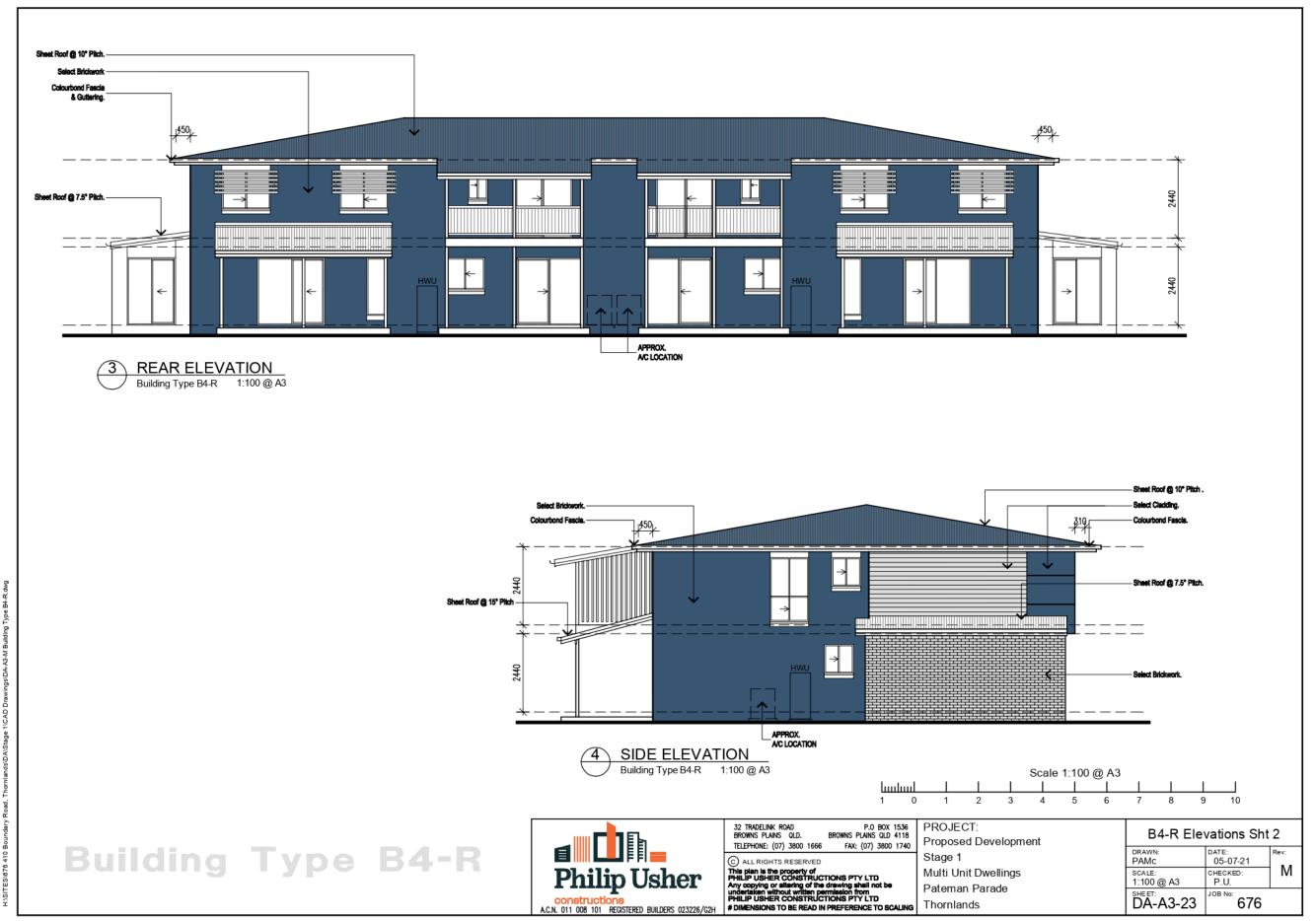




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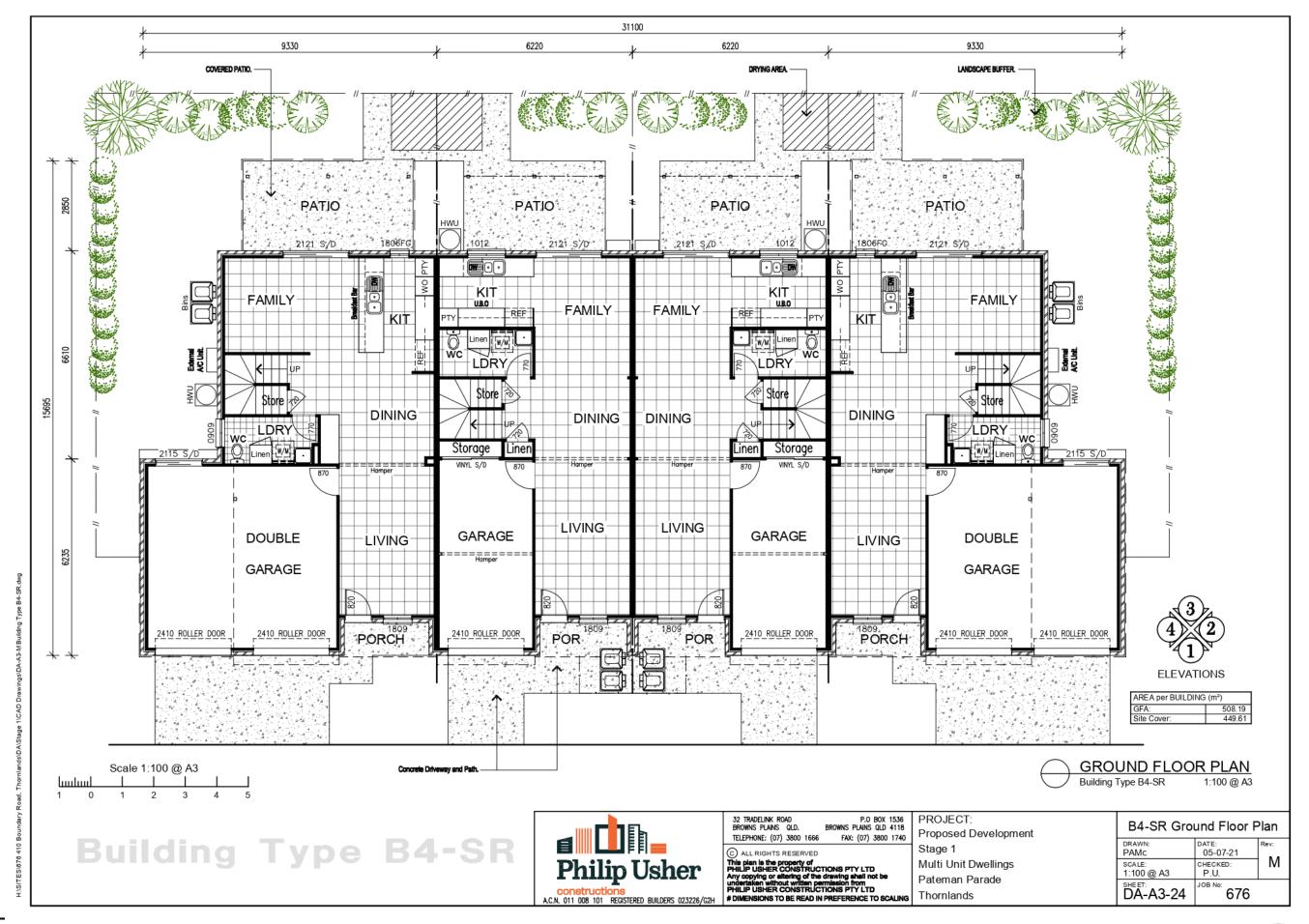


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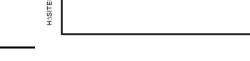
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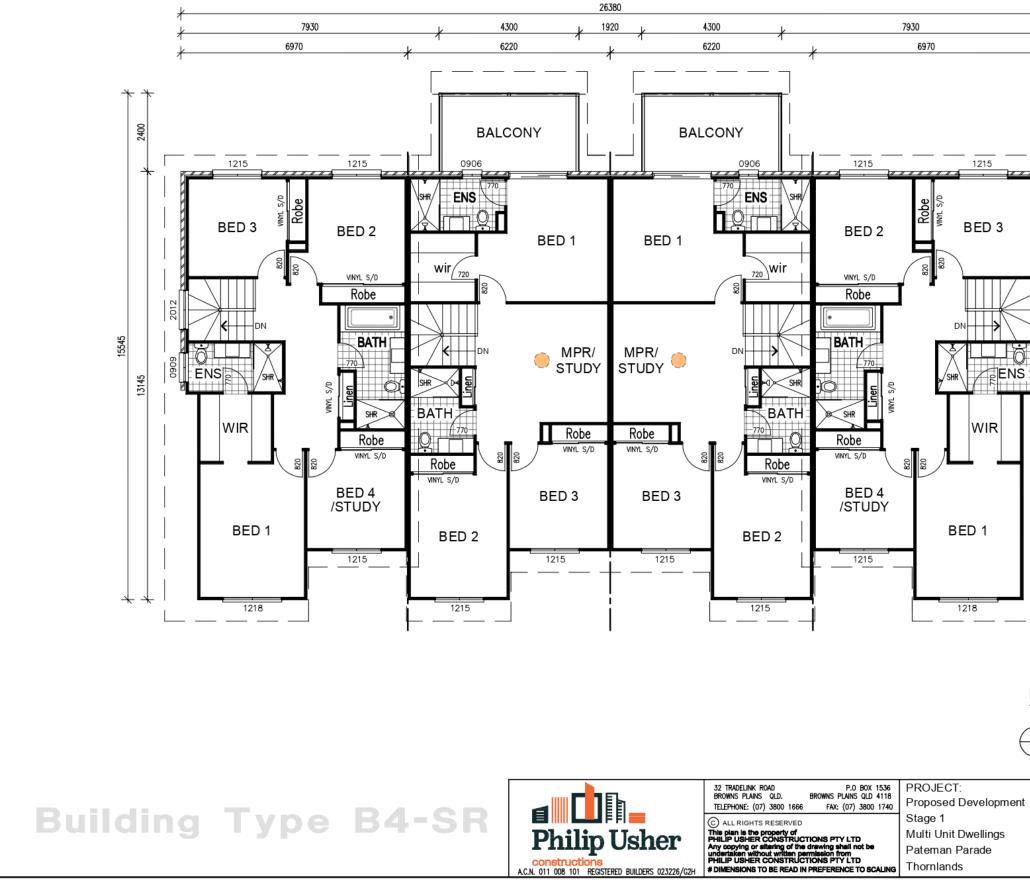
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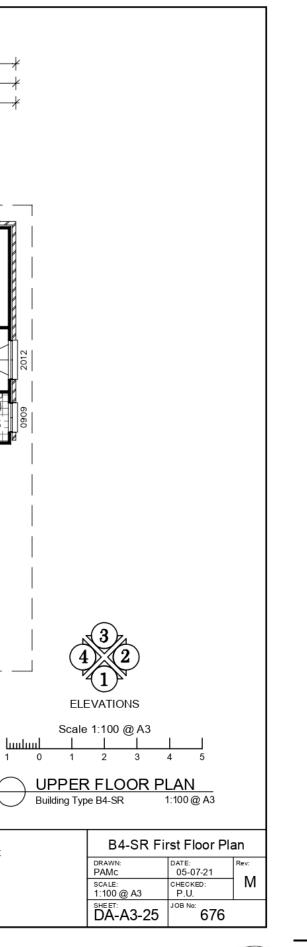


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H:ISITES(676 410 Boundary Road, Thornlands)DA\Stage 1\CAL

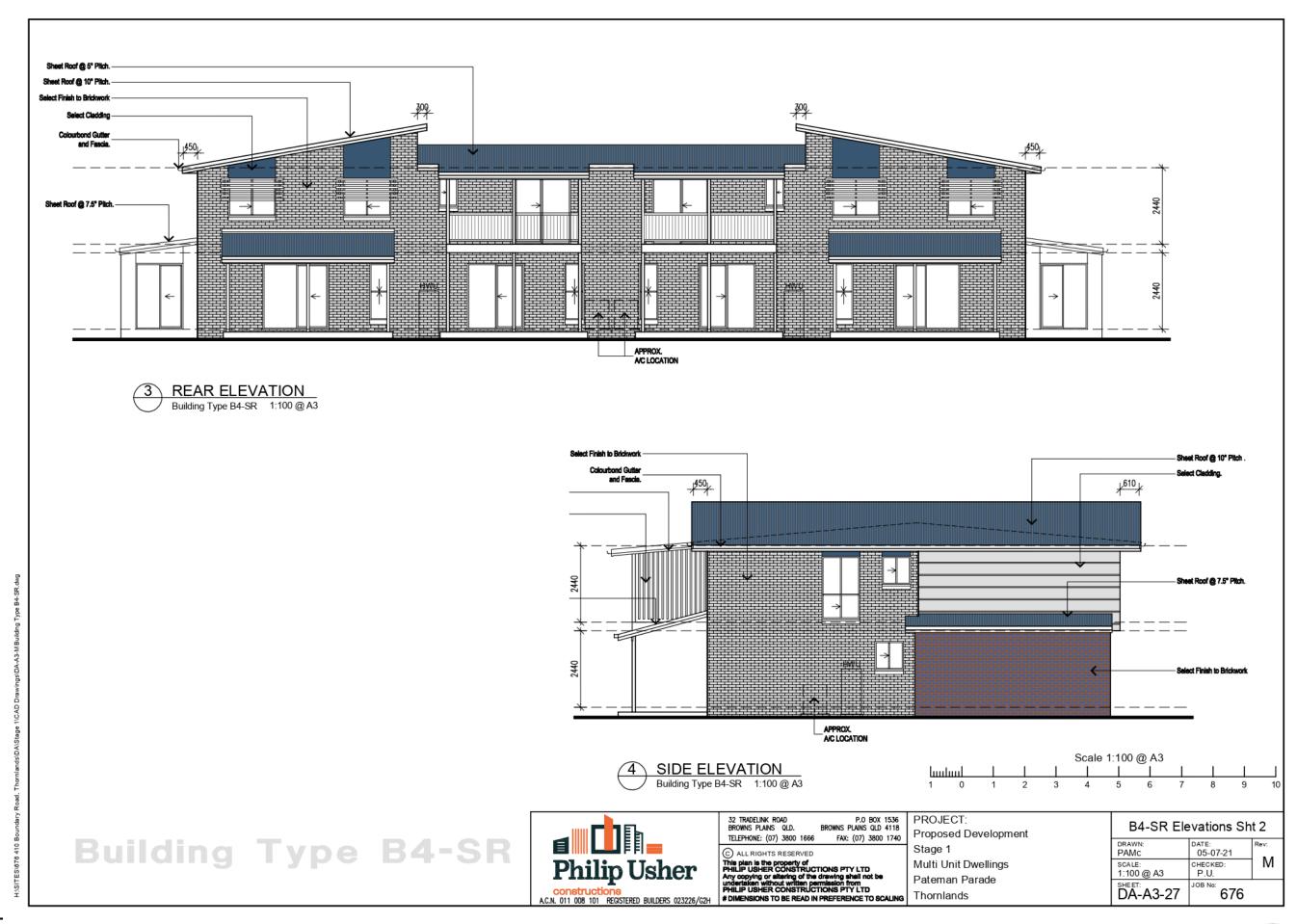


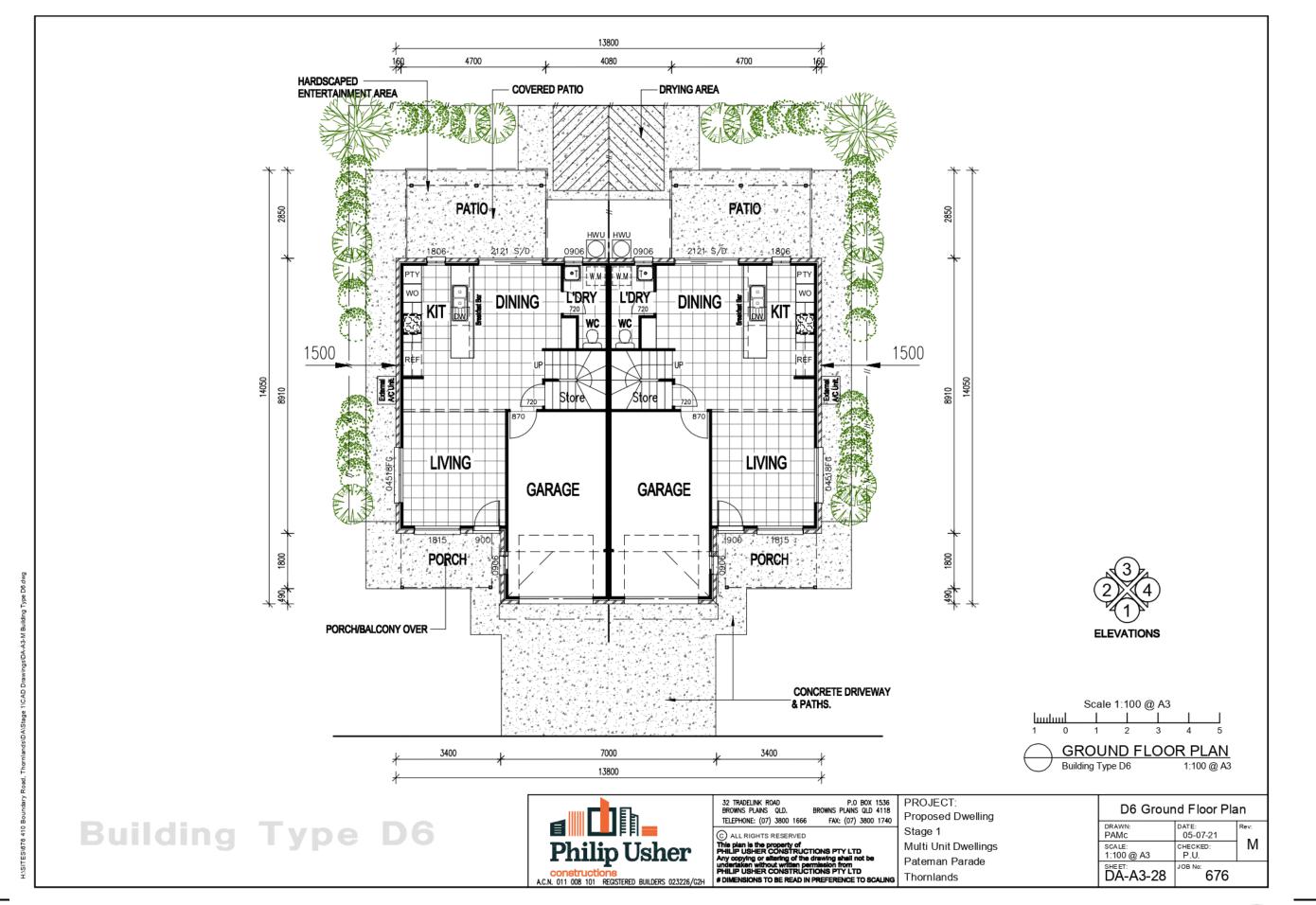




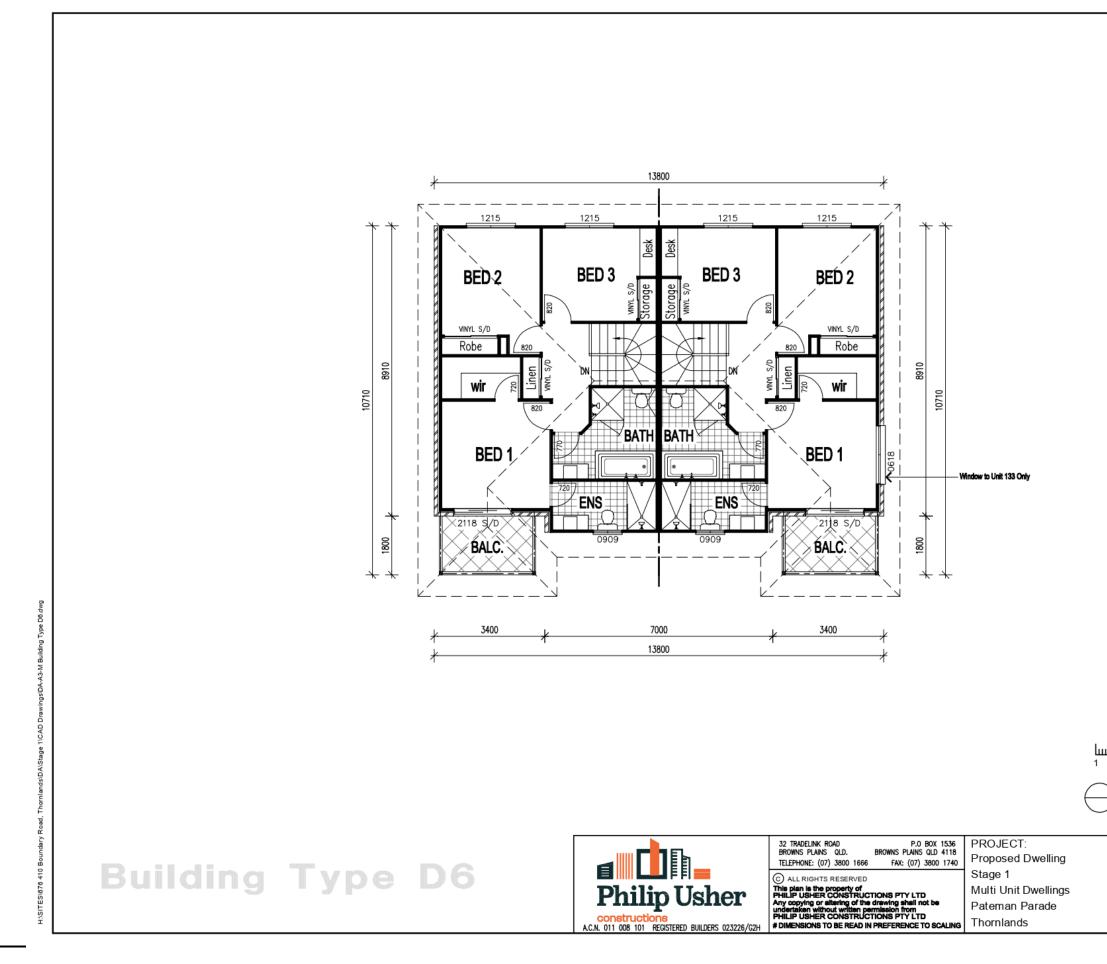


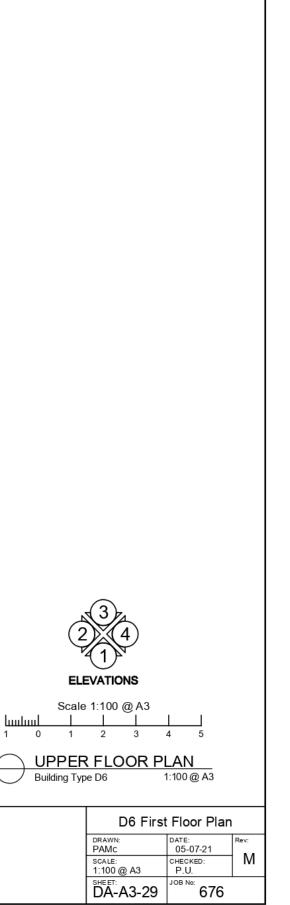
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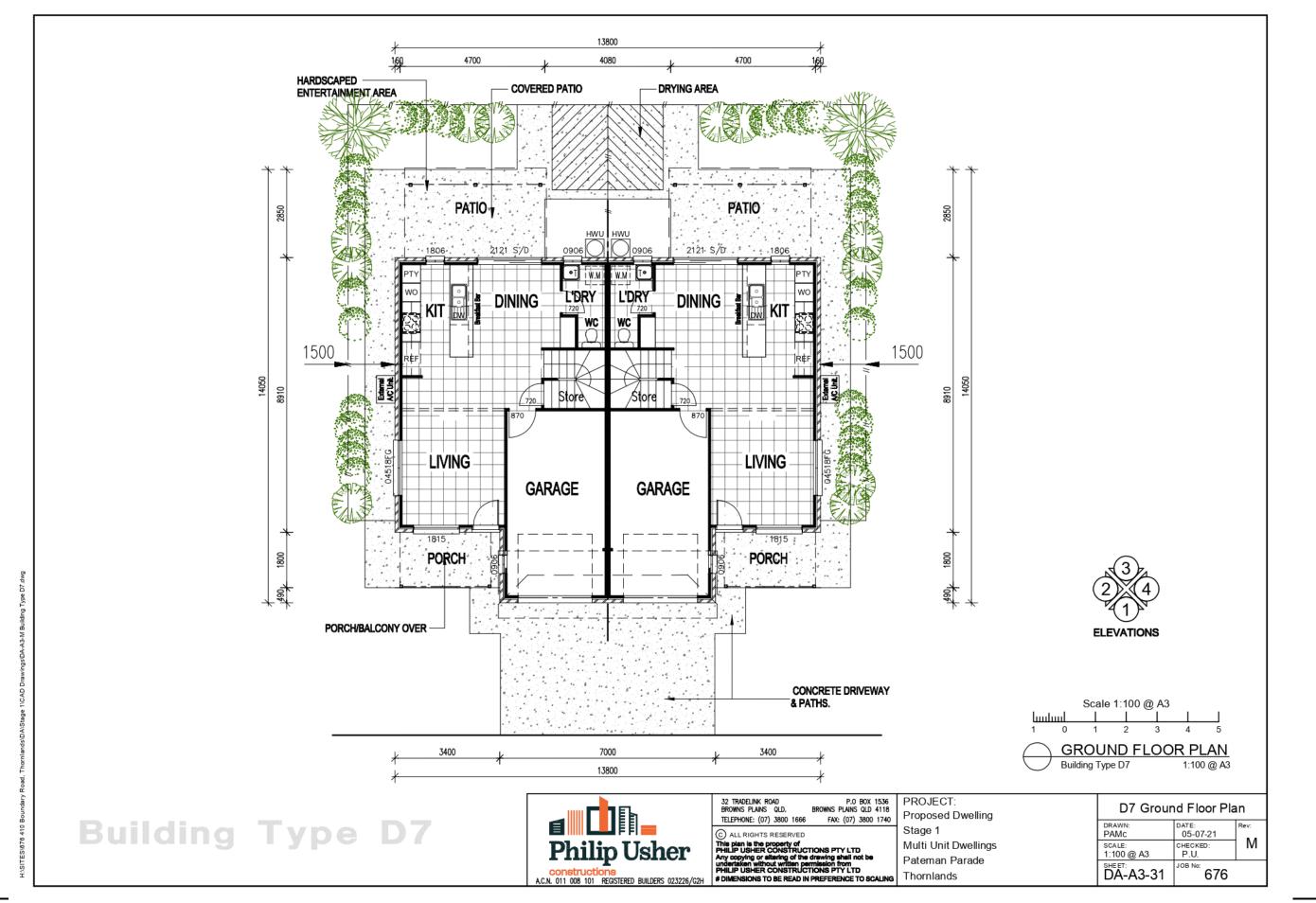
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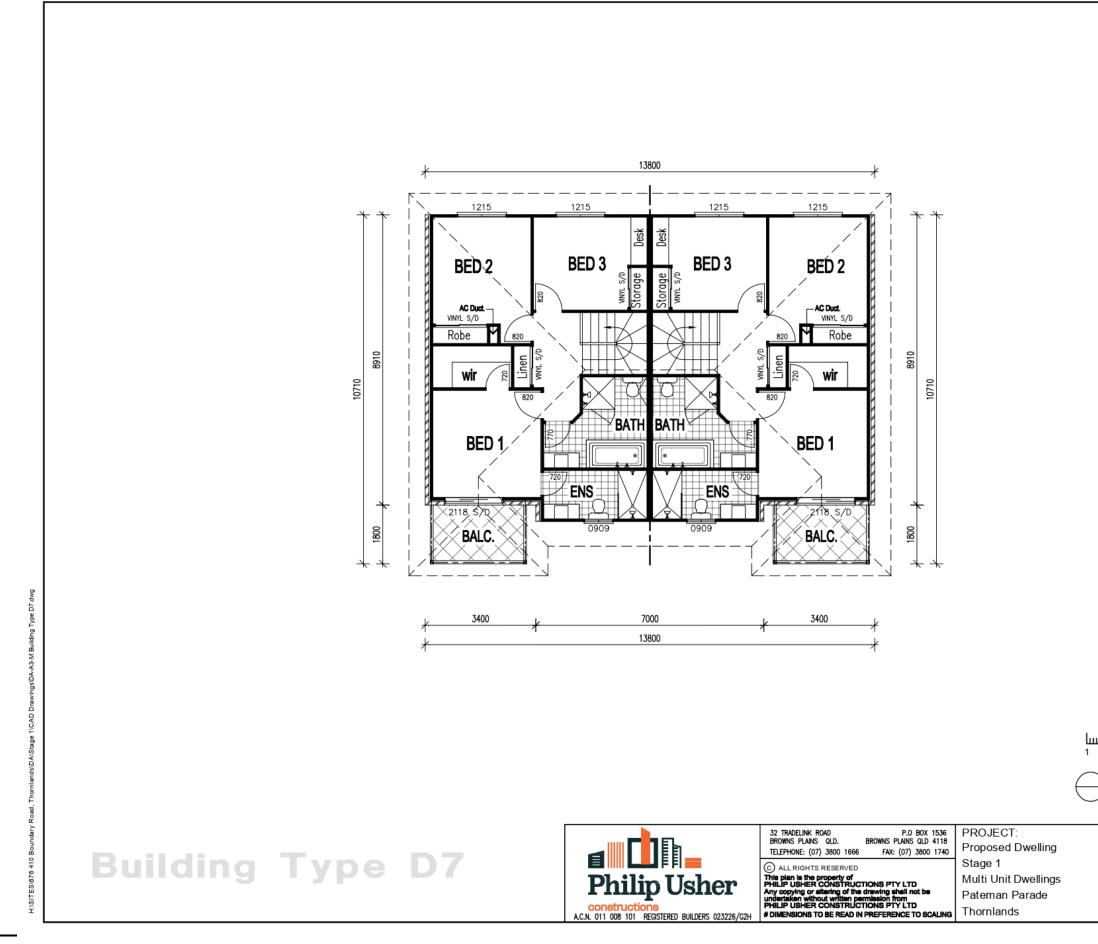


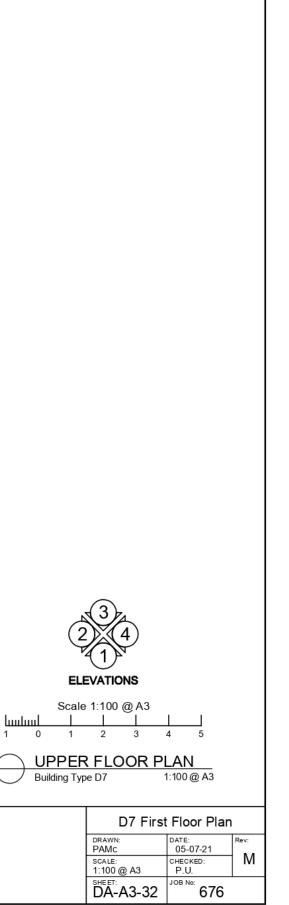


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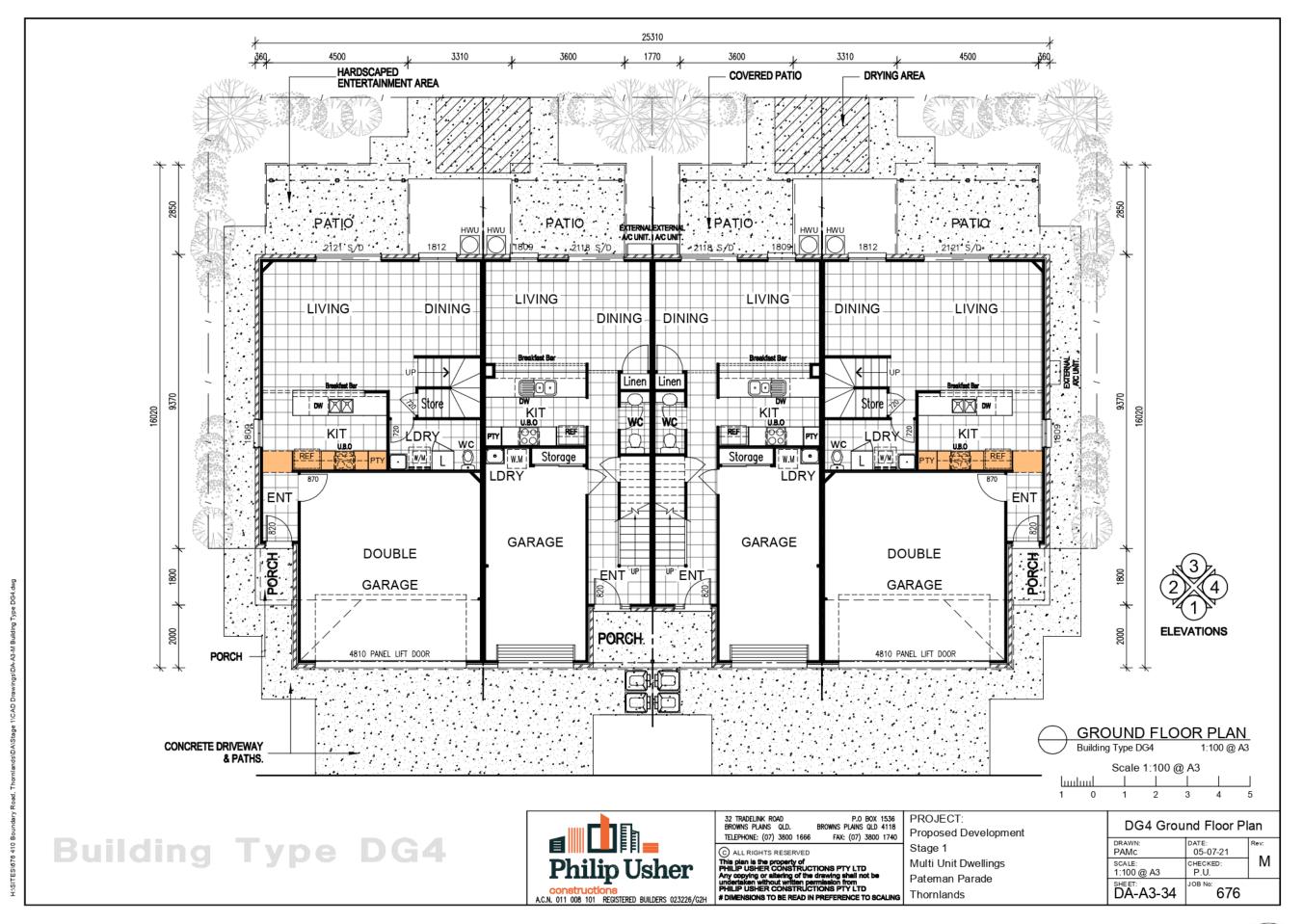


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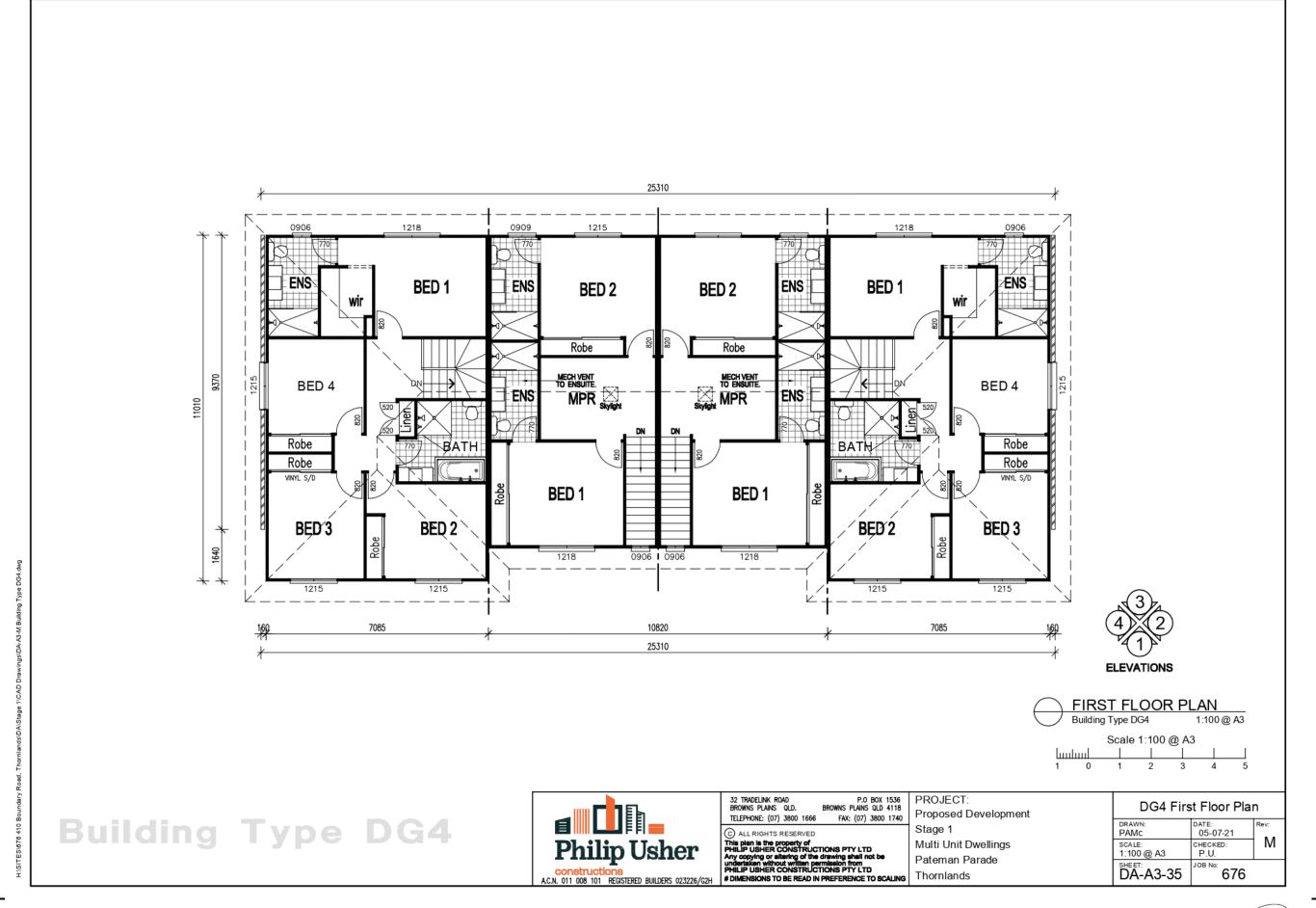


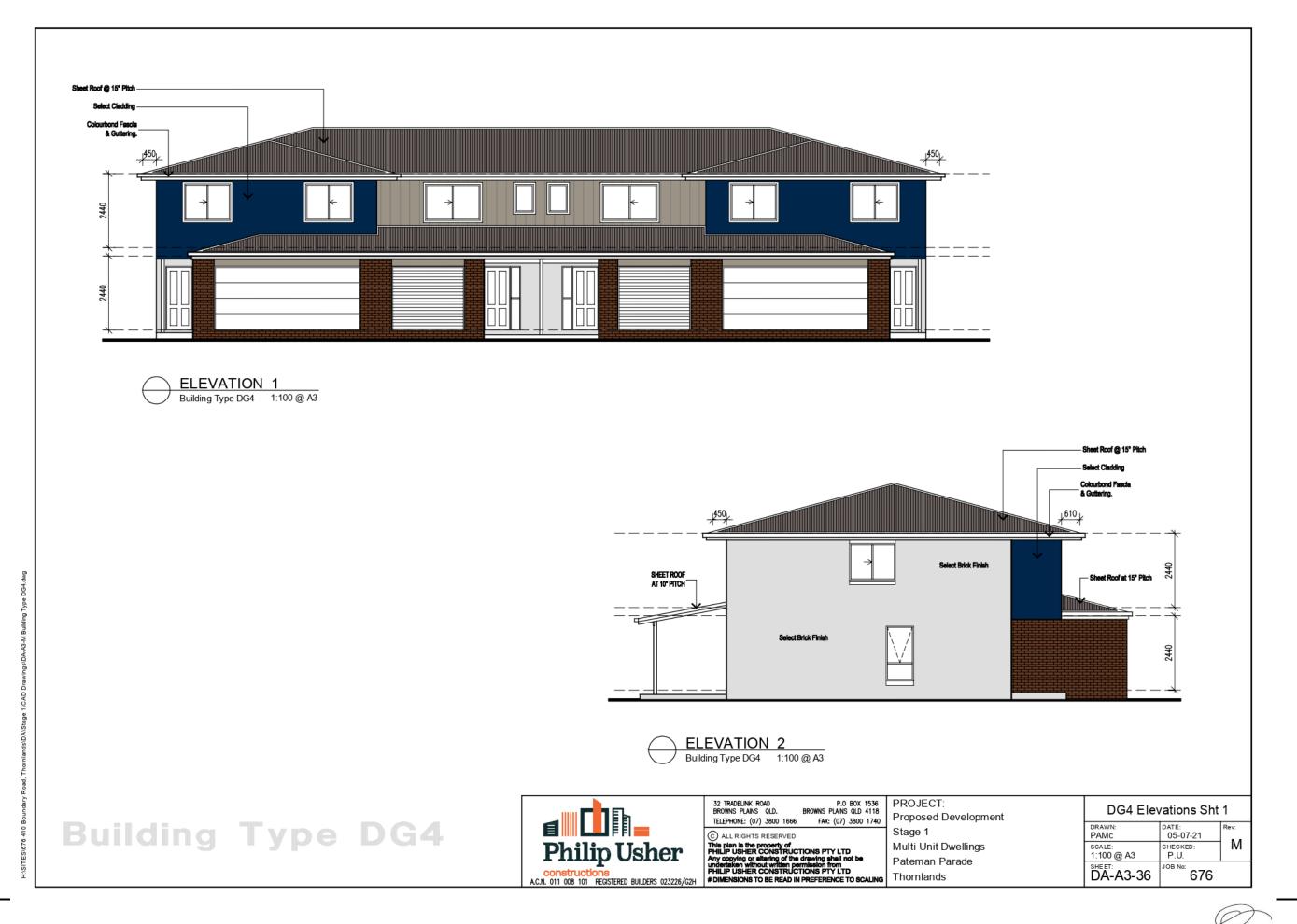




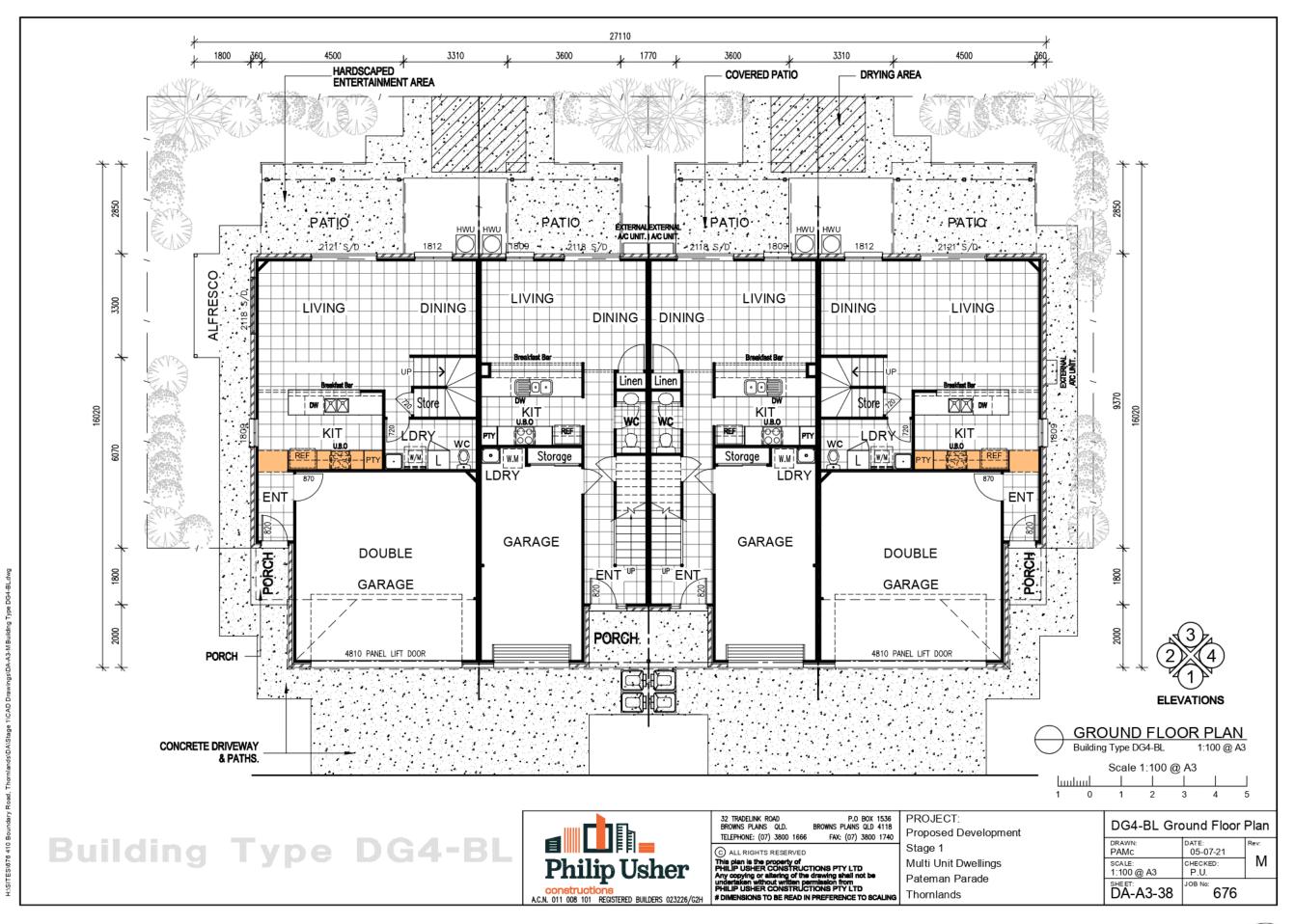


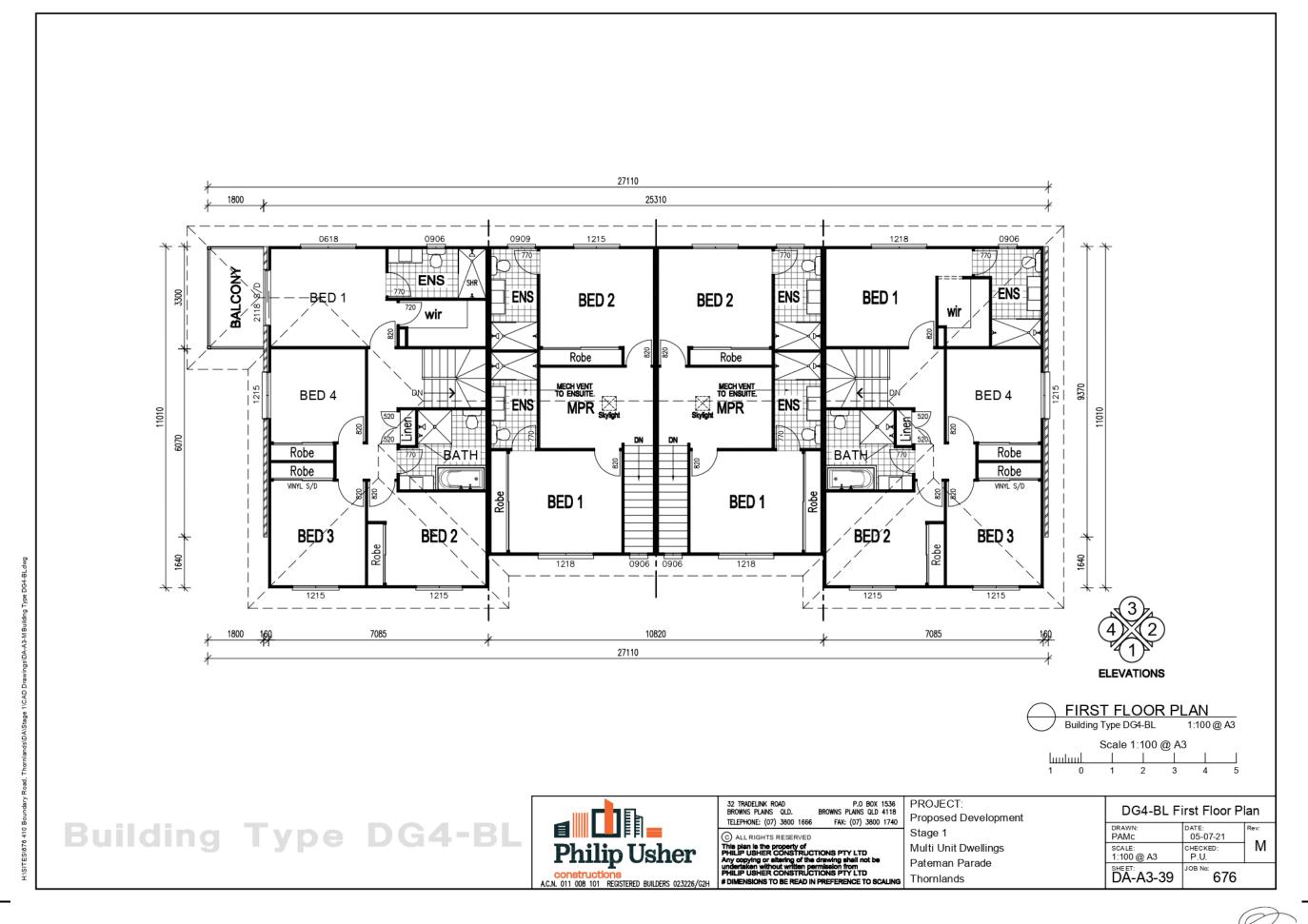
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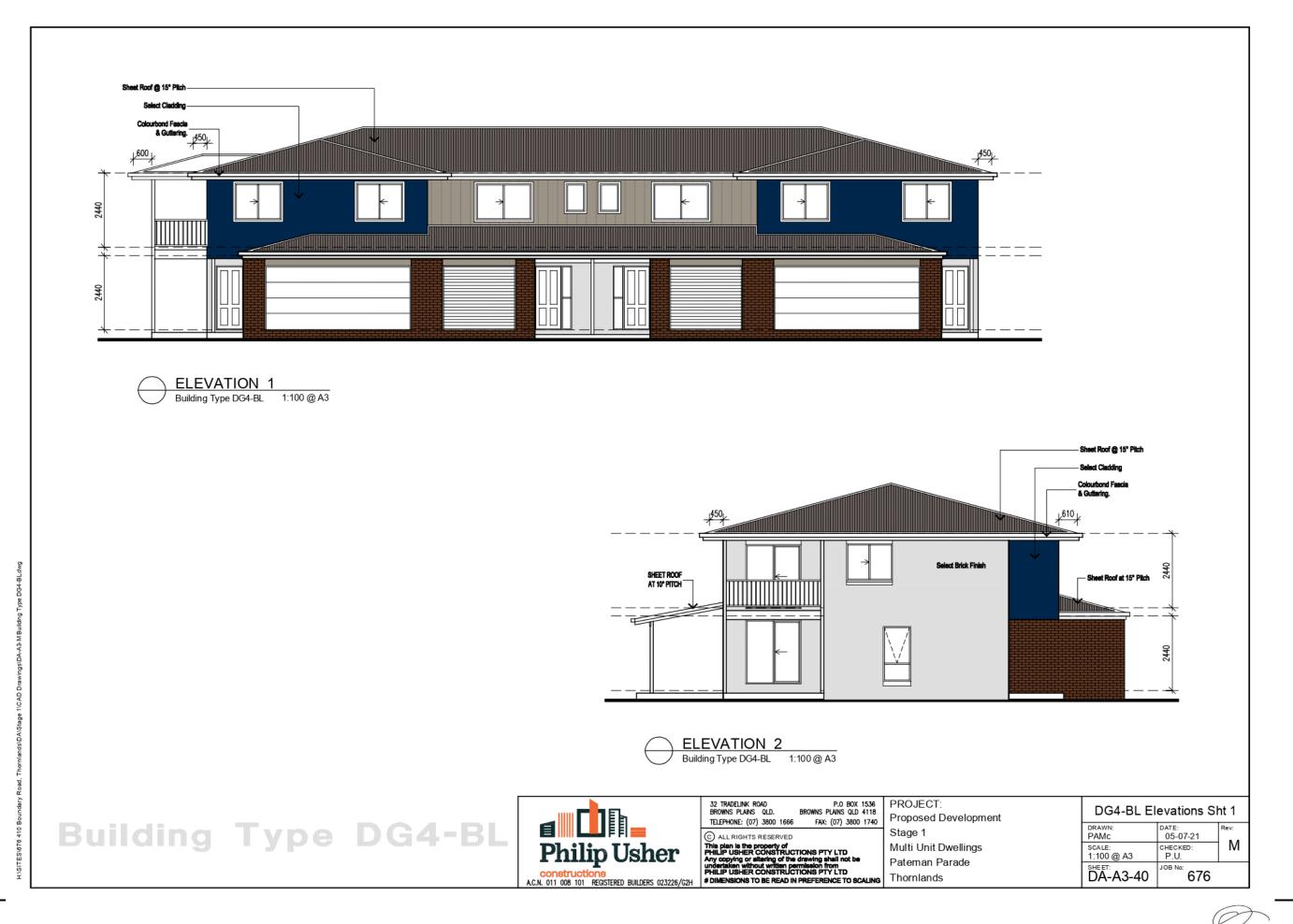


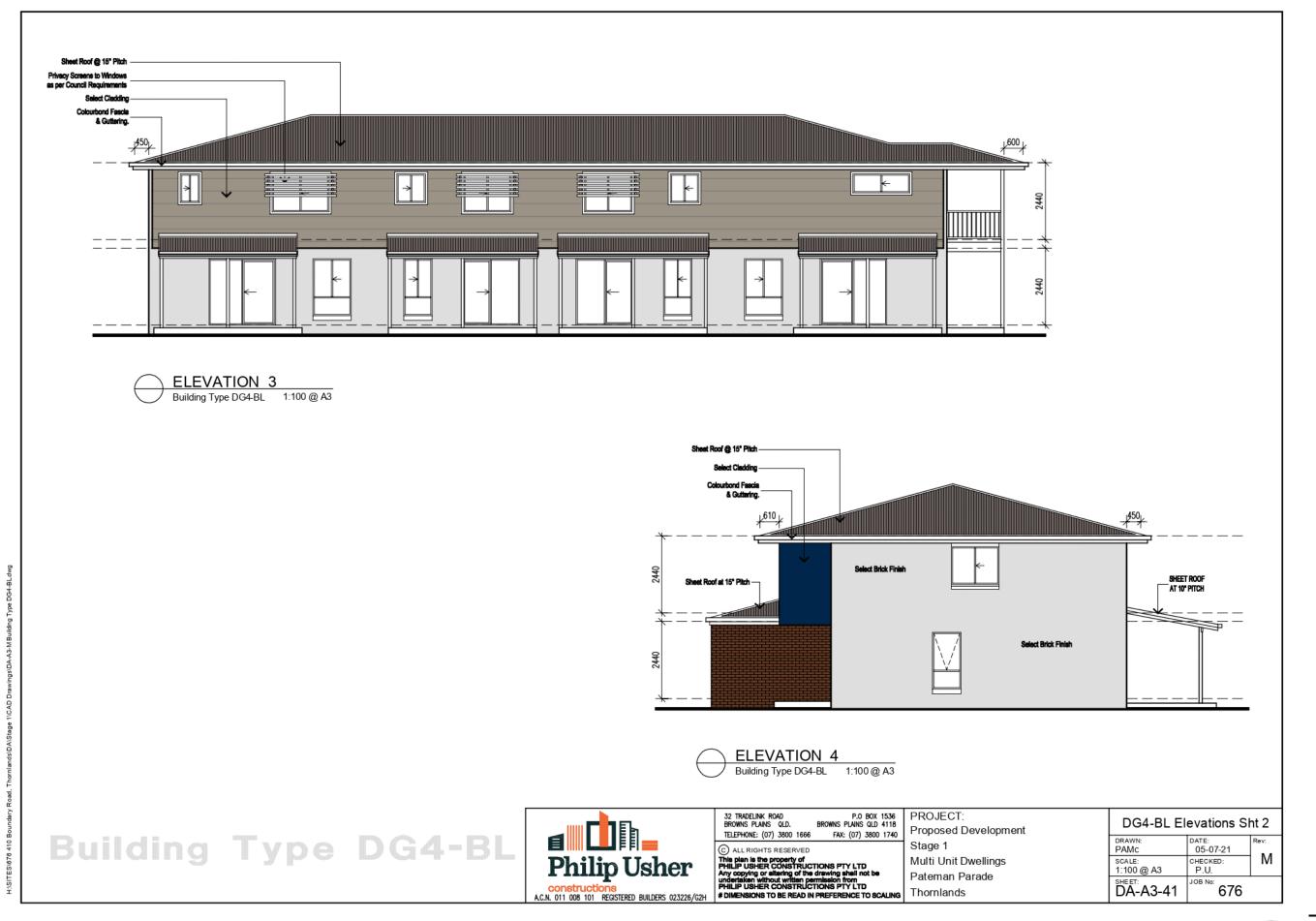


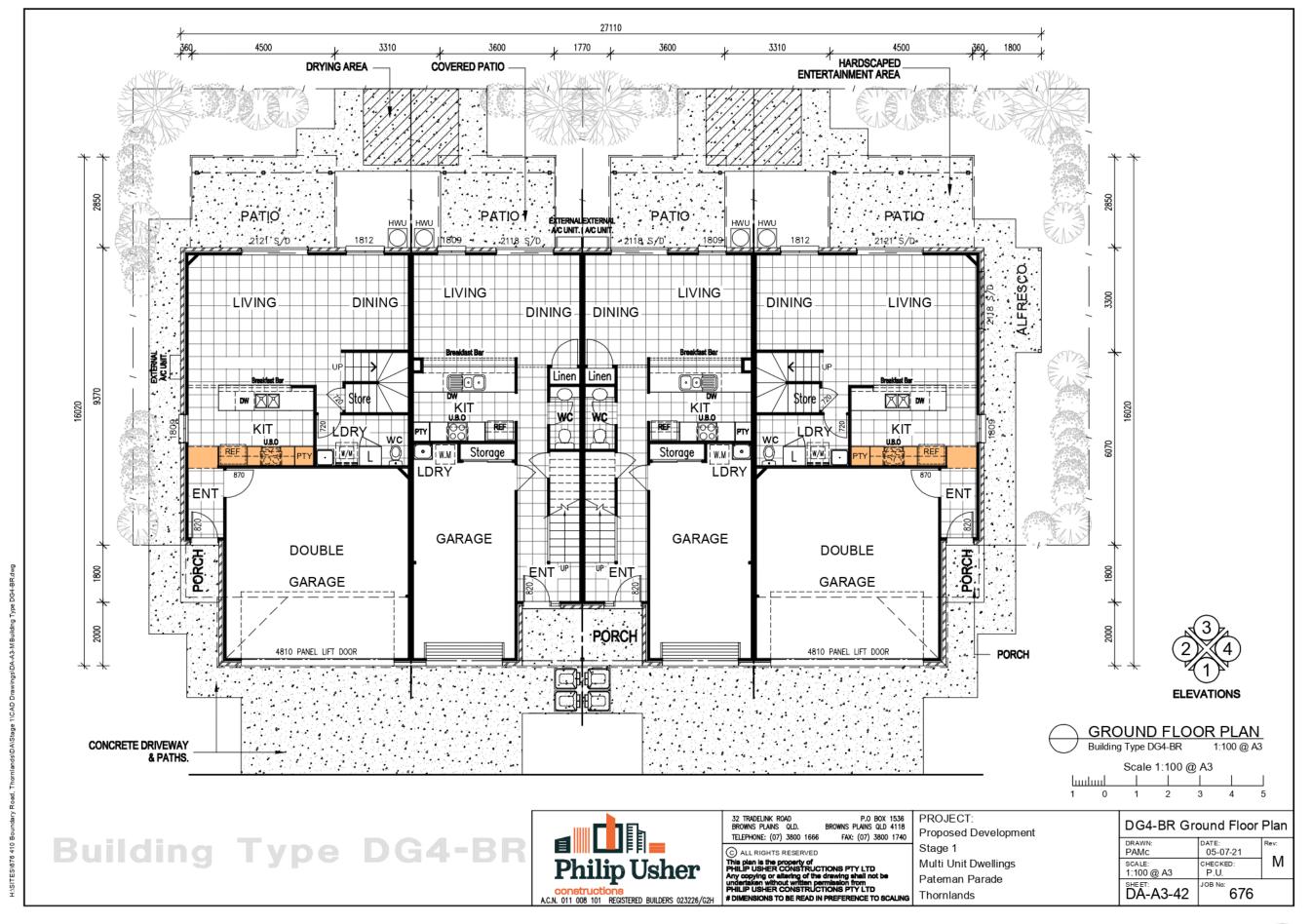


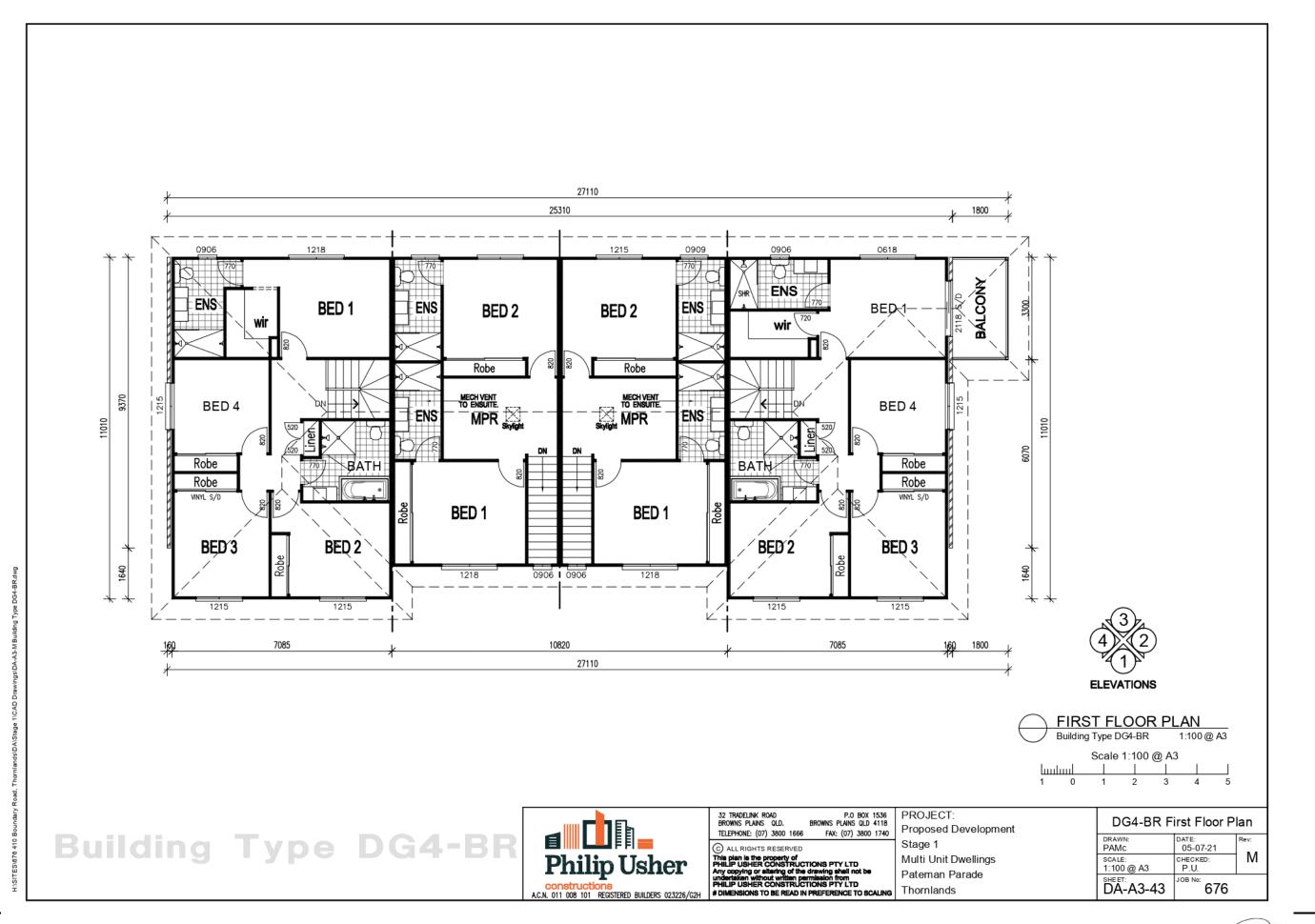


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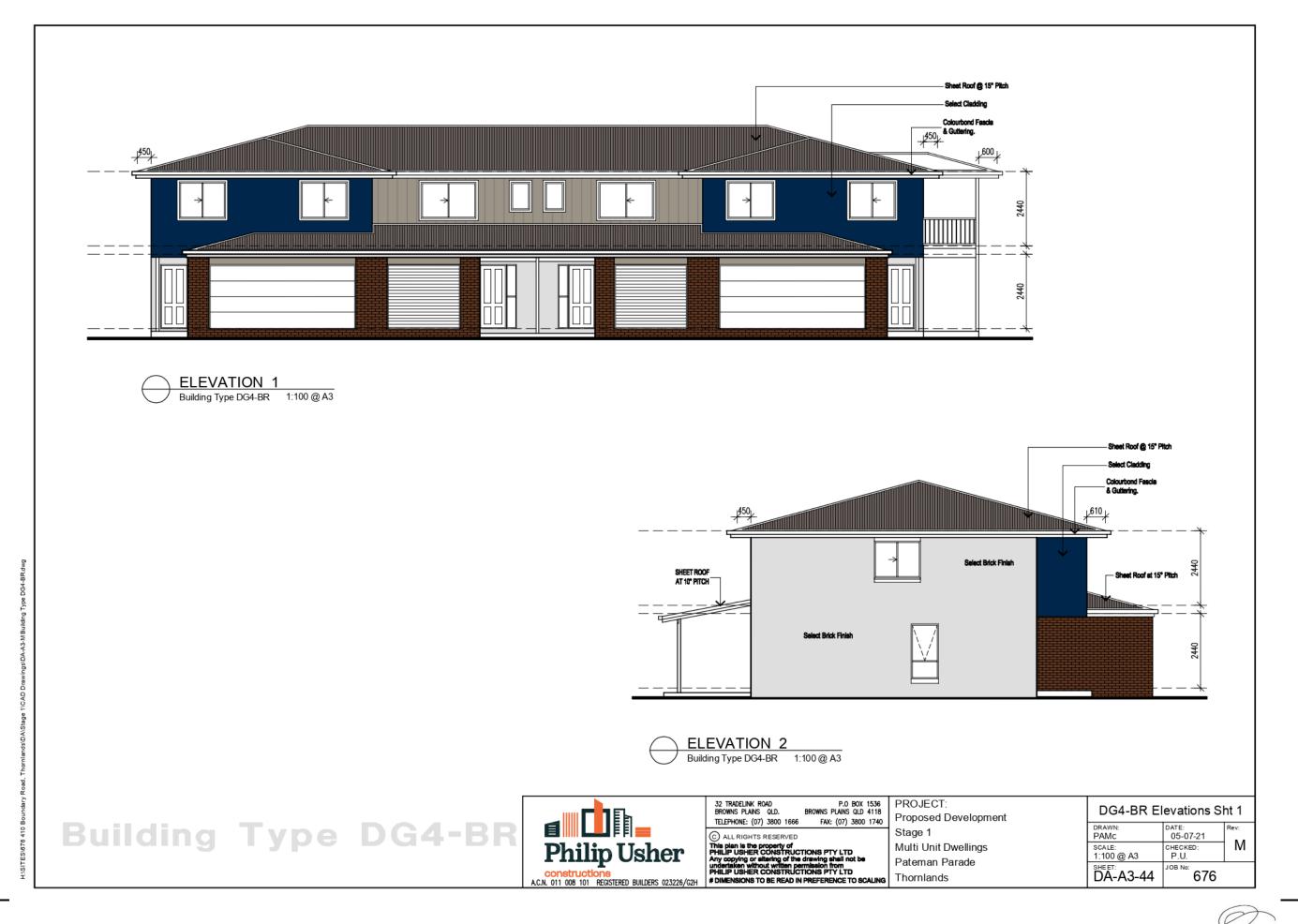




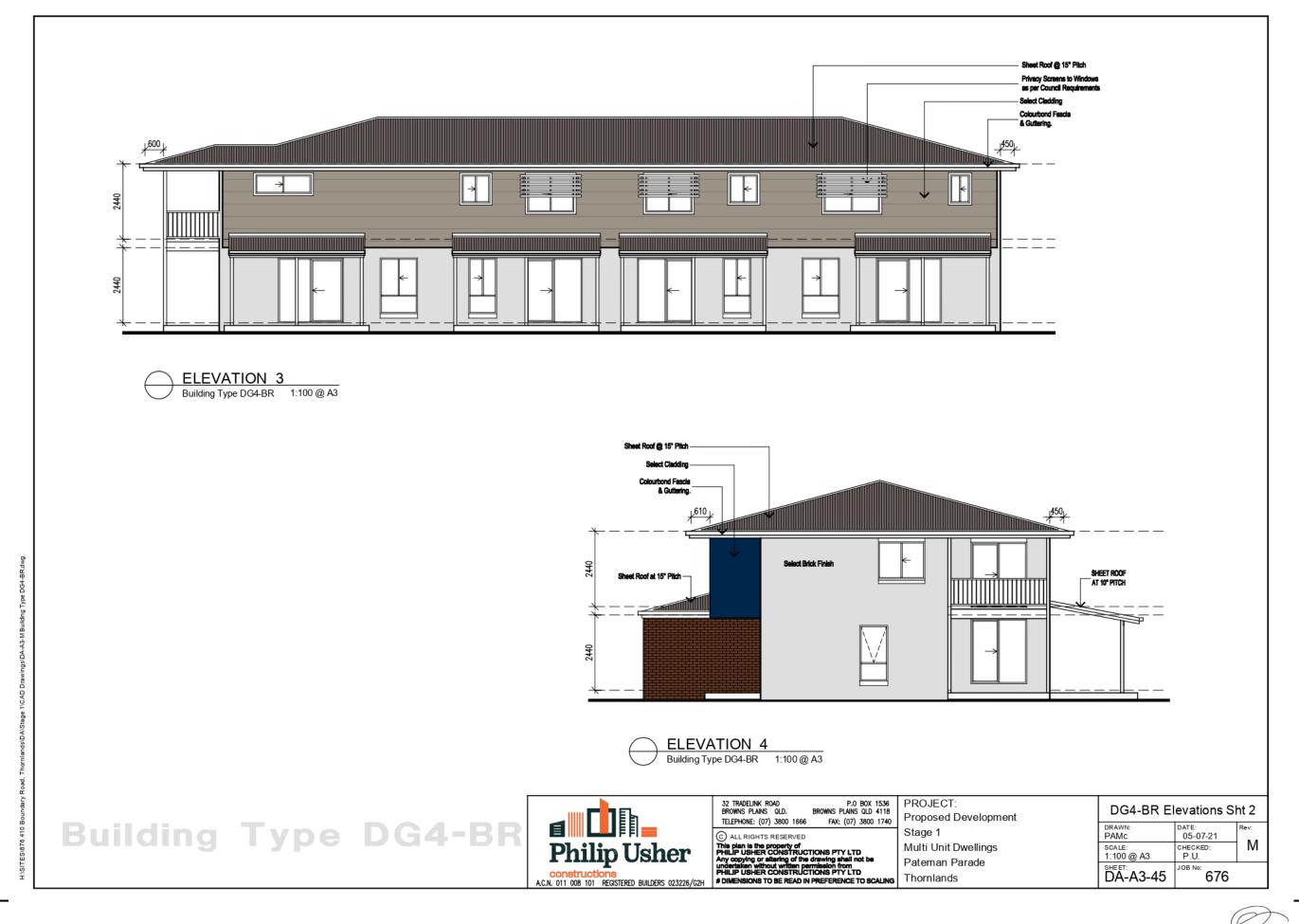


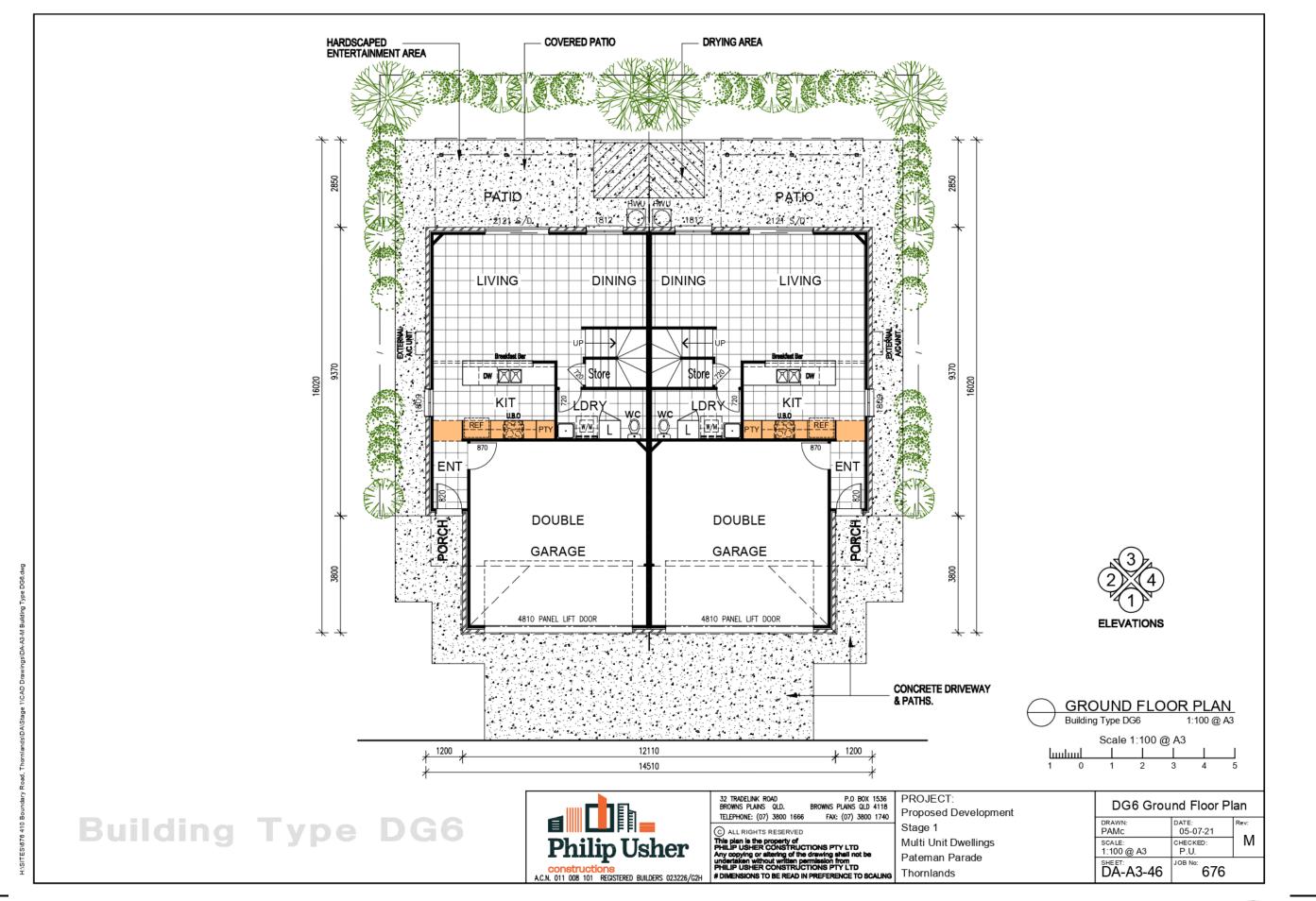


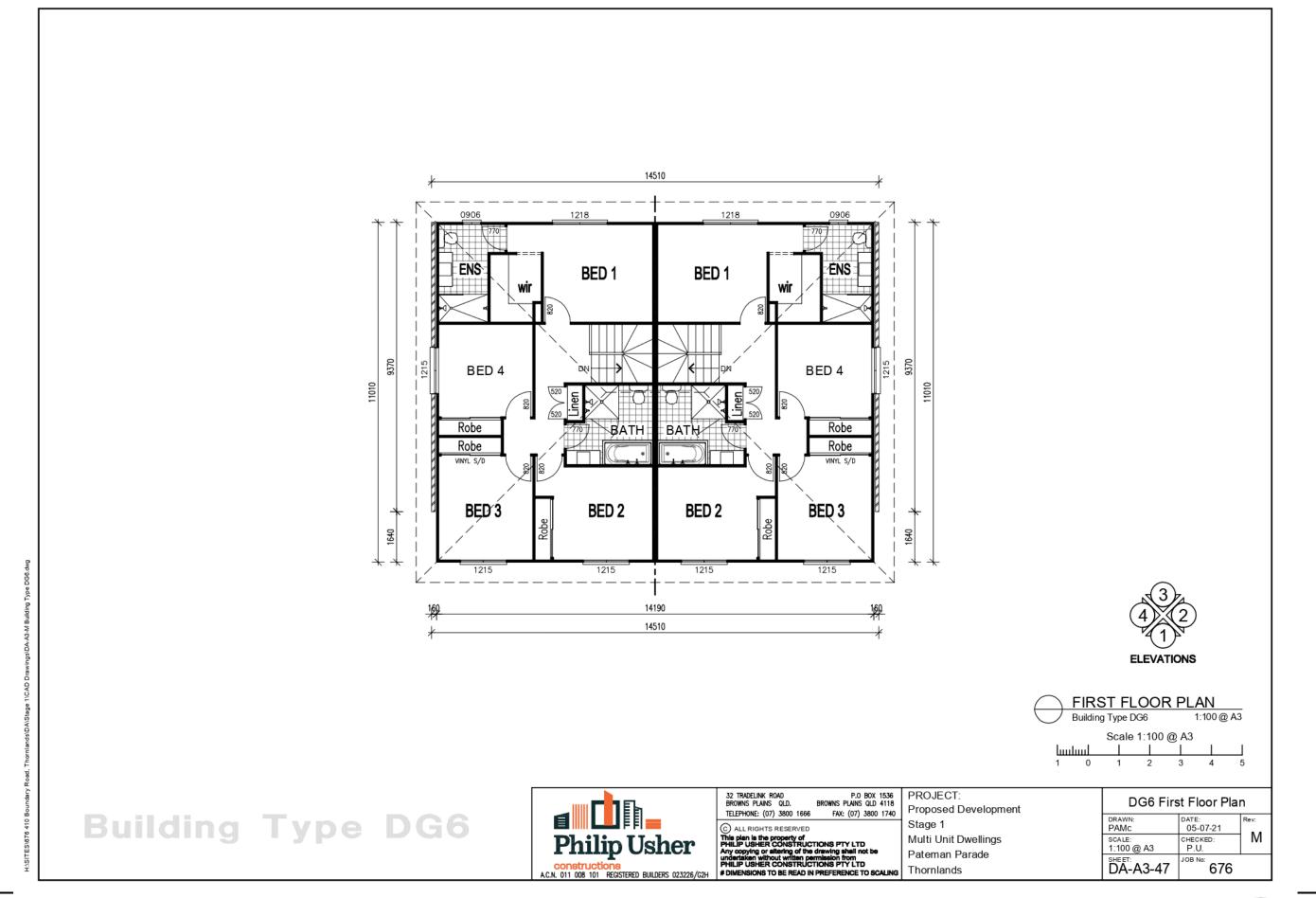
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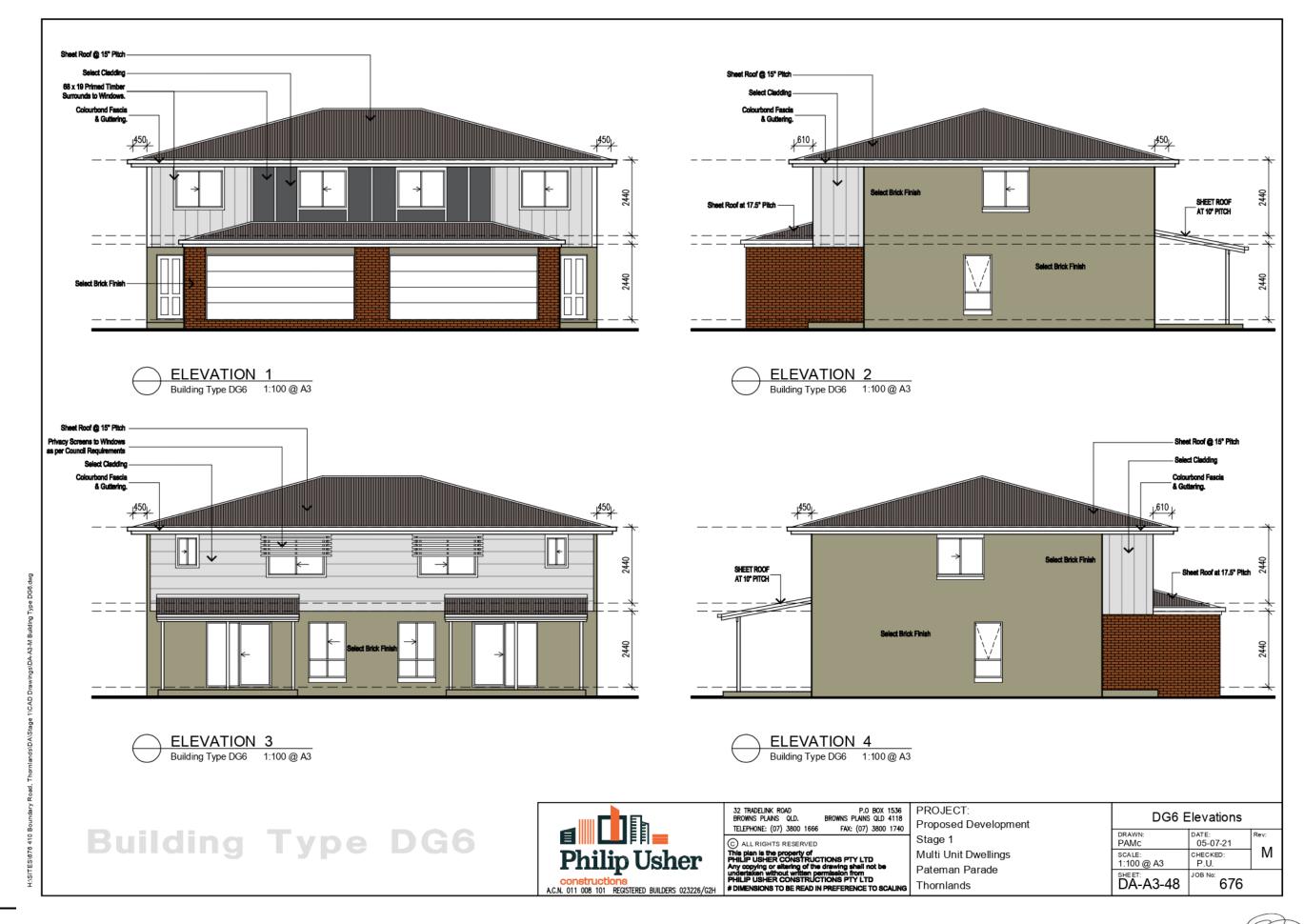
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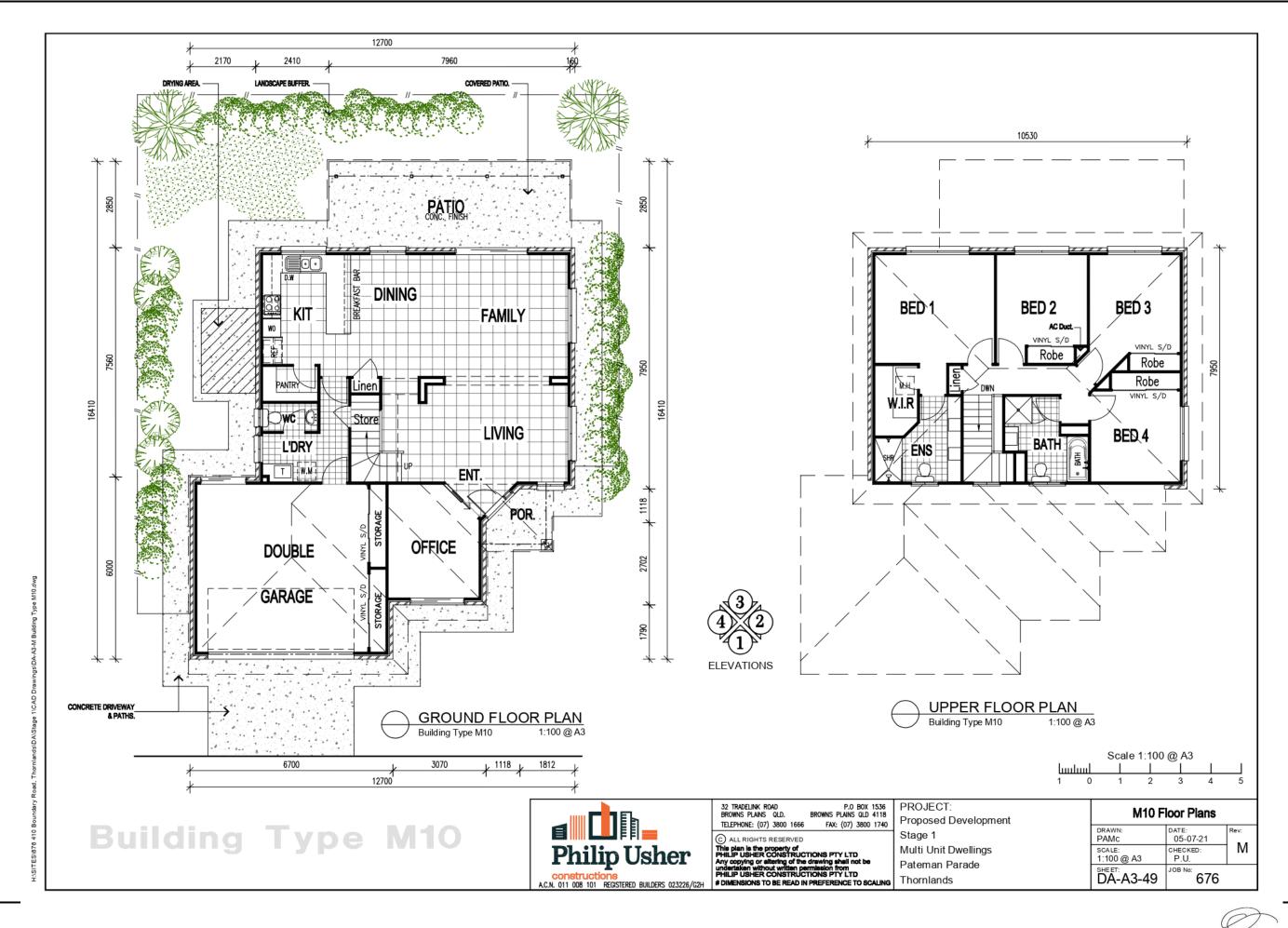


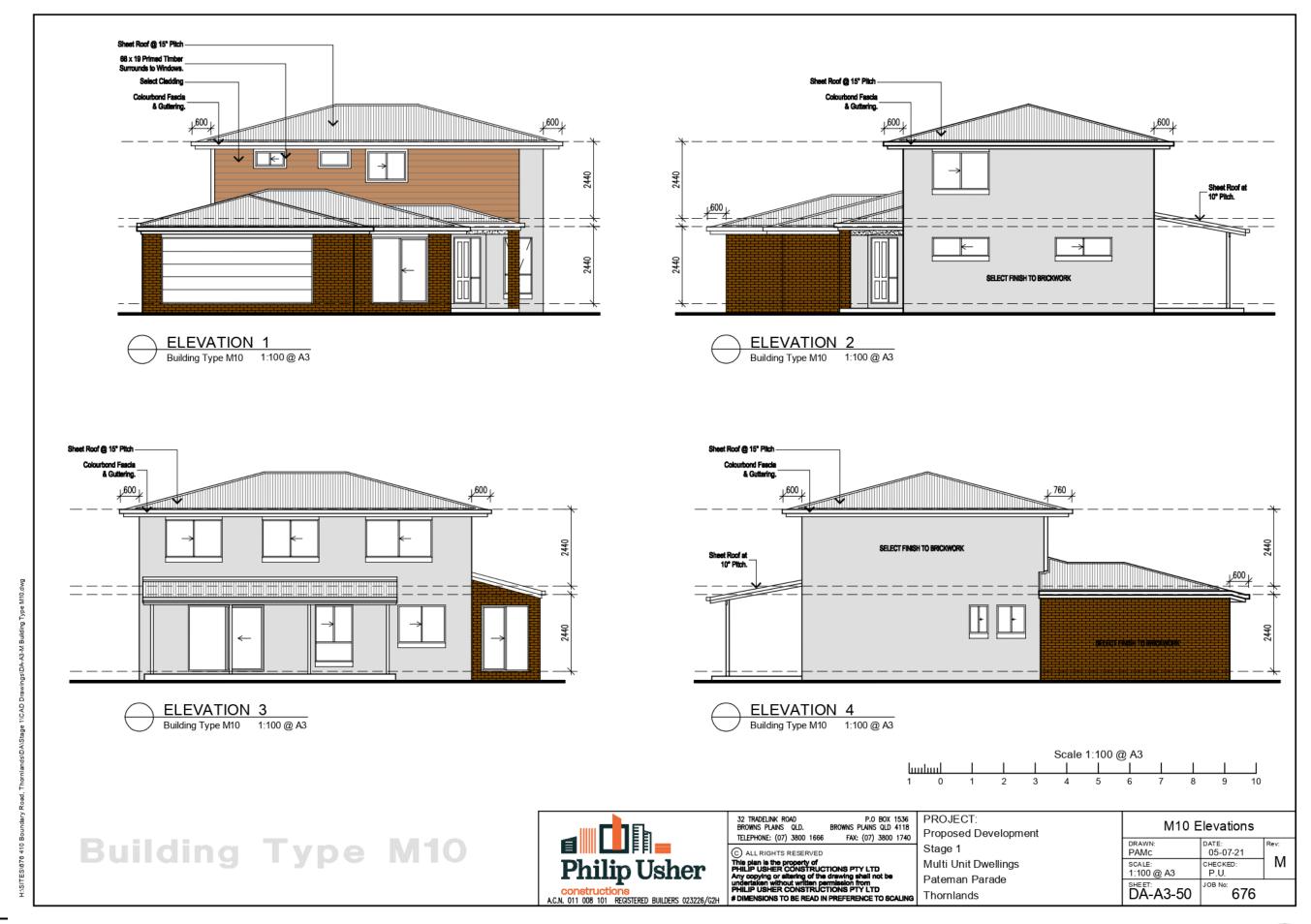


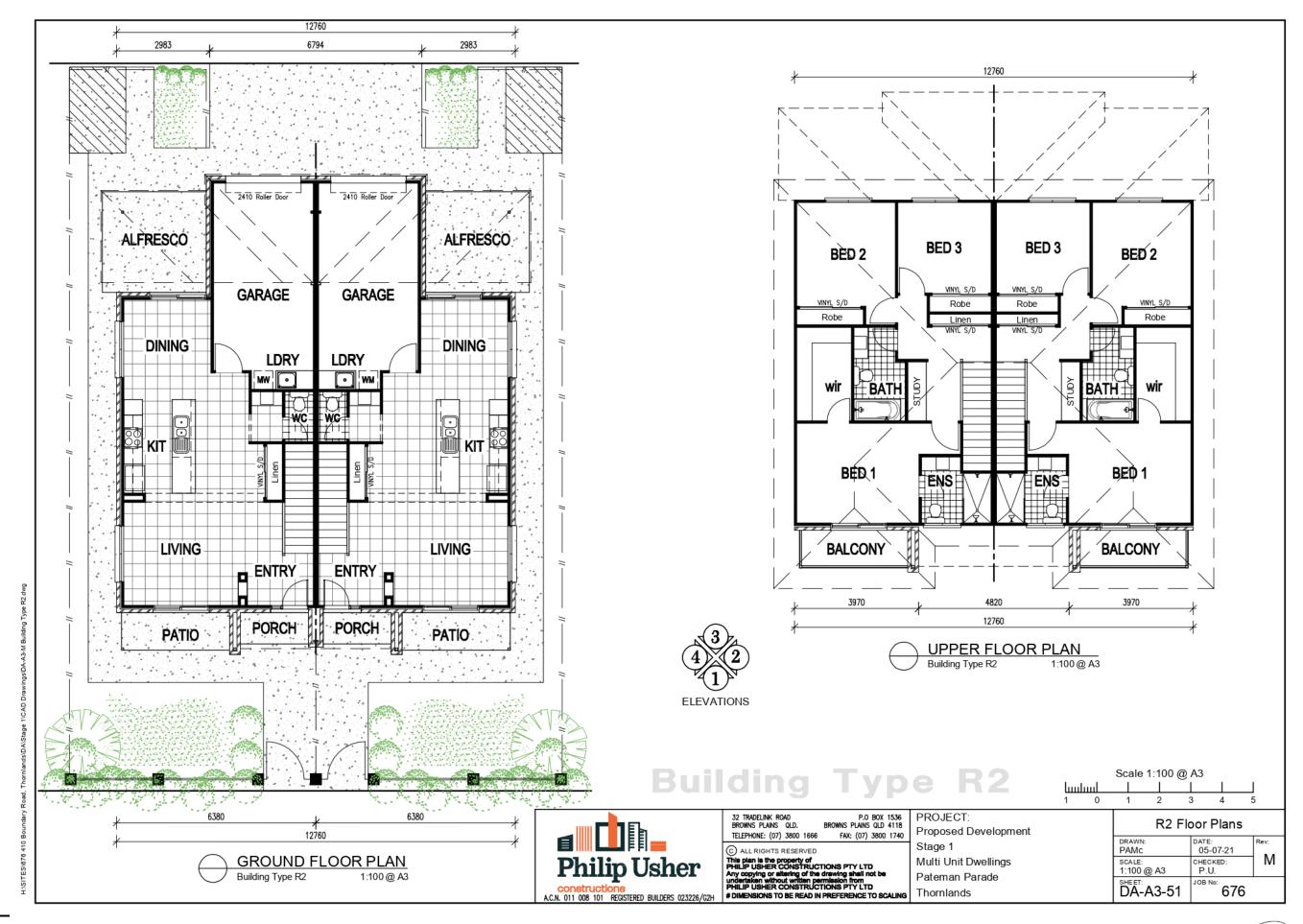
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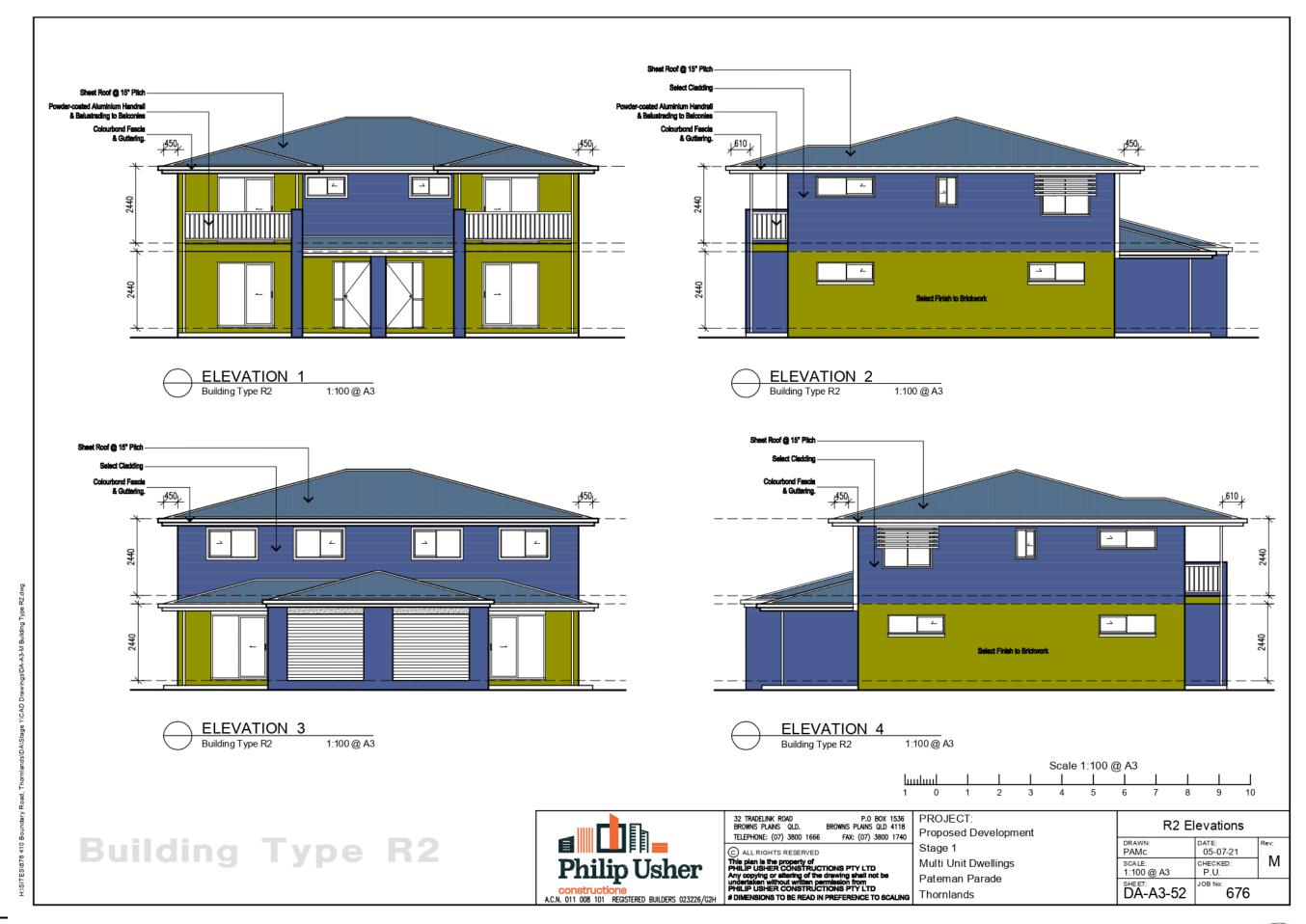
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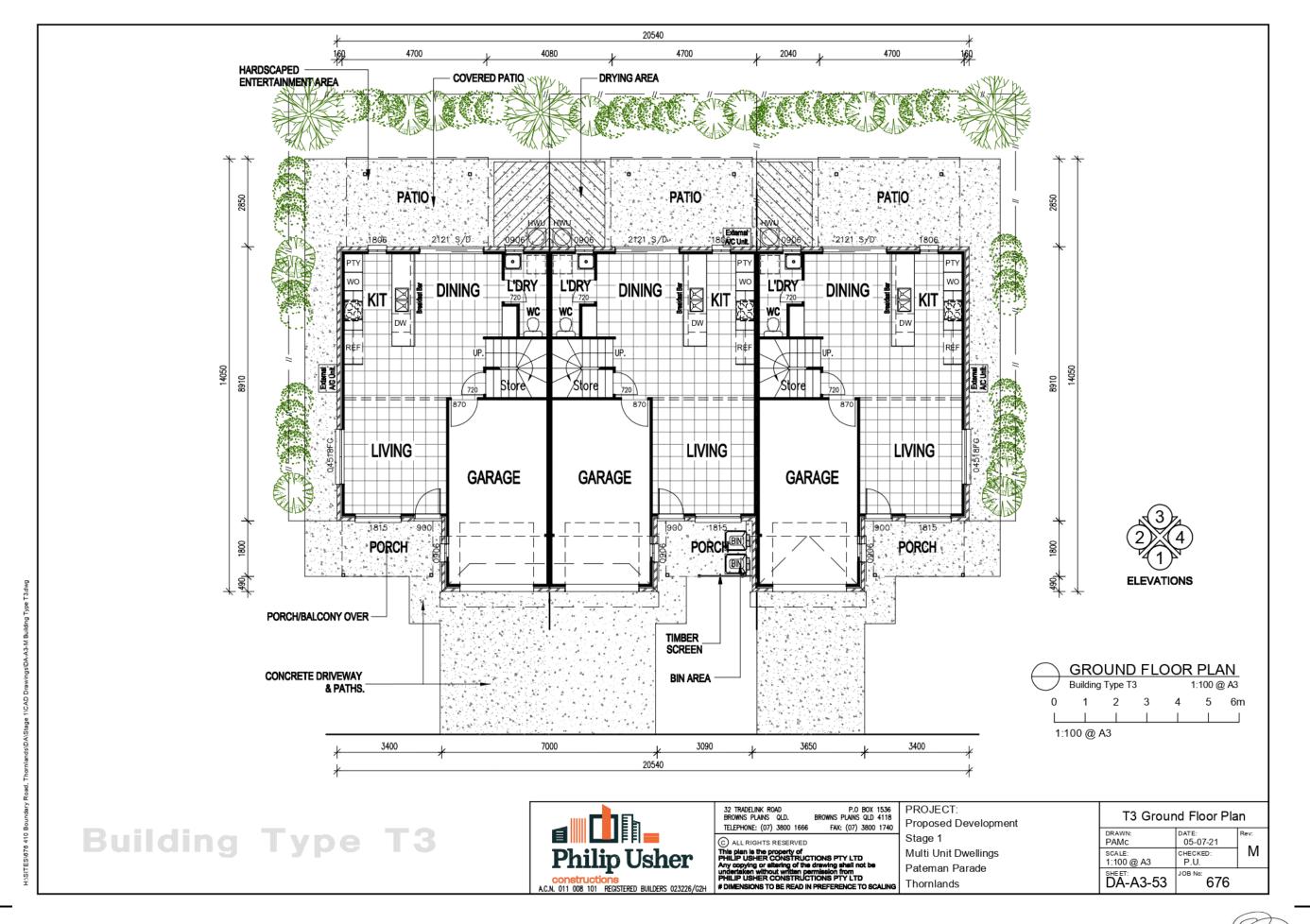


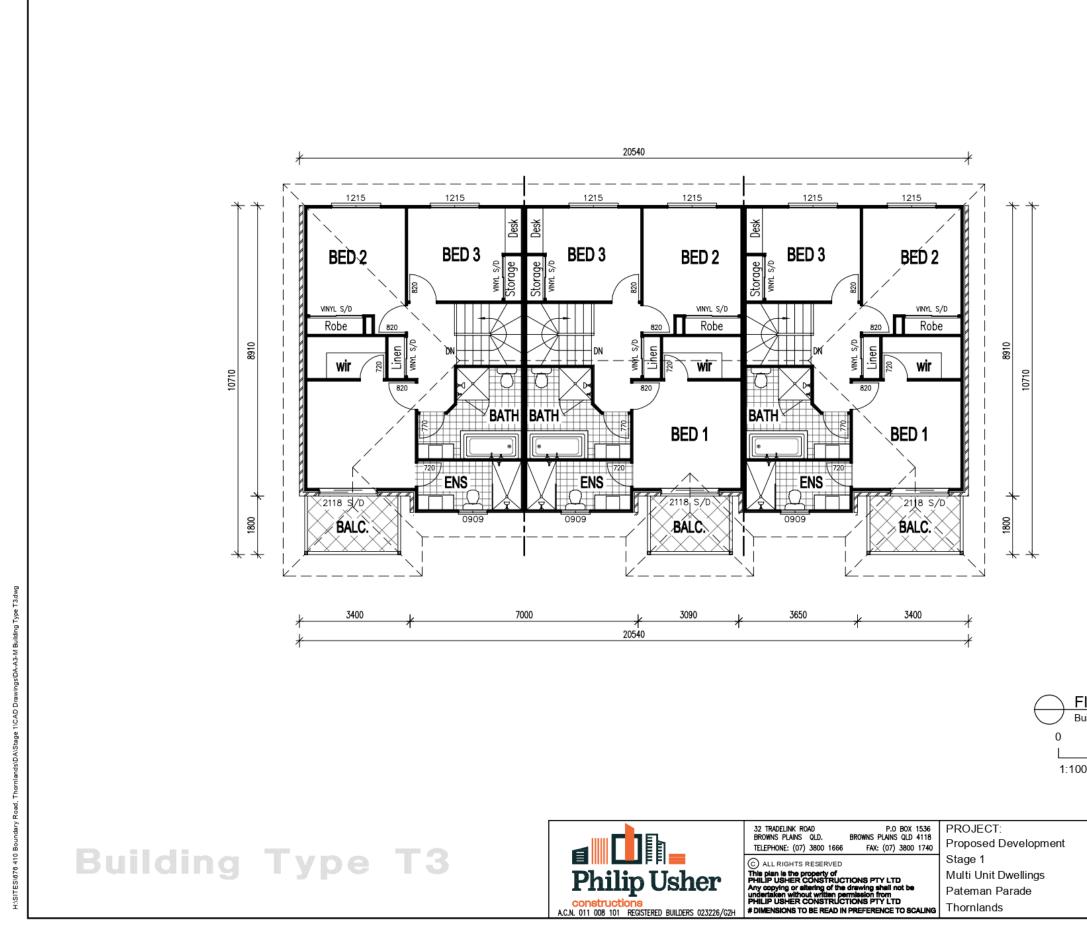


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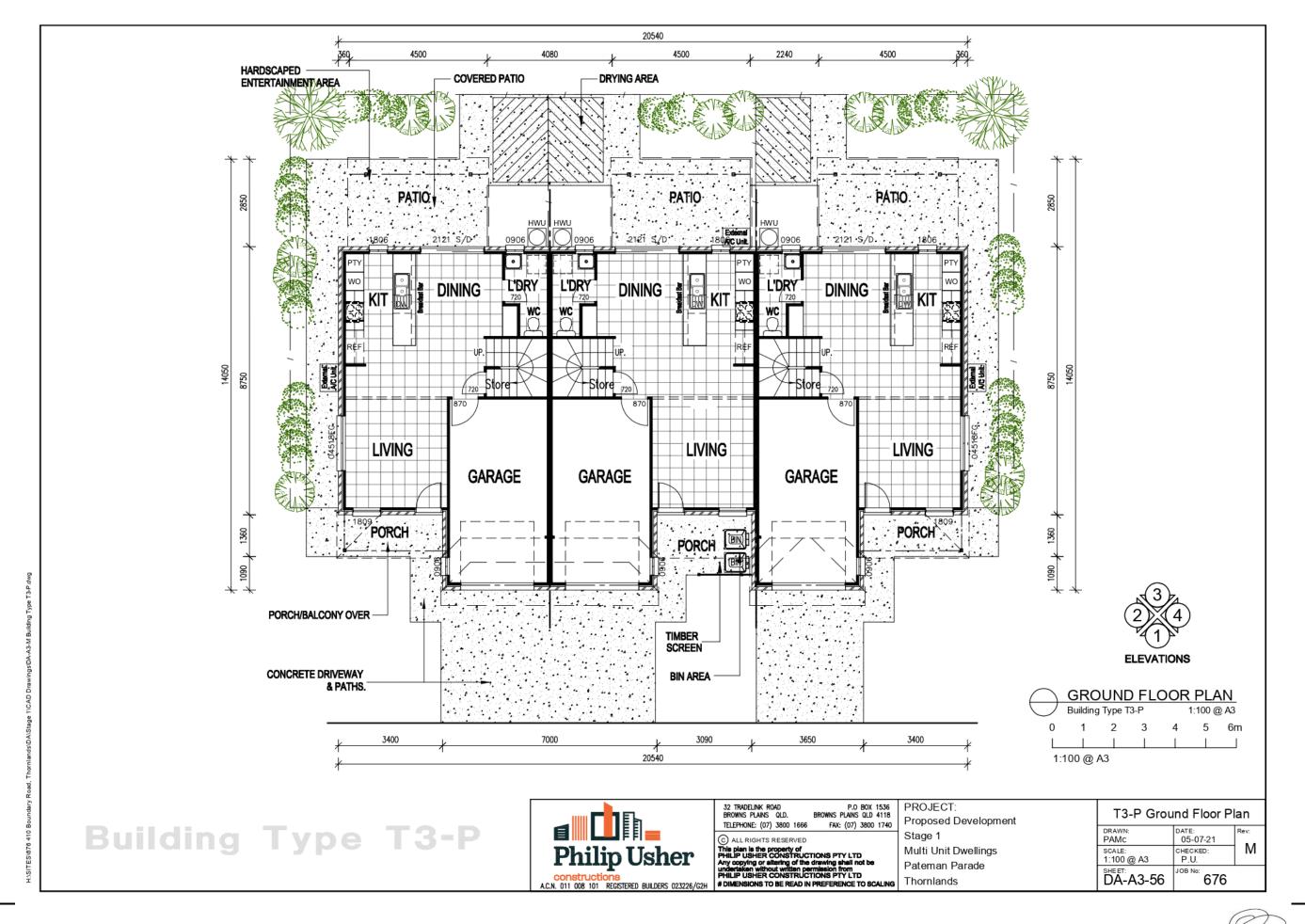


| 2 4 ELEVATIONS | | | |
|--|--|--|-----------|
| IRST FLOOR PLAN uilding Type T3 1:100 @ A3 1 2 3 4 5 6m I I I I I I I 0 @ A3 | | | |
| | DRAWN: PAMC SCALE: 1:100 @ A3 SHEET: | DATE: 05-07-21 CHECKED: P.U. JOB No: | Rev: M |
| | DA-A3-54 | 676 | |

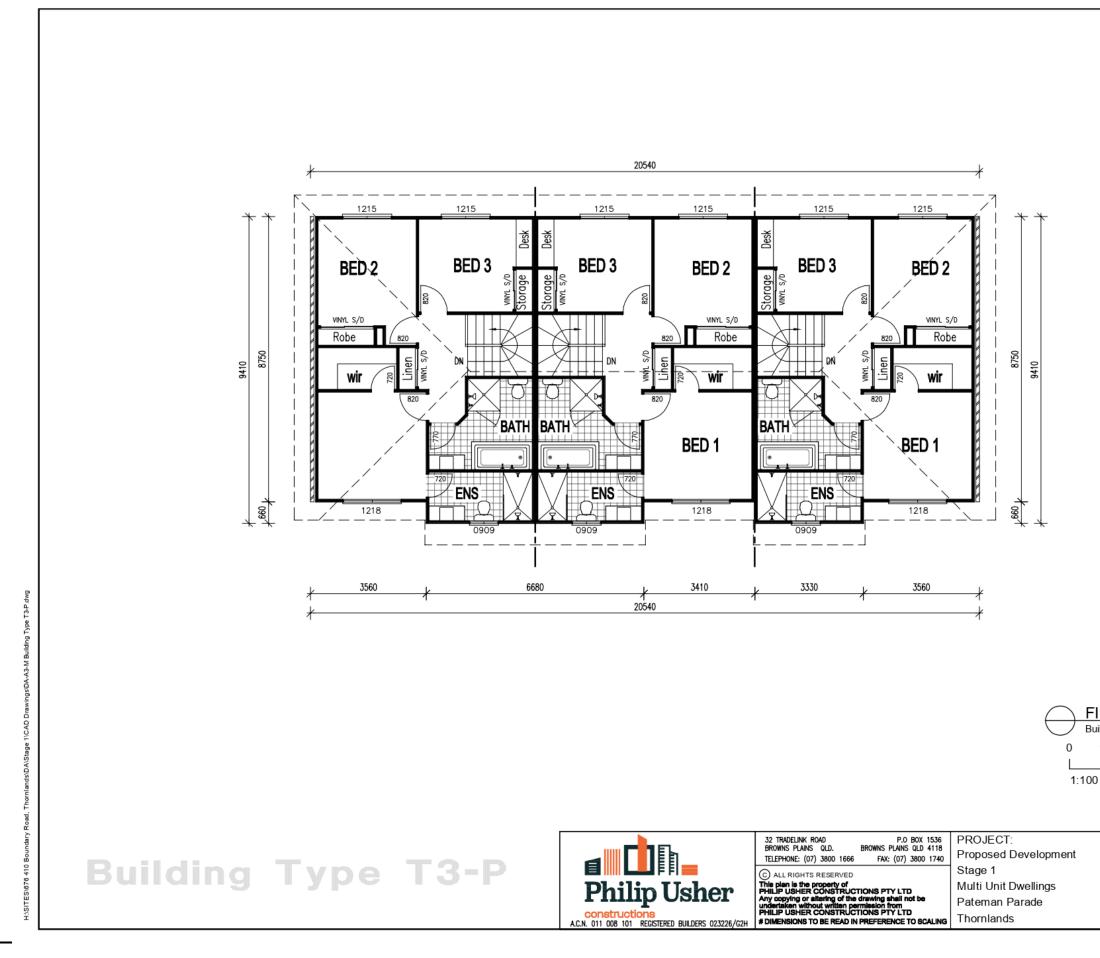
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|--|---|--|
| | | |
| IRST FLO uilding Type T3-P 1 2 | 0R PLAN 1:100@ 3 4 5 | @ A3 6m |
| - | T3-P Firs DRAWN: PAMC SCALE: 1:100 @ A3 SHEET: DA-A3-57 | t Floor Plan 05-07-21 CHECKED: P.U. JOB No: 676 |
| | | |

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DEVELOPMENT APPLICATION

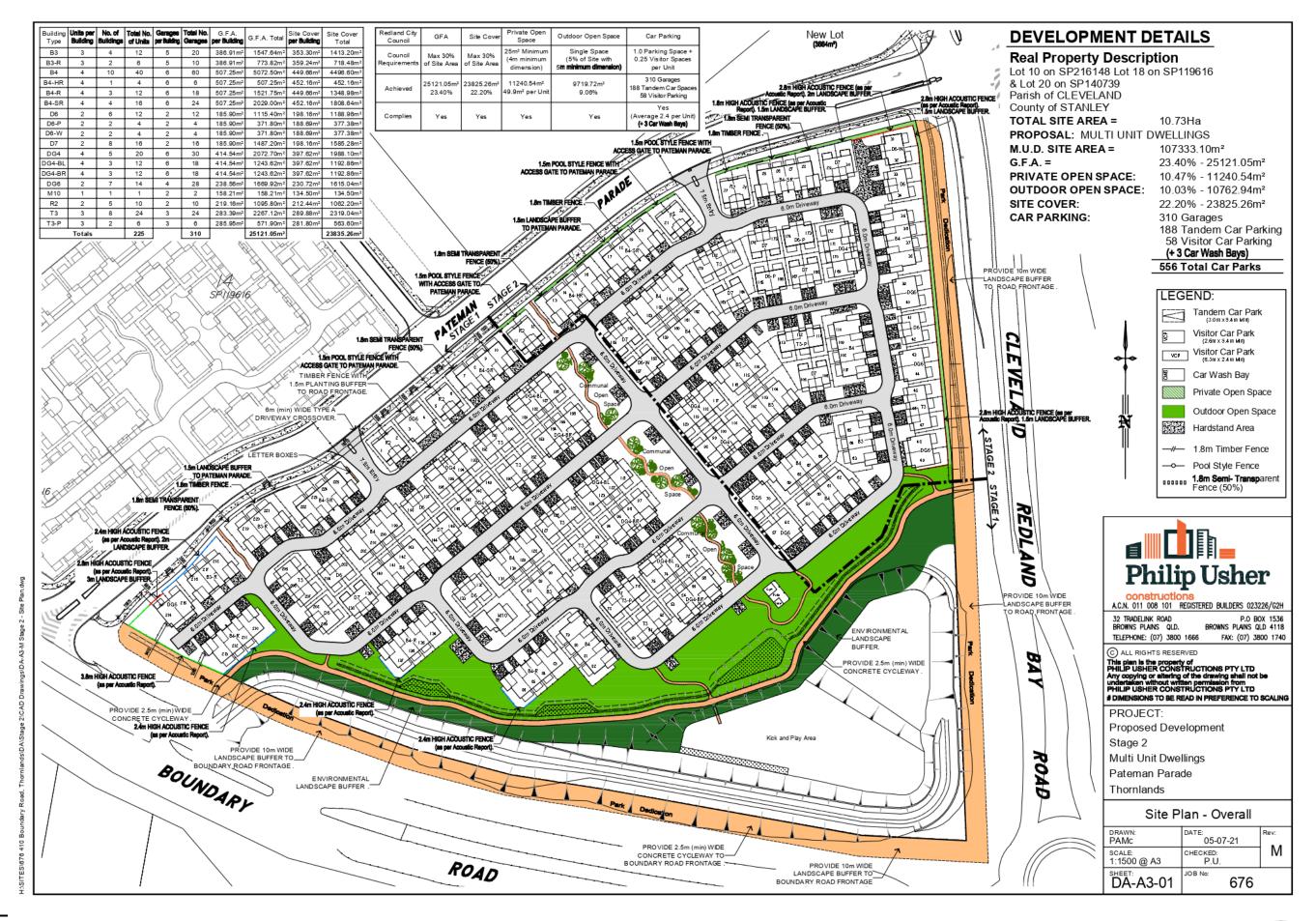
Multi Unit Development (Stage 2 - 106 Units) Pateman Parade, Thornlands Lot 10 on SP216148, Lot 18 on SP119616 and Lot 20 on SP140739 Application Reference: **MCU17/0058**

| м | M RFI Amendments. | | | |
|-------|---|---|--|----------|
| L | RFI Amendments. | | | 12-05-2 |
| к | Unit Designs Updated - Layout Adjusted to | Suit. | | 12-03-2 |
| J | Open Space Area's Adjusted. | | | 16-11-20 |
| 1 | RFI Amendments. | | | 09-10-20 |
| Н | Acoustic Fences Added. Unit 29-30 Design Cl | hange to Suit Acoustic Fence. | | 19-06-20 |
| G | G Various Amendments. | | | 13-05-20 |
| F | F Road Truncations Updated. Layout Amended to Suit. | | | 19-07-18 |
| E | E Swept Path Analysis Added. | | | 10-07-18 |
| D | D Units Addressing Beveridge Road Amended. | | | |
| С | C Units Addressing Beveridge Road Amended. | | | |
| В | B Wetland, New Lot and Tables Areas Added. | | | |
| Α | Original DA Issue | | | 28-08-17 |
| Issue | Amendment | | | Date |
| | Philip Usher | 32 TRADELINK ROAD P.O BOX 1536 BROWNS PLAINS QLD. BROWNS PLAINS QLD 4118 TELEPHONE: (07) 3800 1666 FAX: (07) 3800 1740 C ALL RIGHTS RESERVED This plan is the property of PHILIP USHER CONSTRUCTIONS PTY LTD Any copying or altering of the drawing shell not be undertaken without written permission from PHILIP USHER CONSTRUCTIONS PTY LTD ANY COPYING TO BE READ IN PREFERENCE TO SCALING # DIMENSIONS TO BE READ IN PREFERENCE TO SCALING | PROJECT: Proposed Developm Stage 2 Multi Unit Dwellings Pateman Parade Thornlands | ent |

| | L | SCALE: | CHECKED: | M | |
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| | - 1 | DRAWN: PAMC | DATE: 05-07-21 | Rev: | |
| | ļ | Cover Sheet | | | |
| Starring Pill. | Basue | | | | |
| DA-A3-01 Drawing No. | M Issue | Site Plan - Overall Description | | | |
| DA-A3-02 | M | Open Space - Overall | | | |
| DA-A3-03 | M | Site Plan | | | |
| DA-A3-04 | M | Open Space | | | |
| DA-A3-05 | М | Earthworks | | | |
| DA-A3-06 | Μ | Waste Management | | | |
| DA-A3-07 | М | Streetscape | | | |
| DA-A3-08 | М | B3 Ground Floor | | | |
| DA-A3-09 | М | B3 First Floor | | | |
| DA-A3-10 | М | B3 Elevations Sh | t 1 | | |
| DA-A3-11 | М | B3 Elevations Sh | t 2 | | |
| DA-A3-12 | М | B4 Ground Floor | | | |
| DA-A3-13 | М | B4 First Floor | | | |
| DA-A3-14 | М | B4 Elevations Sh | t 1 | | |
| DA-A3-15 | М | B4 Elevations Sh | t 2 | | |
| DA-A3-16 | М | B4-HR Ground F | loor | | |
| DA-A3-17 | М | B4-HR First Floor | | | |
| DA-A3-18 | М | B4-HR Elevation | s Sht 1 | | |
| DA-A3-19 | м | B4-HR Elevation | s Sht 2 | | |
| DA-A3-20 | М | B4-SR Ground F | oor | | |
| DA-A3-21 | М | B4-SR First Floor | B4-SR First Floor | | |
| DA-A3-22 | м | B4-SR Elevation | s Sht 1 | | |
| DA-A3-23 | М | B4-SR Elevation | s Sht 2 | | |
| DA-A3-24 | М | D6 Ground Floor | | | |
| DA-A3-25 | М | D6 First Floor | | | |
| DA-A3-26 | М | D6 Elevations | | | |
| DA-A3-27 | М | D6-P Ground Flo | or | | |
| DA-A3-28 | М | D6-P First Floor | | | |
| DA-A3-29 | М | D6-P Elevations | | | |
| DA-A3-30 | М | D6-W Ground Floor | | | |
| DA-A3-31 | М | D6-W First Floor | | | |
| DA-A3-32 | М | D6-W Elevations | | | |
| DA-A3-33 | М | D7 Ground Floor | | | |
| DA-A3-34 | М | D7 First Floor | | | |
| DA-A3-35 | М | D7 Elevations | | | |
| DA-A3-36 | М | DG4 Ground Floo | or | | |
| DA-A3-37 | M | DG4 First Floor | | | |
| DA-A3-38 | M | DG4 Elevations S | | | |
| DA-A3-39 | M | DG4 Elevations \$ | | | |
| DA-A3-40 | M | DG6 Ground Floo | or | | |
| DA-A3-41 | M | DG6 First Floor | | | |
| DA-A3-42 | M | DG6 Elevations | | | |
| DA-A3-43 | M | R2 Floor Plans | | | |
| DA-A3-44 | M | R2 Elevations | | | |
| DA-A3-45 | M | T3 Ground Floor | | | |
| DA-A3-46 | M | T3 First Floor | | | |
| DA-A3-40 DA-A3-47 | M | T3 Elevations | | | |
| DA-A3-49 M T3-P First Floor DA-A3-48 M T3-P Ground Floor | | | | | |
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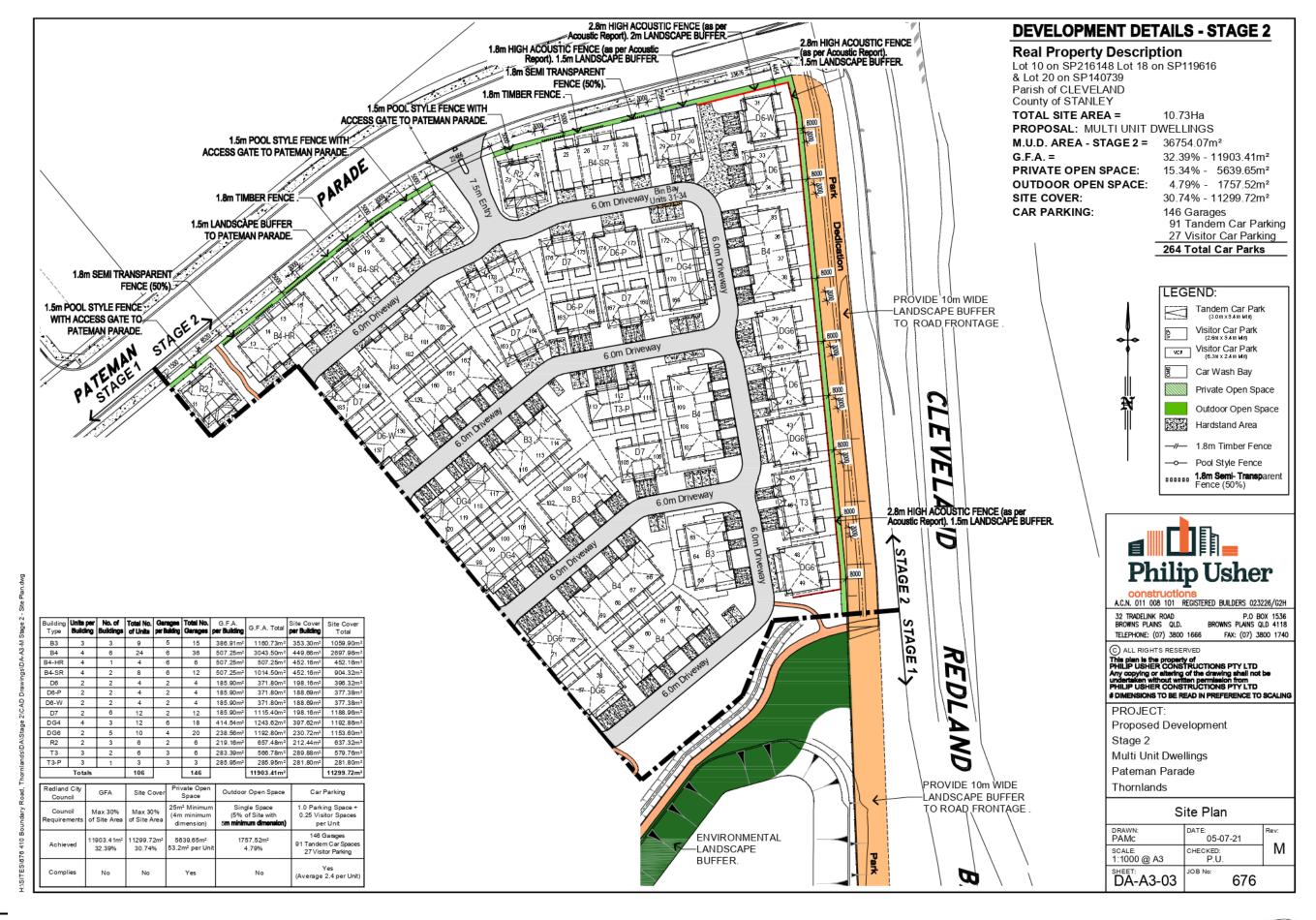
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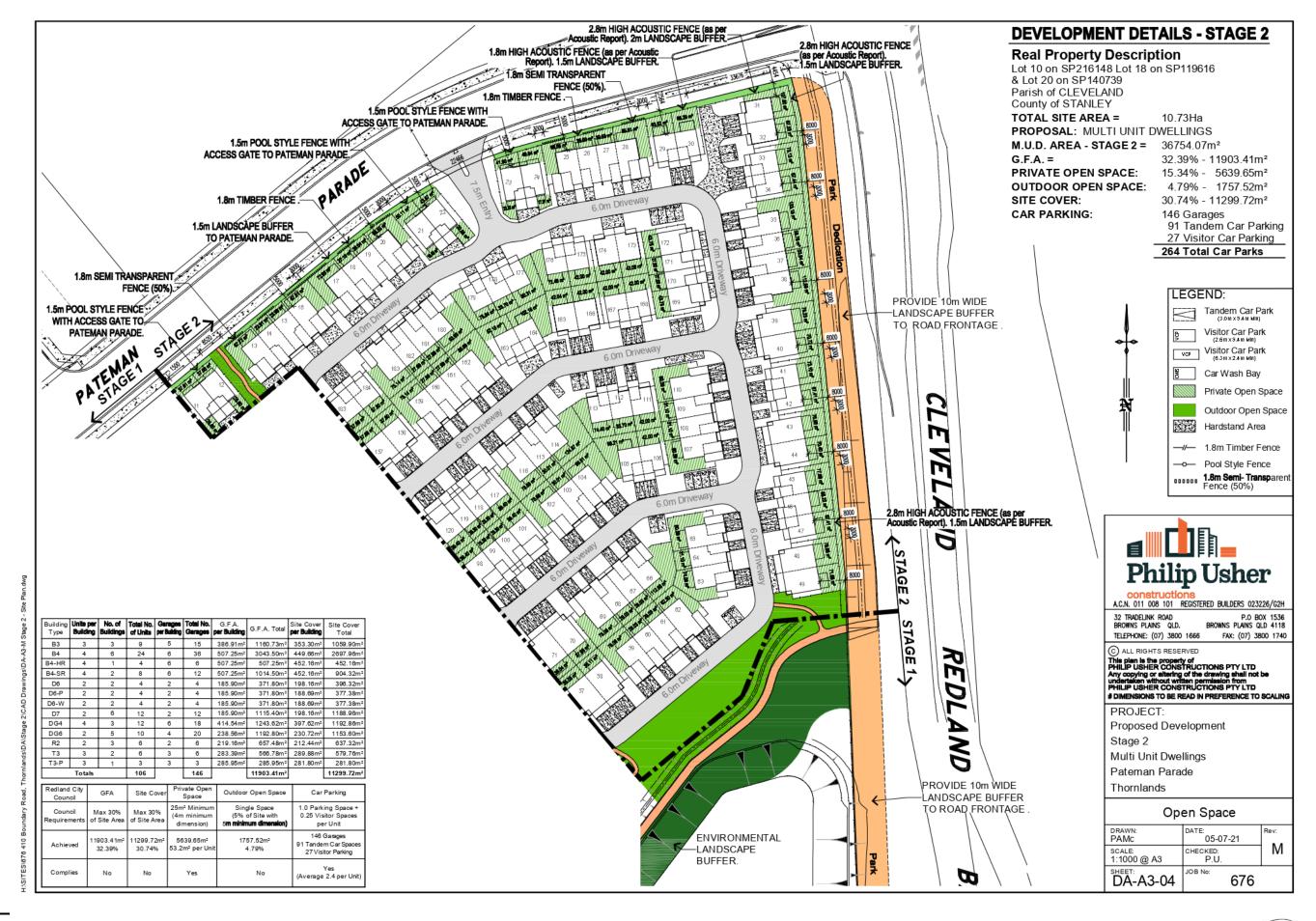
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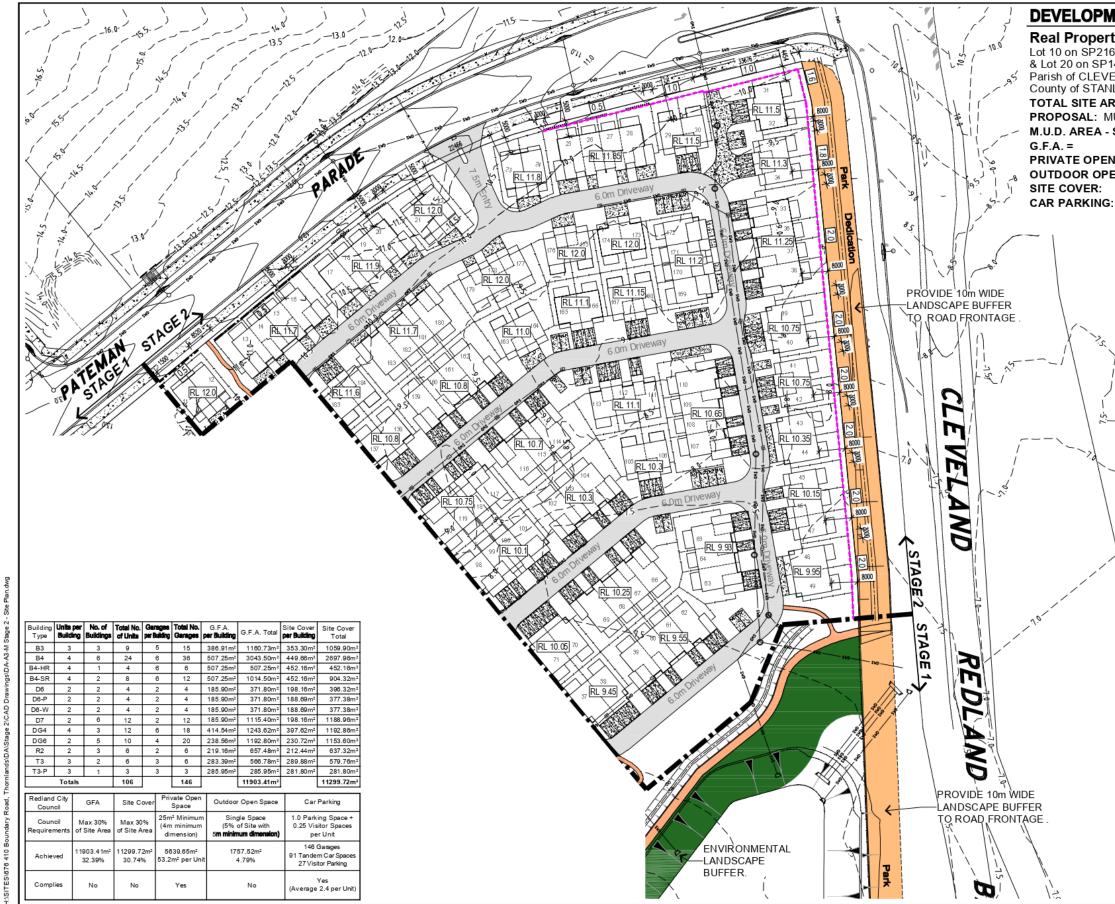


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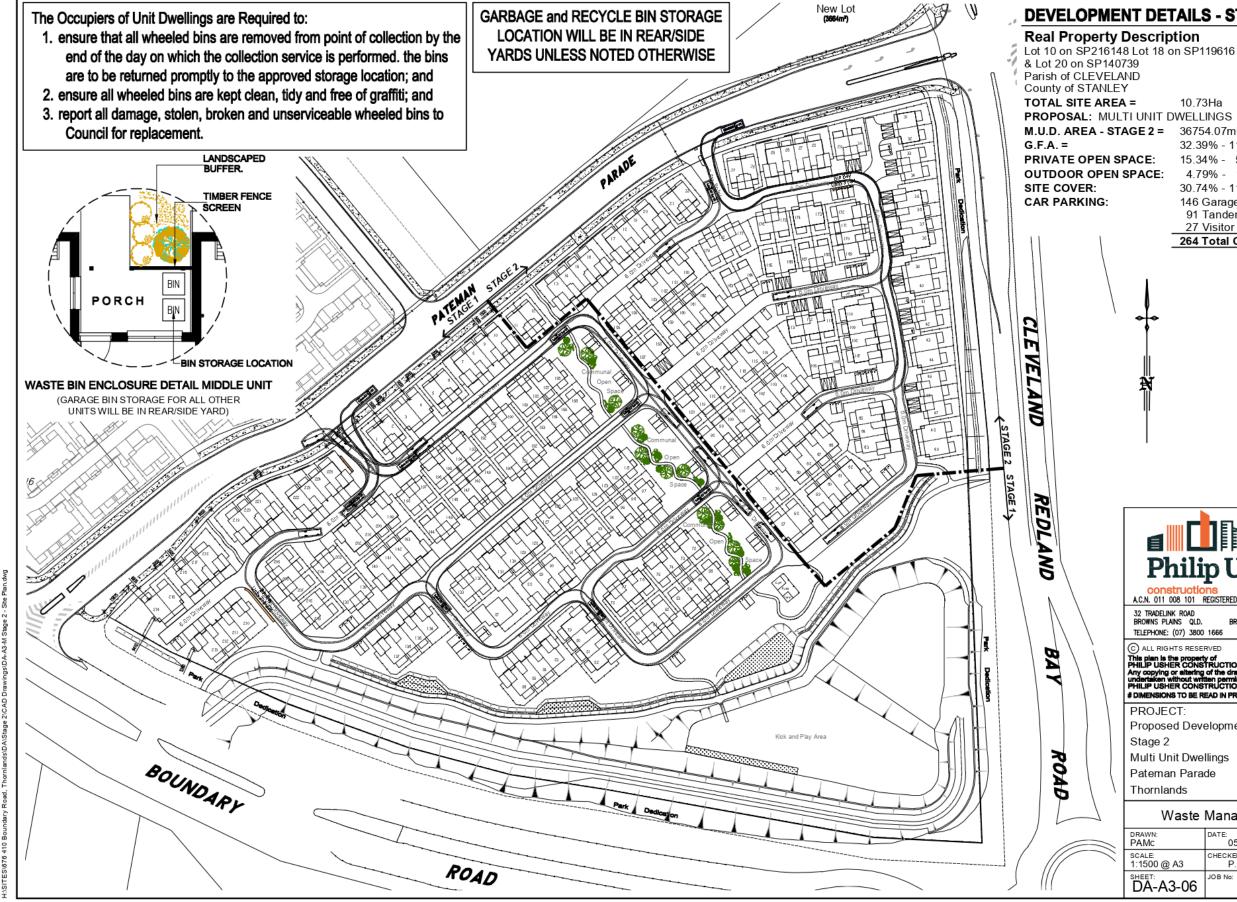


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DEVELOPMENT DETAILS - STAGE 2 Real Property Description Lot 10 on SP216148 Lot 18 on SP119616 & Lot 20 on SP140739 Parish of CLEVELAND County of STANLEY TOTAL SITE AREA = 10.73Ha **PROPOSAL: MULTI UNIT DWELLINGS** M.U.D. AREA - STAGE 2 = 36754.07m² 32.39% - 11903.41m² 15.34% - 5639.65m² PRIVATE OPEN SPACE: OUTDOOR OPEN SPACE: 4.79% - 1757.52m² 30.74% - 11299.72m² 146 Garages 91 Tandem Car Parking 27 Visitor Car Parking 264 Total Car Parks LEGEND: --- Timber Retaining Wall --- Masonary Retaining Wall ____ Retaining Wall RL Earthwork Pad Level 5e **Philip Usher** construc A.C.N. 011 008 101 REGISTERED BUILDERS 023226/G2H 32 TRADELINK ROAD BROWNS PLAINS QLD. P.O BOX 1536 BROWNS PLAINS QLD 4118 TELEPHONE: (07) 3800 1666 FAX: (07) 3800 1740 C ALL RIGHTS RESERVED This plan is the property of PHILIP USHER CONSTRUCTIONS PTY LTD Any copying or altering of the drawing shall not be undertaken without written permission from PHILIP USHER CONSTRUCTIONS PTY LTD # DIMENSIONS TO BE READ IN PREFERENCE TO SCALING PROJECT: Proposed Development Stage 2 Multi Unit Dwellings Pateman Parade Thornlands Earthworks DRAWN PAMc 05-07-21 Μ scale: 1:1000 @ A3 CHECKED: P.U. JOB No DA-A3-05 676



DEVELOPMENT DETAILS - STAGE 2

Real Property Description

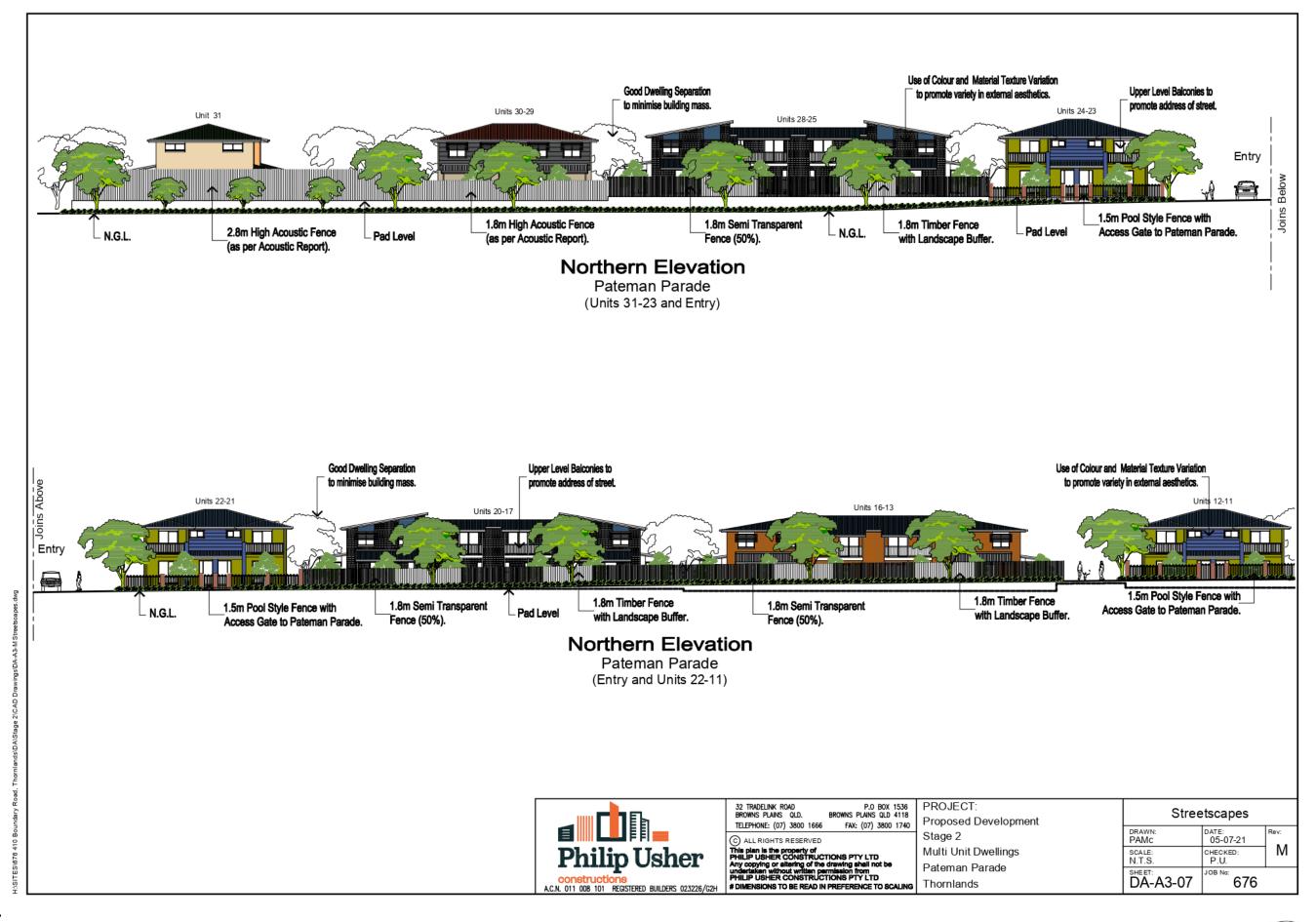
& Lot 20 on SP140739 Parish of CLEVELAND TOTAL SITE AREA = 10.73Ha **PROPOSAL: MULTI UNIT DWELLINGS** M.U.D. AREA - STAGE 2 = 36754.07m² 32.39% - 11903.41m²

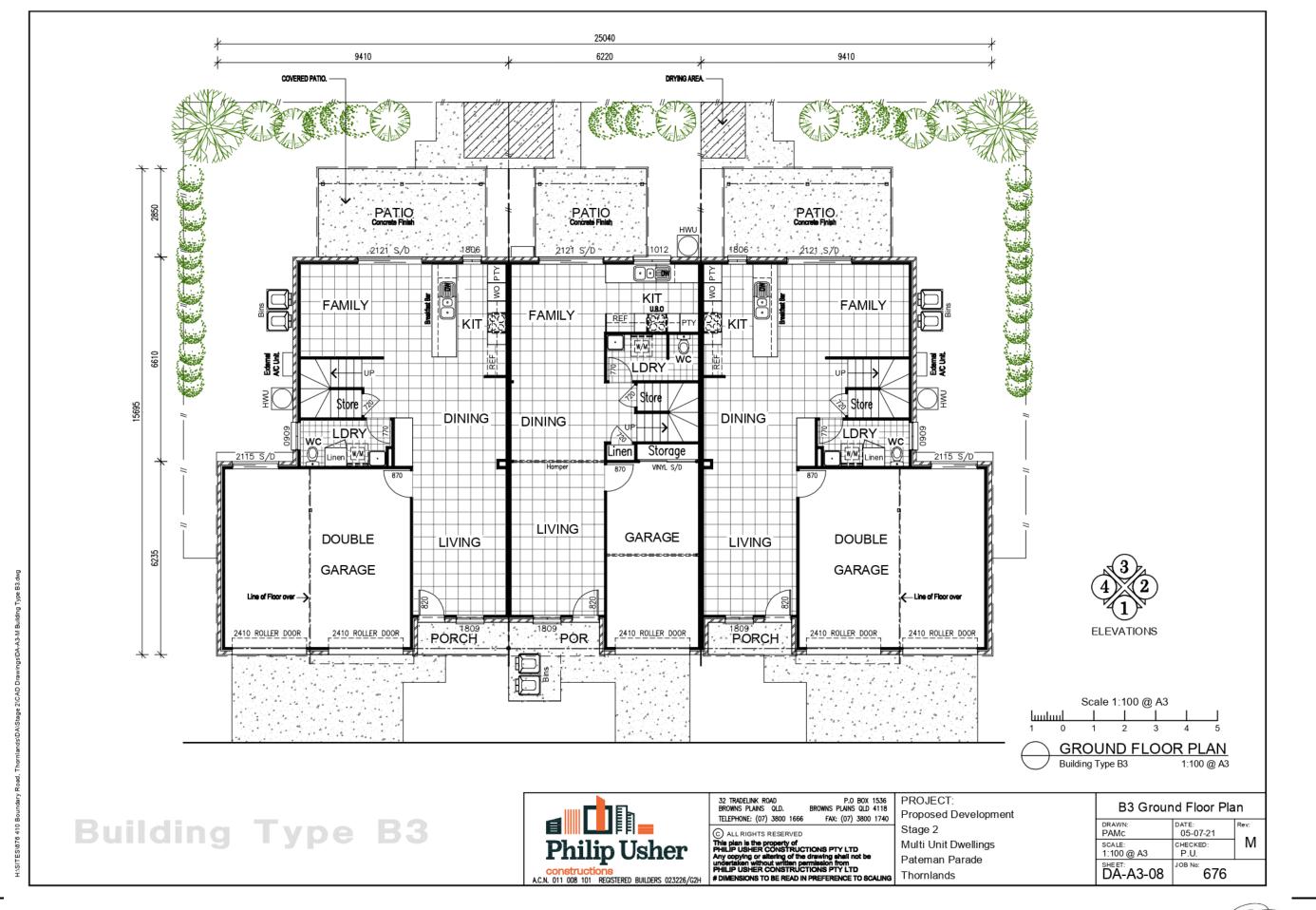
OUTDOOR OPEN SPACE:

15.34% - 5639.65m² 4.79% - 1757.52m² 30.74% - 11299.72m² 146 Garages 91 Tandem Car Parking 27 Visitor Car Parking 264 Total Car Parks

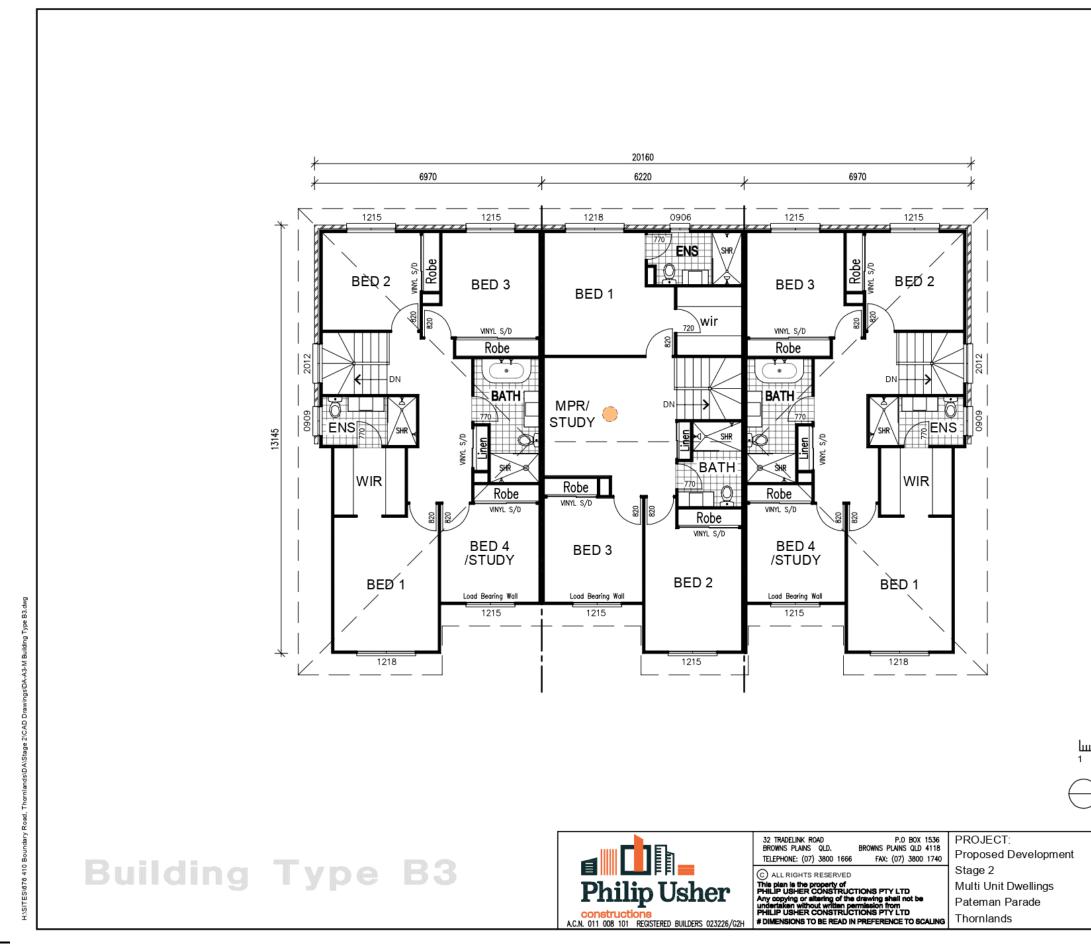
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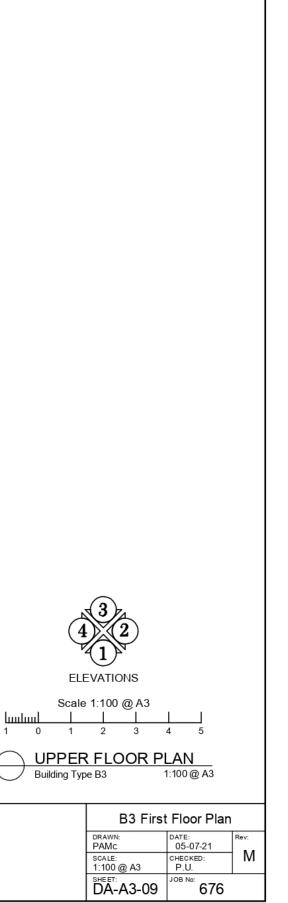
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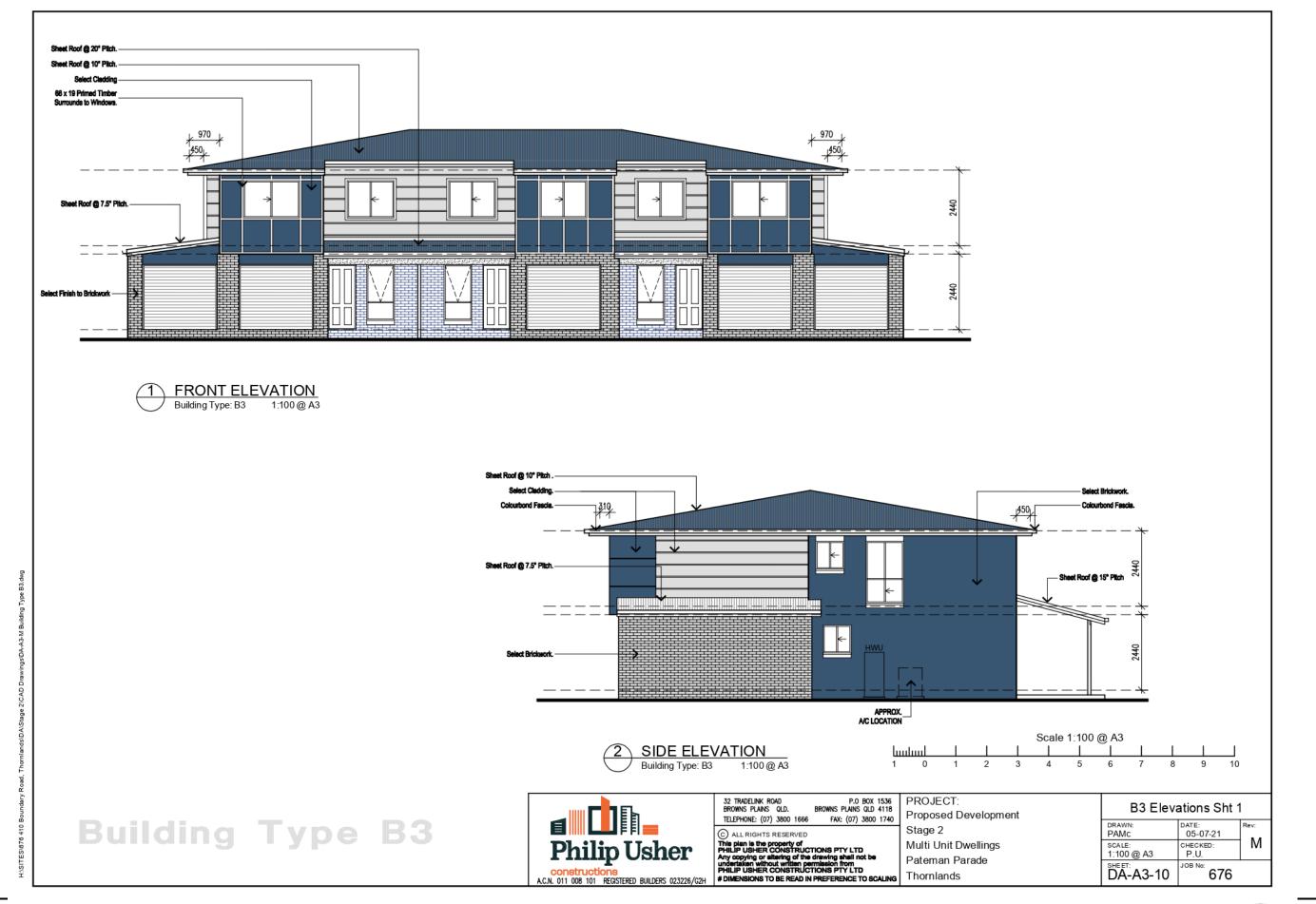


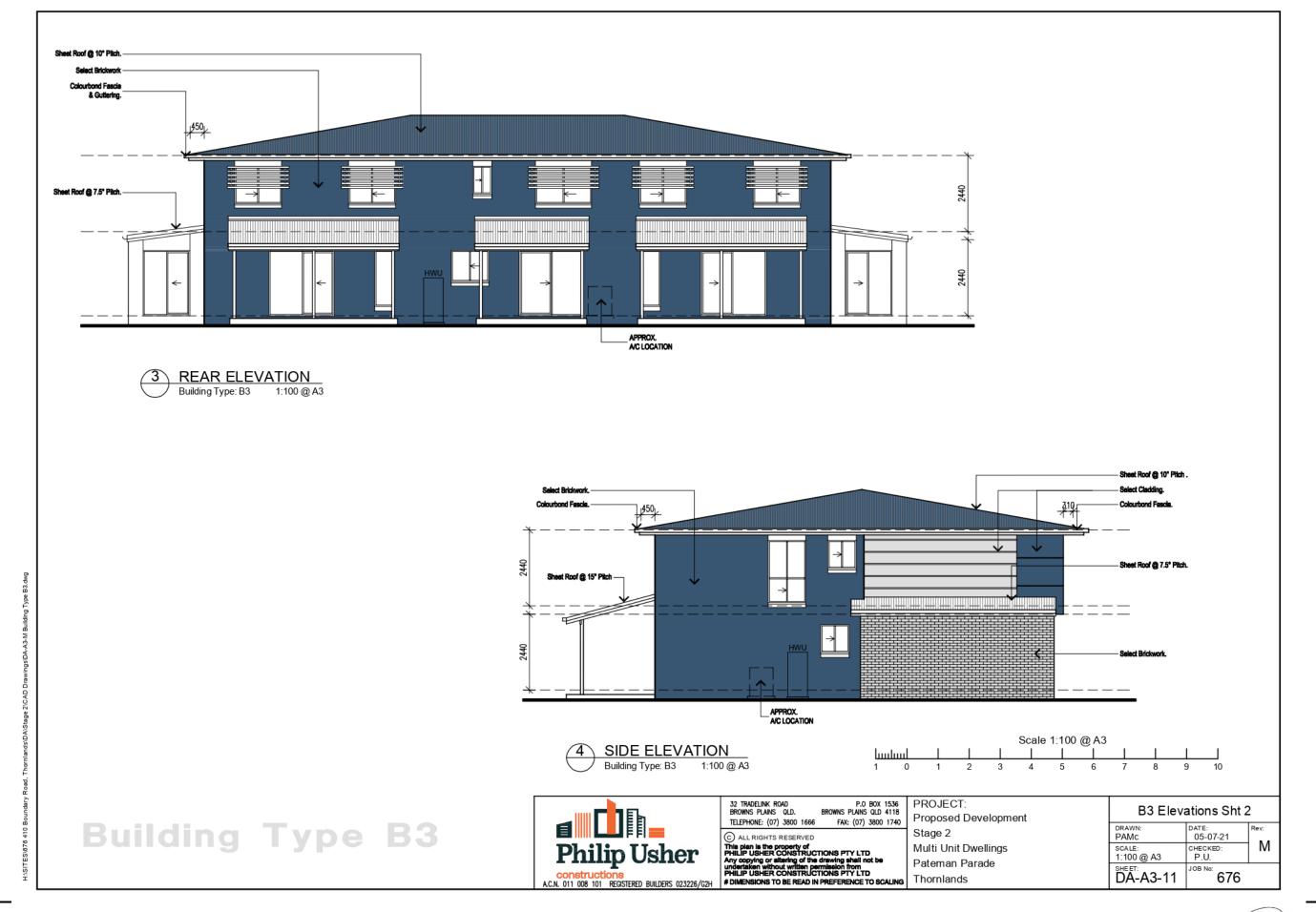


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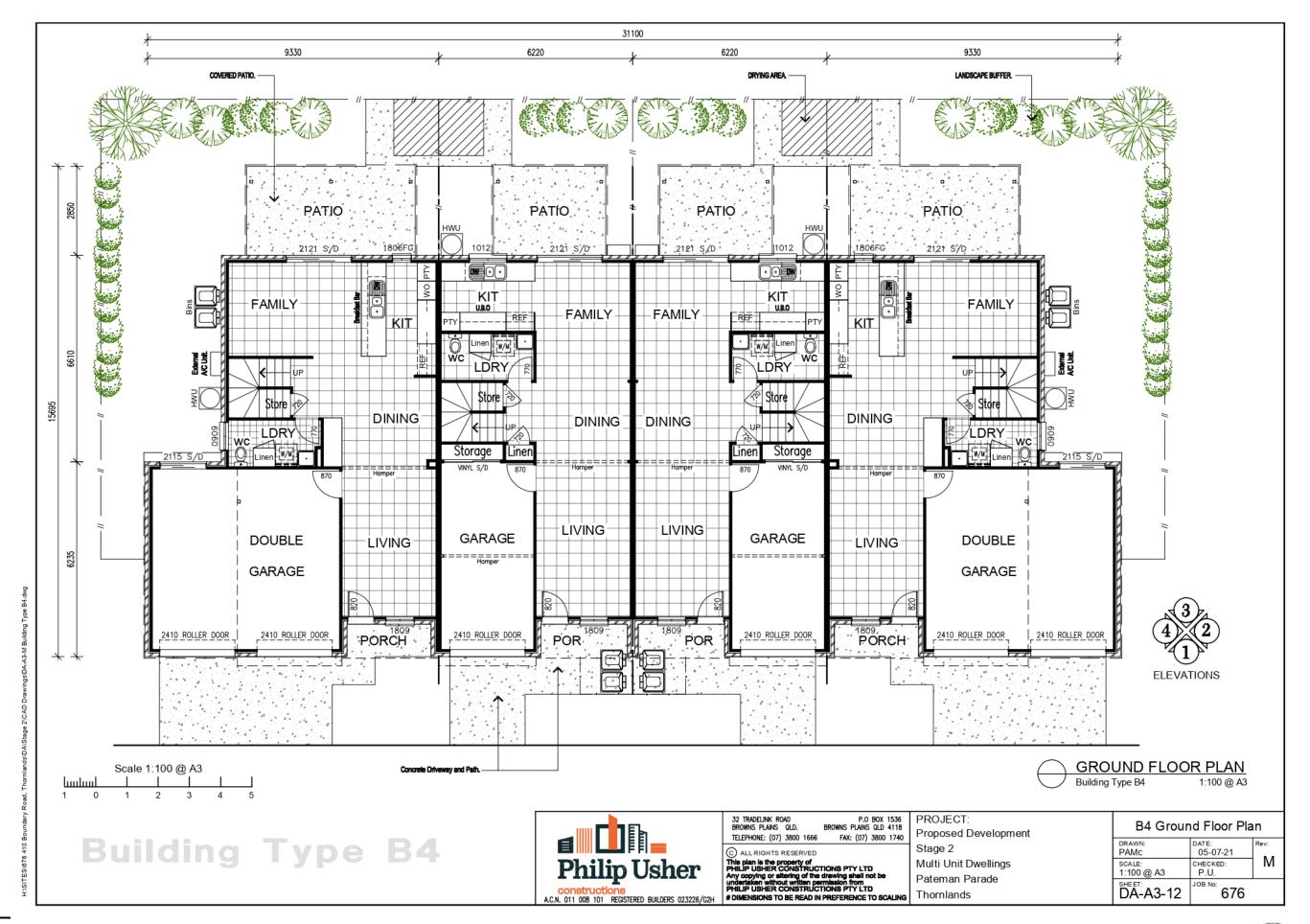


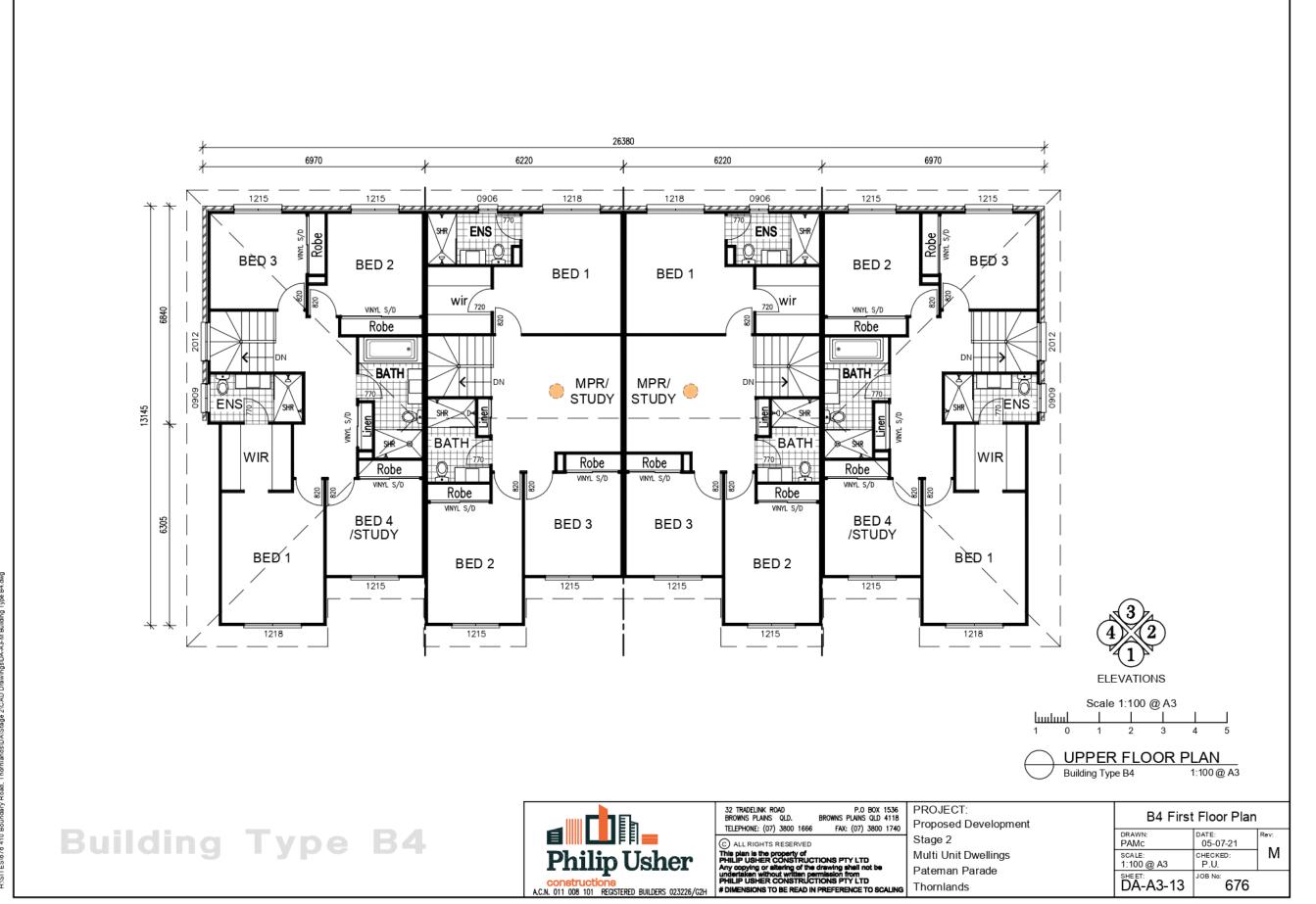


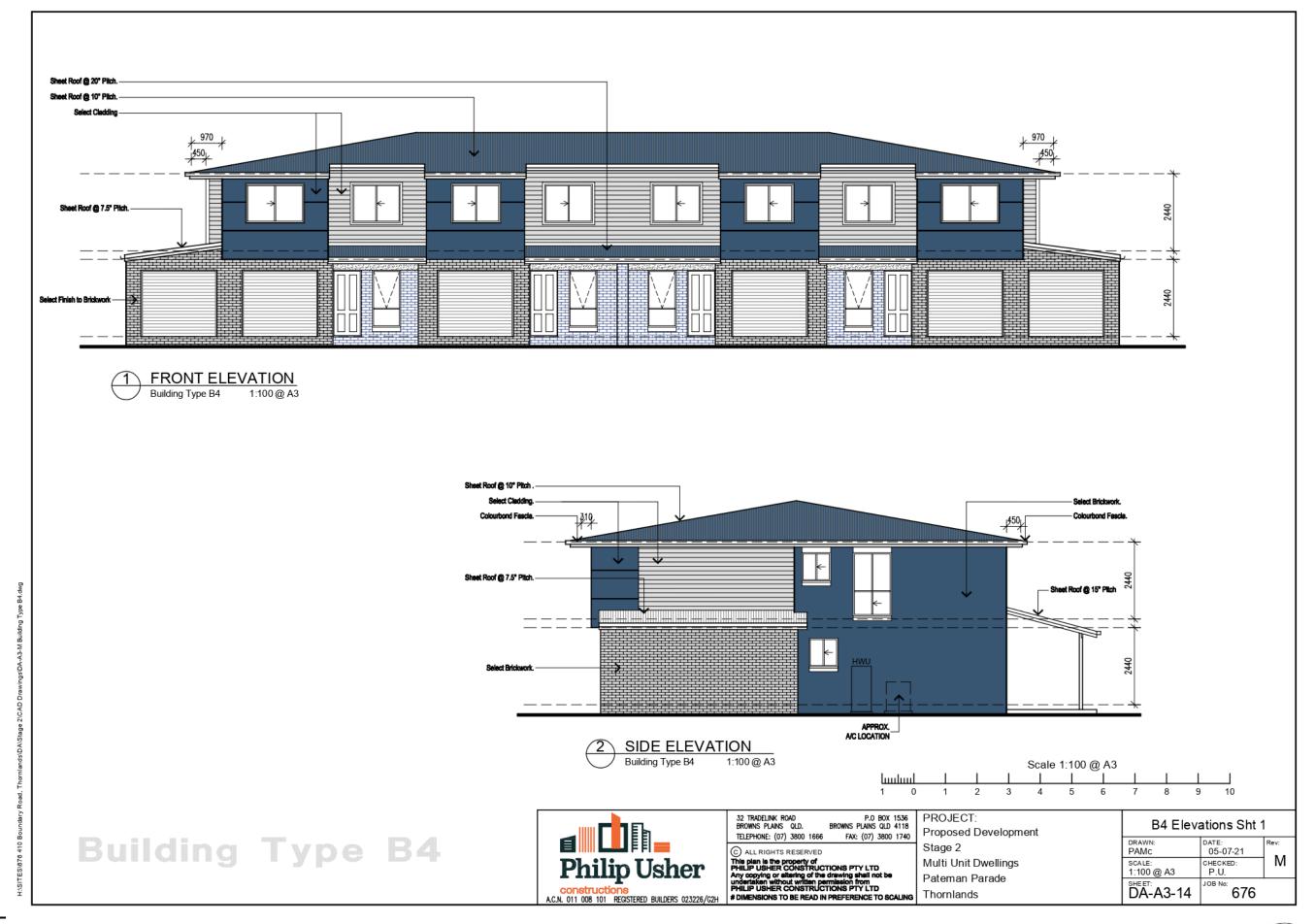




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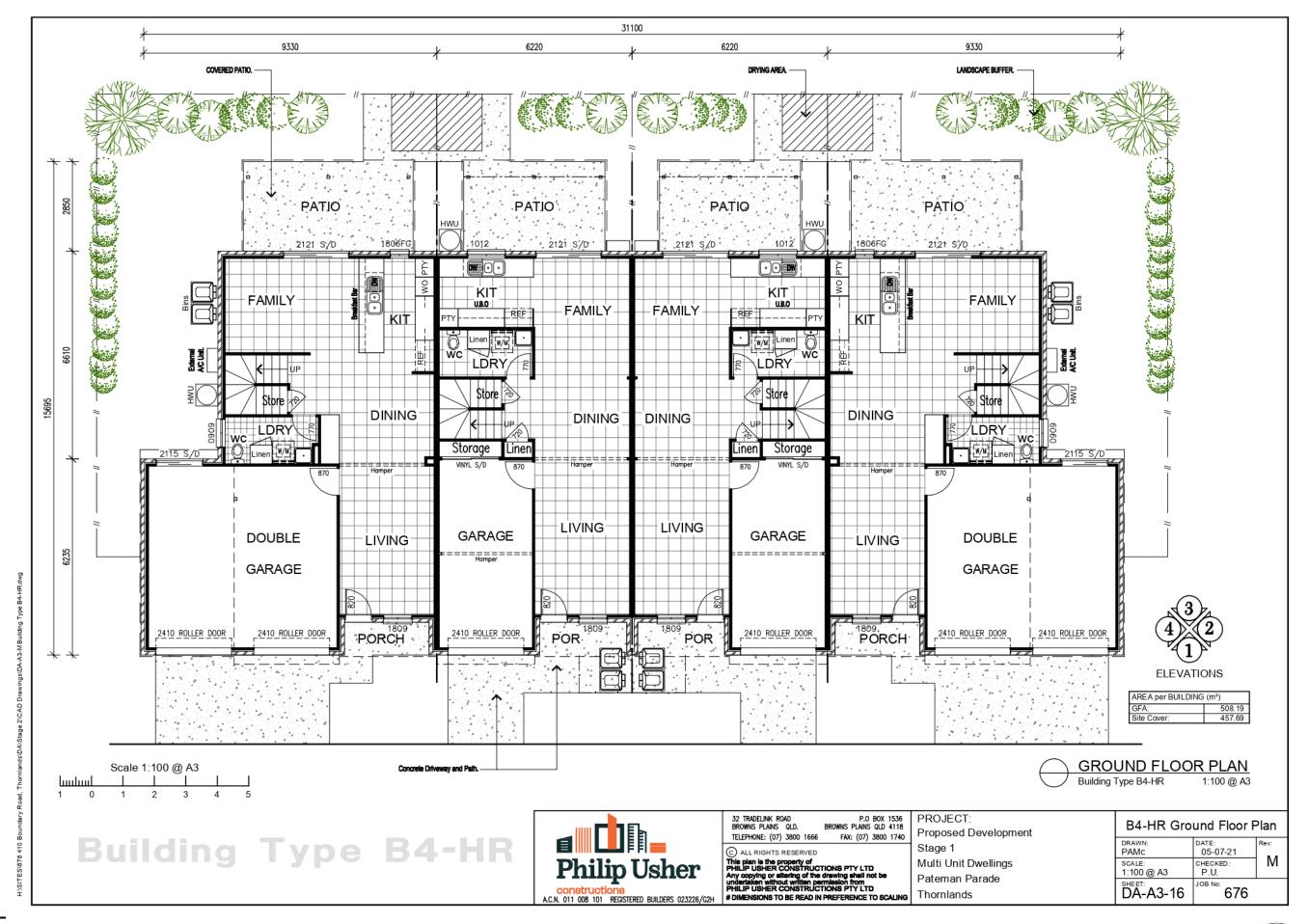


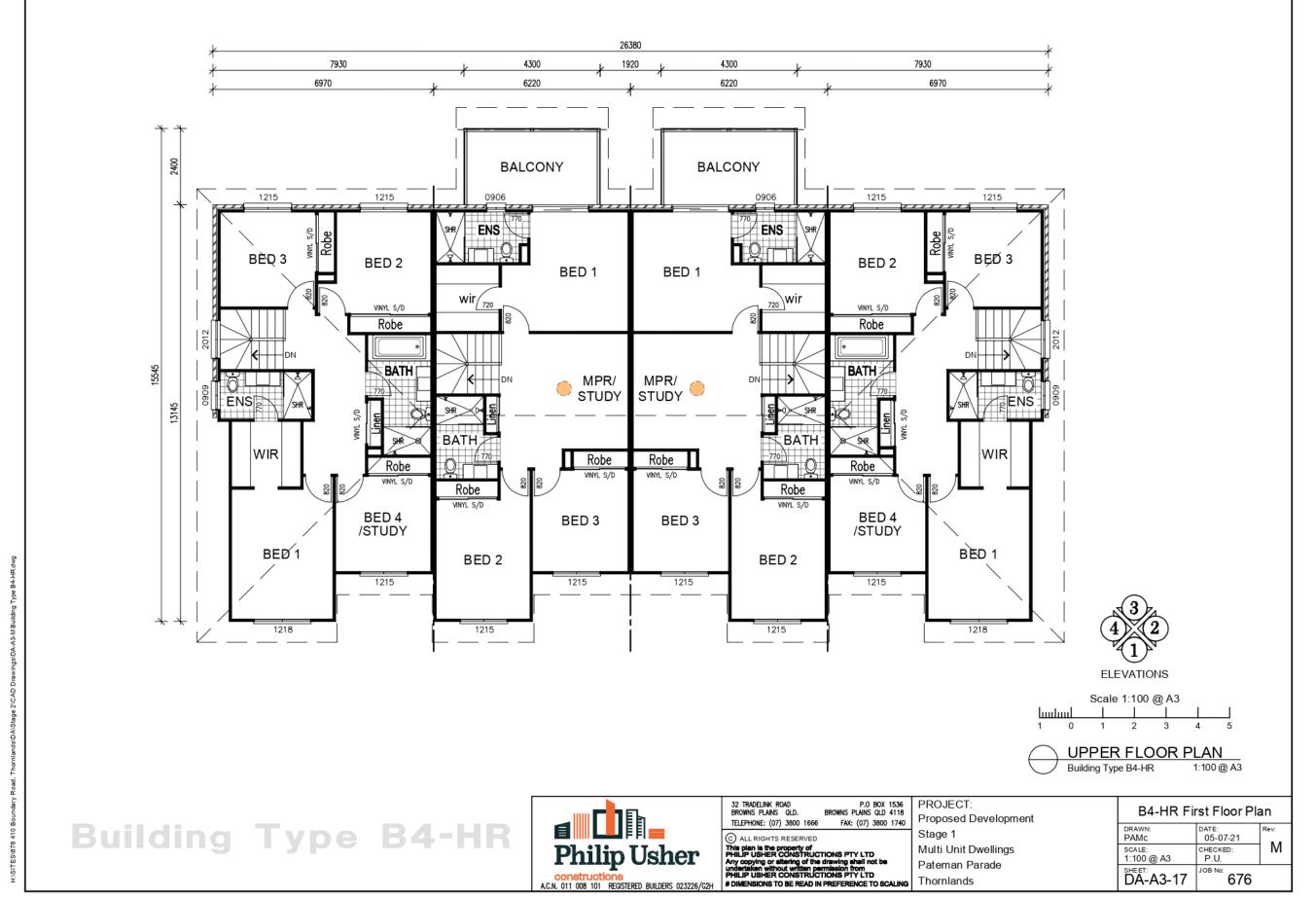


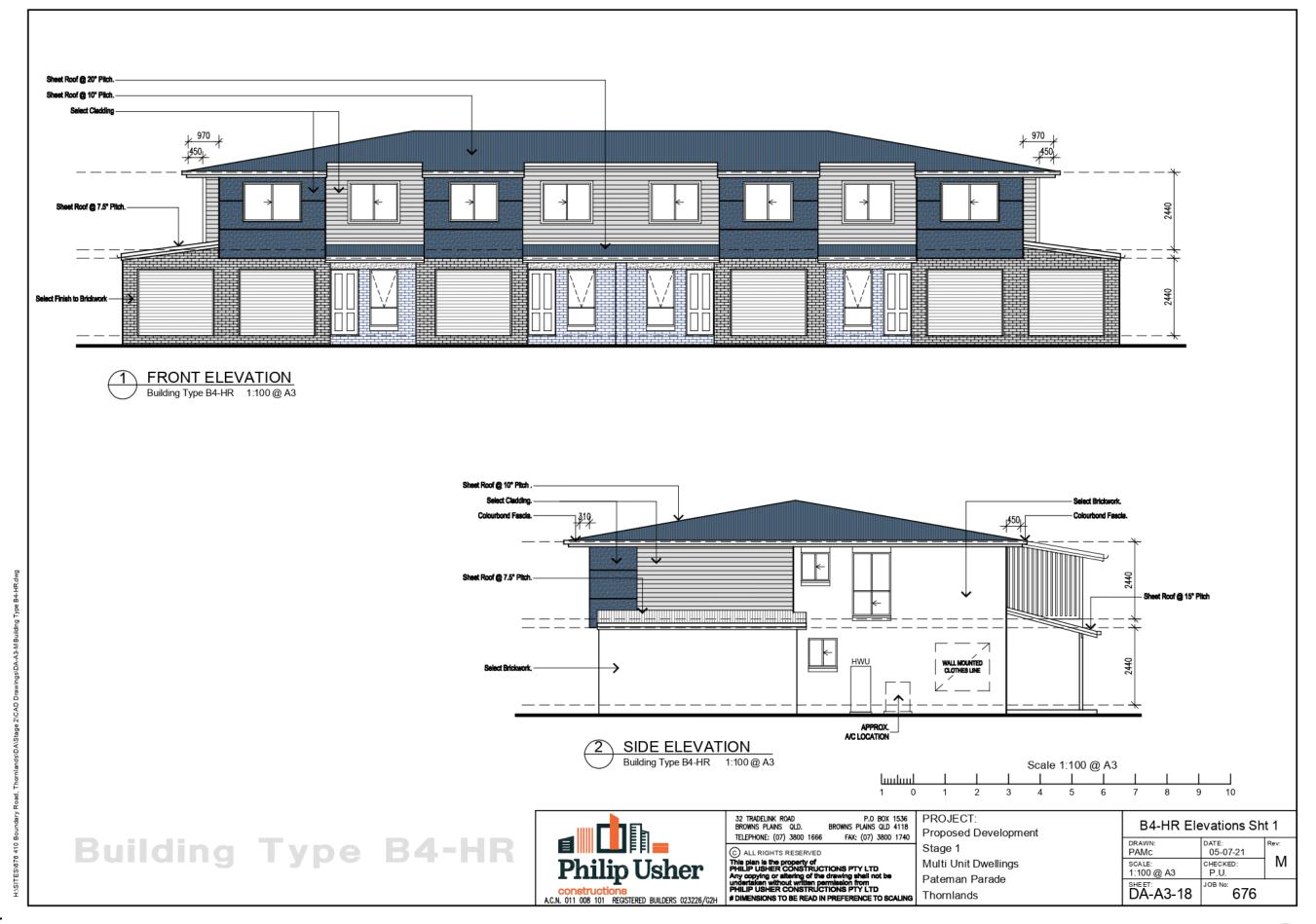
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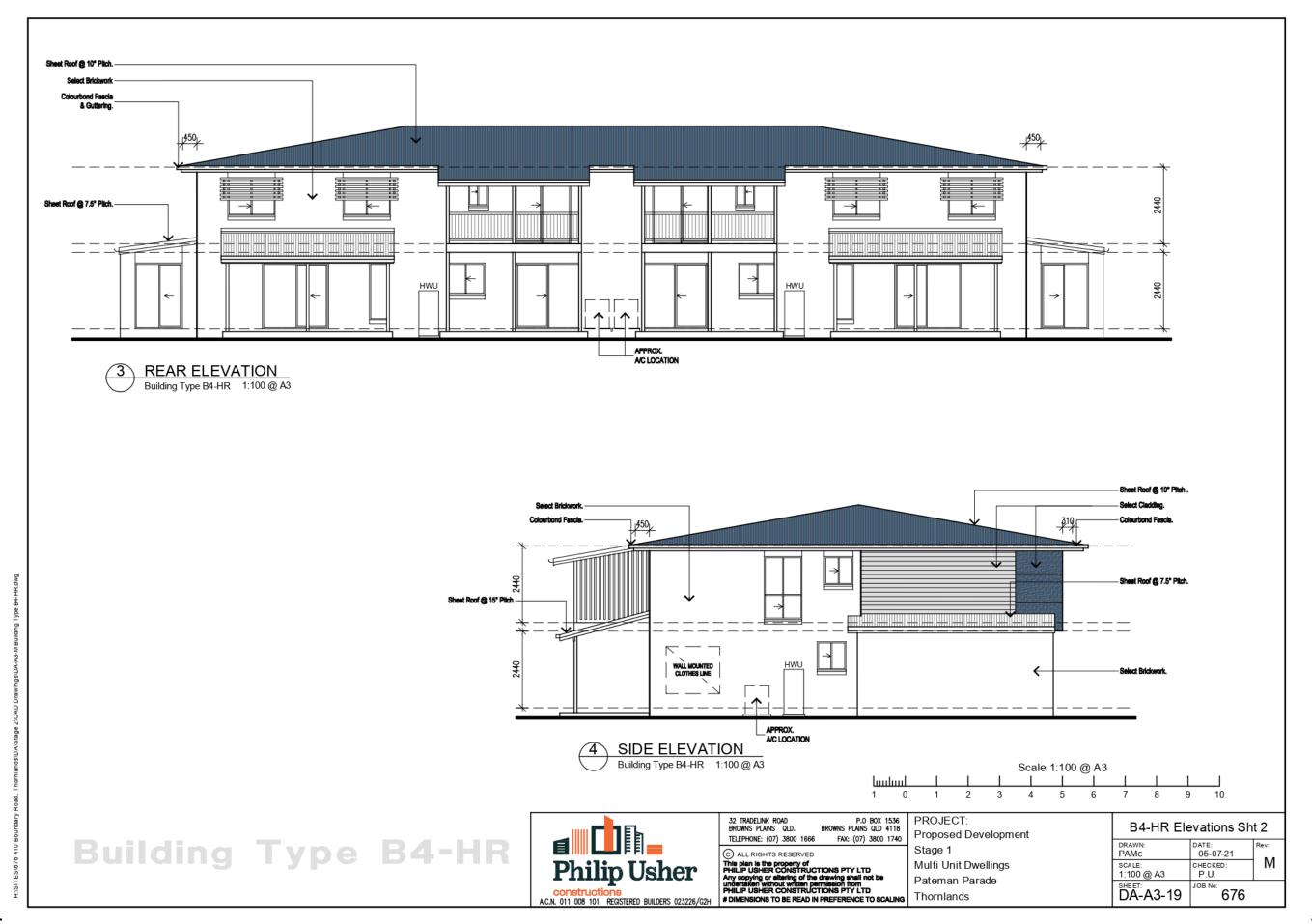


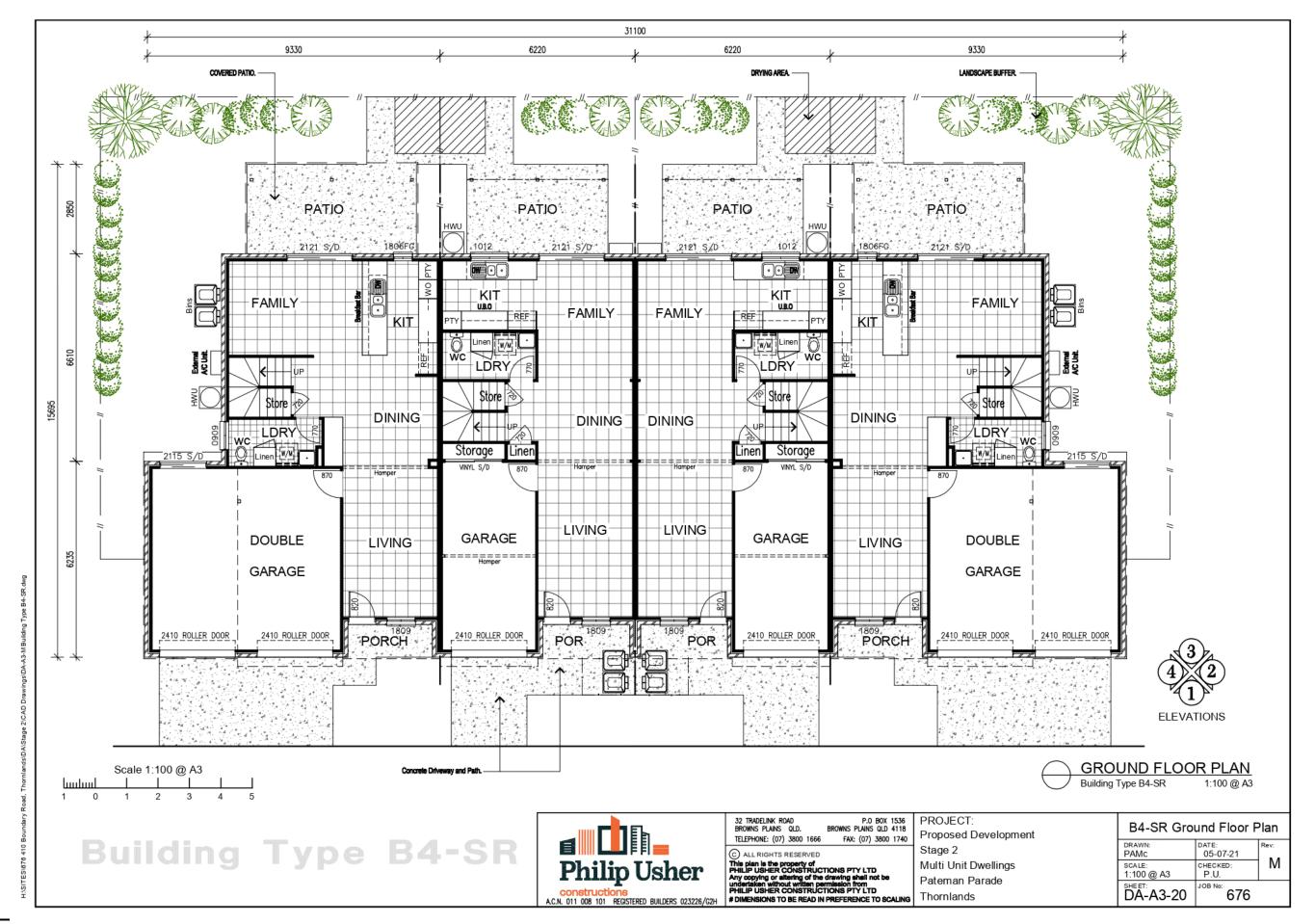
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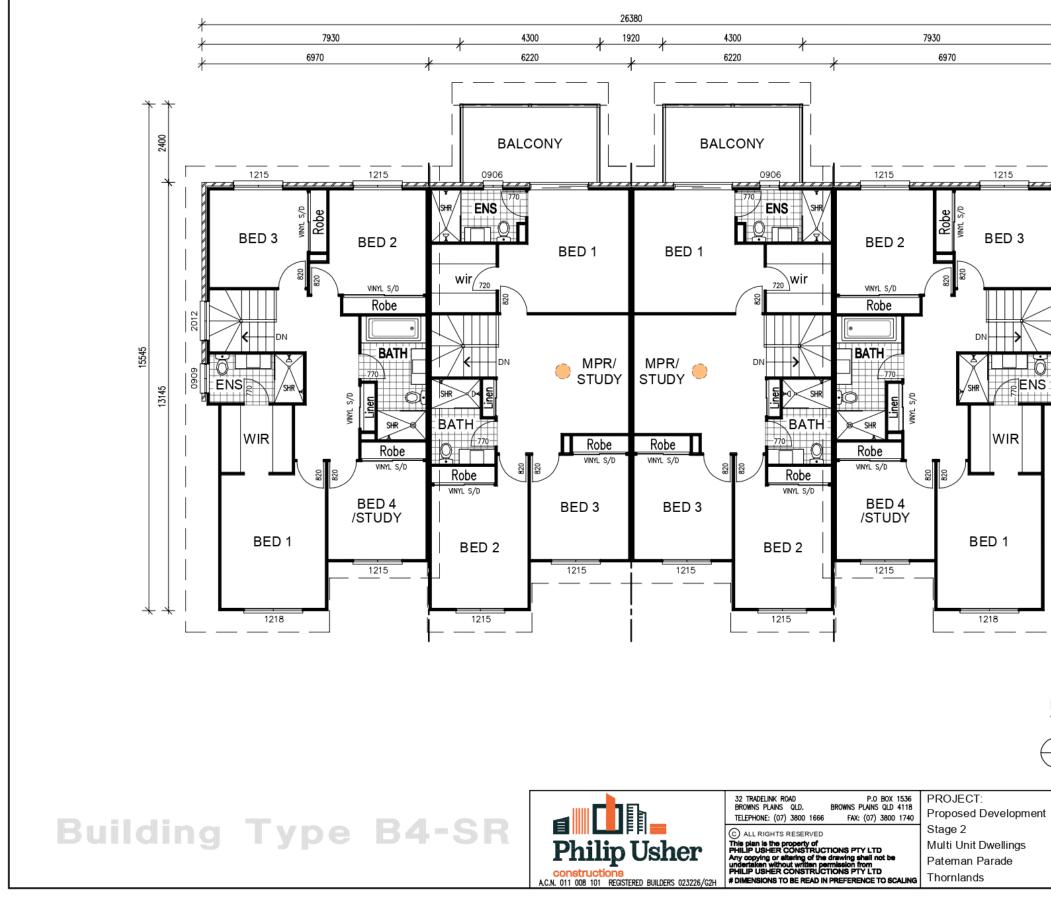


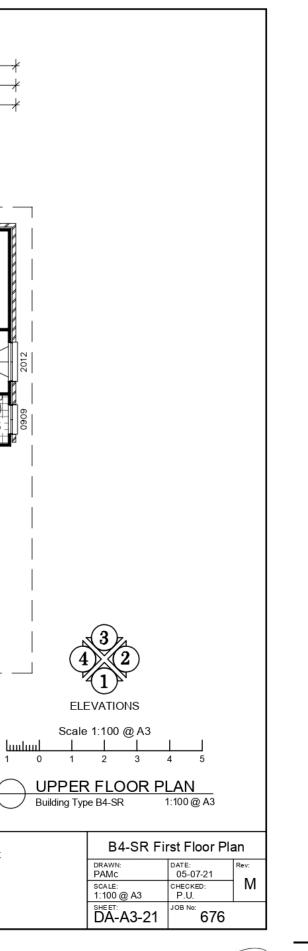




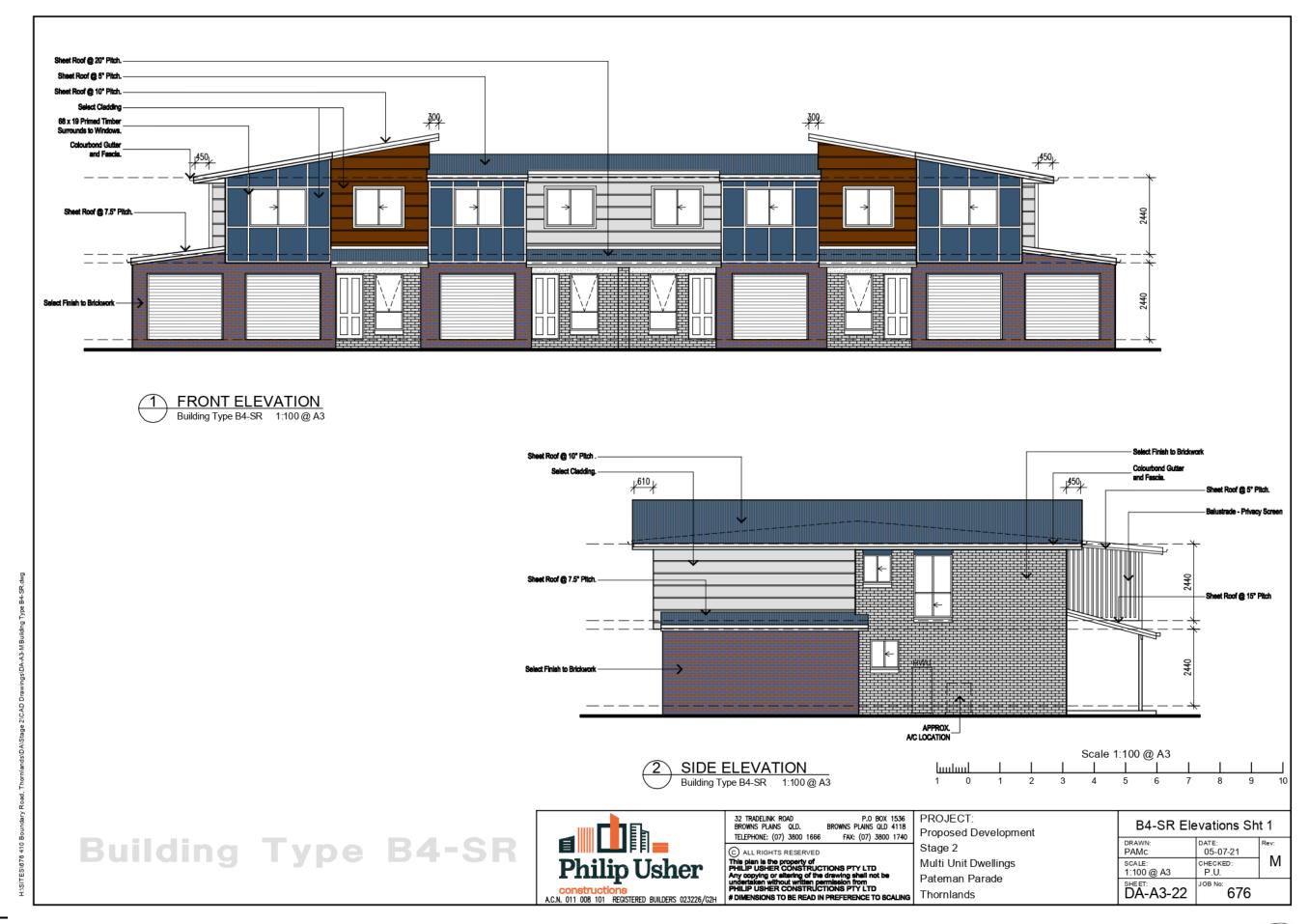


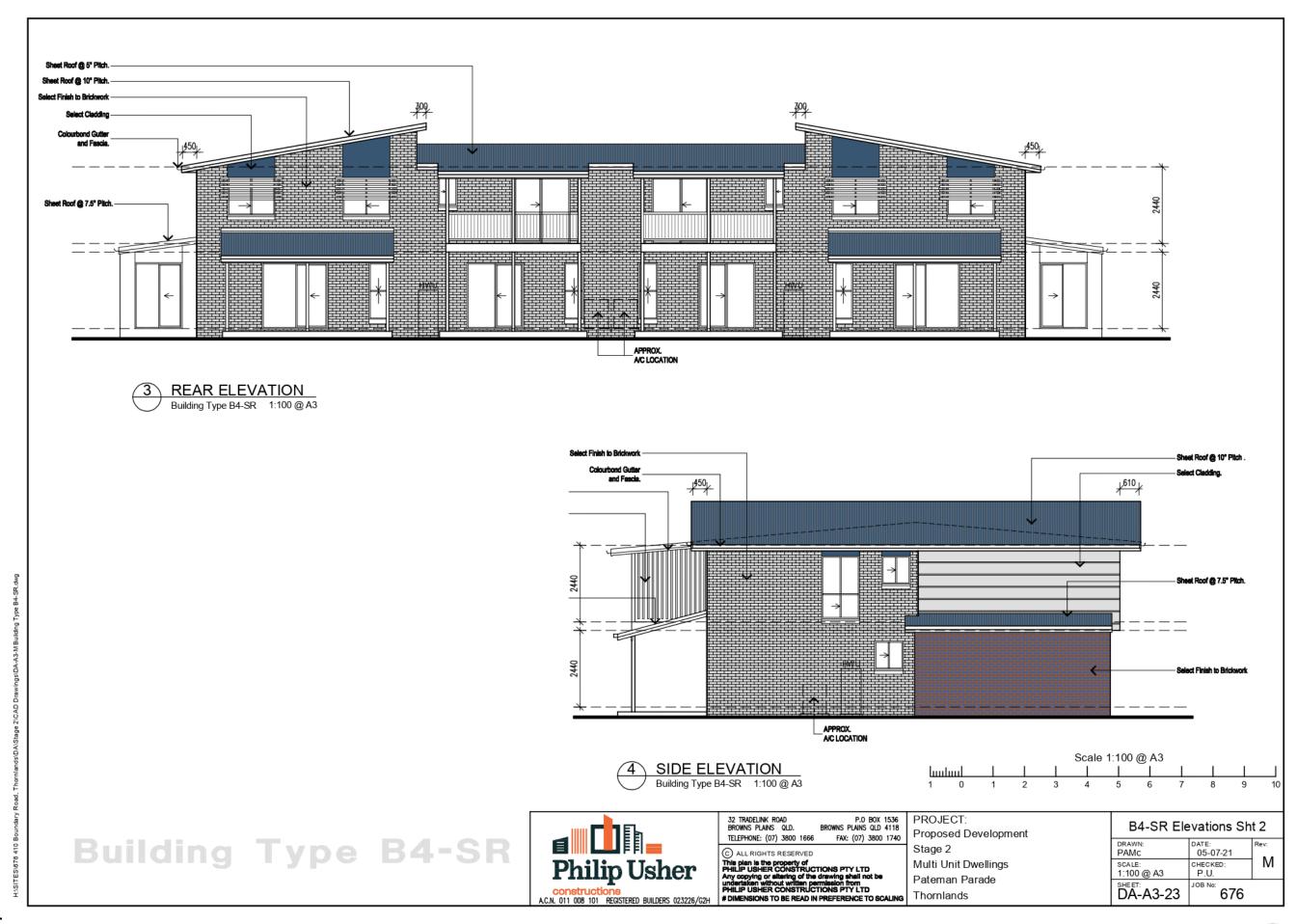
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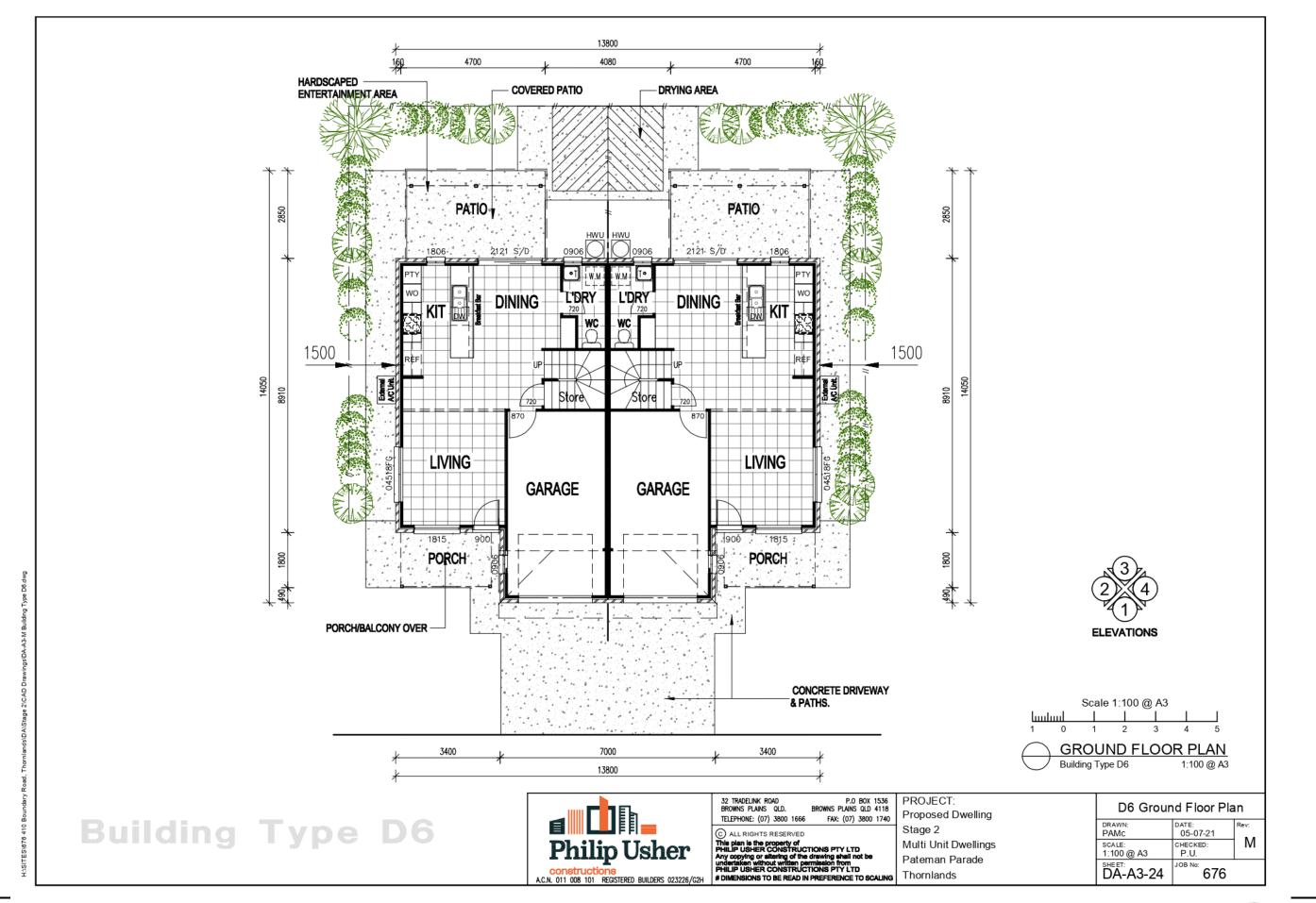


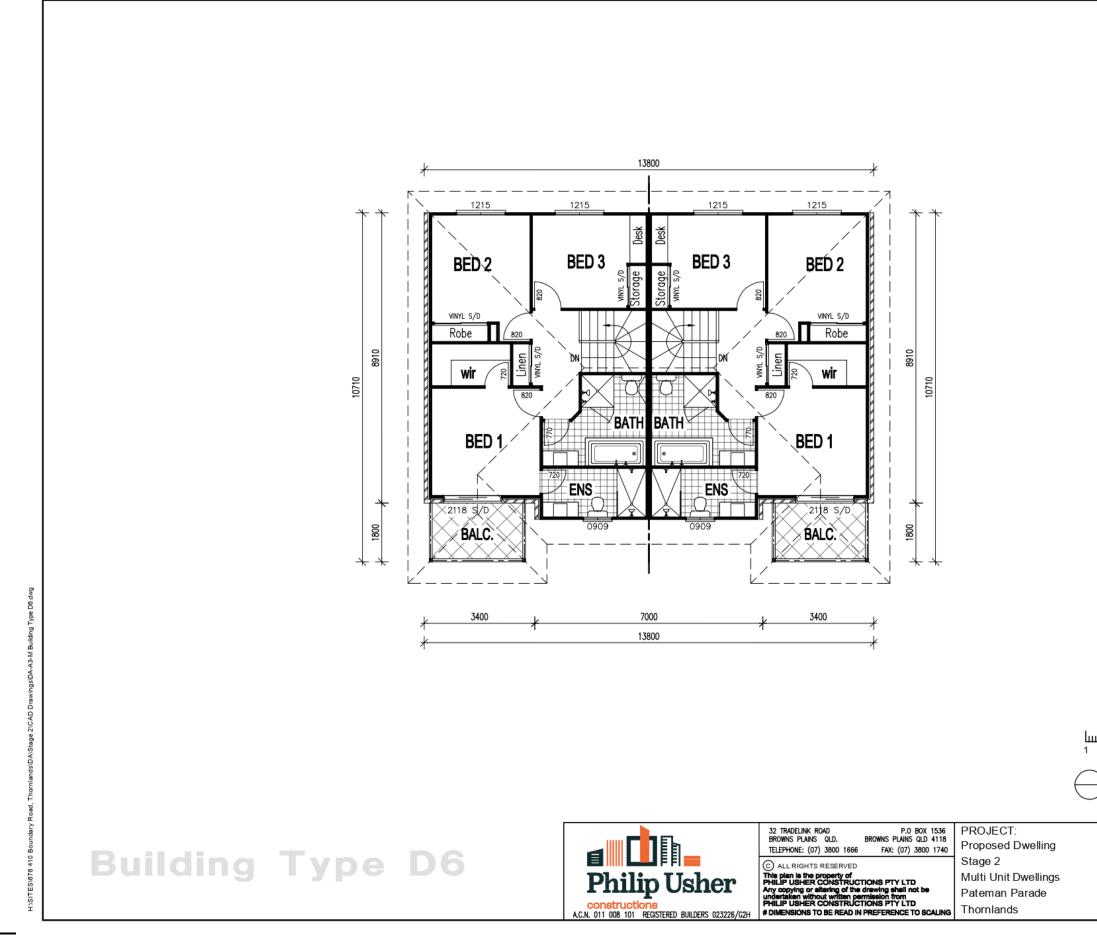
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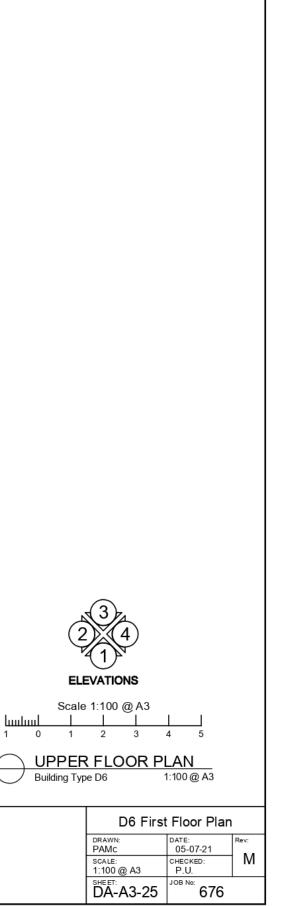




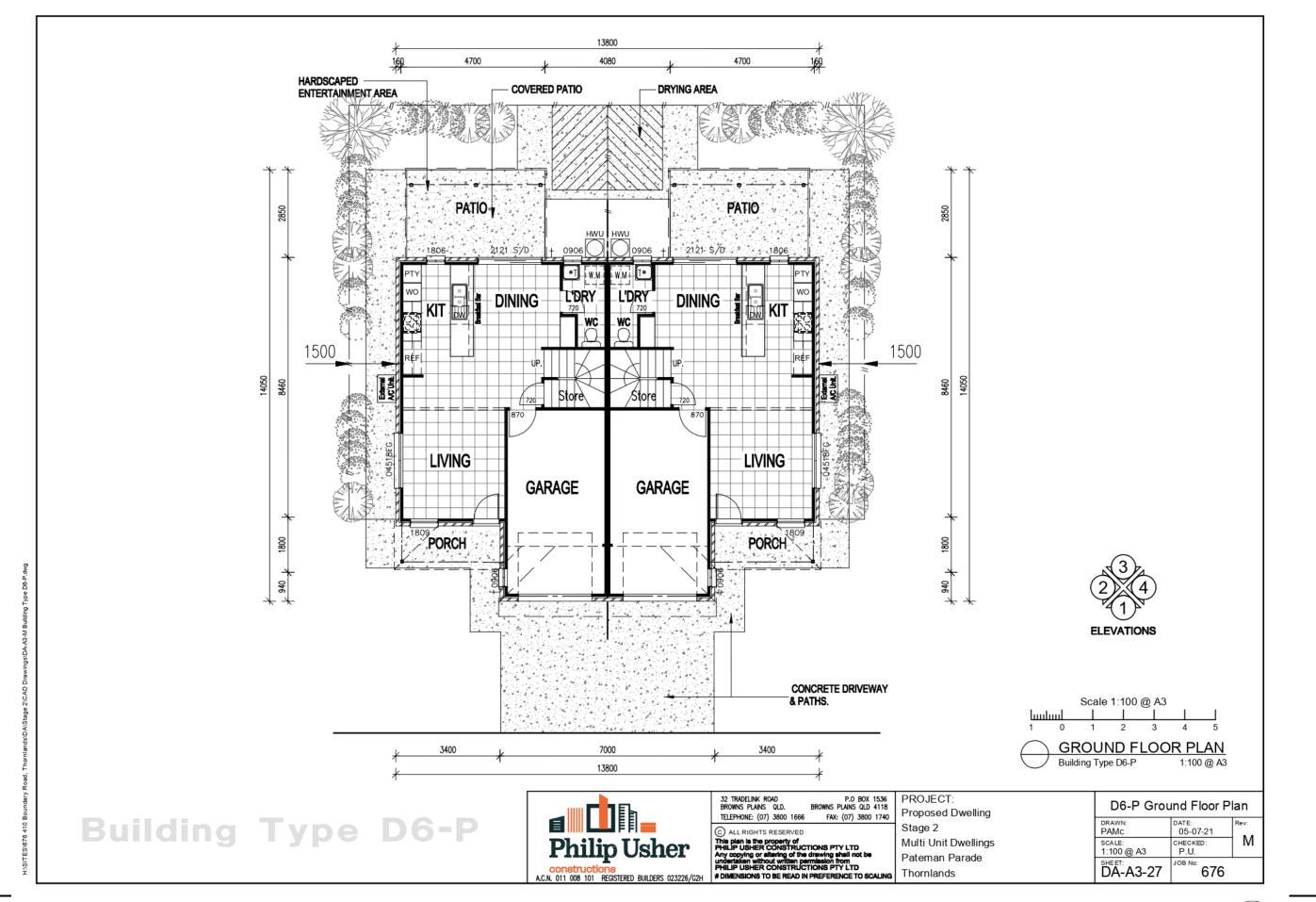
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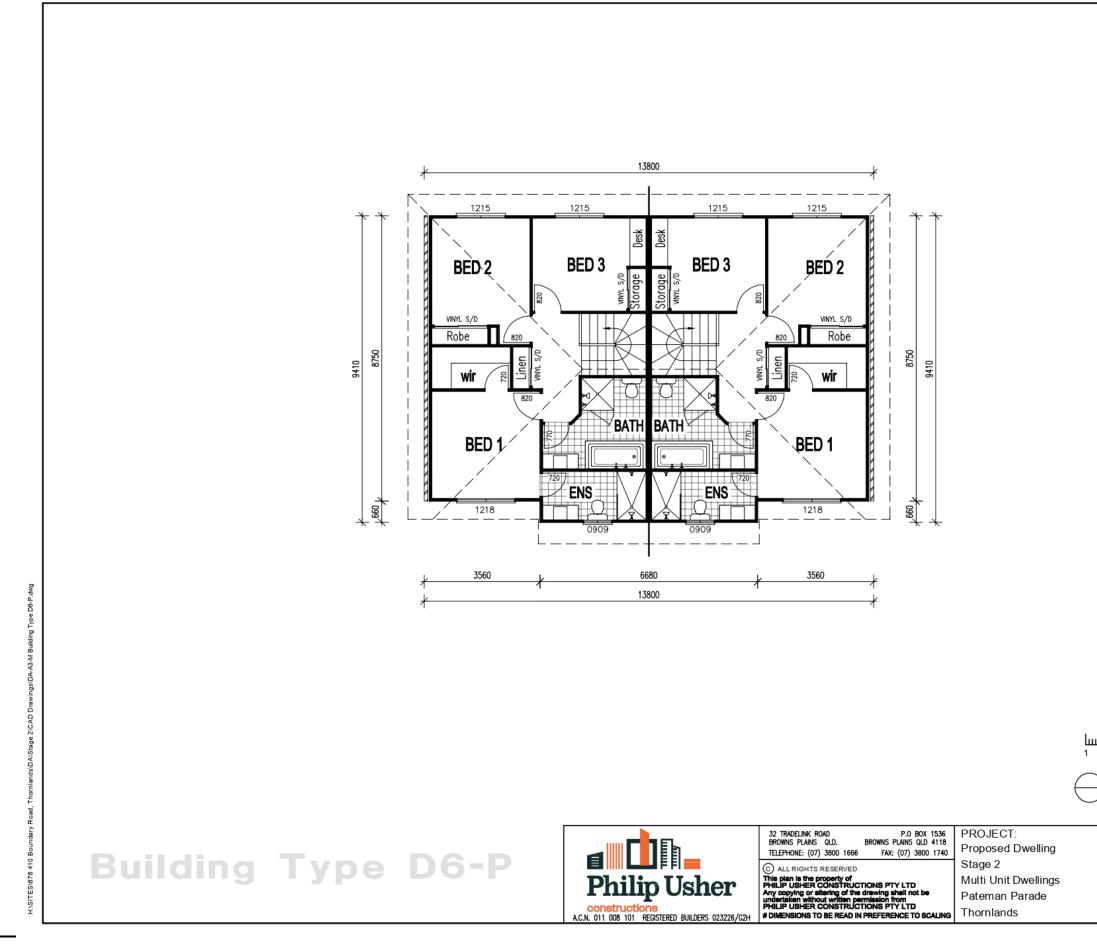


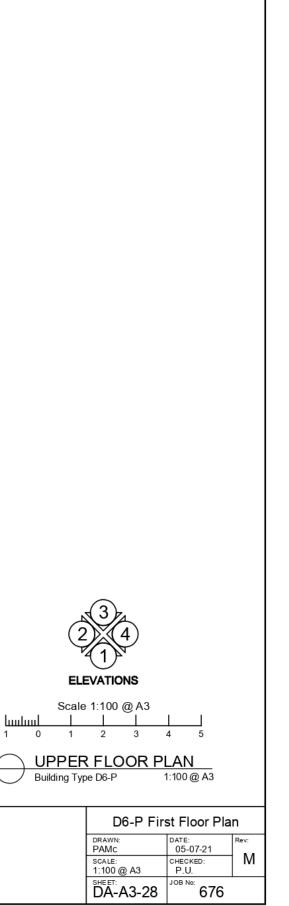


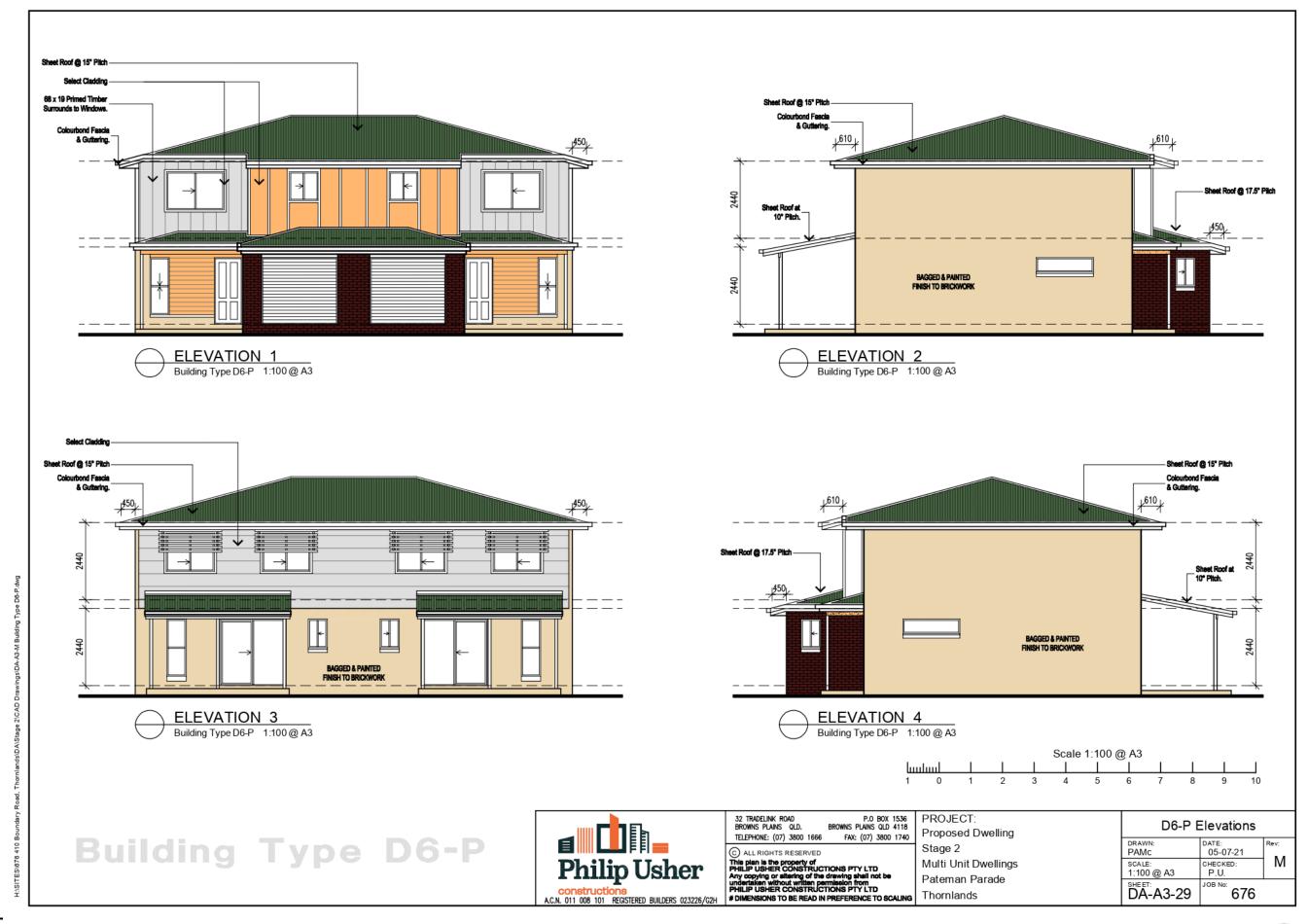


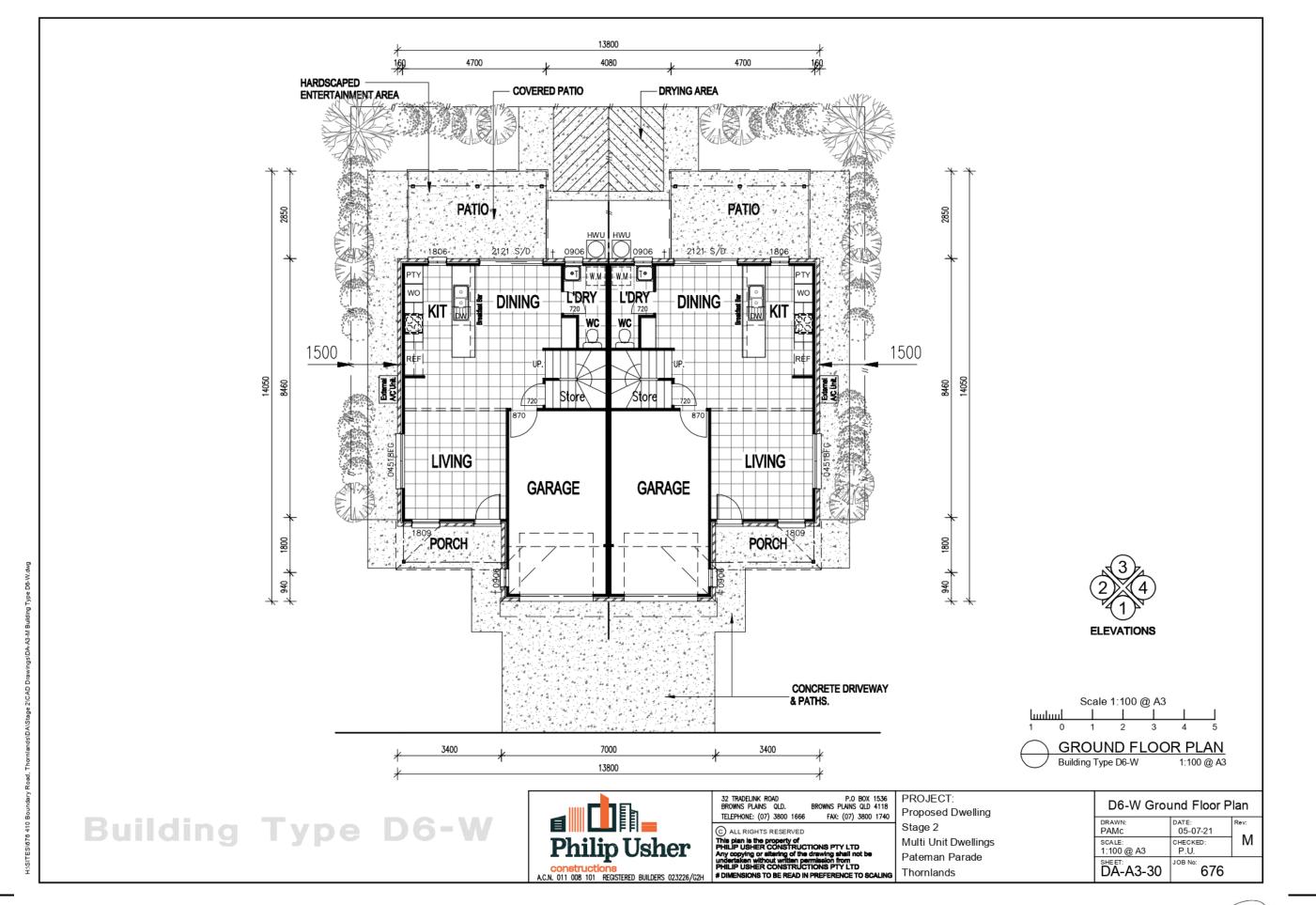




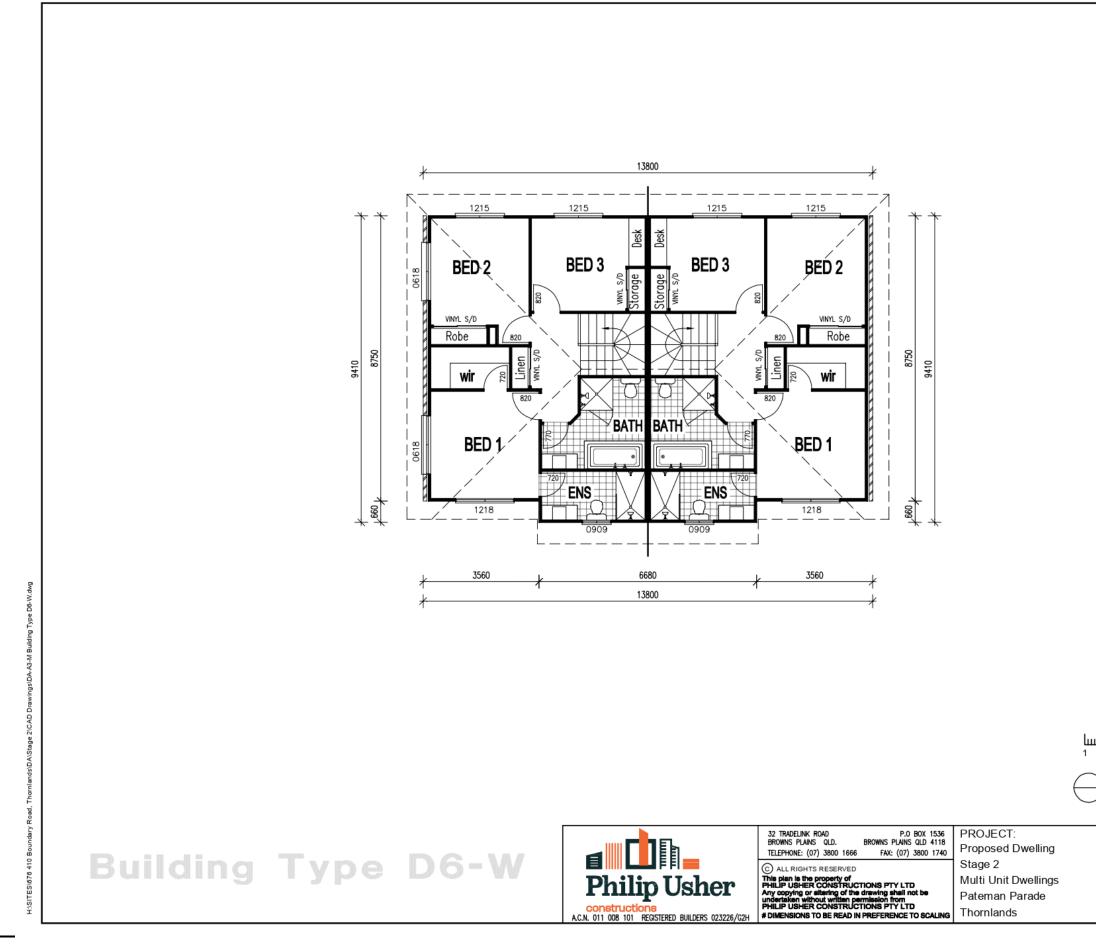


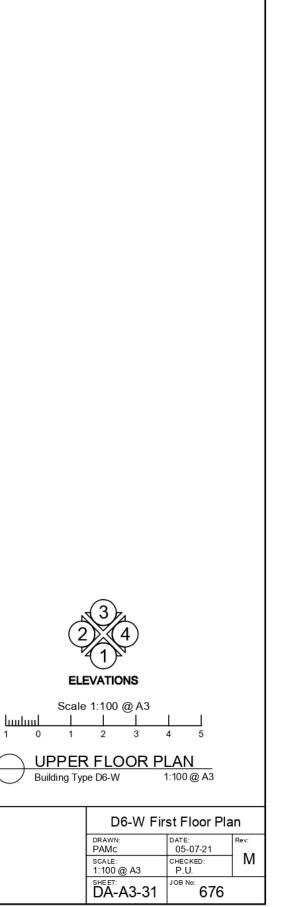


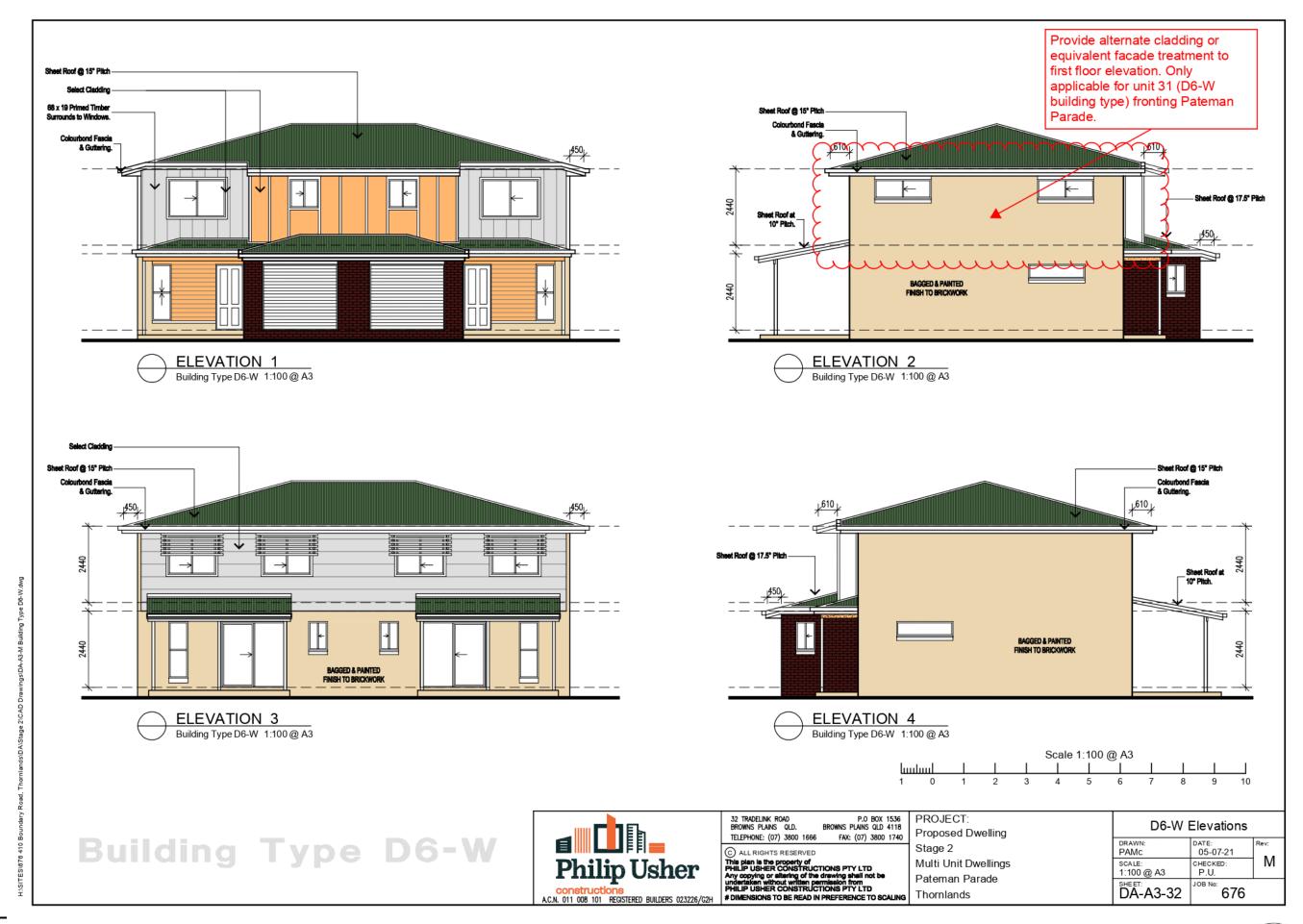


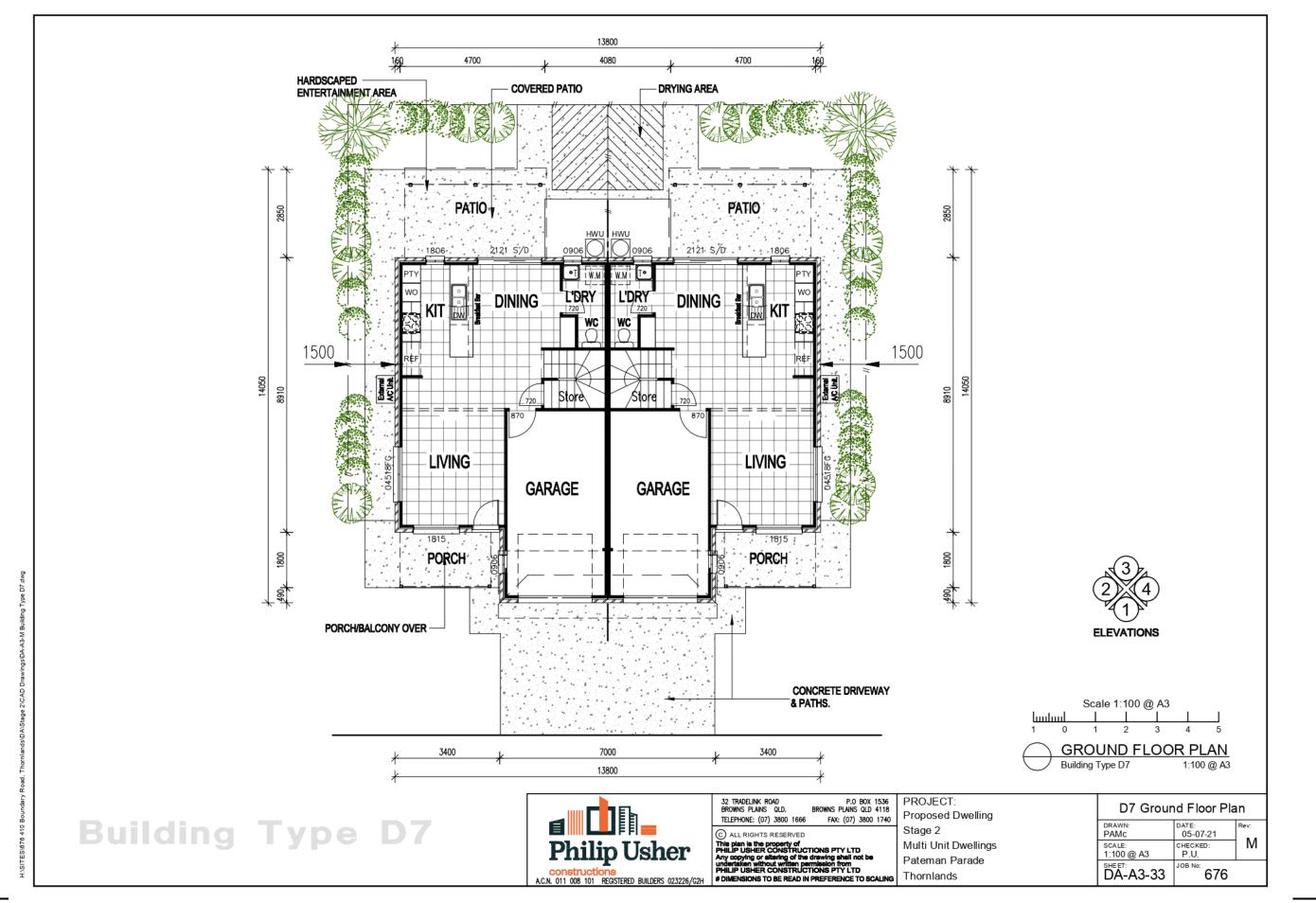


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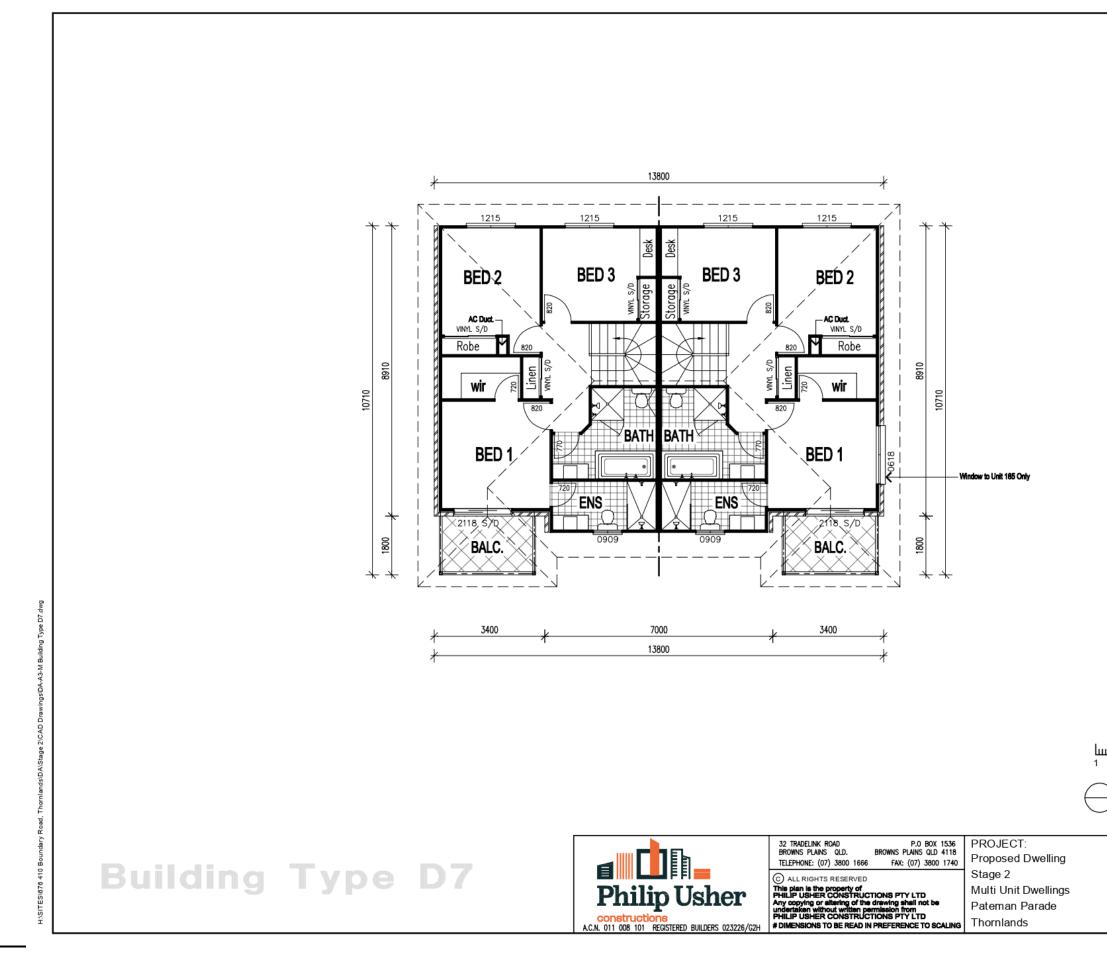


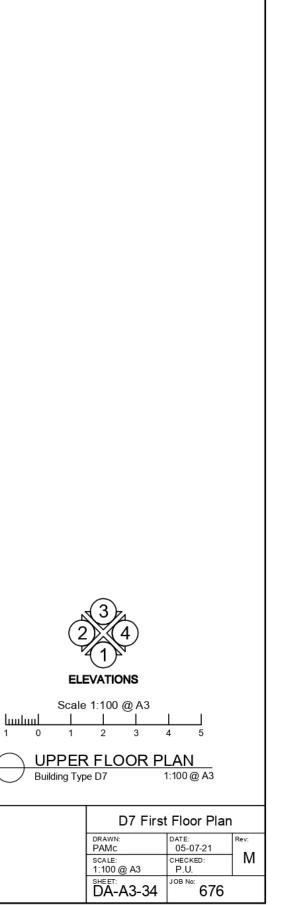


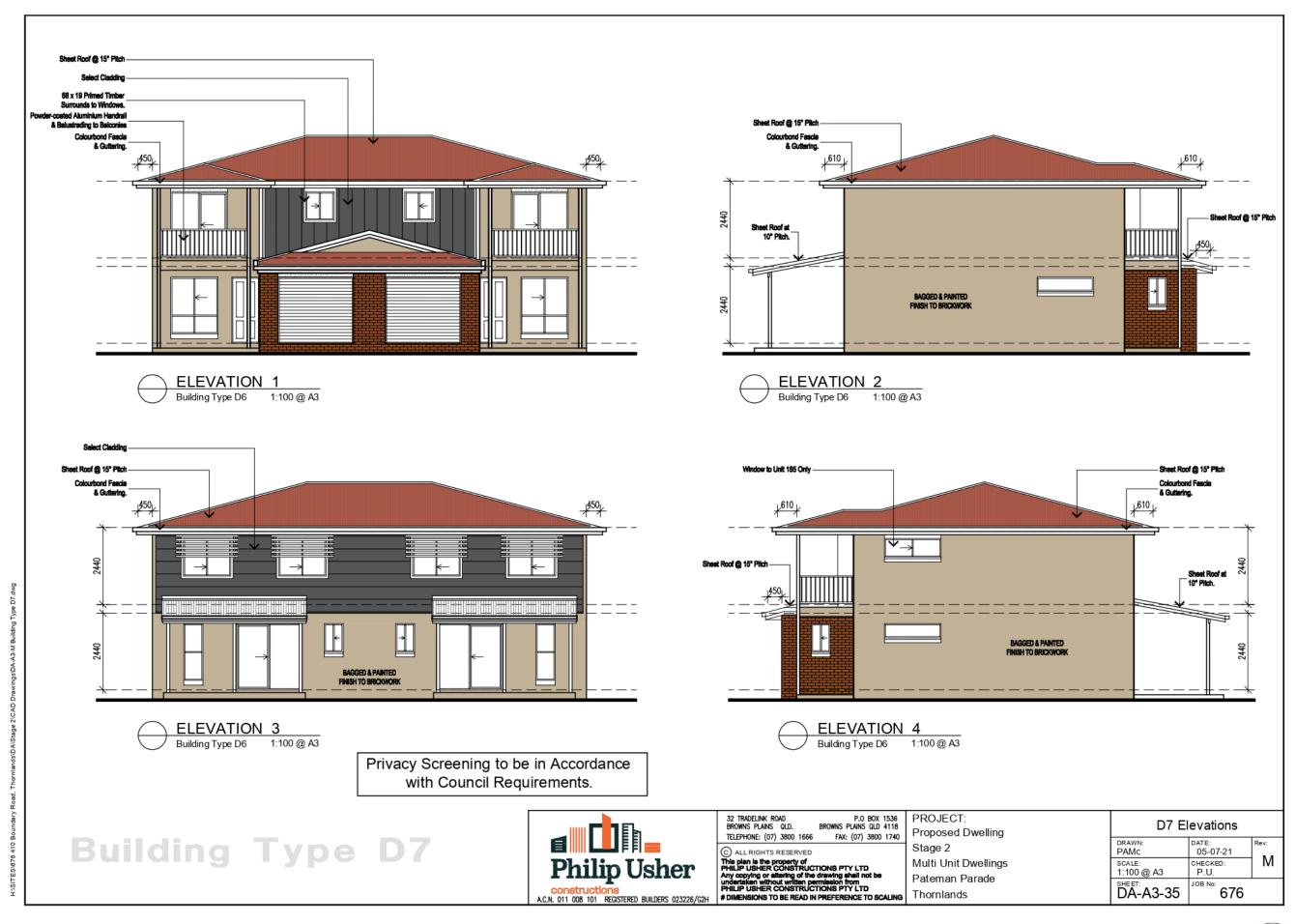


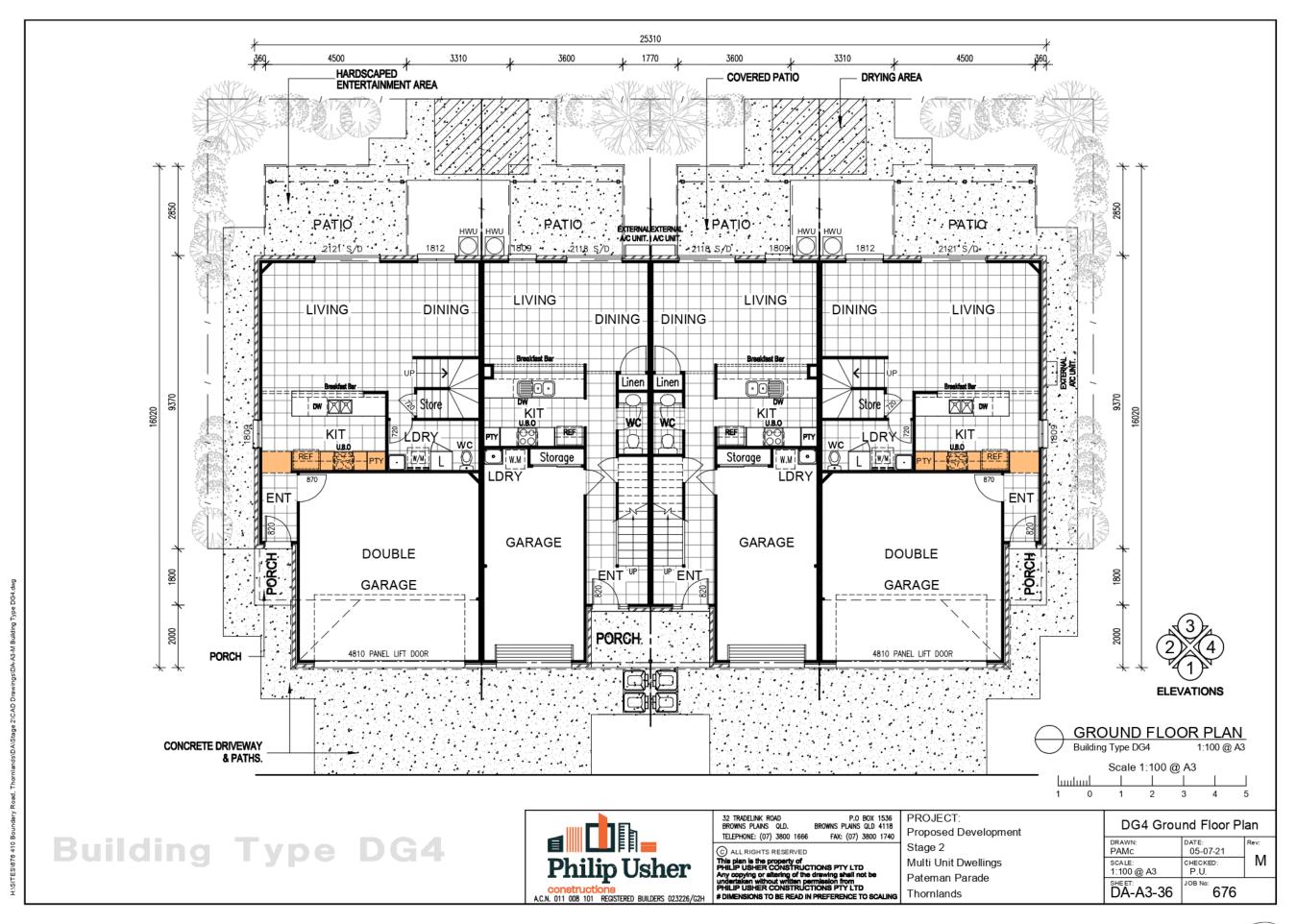


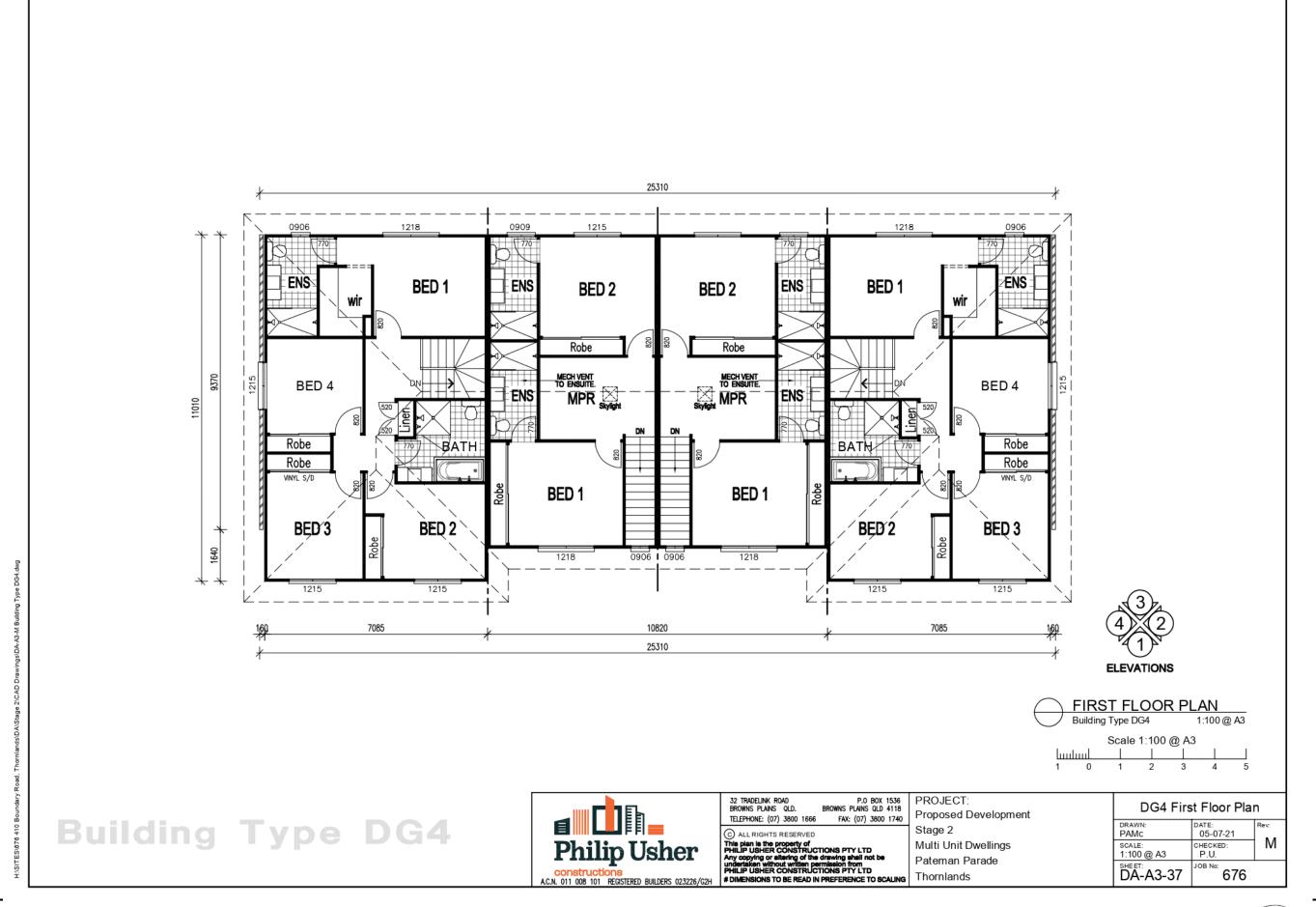
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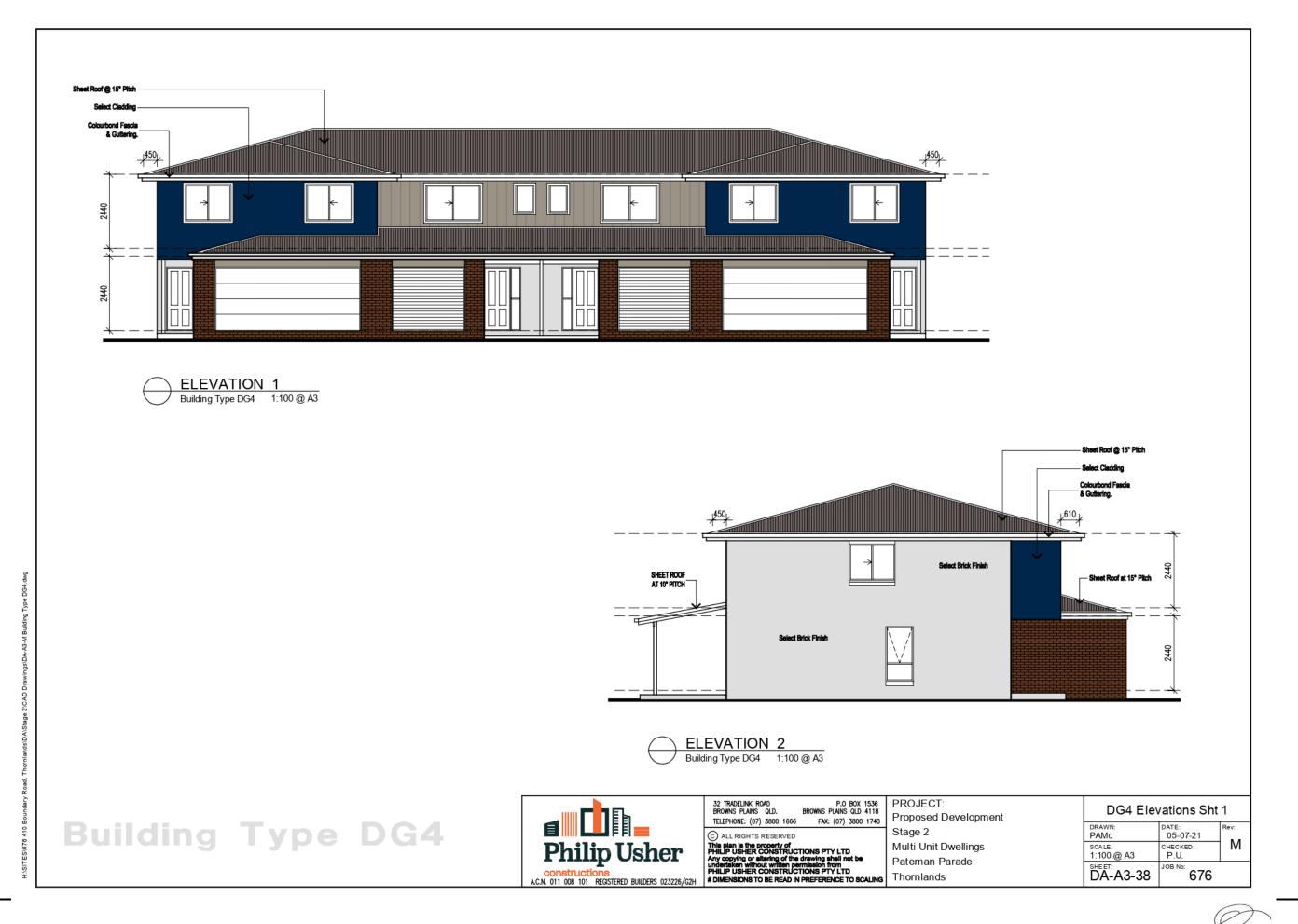


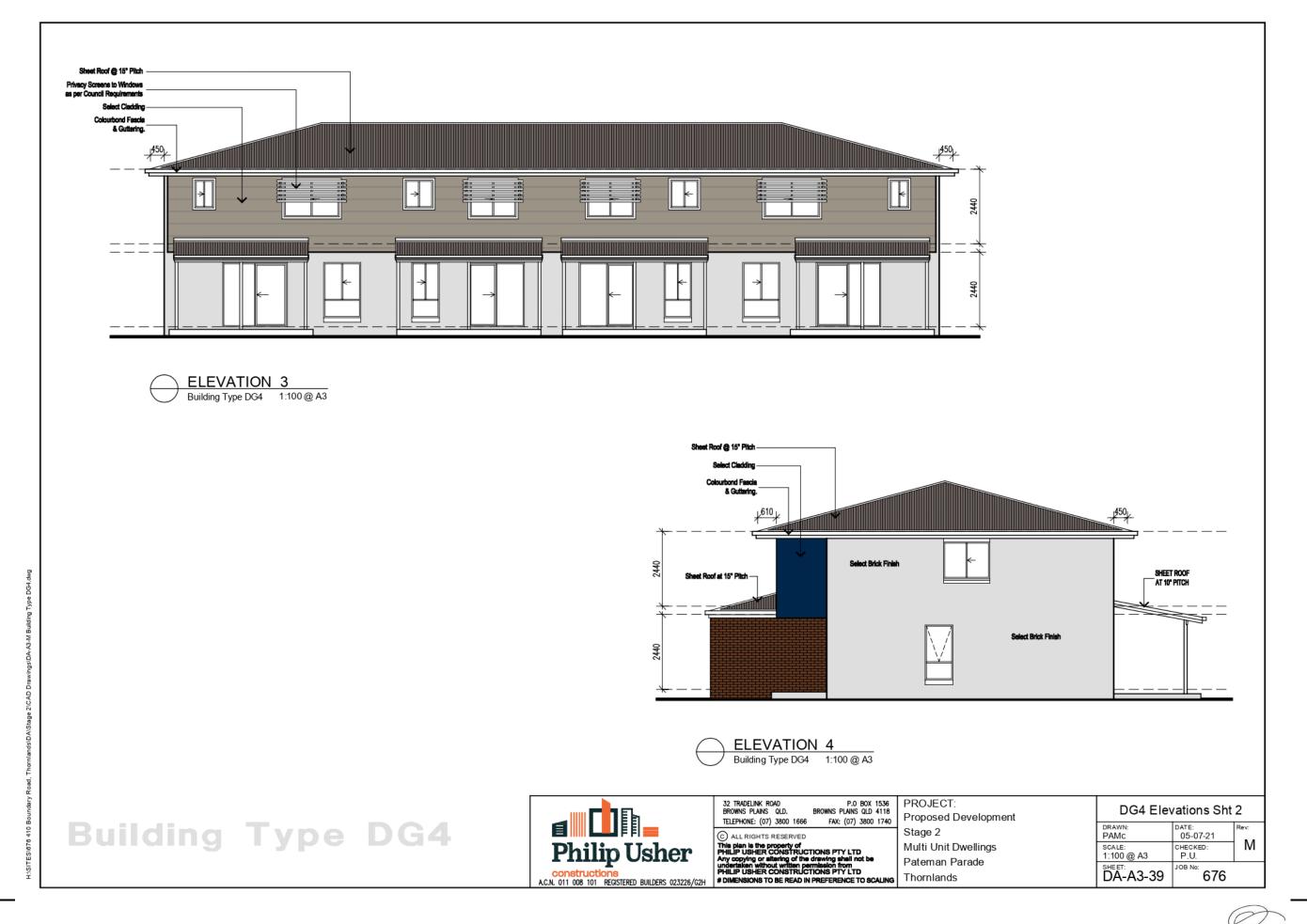


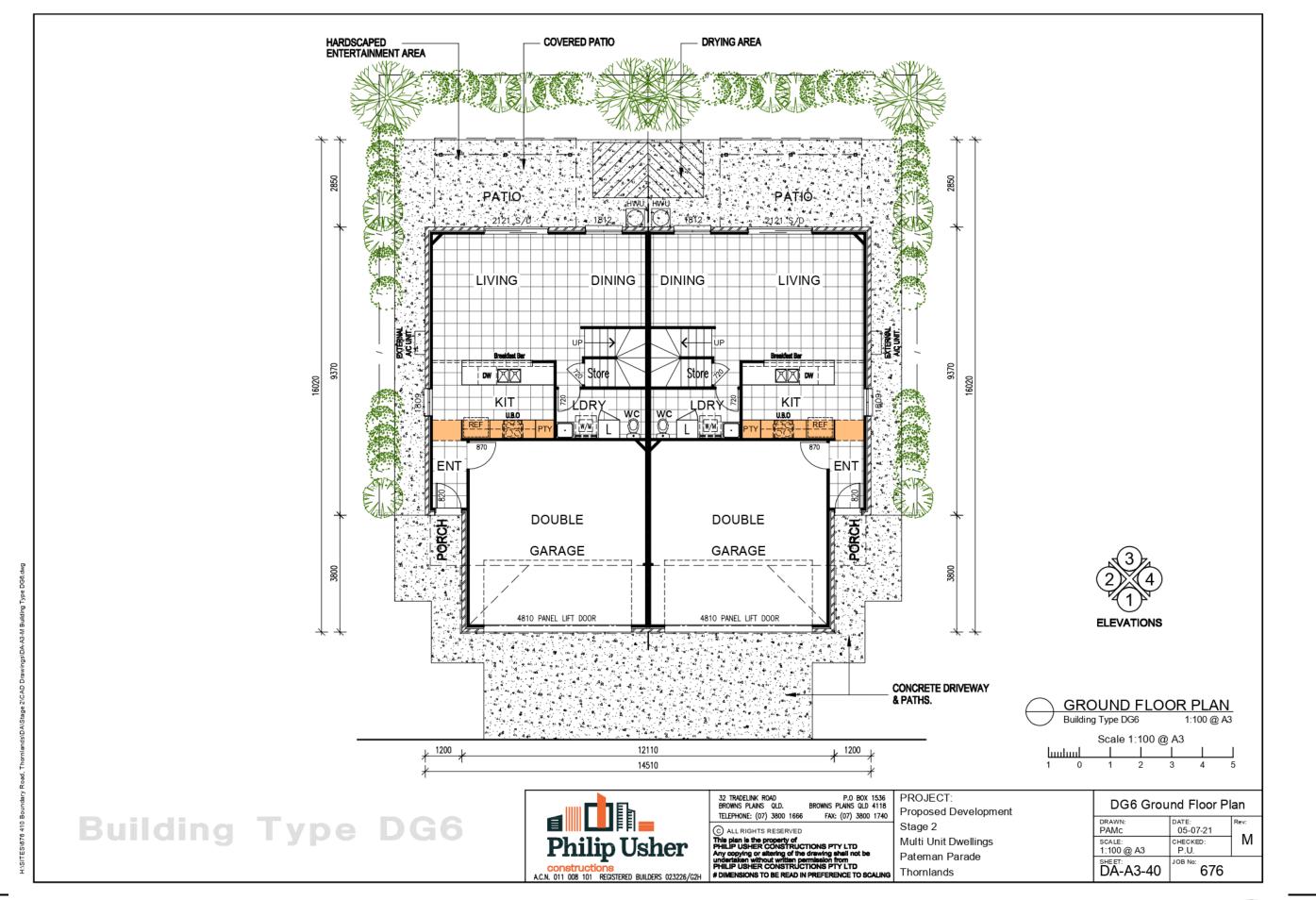


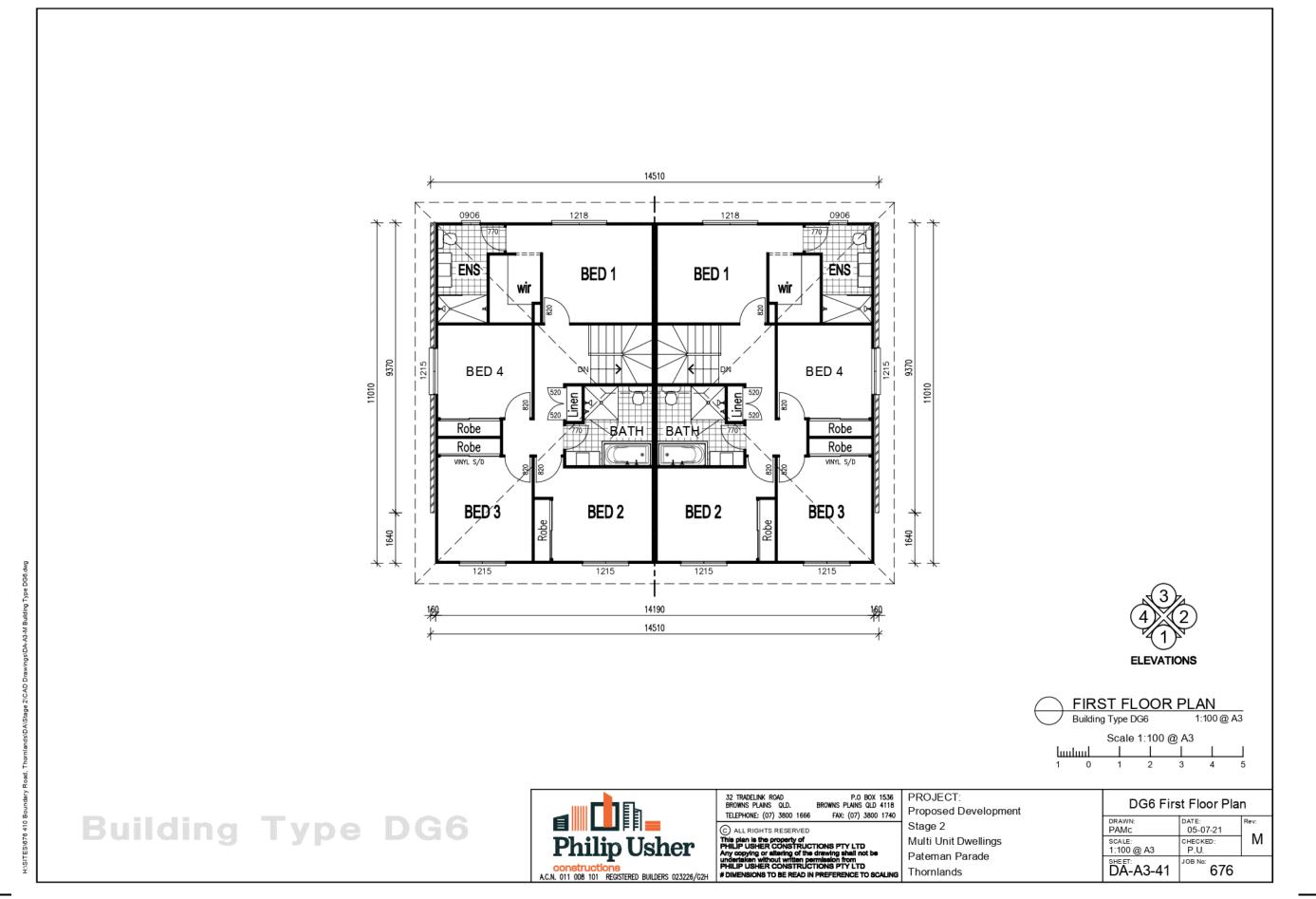


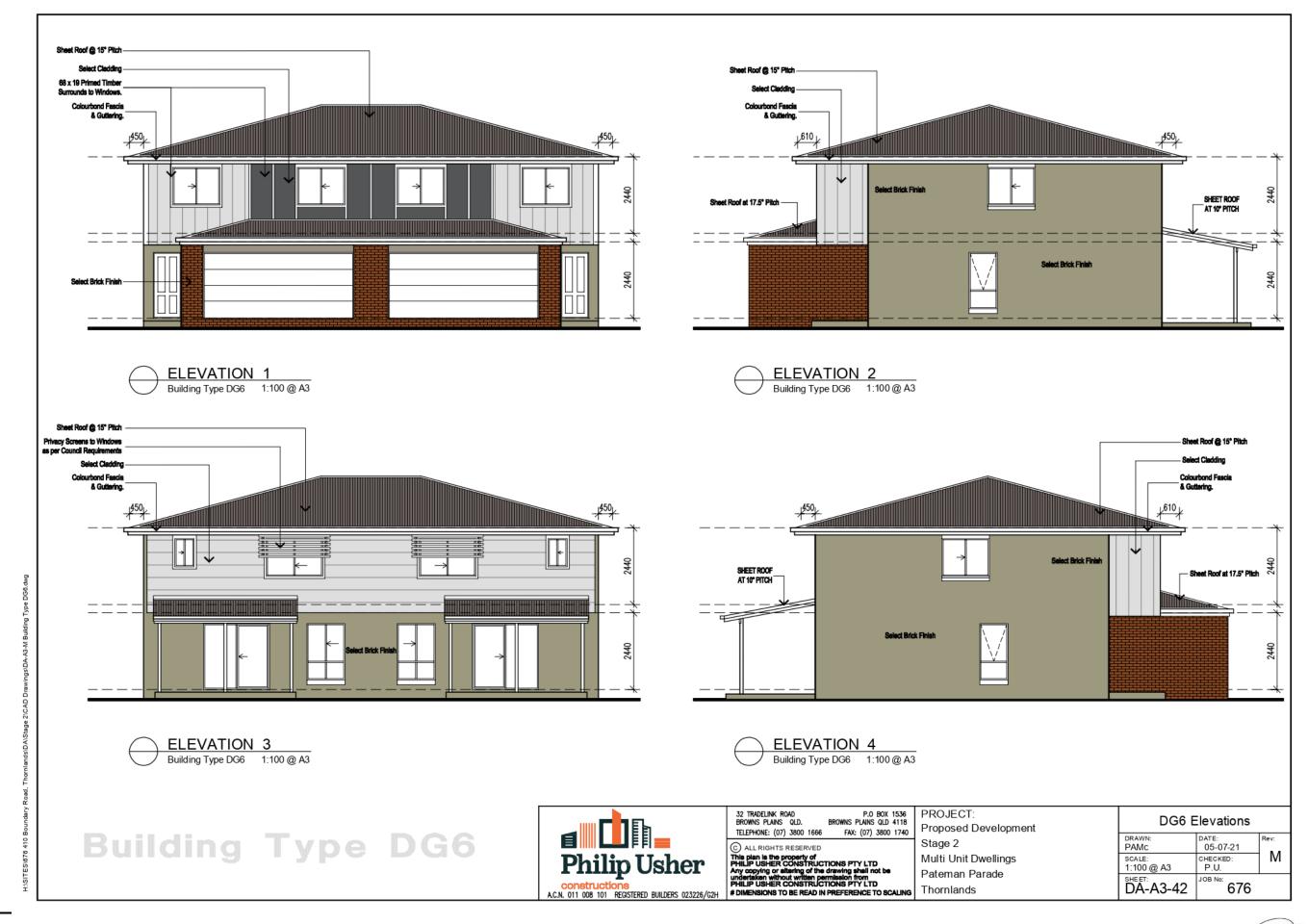
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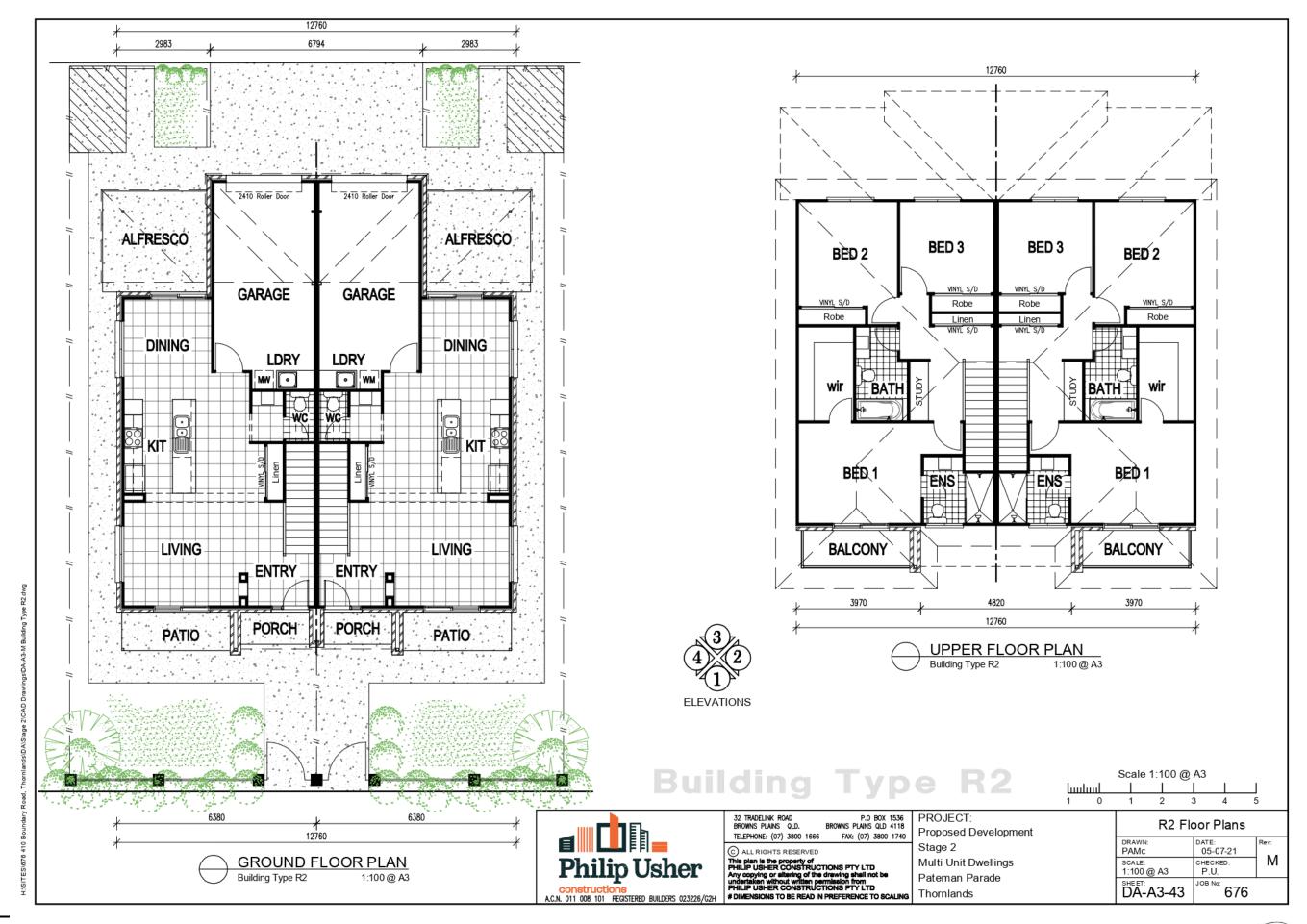


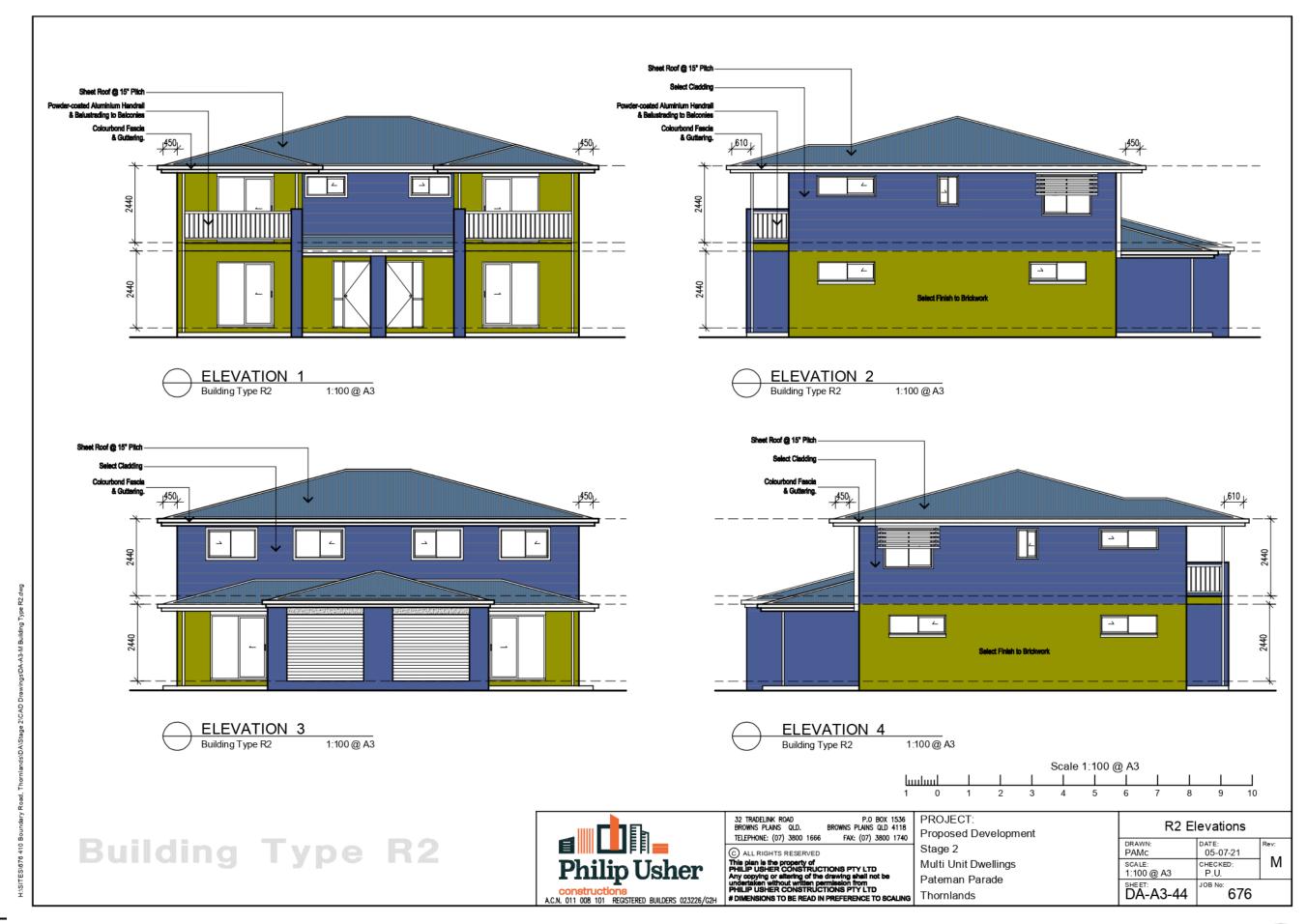


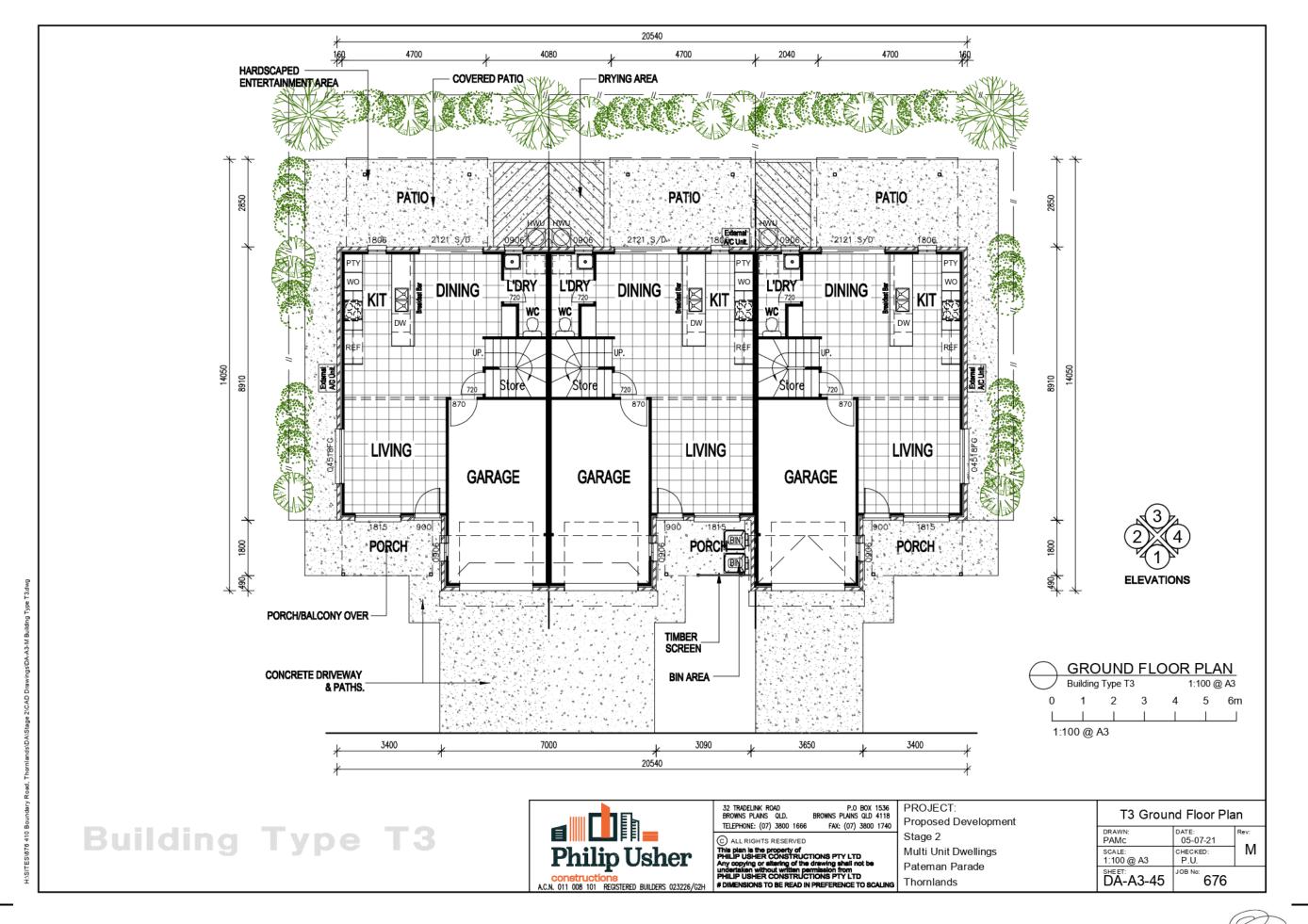


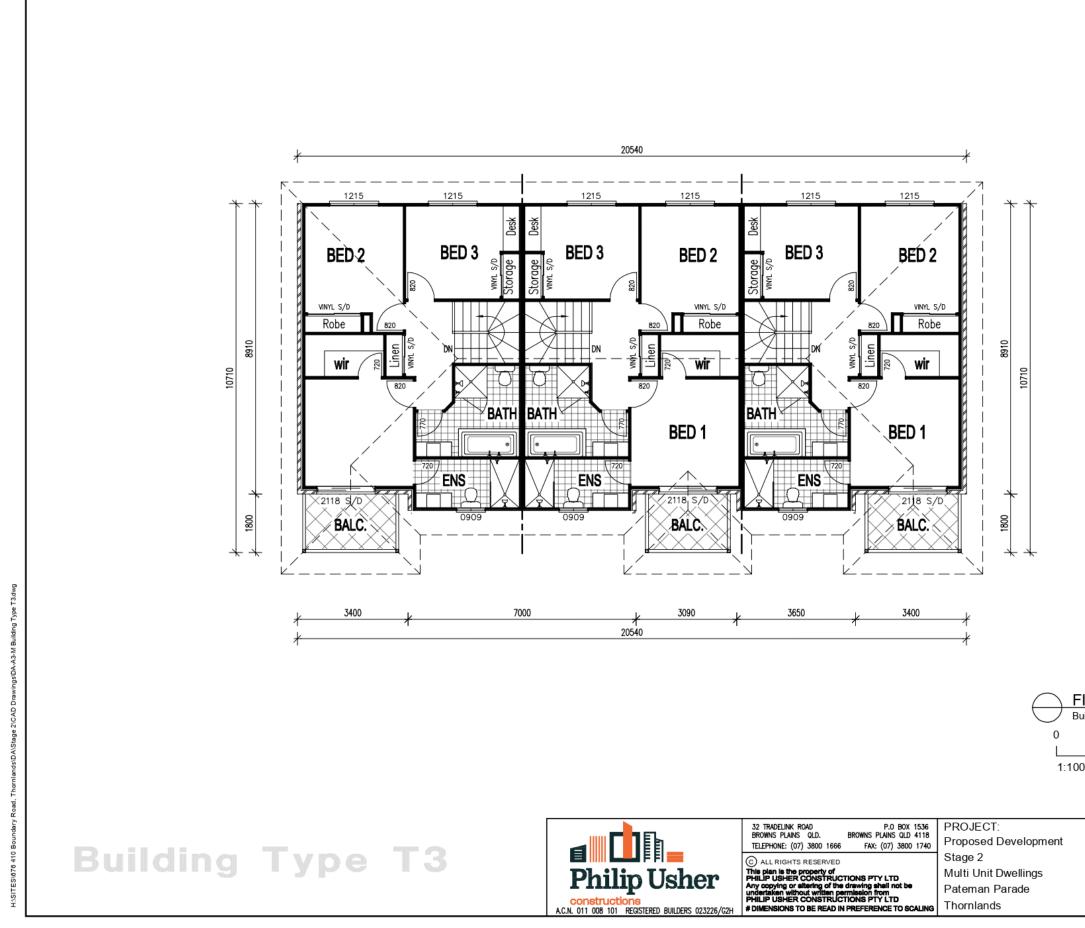








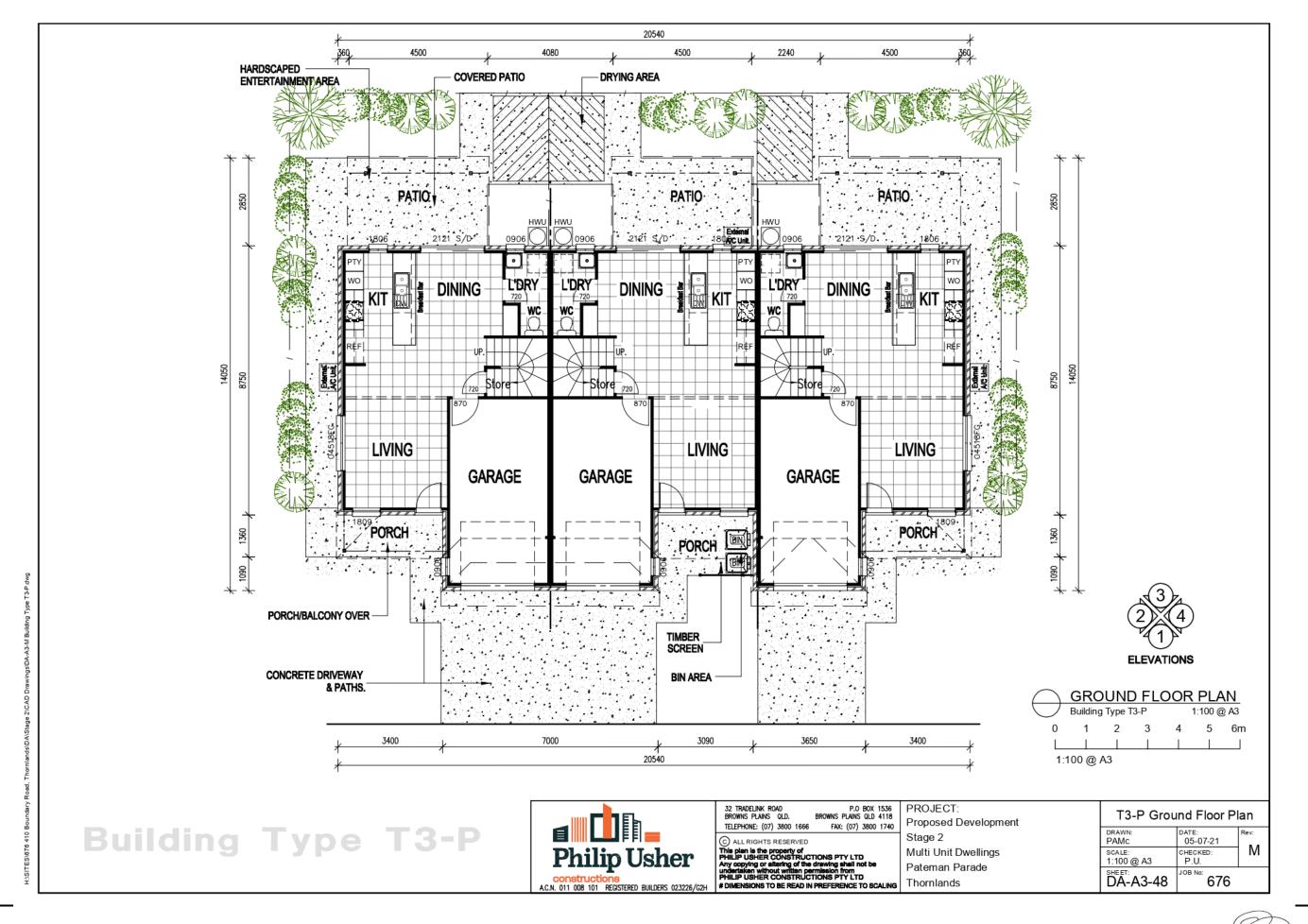




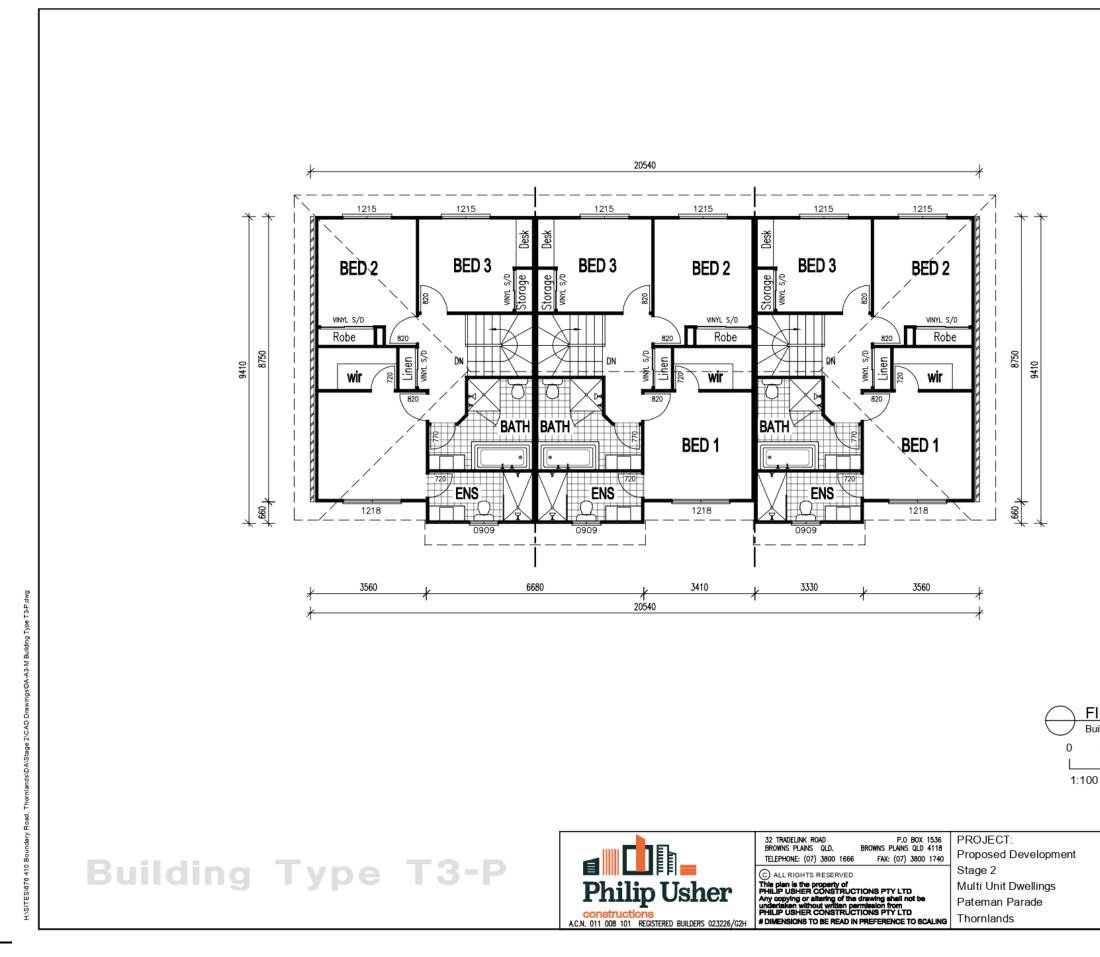
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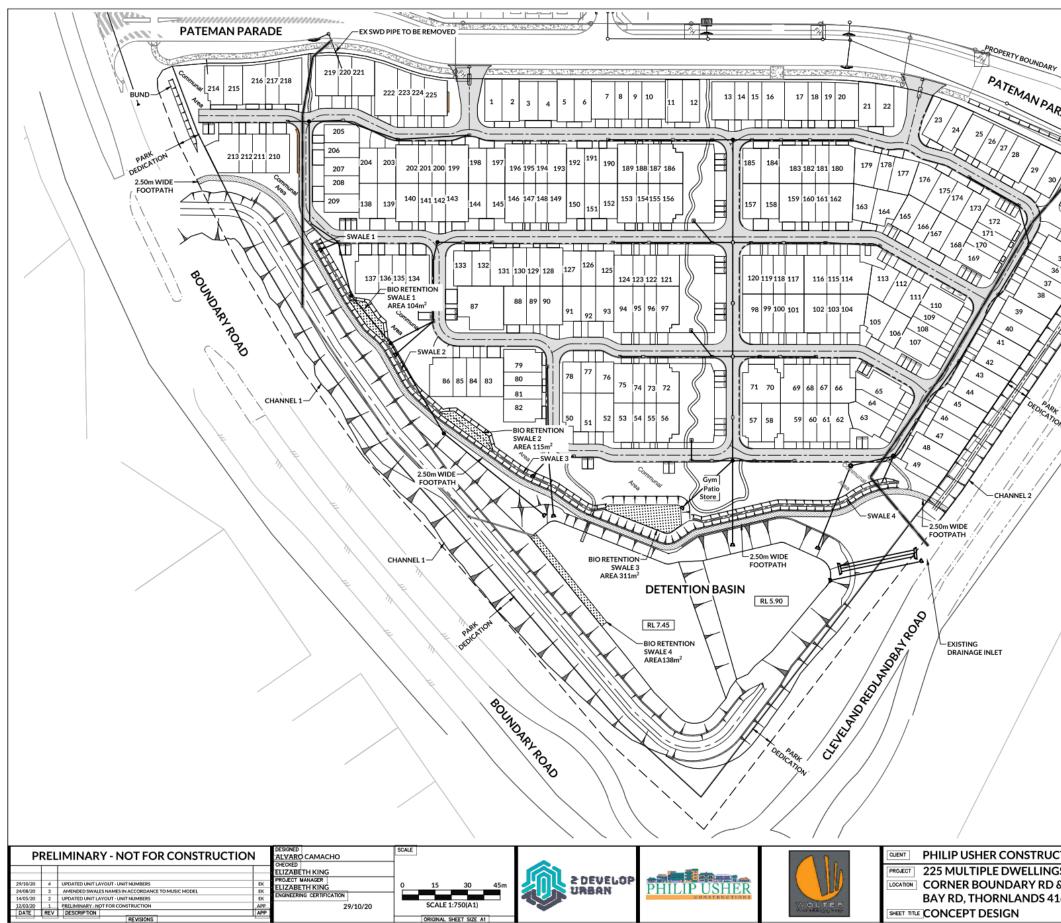


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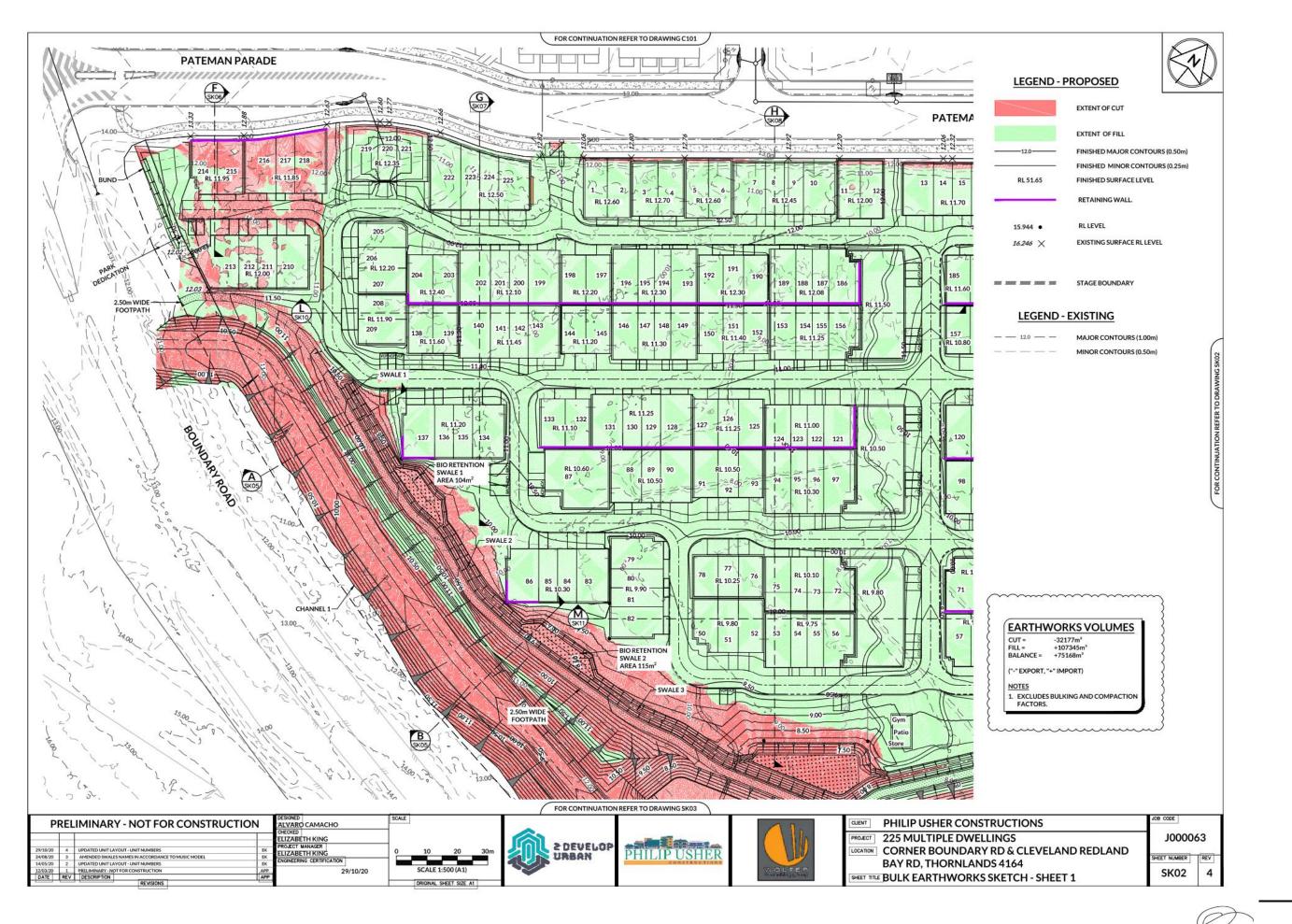


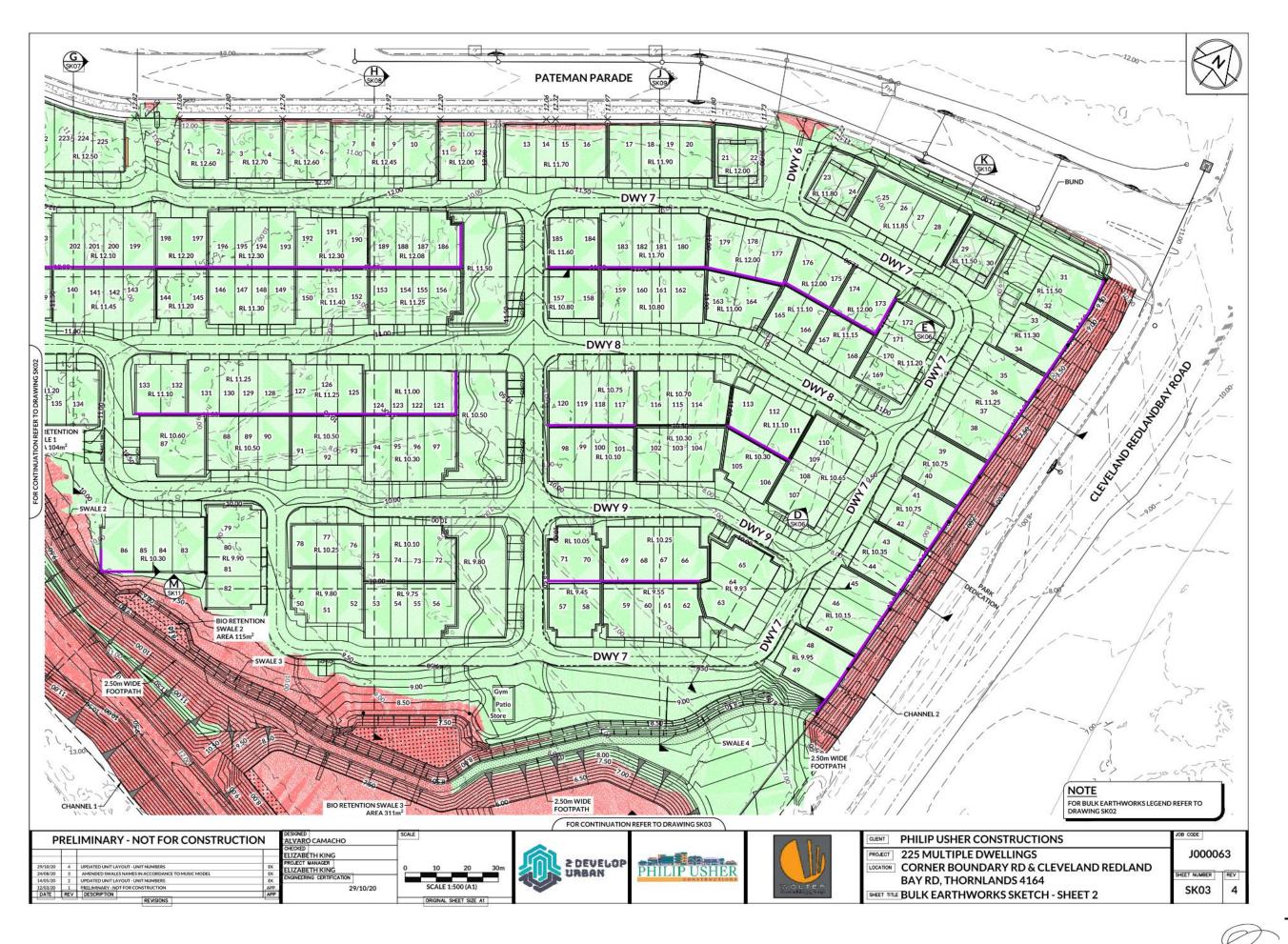
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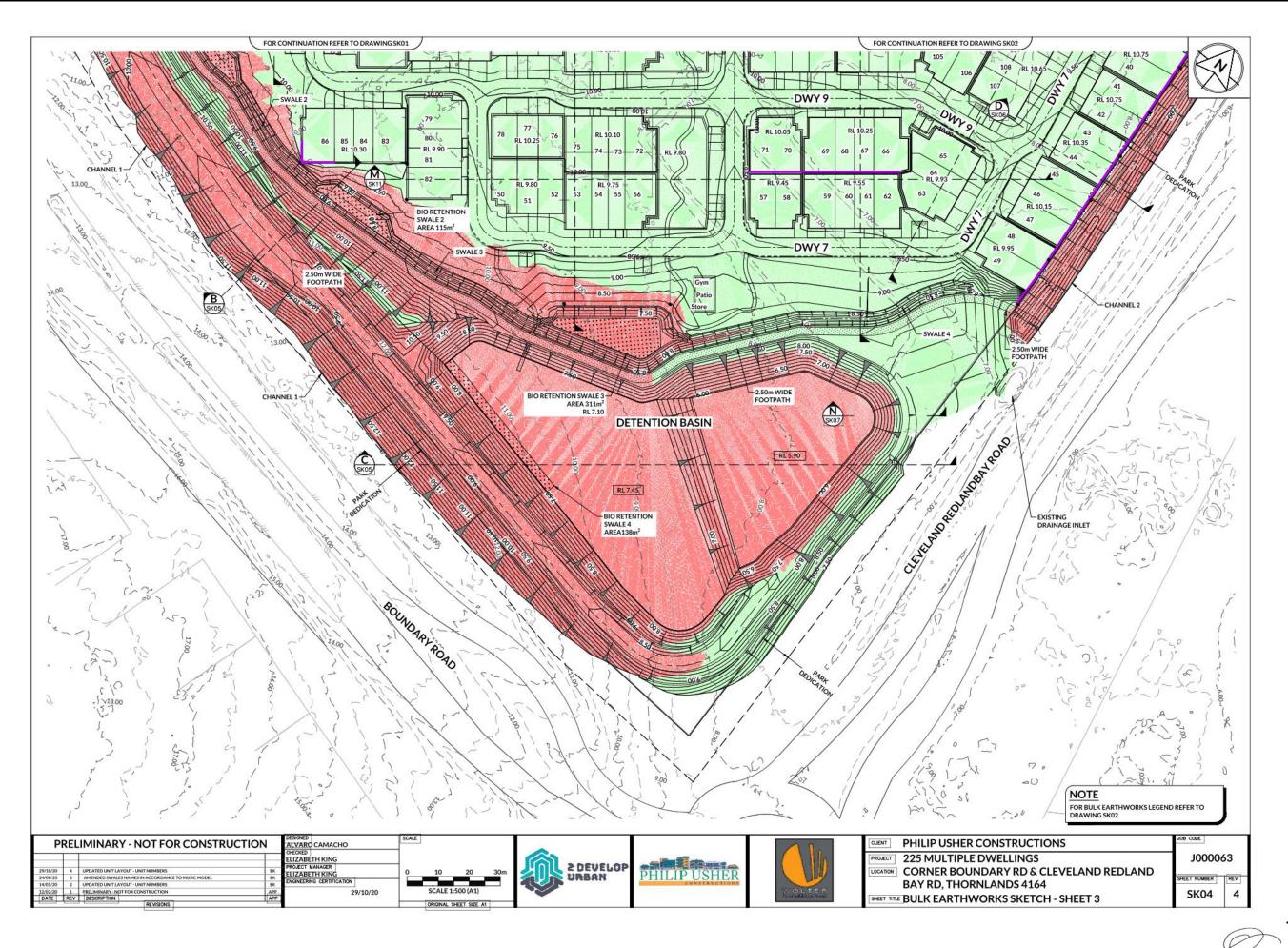


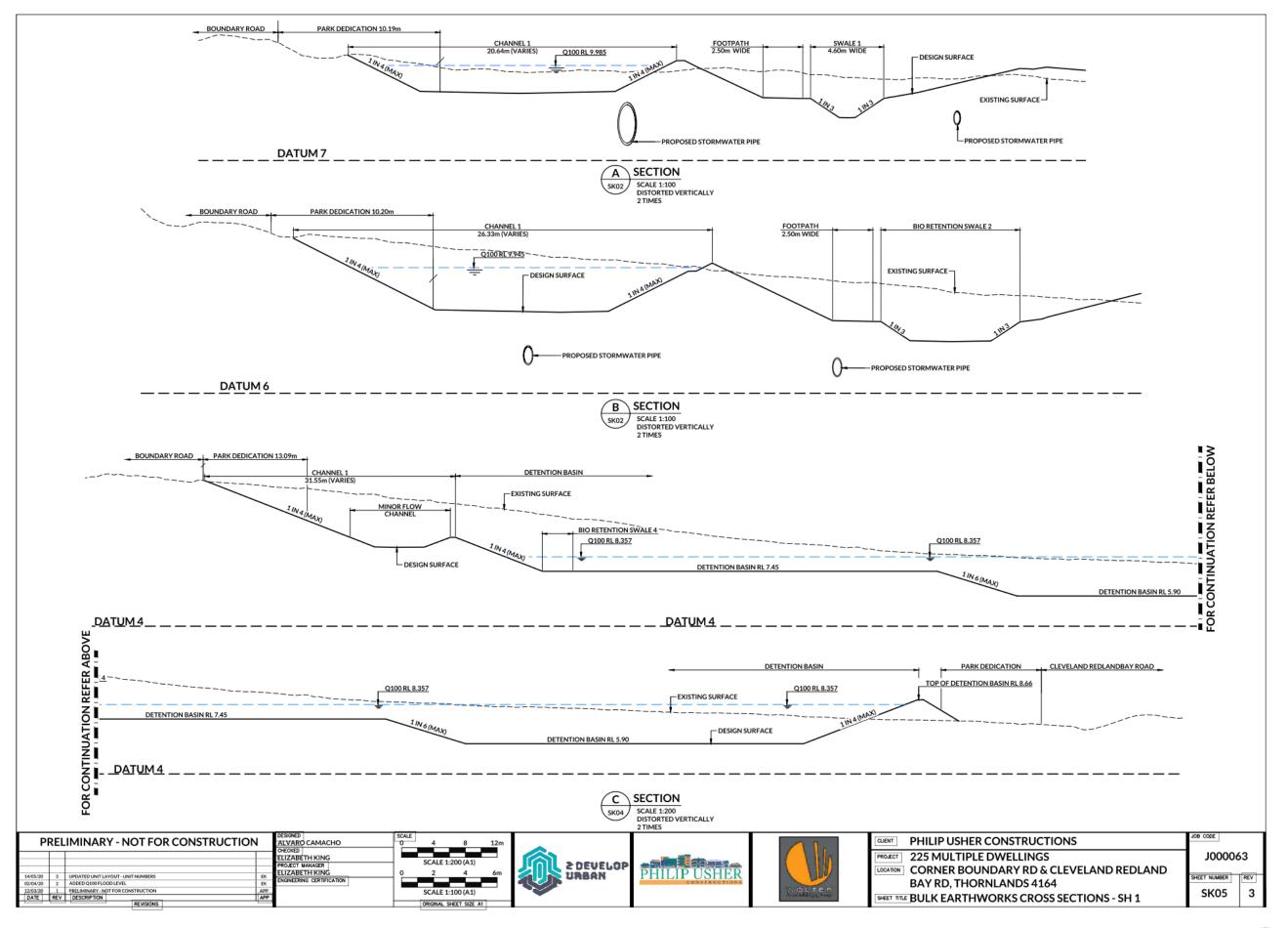


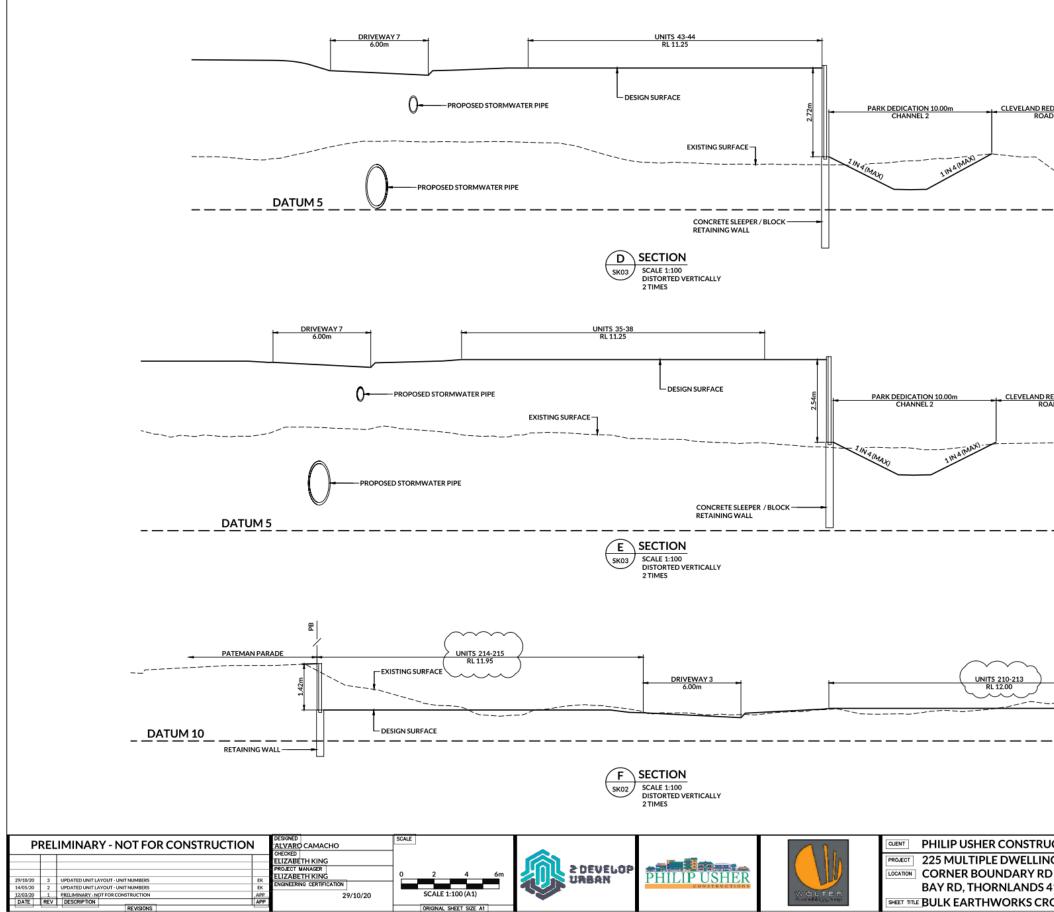
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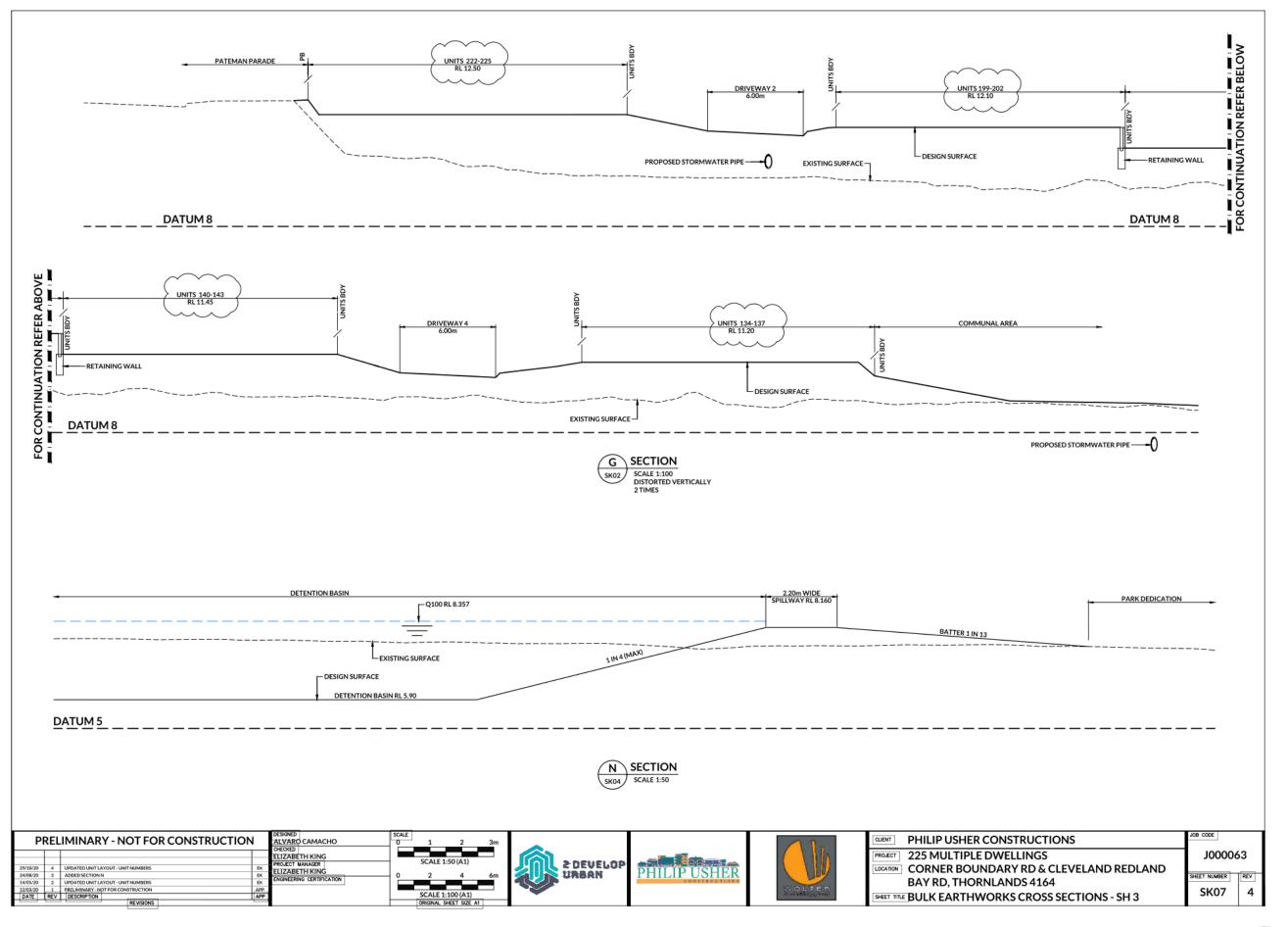


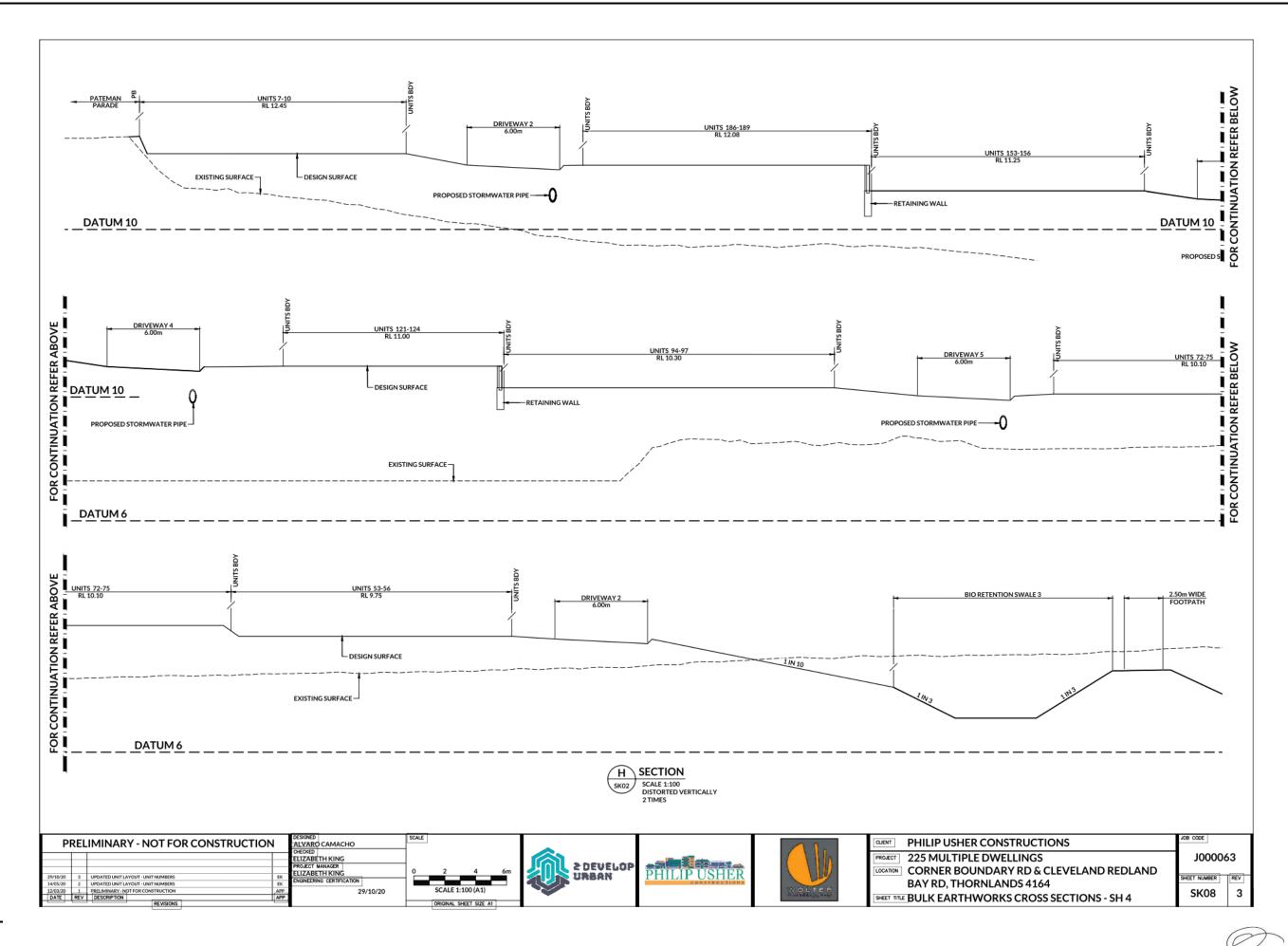




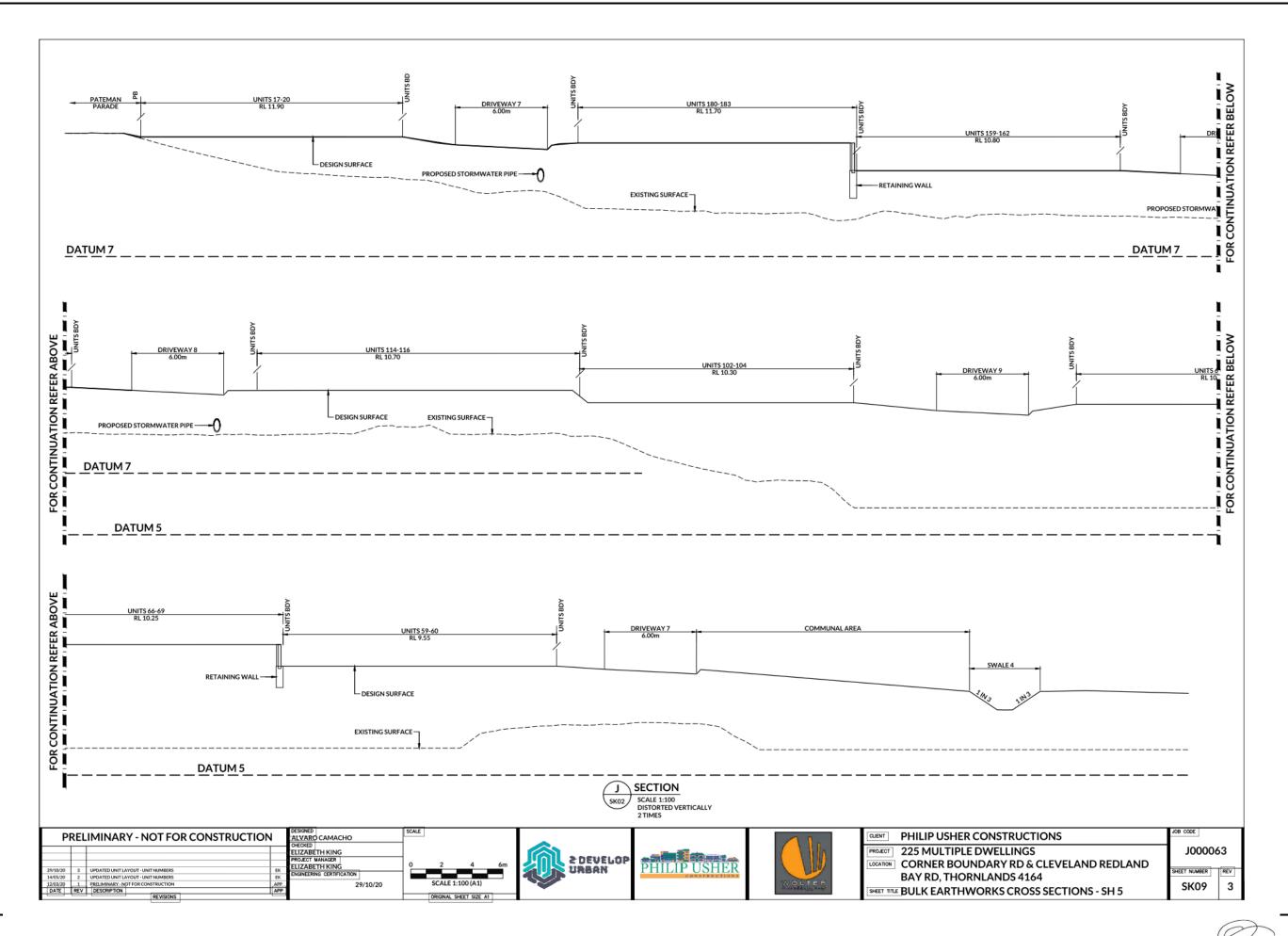


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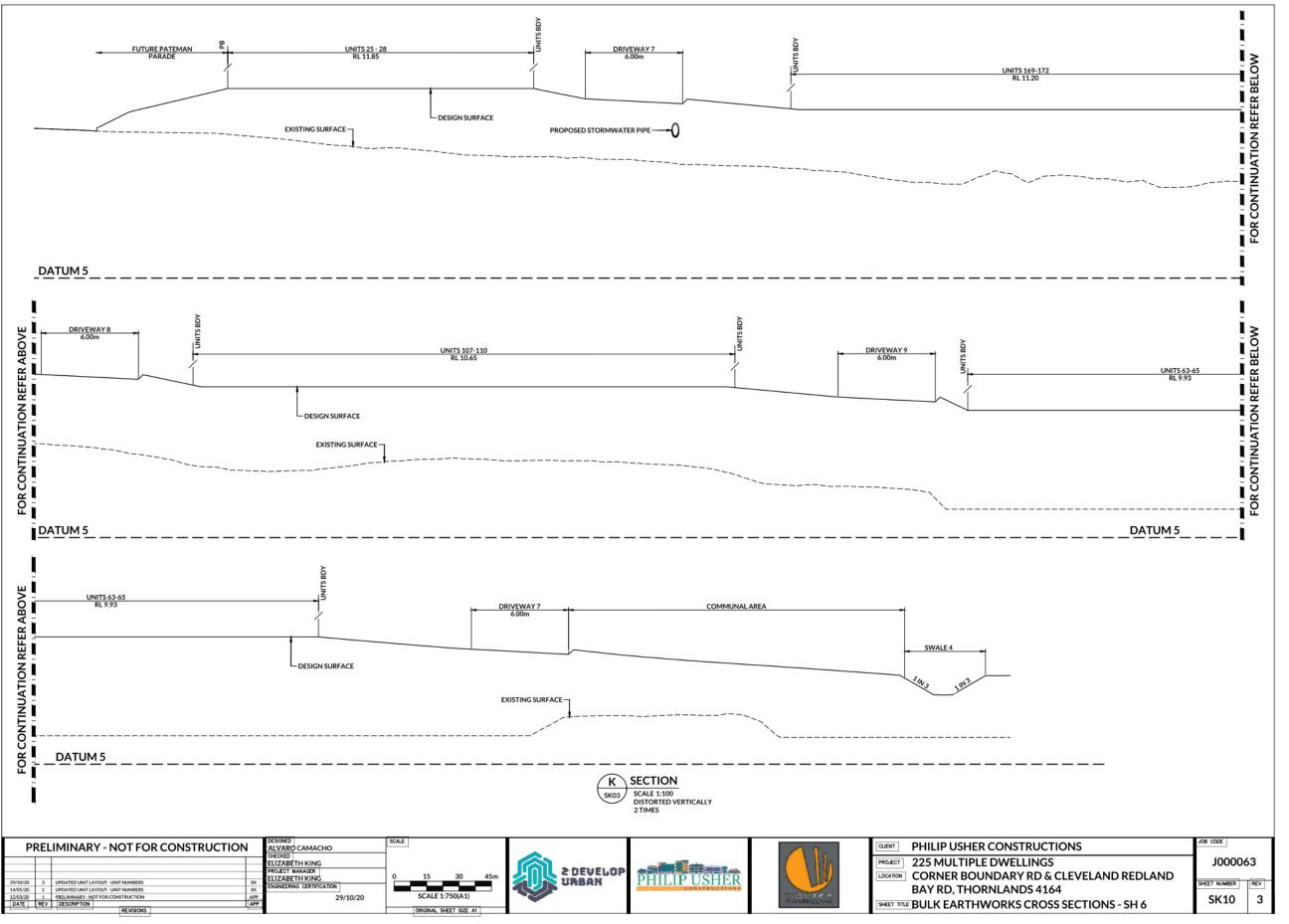


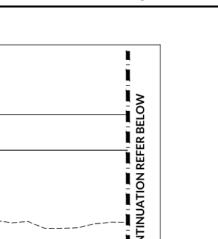


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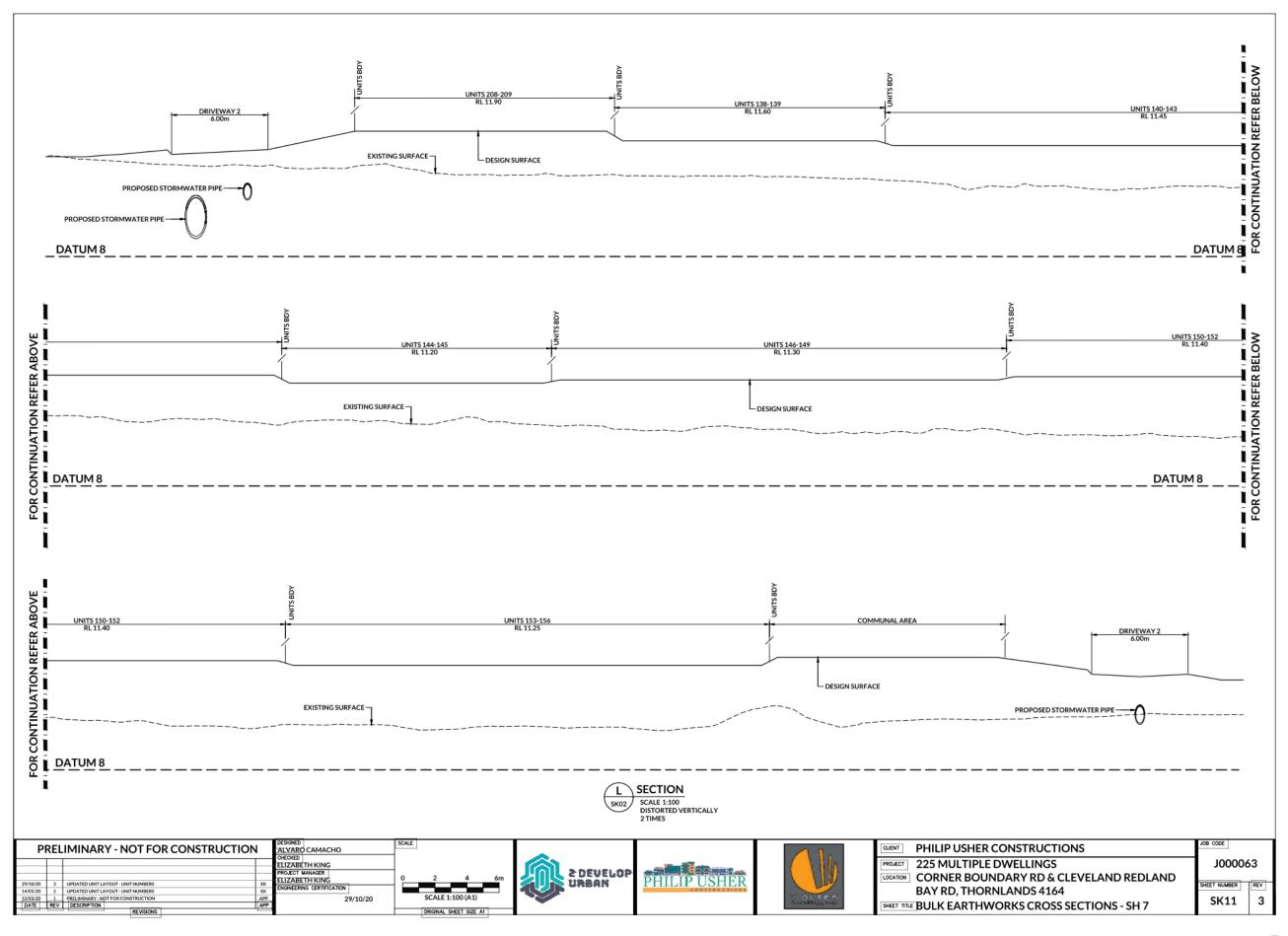


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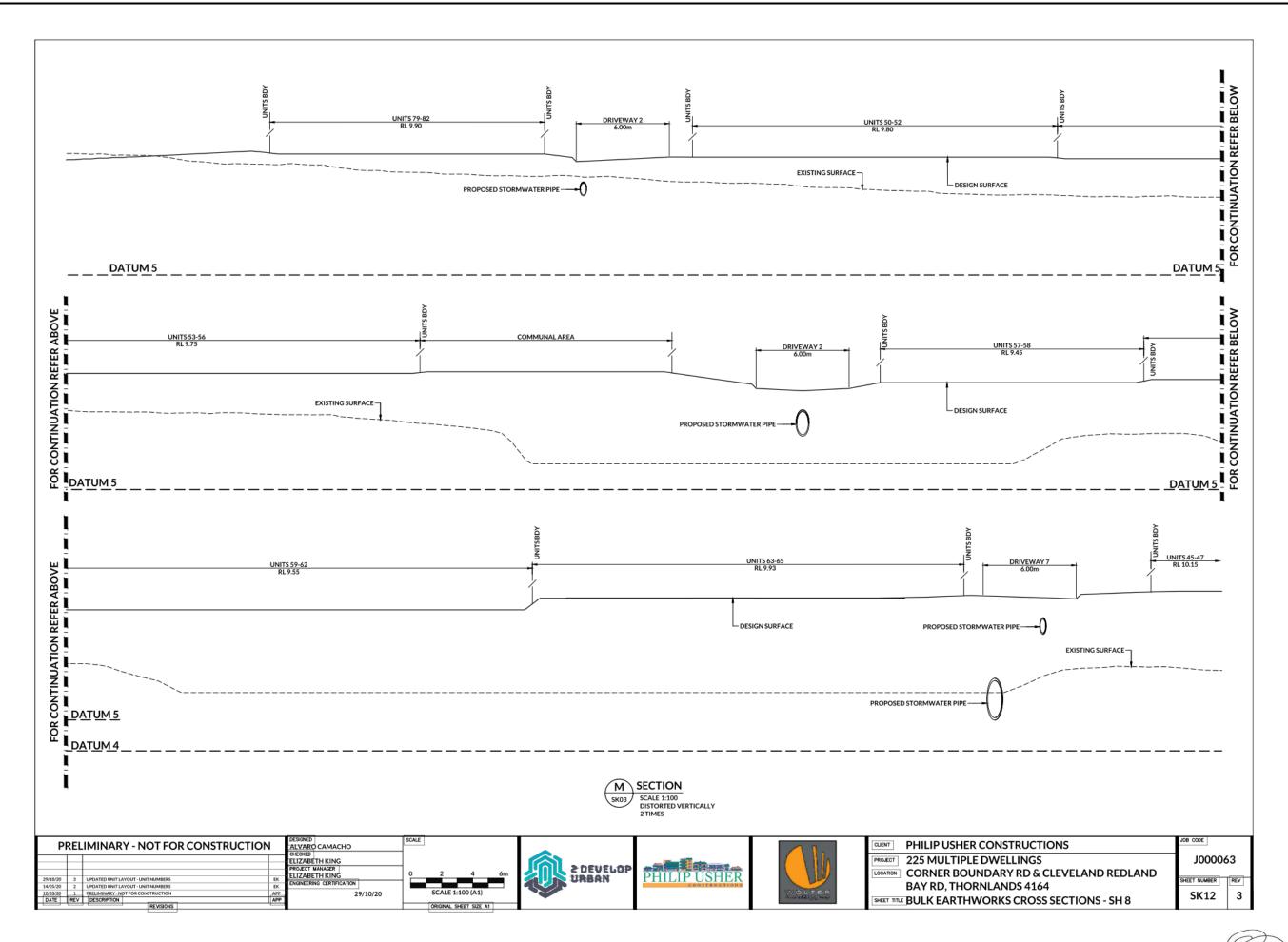




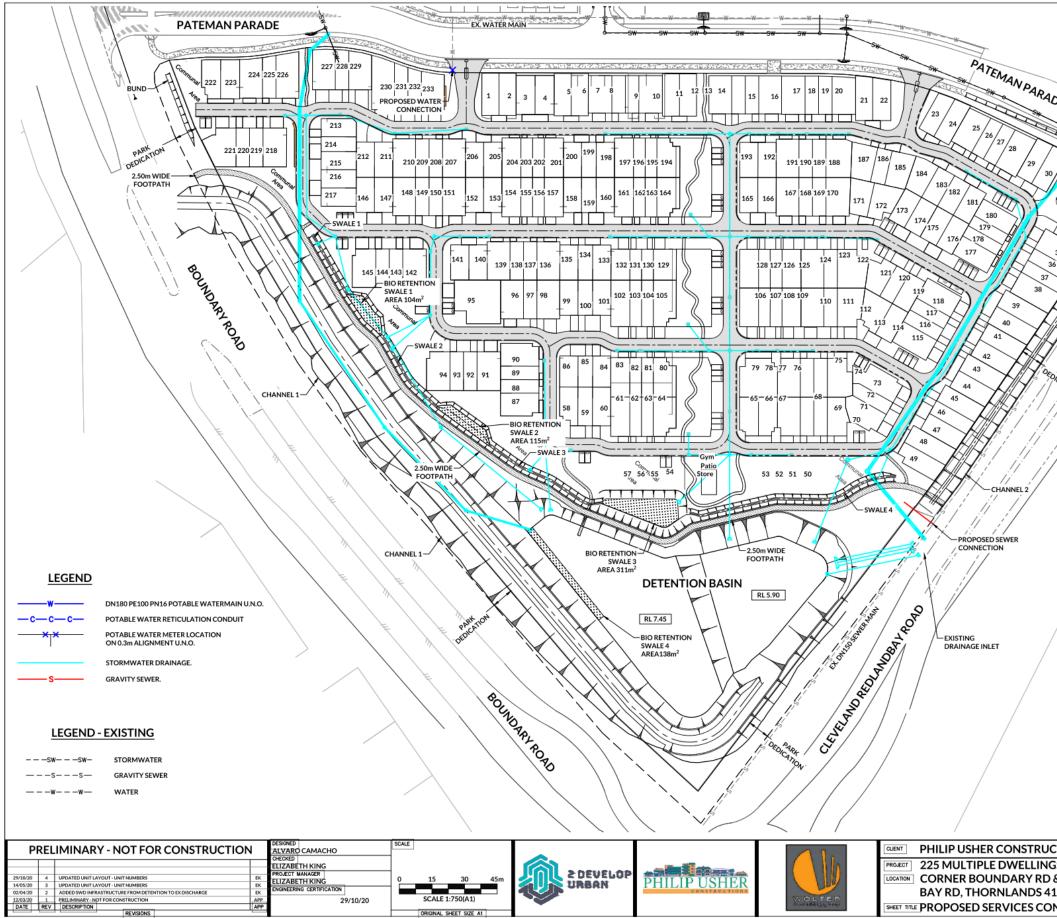
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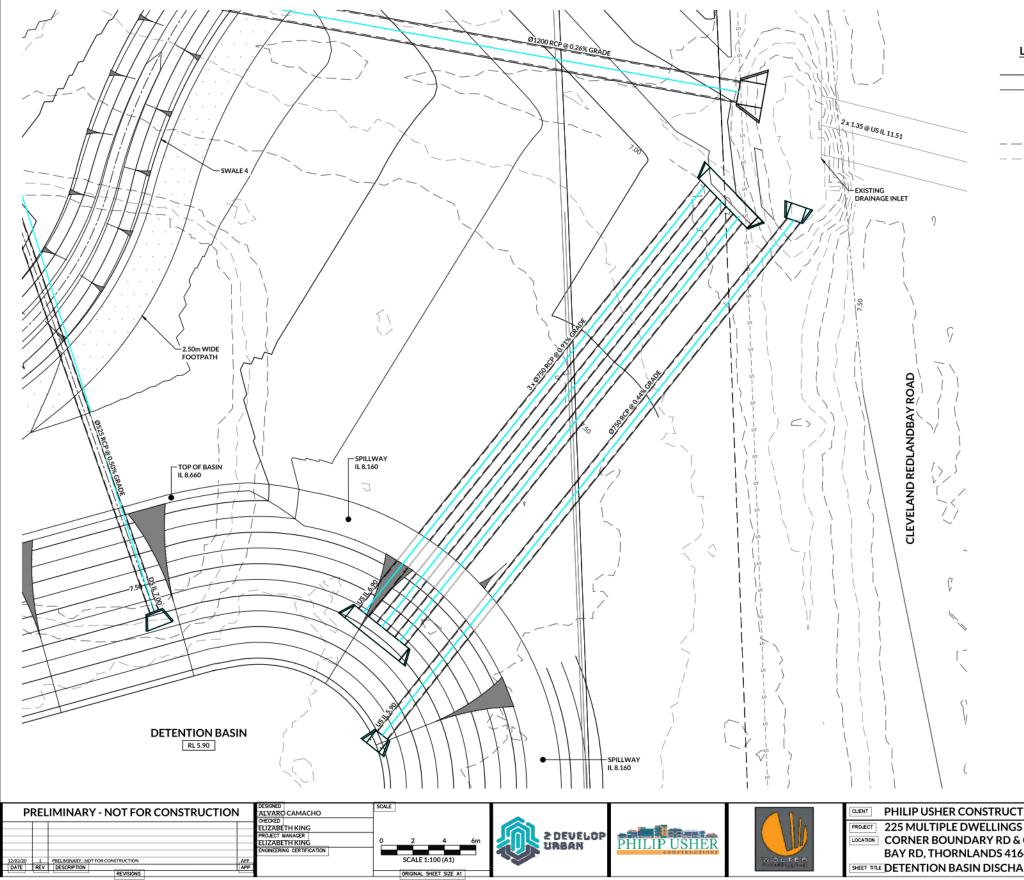




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LEGEND - PROPOSED

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FINISHED MAJOR CONTOURS (0.50m) FINISHED MINOR CONTOURS (0.25m)

LEGEND - EXISTING

MAJOR CONTOURS (0.50m) MINOR CONTOURS (0.25m)

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| ; CLEVELAND REDLAND | J000063 | 63 |
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| ARGE LAYOUT PLAN | SK14 | 1 |
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Philip Usher Constructions Pty Ltd

Cnr Boundary Rd & Cleveland Redland Bay Road, Thornlands Flood Impact Assessment

February 2016

WATER | ENERGY & RESOURCES | ENVIRONMENT | PROPERTY & BUILDINGS | TRANSPORTATION



Abbreviations

- 1D One-Dimensional
- 2D Two-Dimensional
- AEP Annual Exceedance Probability
- AHD Australian Height Datum
- ARI Average Recurrence Interval
- AR&R Australian Rainfall & Runoff
- BoM Bureau of Meteorology
- RCC Redland City Council
- CL Continuing Loss (mm/hr)
- DEM Digital Elevation Model
- IE Institute of Engineers
- IL Initial Loss (mm)
- LIDAR Light Detection and Ranging

GHD | Report for Philip Usher Constructions Pty Ltd - Cnr Boundary Rd & Cleveland Redland Bay Road, Thornlands, 41/26773 | i



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Appendices

Appendix A Site Development Layout

Appendix B Existing Condition Maps

Appendix C Developed Condition Maps

Appendix D Peak Flood Level Difference Maps

Appendix E Climate Change Assessment and Model Sensitivity Test

1. Introduction

1.1 Background

GHD was commissioned by Philip Usher Constructions Pty Ltd to undertake a Flood Impact Assessment for the proposed development on the corner of Boundary Road and Cleveland Redland Bay Road, Thornlands.

Previously in January 2006, GHD undertook flood mapping for the South East Thornlands Structure Plan for Redlands City Council (RCC). A hydrological model was developed for the greater Eprapah Creek catchment (28.5 km²) as part of the investigation. The proposed development at the corner of Boundary Road and Cleveland Redland Bay Road is within the extents of the hydrological model that was developed for the South East Thornlands Structure Plan.

It should be noted that all tasks associated with the preparation of this report have been undertaken on the basis that the data provided by others is correct. In particular GHD has relied on the accuracy of the LiDAR aerial survey and post-development design surface provided by Wolter Consulting and ETS Engineering respectively. Further qualifications relating to specific aspects of this study are discussed in Section 1.2.

1.2 Limitations

This report has been prepared by GHD for Philip Usher Constructions Pty Ltd and may only be used and relied on by Philip Usher Constructions Pty Ltd for the purpose agreed between GHD and the Philip Usher Constructions Pty Ltd as set out in section 1.1 of this report.

GHD otherwise disclaims responsibility to any person other than Philip Usher Constructions Pty Ltd arising in connection with this report. GHD also excludes implied warranties and conditions, to the extent legally permissible.

The services undertaken by GHD in connection with preparing this report were limited to those specifically detailed in the report and are subject to the scope limitations set out in the report.

The opinions, conclusions and any recommendations in this report are based on conditions encountered and information reviewed at the date of preparation of the report. GHD has no responsibility or obligation to update this report to account for events or changes occurring subsequent to the date that the report was prepared.

The opinions, conclusions and any recommendations in this report are based on assumptions made by GHD described in this report. GHD disclaims liability arising from any of the assumptions being incorrect.

GHD has prepared this report on the basis of information provided by Philip Usher Constructions Pty Ltd and others who provided information to GHD (including Government authorities)], which GHD has not independently verified or checked beyond the agreed scope of work. GHD does not accept liability in connection with such unverified information, including errors and omissions in the report which were caused by errors or omissions in that information.



2. Catchment Overview

The catchment comprises of two distinct areas; and area of 23.83 ha between Boundary Road and Cleveland-Redland Bay Road (containing the subject site) and an area of 14.47 ha upstream of Boundary Road. Three lots describe the site; Lot 10 on SP216148, Lot 18 on SP119616 and Lot 20 on SP140738. The catchment containing the site grades from RL 20.75 at the north-west to RL 5.75 at the south-east. The catchment upstream of Boundary Road grades from RL 29.5 at the southern boundary of the catchment to RL 12 at the boundary adjacent to Boundary Road.

Stormwater drainage infrastructure on the catchment is described below as per the Wolter Consulting Detail survey (dated 20.08.13, Reference Number SB2866_02_A). Photographs taken from a site visit are also presented.

 Boundary Road U/S culverts 4 x 0.8m x.0.45m box culverts (photograph shows downstream end of culverts);



D/S Boundary Road culverts 2 x 1350dia (photograph shows upstream end of culverts);





- Cleveland Redland Bay Road (recently re-located) 3 x 2.1m x 1.2m box culverts (photograph shows upstream end of culverts within downstream site boundary).



A catchment layout is provided in Figure 2-1.

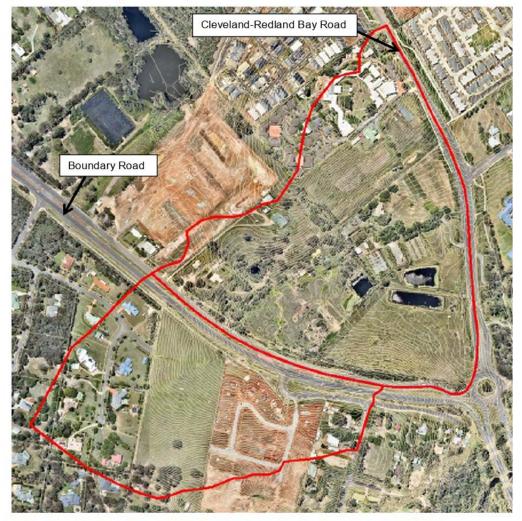


Figure 2-1 General catchment layout and topography (1 m contour lines)

2.1.1 Existing Scenario

Land use on the existing site catchment is predominantly scattered areas of brush & dense tree coverage, farm dams and empty sheds (calculated as 18% impervious, at higher end of rural category). Land use on the upstream catchment is predominantly low density urban residential with areas of open space.

Council's Planning Scheme V5.2.2013 and their Planning and Development Online system identifies part of the area as flood prone land (GHD's "South East Thornlands Urban Community Structure Plan – Flood Mapping Report May 2007).

The general surface water flow patterns in the local area are from the south-west, through the Boundary Road culverts, through the middle of the subject site, and then through the Cleveland-Redland Bay Road culverts to the east. Flow discharges to Eprapah Creek and ultimately Moreton Bay.

2.1.2 Proposed Development

For the purpose of this assessment, post development land use was based on Council's Planning Scheme V5.2.2013 and their Planning and Development Online system. The zoning includes areas of medium density to low density urban residential, park residential, open space, minor roads and areas designated to community purposes (calculated as 52% impervious using Planning Scheme & QUDM).

Appendix A includes the site development layout as provided by Philip Usher Constructions Pty Ltd.



3. Flood Assessment

3.1 Introduction

The following section of this report is separated into the hydrologic assessment and the subsequent hydraulic assessment.

3.2 Hydrologic Modelling

3.2.1 Overview

A hydrologic model was developed to calculate upstream catchment flows and flows off the site catchment. The two catchment areas shown in Figure 2-1 were broken down into seven subcatchments to represent in greater detail inflows from the different contributing areas. These hydrograph outputs were used in the subsequent hydraulic modelling. Flows were cross checked against results from the previous GHD (2006) Study.

3.2.2 Software

XP-RAFTS hydrologic modelling software is based on the RSWM model developed by the Snowy Mountains Engineering Corporation (SMEC) and is an industry standard rainfall-runoff routing analysis package. It is capable of modelling changes due to development for both rural and urban sub-catchments and is an accepted model used to quantify flood flows from catchments as specified in ARR.

XP-RAFTS was used in this study to estimate the runoff hydrograph from an individual subcatchment based on rainfall intensities, temporal patterns and the definition of parameters describing the sub-catchment characteristics. These parameters include the sub-catchment area, slope, roughness and fraction of impervious area. Sub-catchment outflow hydrographs are routed downstream through the model via links (either lag links or routing links) that connect these sub-catchments.

3.2.3 Hydrological model setup

The layout of the XP-RAFTS model used in this study is shown in Figure 3-1. All model parameters were left as per the program defaults unless noted herein.





Figure 3-1 XP-RAFTS model schematisation

Sub-catchment delineation

Sub-catchments were delineated within ArcGIS using the 1 m LIDAR DEM and survey data provided by Wolter Consulting. Manual edits to calculated catchment boundaries were made where required.

A total of seven catchments were used in the XP-RAFTS model, plus two junction nodes representing the flows arriving at the culverts upstream and downstream the site. Routing (lag) reaches were used in those cases where the hydrograph generated by a given sub-catchment travels along a drain before reaching the culvert inlet.

For those sub-catchments with distinctive pervious and impervious areas, two sub-catchments were used in each XP-RAFTS node, as per the software's user manual recommendation. Fraction impervious values for each land use were adopted as the largest of those provided in Table 10.3.18 of Redland's Priority Infrastructure Plan¹ and Table 4.5.1 of Queensland Urban Drainage Manual (Third edition 2013 – provisional).

¹ http://www.redland.qld.gov.au/PlanningandBuilding/RPS/Documents/V5.1_Documents/10.00%20with%20Maps%20-%20Large%20File%20Size.pdf

| Node Name | Area (ha) | % Impervious | Slope (%) | Lag time (minutes) |
|----------------|-----------|--------------|-----------|-----------------------|
| US West (1) | 2.521 | 0 | 3.38 | 5 |
| US West (2) | 2.521 | 100 | 5.50 | 5 |
| US Midwest (1) | 2.997 | 0 | 4.06 | 0 |
| US Midwest (2) | 1.285 | 100 | 4.00 | 0 |
| US Mideast (1) | 2.400 | 0 | 5.96 | 0 |
| US Mideast (2) | 1.029 | 100 | 5.90 | 0 |
| US East (1) | 0.924 | 0 | 2.04 | 5 |
| US East (2) | 0.396 | 100 | 2.04 | 5 |
| Road (1) | 0.285 | 0 | 1.98 | 0 |
| Road (2) | 0.664 | 100 | 1.90 | 0 |
| DS South (1) | 11.998 | 0 | 1.43 | 0 |
| DS South (2) | 0.000 | 100 | 1.45 | 0 |
| DS North (1) | 8.439 | 0 | 2.06 | 0 |
| DS North (2) | 2.110 | 100 | 2.00 | 0 |
| | | | | |

Table 3-1 describes the sub-catchment characteristics used in the XP-RAFTS model representative of existing conditions.

| Table 3-1 XP-RAFTS | catchment | properties - | - existing | condition |
|--------------------|-----------|--------------|------------|-----------|
|--------------------|-----------|--------------|------------|-----------|

Table 3-2 shows the characteristics of the subcatchments for the developed condition. Developed areas were sourced from Philip Usher Construction's site layout plan, shown in Appendix A.

| Node Name | Area (ha) | % Impervious | Slope (%) | Lag time (minutes) |
|----------------|-----------|--------------|-----------|-----------------------|
| US West (1) | 2.521 | 0 | 3.38 | 5 |
| US West (2) | 2.521 | 100 | 5.50 | 5 |
| US Midwest (1) | 2.997 | 0 | 4.06 | 0 |
| US Midwest (2) | 1.285 | 100 | 4.00 | 0 |
| US Mideast (1) | 2.400 | 0 | 5.96 | 0 |
| US Mideast (2) | 1.029 | 100 | 5.90 | 0 |
| US East (1) | 0.924 | 0 | 2.04 | 5 |
| US East (2) | 0.396 | 100 | 2.04 | 5 |
| Road (1) | 0.285 | 0 | 1.98 | 0 |
| Road (2) | 0.664 | 100 | 1.90 | 0 |
| DS South (1) | 8.782 | 0 | 1 43 | 0 |
| DS South (2) | 3.215 | 100 | 1.43 | 0 |
| DS North (1) | 7.226 | 0 | 2.06 | 0 |
| DS North (2) | 3.323 | 100 | 2.06 | 0 |

Table 3-2 XP-RAFTS catchment properties – developed condition

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Detention basins

One retarding basin was included in the XP-RAFTS model, representing the basin constructed as part of an existing upstream residential subdivision development (corresponding to the US Mideast subcatchment, see Figure 3-1). Geometric characteristics of the detention basin are shown in Table 3-3. This data was extracted from a DRAINS model of the site, provided to GHD by ETS Engineering.

Table 3-3 Upstream development detention basin - storage characteristics

| Stage (mAHD) | Area (m ²) | Volume (m ³) |
|--------------|------------------------|--------------------------|
| 13.000 | 530 | 0 |
| 13.500 | 815 | 336.25 |
| 14.000 | 1130 | 822.50 |

The basin outlet characteristics in XP-RAFTS were tweaked to achieve the same outflows as the provided DRAINS model.

Equal area slopes

Equal area slopes for each sub-catchment (XP-RAFTS node) were calculated based on the merged DEM and the longest flow path lines. Table 3-1 and Table 3-2 provide the equal area slopes for each sub-catchment.

Roughness

A uniform Manning's 'n' roughness value of 0.06 was initially used for the pervious component of the XP-RAFTS nodes, which is a reasonable estimate of average rural/urban catchment conditions. A Manning's 'n' roughness value of 0.02 was used for the impervious components of the nodes.

Losses

The initial / continuing rainfall loss model was used to estimate design storm hydrographs. An initial loss of 0 mm and a uniform continuing loss of 2.5 mm/hr were applied to the pervious areas of the catchment. An initial loss of 0 mm reflects the fact that design storm 'bursts' may occur following a period of lower intensity rainfall that saturates the catchment. The uniform continuing loss of 2.5 mm/hr was based on Table 3.6 of ARR Book 2. For the impervious areas, both the initial and continuing loss parameters were set to 0 mm.

Storm events

The "automatic storm generator" function with XP-RAFTS was used to select the appropriate combinations of rainfall, temporal patterns and simulation parameters and then generate design storm hydrographs for the 2-year, 10-year, 50-year and 100-year ARI events. The routing increment was set to 1 minute, and the simulation time was set to 10 times the storm duration. XP-RAFTS was used to generate design storm hydrographs for the events shown in Table 3-1.

Table 3-4 XP-RAFTS Design Storm Events

| ARI (years) | Storm Duration |
|-------------------|--|
| 2, 10, 20, 50 and | ARR Zone 3 Temporal Pattern |
| 100 | 30 minutes, 45 minutes, 1 hour, 1.5 hours, 2 hours and 3 hours |

Results for the existing and developed cases show that the critical storm duration is 60 minutes for both culverts (upstream and downstream the site).



Storage coefficients and exponents

XP-RAFTS treats each sub-catchment as a conceptual lumped storage, with a storagedischarge relationship given by:

Equation 1: S = Bqⁿ⁺¹ Where: S = storage (m³) q = discharge (m³/s) B = storage delay time coefficient n = storage non-linearity exponent

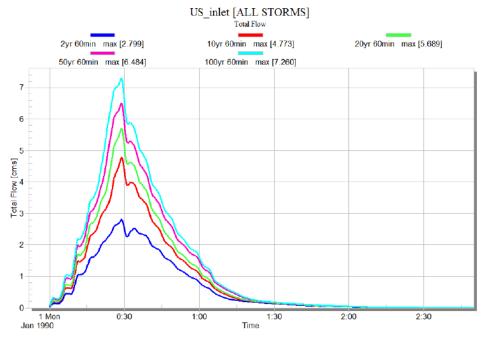
In the absence of calibration or regional regression estimates, B is calculated internally by XP-RAFTS according to the following:

| Equation 2: | | $B_{AV} = 0.285 \ A^{0.52} \ (1+U)^{-1.97} \ S_{C}^{-0.5}$ |
|-------------|--------|--|
| | Where: | A = sub-catchment area (km ²) |
| | | U = fraction of the sub-catchment that is urbanised |
| | | S_c = main drainage slope of the sub-catchment (%) |

The default value for n (-0.285) and the default equation for B have been left unmodified. The global storage modification factor, B_X , has been left as default (i.e. $B_X = 1$).

3.2.4 Hydrological model results

Figure 3-2 and Figure 3-3 show the hydrograph results from the hydrological model, for the different events (return periods) analysed. These hydrographs represent the sum of the respective sub-catchment hydrographs, lumped at the culvert inlets upstream (Figure 3-2) and downstream (Figure 3-3) the site.





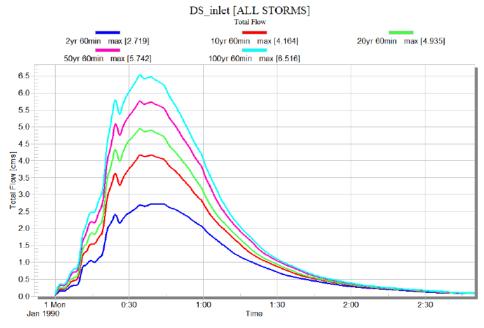


Figure 3-3 Downstream culvert inlet hydrographs – Existing case

Figure 3-4 and Figure 3-5 respectively show the upstream and downstream culvert inlet hydrographs for the developed case. Results for the upstream culver inlet for the developed case coincide with those for the existing case, since any future developments upstream will have to include sufficient on-site peak flow attenuation to at least not worsen the existing condition.

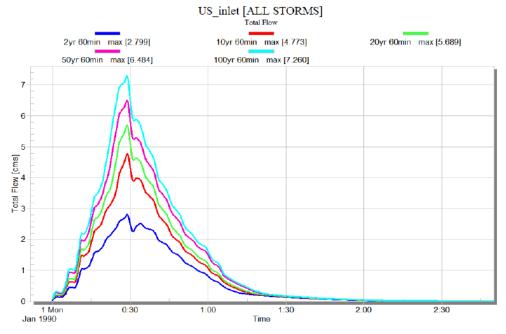


Figure 3-4 Upstream culvert inlet hydrographs - Developed case

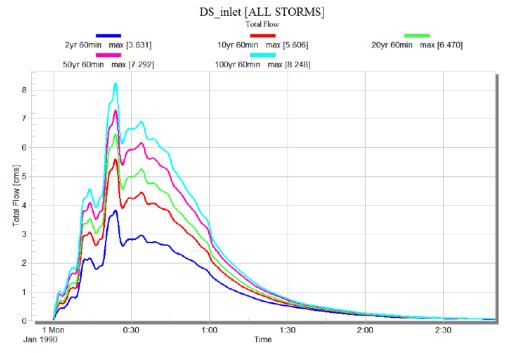


Figure 3-5 Downstream culvert inlet hydrographs - Developed case

The comparison of downstream culvert inlet hydrographs between the existing and developed case (Figure 3-3 and Figure 3-5) shows that in the developed case peak flows increase approximately by 20% to 25% with respect to the base case ones. It can also be seen that the increase of impervious area causes the hydrographs to peak earlier than for the existing case.

3.2.5 Comparison to previous studies

The results of previous studies were reviewed as part of this hydrological assessment. Direct comparison of results is not possible in all cases, as the hydrological methods and catchments vary. The review of other studies confirmed the general appropriateness of the current hydrological model in determining design storm hydrographs for the current purpose.



3.3 Hydraulic Modelling

3.3.1 Overview

A hydraulic model was developed to determine flooding extents and levels within the site.

3.3.2 Software

TUFLOW (WBM-BMT, 2010) is a 2D unsteady flow hydrodynamic modelling tool developed by WBM-BMT. TUFLOW is oriented towards establishing flow and inundation patterns in coastal waters, rivers and floodplains as well as urban areas. TUFLOW solves the depth-averaged 2D shallow water equations for flows such as for the free-surface flows occurring from floods and tides based on the creation of an appropriate-resolution DEM, surface inflows, surface roughness and boundary conditions. TUFLOW is recognised as an industry standard 2D hydrodynamic modelling package within Australia and is well-suited to the modelling of the waterways and floodplains within the site.

3.3.3 Model Extent & Resolution

The extent of the model was selected to cover the floodplains within the overall catchment area. A 2 m grid size was chosen as a reasonable balance between model run times and appropriate representation of the small-scale features of the floodplain.

3.3.4 Timestep

A 0.5 second time step was used for both the 1D and 2D models respectively.

3.3.5 Digital Elevation Model

As mentioned previously, the DEM provided by Wolter Consulting was used as a base for the representation of topography in the hydraulic model. The following steps were also undertaken for DEM refinement:

- Creation of TUFLOW modified specific elevation points based on user-defined polygons to smooth the topography in cells immediately upstream and downstream of culverts. This is a requirement for model stability and has a negligible impact on peak flood level results.
- 2. Application of TUFLOW modified specific elevation points based on user-defined ridge lines. This technique was used to appropriately represent critical hydraulic controls such as road crests where such features are smaller than TUFLOW's normal sampling density as well as the edge of the 1D nested regions to ensure smooth flow transition.

3.3.6 Roughness

The spatial distribution of hydraulic roughness was defined by polygons representing areas of homogenous surface type. The polygons were generated manually using the aerial imagery and photographs from a site visit. The roughness categories and Manning's "n" values shown in Table 4-1 were used in the hydraulic model.

Table 3-5 TUFLOW Roughness Categories

| Category | Manning's "n" | Description |
|----------|---------------|------------------|
| 1 | 0.06 | Open ground |
| 2 | 3.0 | Building |
| 3 | 0.1 | Dense vegetation |
| 4 | 0.02 | Road |
| 5 | 0.03 | Pond/ open water |

3.3.7 Hydraulic Structures

Three sets of hydraulic structures were included as 1D components nested within and coupled to the 2D domain. These structures are Culvert 1, Culvert 2 and Culvert 3 as identified on Figure 3-6. Culverts 1 and 2 are located on Boundary Road and Culvert 3 is located on Cleveland Redland Bay Road. Table 3-6 contains the parameters used in the TUFLOW model. The estimated parameters were based on the survey data supplied by Wolter Consulting, photographs taken on site and the aerial image.

Table 3-6 1D Culvert Modelling Parameters

| Parameter | Culvert 1- Footpath Adjacent to Boundary Road | Culvert 2 – Boundary Road | Culvert 3 – Cleveland Redland Bay Road |
|---------------------------|---|---------------------------------|--|
| Shape | Box | Pipe | Box |
| Material | Concrete | Concrete | Concrete |
| Diameter (m) | - | 1.35 | - |
| Width x Height | 0.8 x 0.45 | - | 2.1 x 1.2 |
| Number of barrels | 4 | 2 | 3 |
| Manning's "n" | 0.013 | 0.013 | 0.013 |
| Length (m) | 3.72 | 43 | 28.3 |
| Upstream invert (m AHD) | 12.95 | 11.47 | 5.72 |
| Downstream invert (m AHD) | 12.89 | 10.58 | 5.5 |
| Blockage (%) | 50 | 50 | 50 |
| Entry loss | 0.5 | 0.5 | 0.5 |
| Exit loss | 1 | 1 | 1 |

3.3.8 Boundary Conditions

The TUFLOW model has boundaries to represent the inflow of water from external catchments, the distribution of local runoff within the model, and to allow water to leave the model at the downstream end. The external flows and the downstream boundary are represented by lines along the model's edge and the internal flows are polygons within the 2D domain. For all internal boundaries, runoff hydrographs (i.e. pre-determined flow rates from the hydrological model) are used.

TUFLOW automatically reads the hydrographs generated by XP-RAFTS for each storm event. The internal and external boundaries are listed in Table 3-7, with the names corresponding to the applicable nodes from the XP-RAFTS model. The boundary locations are shown on Figure 3-6.



Table 3-7 TUFLOW Model Boundaries

| Boundary Type | Name |
|------------------------------|--------------------|
| External (2d_bc) - tailwater | Outlet |
| Internal Hydrograph (2d_sa) | US_inlet, DS_inlet |

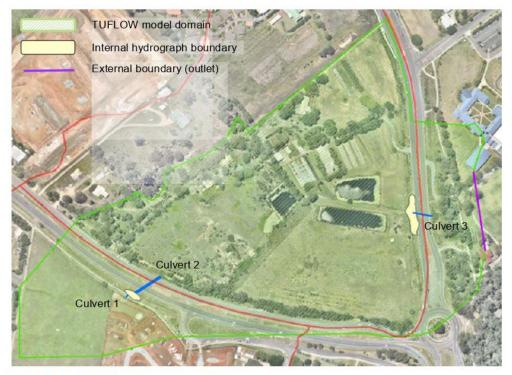


Figure 3-6 TUFLOW model schematisation

3.3.9 Initial Water Levels

An initial water level of 6.5 m AHD (equal to the constant downstream tailwater level) was applied at the start of the simulation.

3.3.10 Storm Events

The 2-year, 10-year, 20-year, 50-year and 100-year ARI hydrographs corresponding to the 60minute duration storm events (critical storm duration) were modelled in TUFLOW. The 90minute, 100 year ARI event was also analysed, however the resulting water levels and velocities were across the entire model domain lower than the 60-minute results. Thus, the 60-minute storm event results were only considered.

3.3.11 Results

Appendix B shows peak flood levels mapped for the existing case, Appendix C shows peak flood levels mapped for the developed case and Appendix D shows mapped afflux values (difference in existing and developed case peak flood levels). For simplicity reasons, afflux values are only shown in those areas where the developed peak water level is higher than the existing one (positive afflux).



| Event ARI | Peak flow existing (m ³ /s) | Peak flow developed (m ³ /s) | Difference (m ³ /s) |
|-----------|---|--|--------------------------------|
| 2 year | 5.224 | 5.377 | +0.153 |
| 10 year | 6.512 | 5.542 | -0.970 |
| 20 year | 7.679 | 6.341 | -1.338 |
| 50 year | 8.709 | 7.215 | -1.494 |
| 100 year | 9.442 | 7.870 | -1.572 |

Table 3-8 Peak flows through Culvert 3 (downstream of site)

Results in Table 3-8 indicate that only for the 2 year ARI there seems to be an increase of peak flows arriving to the culvert downstream the site –causing an increase of water levels of up to 5 mm immediately downstream the site, as shown in Appendix B, C and D. The likely reason for this increase of flow at the culvert downstream the site is that the design of the approach channel arriving to the culvert is more efficient in the developed case than in the existing case. This causes that for small events in which on-site attenuation is not significant, the culvert in the developed case conveys more flow than in the existing case.

Additionally, for the 2 year ARI event there is an increase in water levels of up to 3 mm immediately upstream the entrance of the culvert located upstream the site. At this location, rarer events produce peak water levels that are in all cases lower than the existing ones.

For the 10-year ARI and rarer events it can be seen that peak flows arriving at the culvert downstream the site are approximately 15% to 20% smaller than those for the existing case, due to the effect of on-site attenuation. These results seem to indicate that there is room to optimise the on-site attenuation configuration, potentially reducing earthworks.

3.3.12 Climate Change Assessment

A climate change scenario was tested in the model by increasing inflows by 20% in the 100year ARI event. It was not necessary to modify tailwater conditions for this assessment, since tailwater levels are well above projected climate changed sea levels.

Results for this assessment are shown in Appendix E. Maximum water levels for the climate change scenario are up to 0.20 m higher than for the existing condition scenario, and up to 0.15 m higher than for the developed condition scenario. No increases in peak water levels are observed upstream or downstream the site.

3.3.13 Sensitivity Test

A sensitivity test was performed to estimate the impacts on results due to an underestimation of Manning's roughness values. Thus, Manning's roughness values shown in Table 3-5 were increased by 20% for the 100-year ARI event.

Results of this sensitivity test are presented in Appendix E. Maximum water levels for the +20% Manning's scenario are up to 0.03 m higher than for both the existing and developed condition scenarios. No increases in peak water levels are observed upstream or downstream the site.



4. Conclusion

Hydrological and hydraulic modelling has been performed to estimate changes to maximum water levels at the proposed development on the corner of Boundary Road and Cleveland Redland Bay Road, Thornlands, as well as properties located upstream and downstream the site.

A range of storm events and design return periods were analysed, as well a sensitivity test and a climate change scenario.

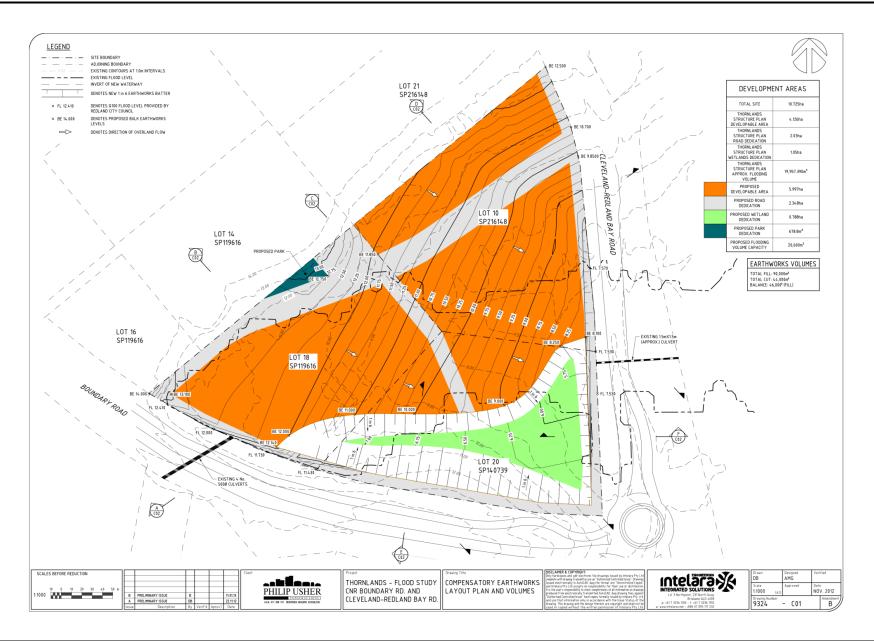
Results from these analyses indicate that the proposed on-site storage design has sufficient capacity to attenuate the increase in peak flows generated by the change in land use due to the site development. Moreover, there is potential for an optimisation of earthwork volumes, as peak flows exiting the site for the developed scenario are about 15% lower than those for the existing one in the 1% AEP event.

Results for the 2 year ARI scenario indicate that the is a slight increase in peak flows in the culvert downstream the site, likely due to a culvert inlet design and site flow path that is more efficient than the existing inlet characteristics. This causes an increase of up to 5 mm in maximum water levels in the property immediately downstream the site. There is no significant increase in flood extent associated with the marginally higher flood levels, and the increases do not affect any existing buildings. Additionally, there is up to 3 mm increase in water levels immediately upstream the site, for the 2 year ARI scenario (although these increases are constrained entirely within the road reserve). For all other events, the proposed development reduces peak flood levels and flow rates external to the site.



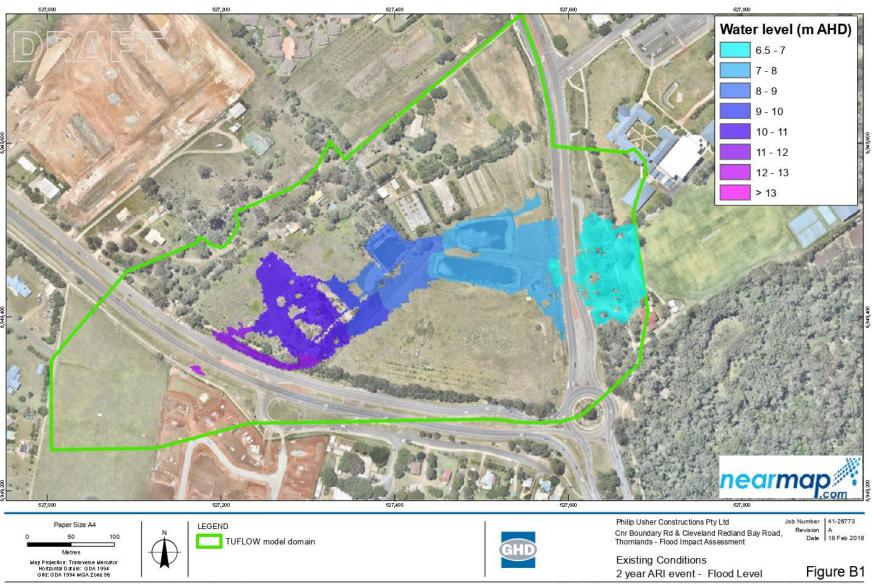
0 Page 379

Appendices





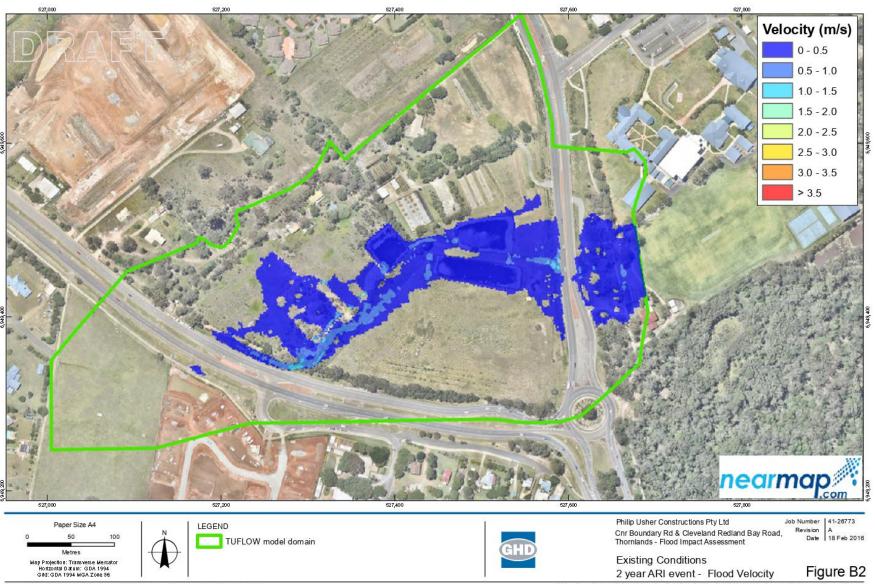
Appendix B Existing Condition Maps



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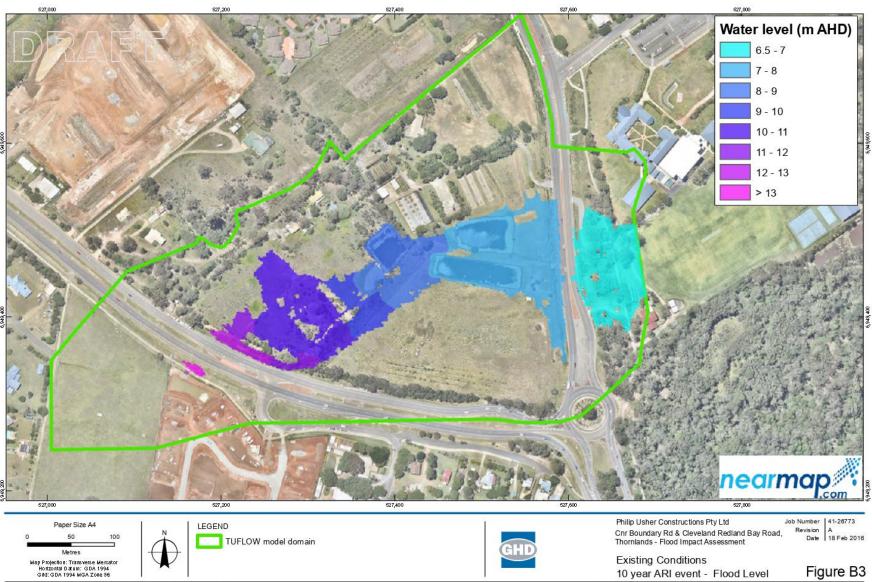
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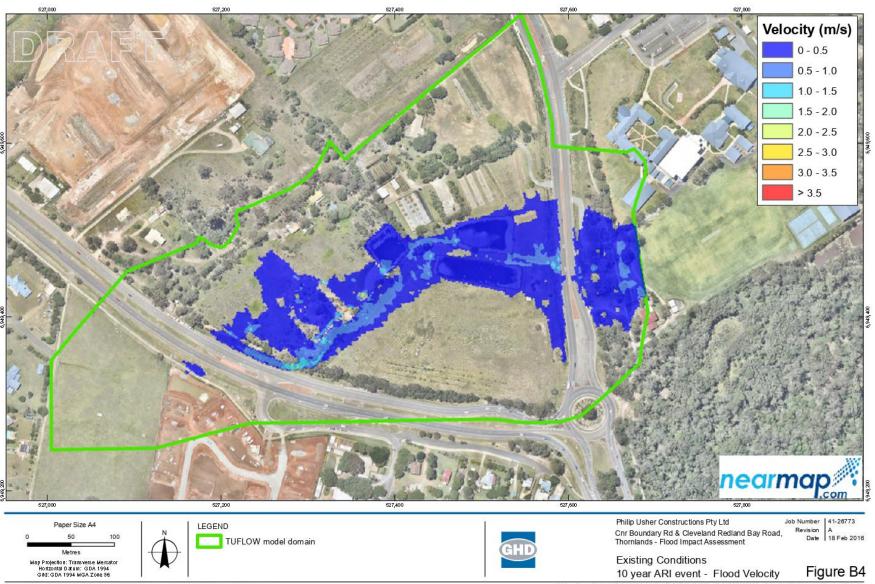
145 Ann Street Brisbane QLD 4000 Australia T 61 7 3316 3000 F 61 7 3316 3333 E bremall@ghd.com W www.ghd.com



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145 Ann Street Brisbane QLD 4000 Australia T 61 7 3316 3000 F 61 7 3316 3333 E bremali@ghd.com W www.ghd.com

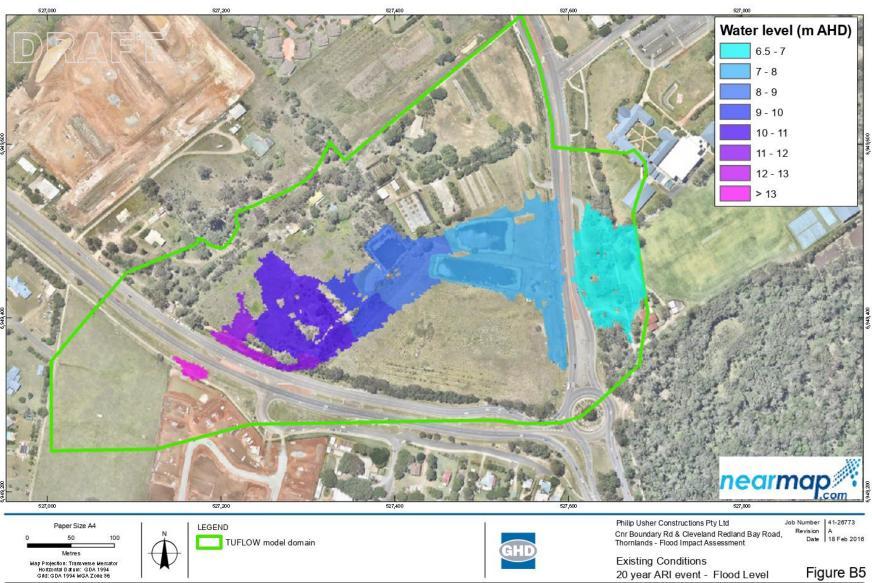
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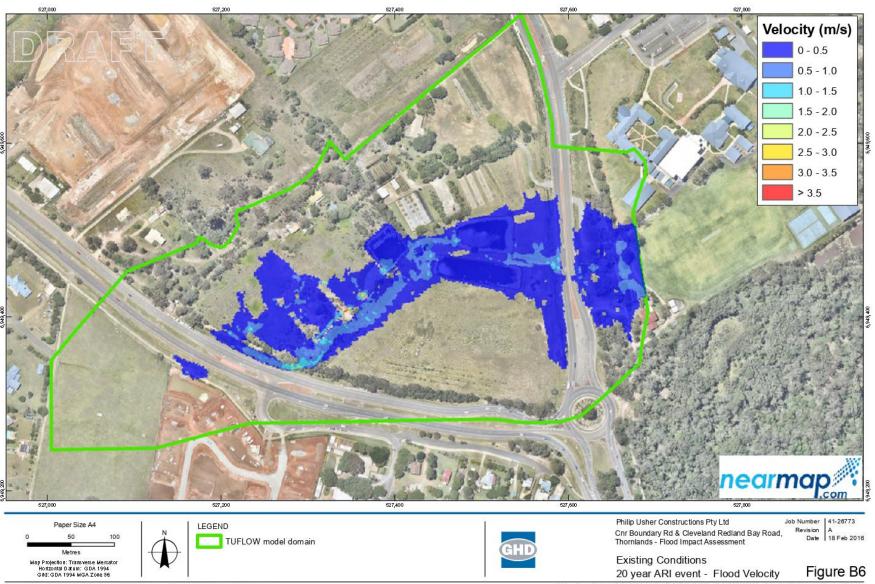
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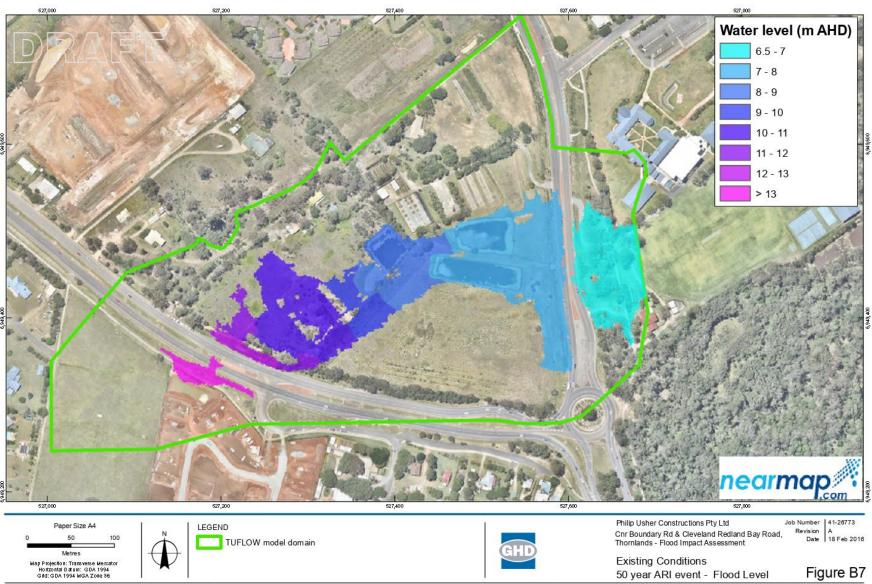
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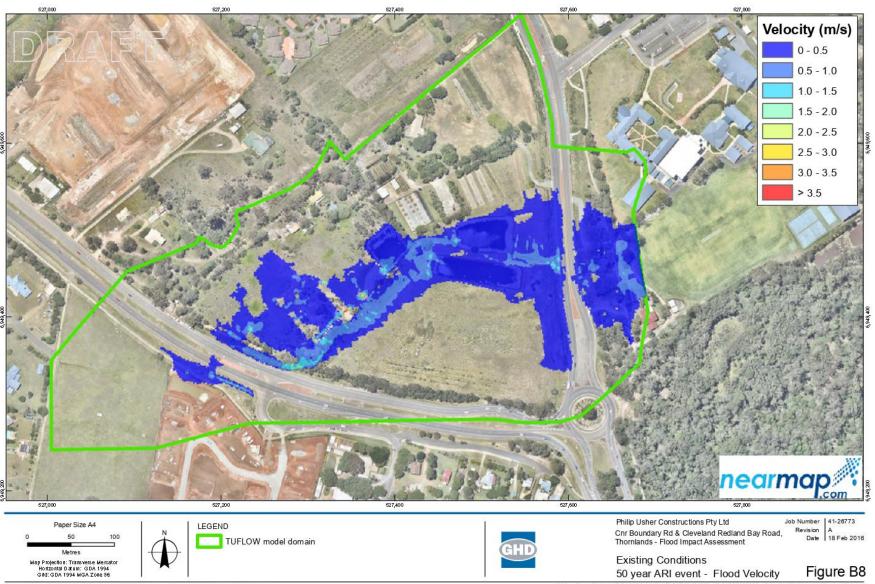
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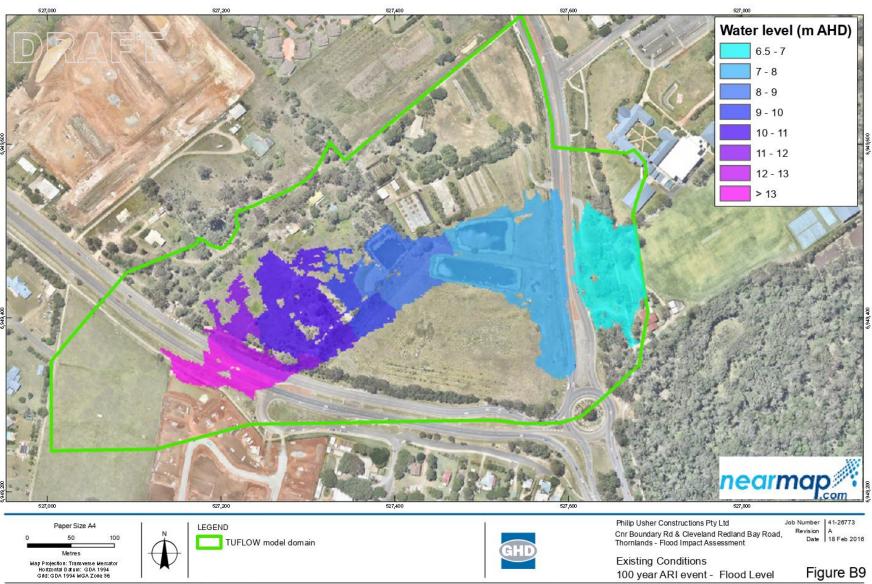
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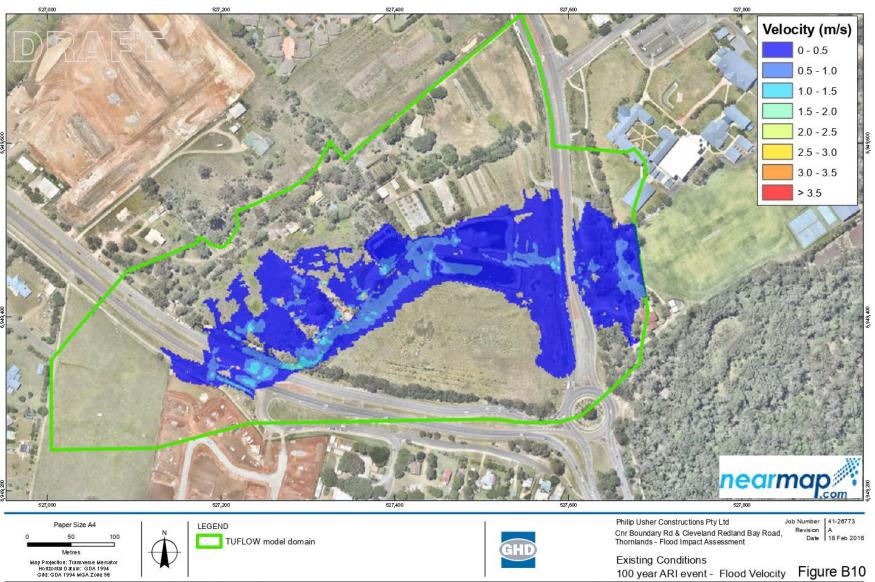
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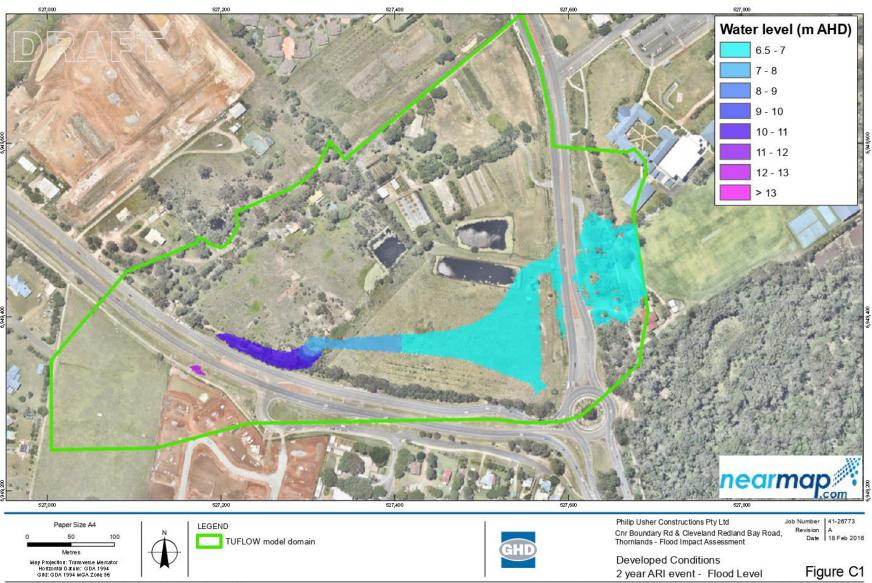


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Appendix C Developed Condition Maps

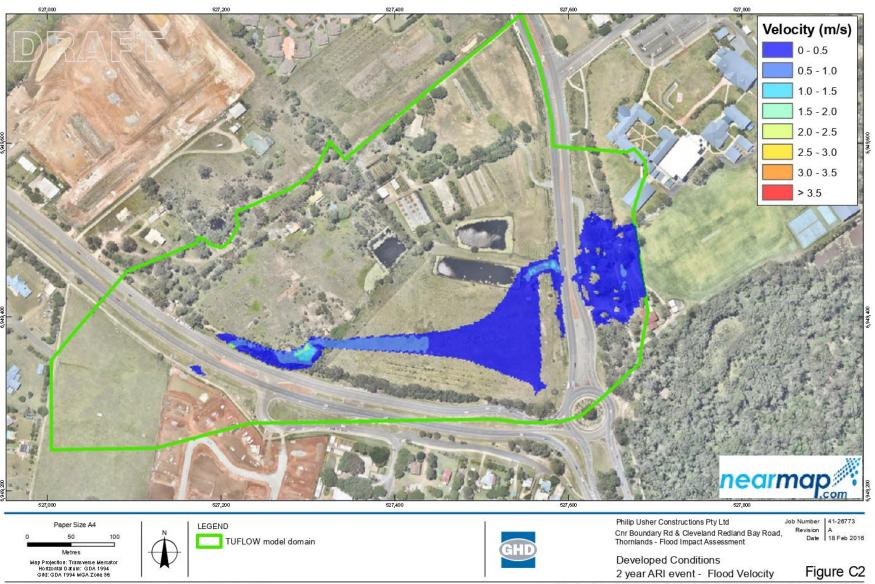




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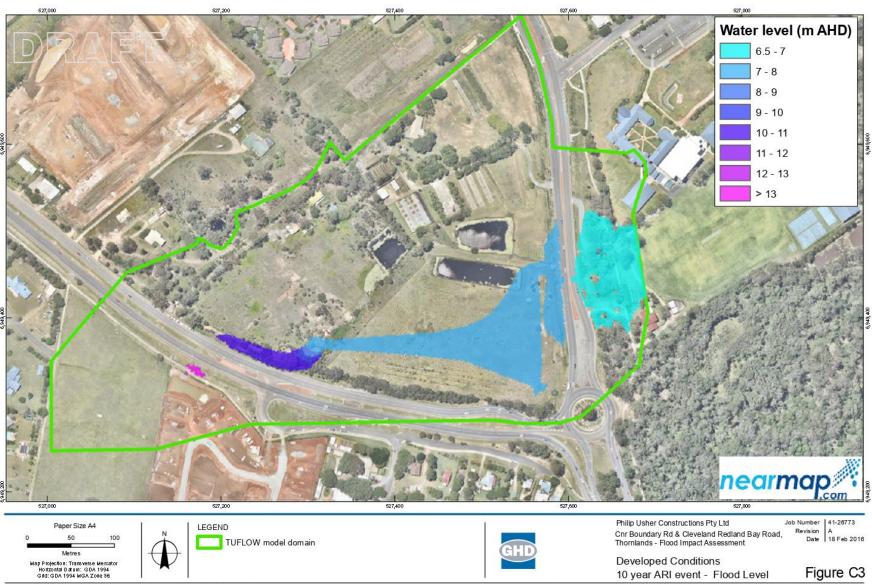
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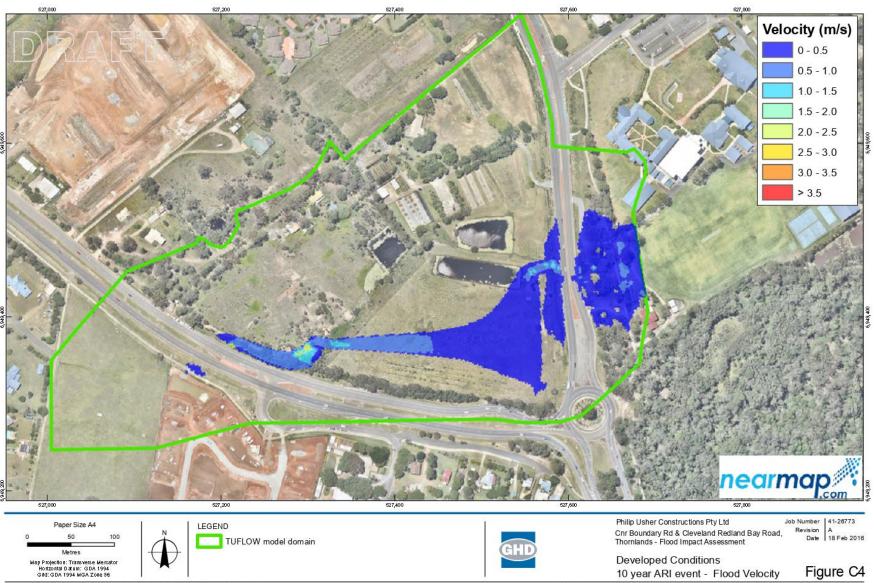
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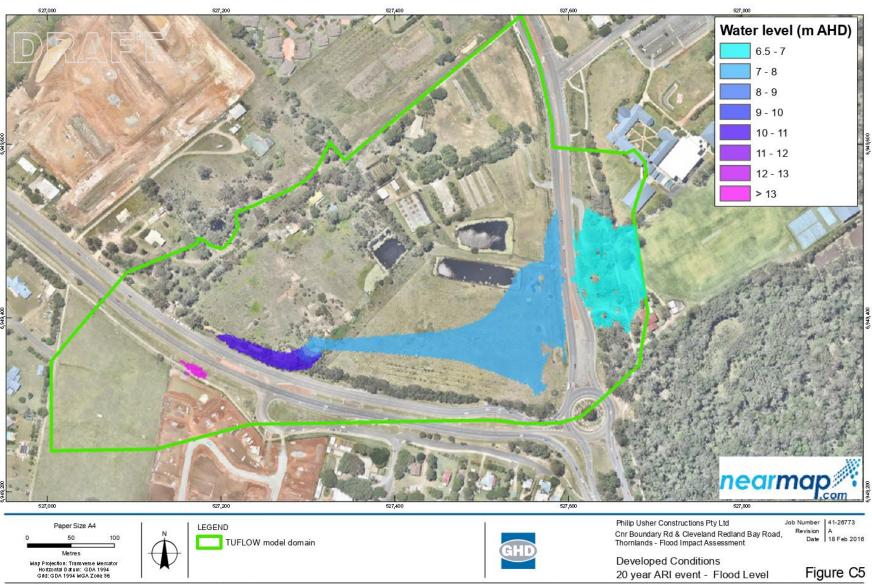
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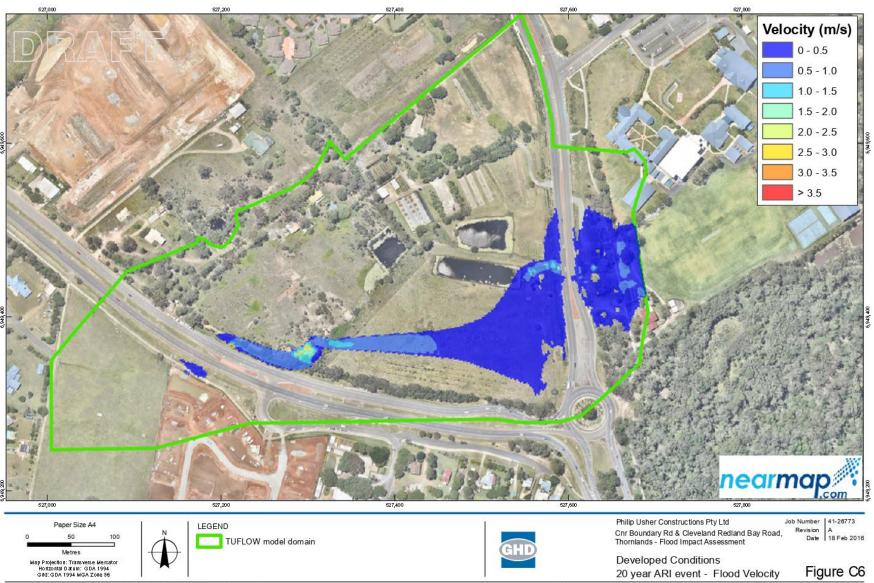
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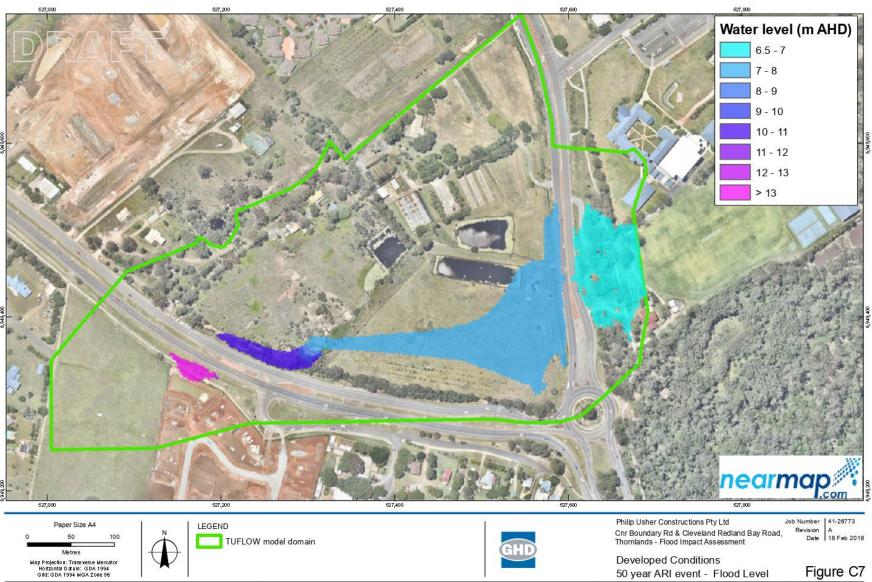
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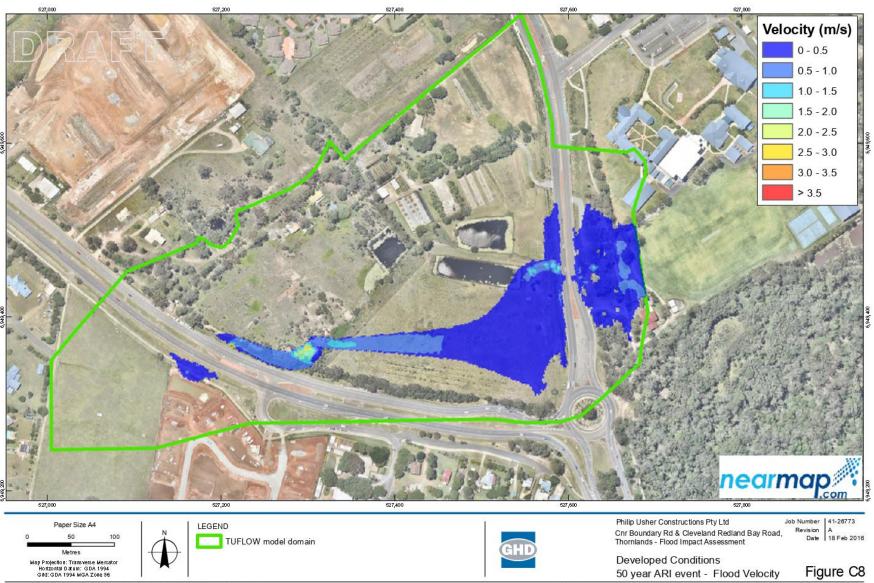
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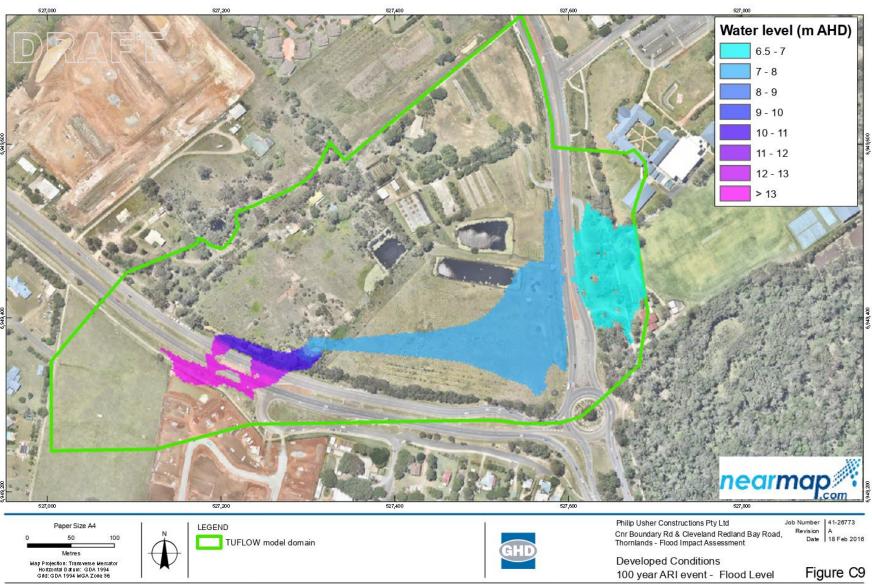
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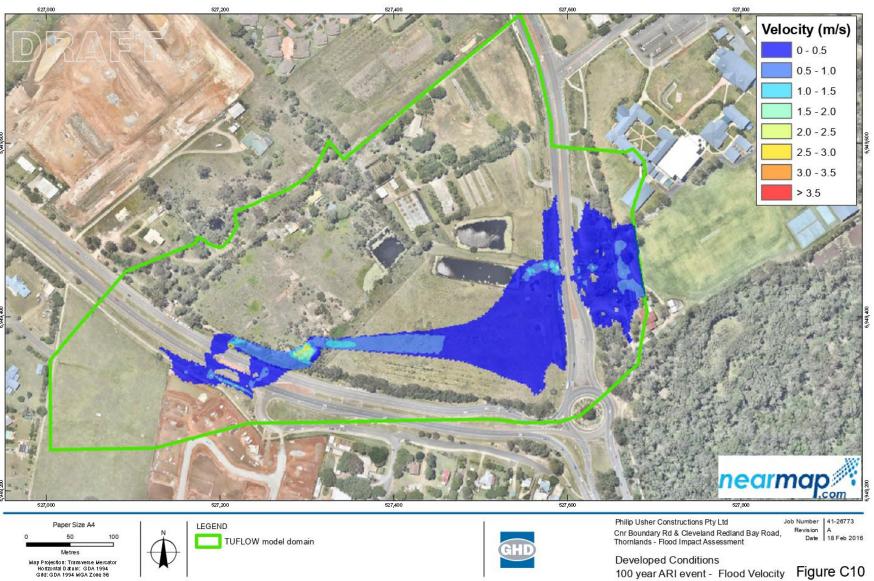
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Appendix D Peak Flood Level Difference Maps

22 | GHD | Report for Philip Usher Constructions Pty Ltd - Cnr Boundary Rd & Cleveland Redland Bay Road, Thornlands, 41/26773



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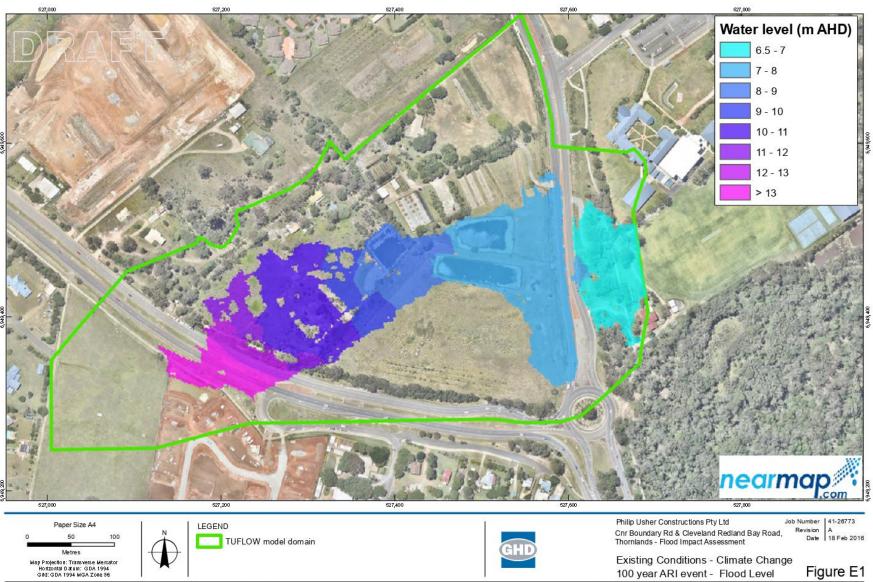
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Appendix E Climate Change Assessment and Model Sensitivity Test

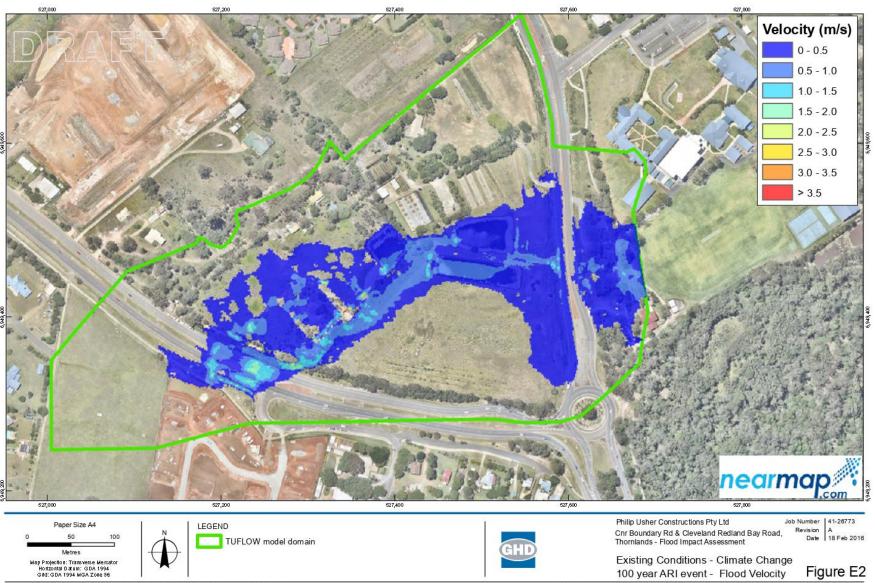
GHD | Report for Philip Usher Constructions Pty Ltd - Cnr Boundary Rd & Cleveland Redland Bay Road, Thornlands, 41/26773 | 23



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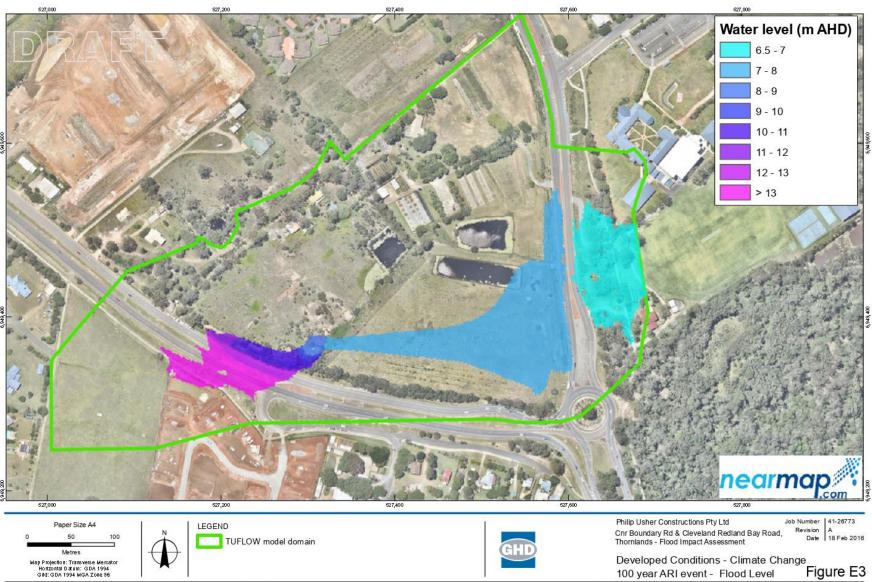
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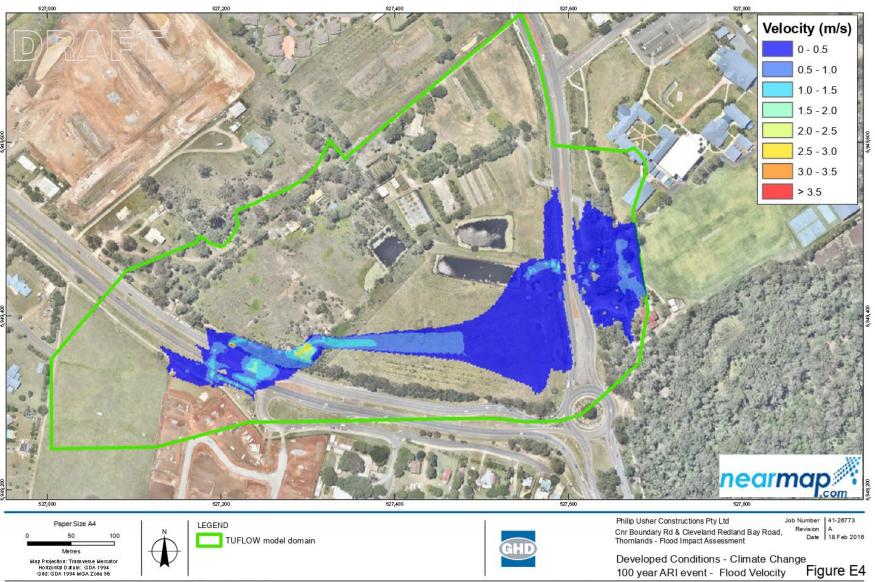
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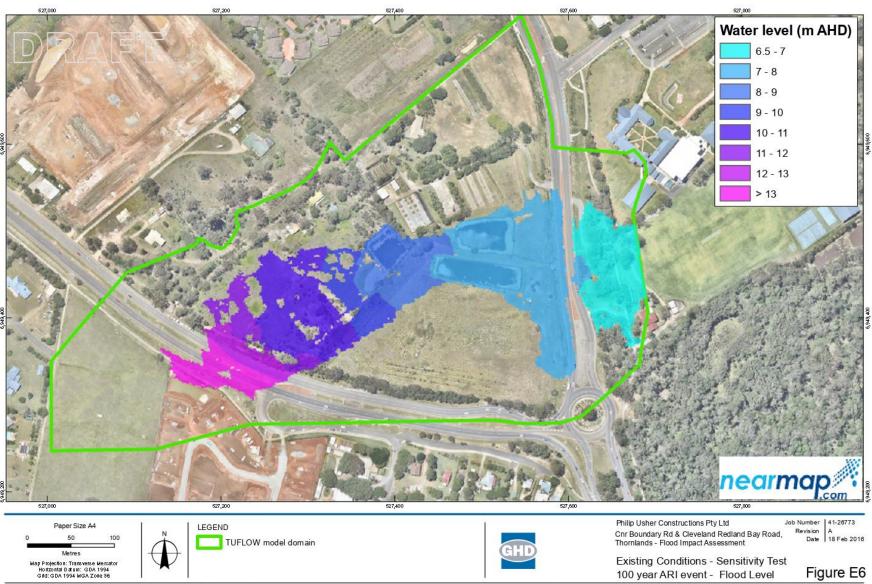
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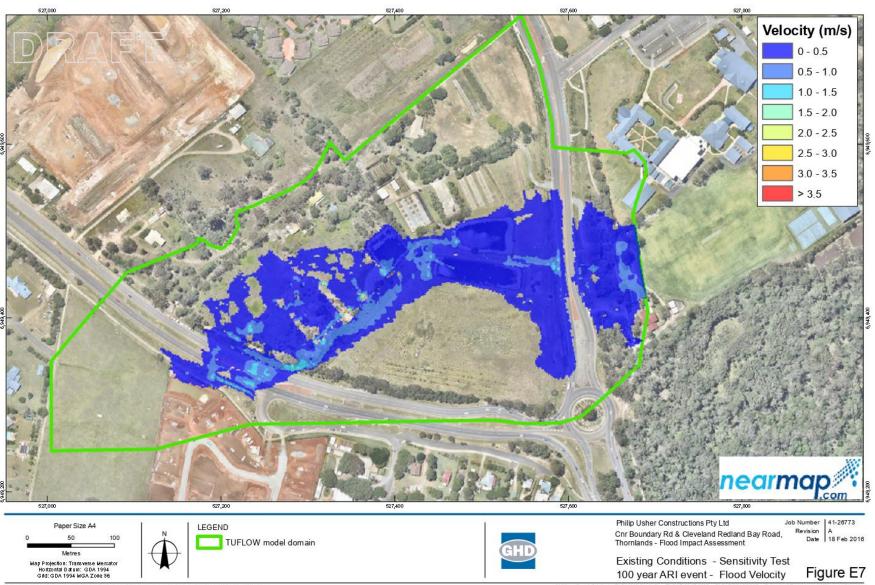
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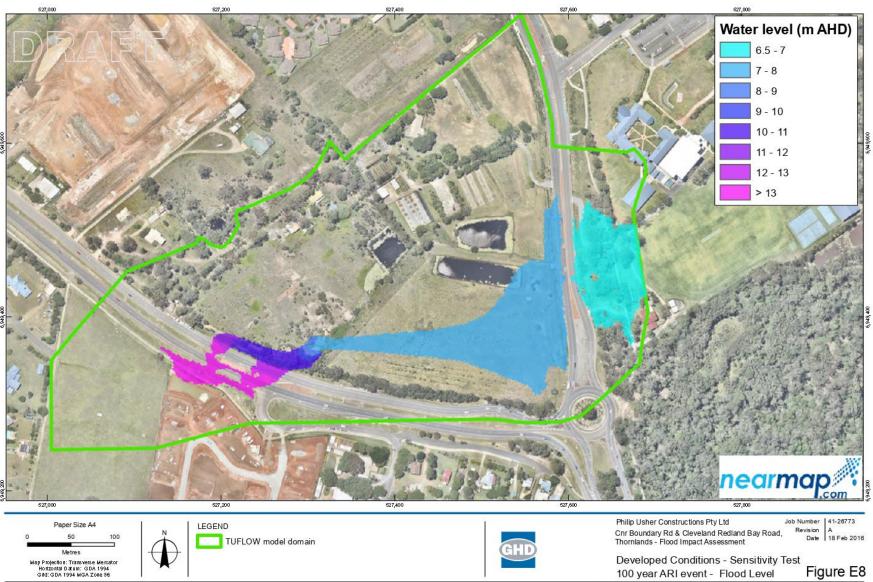
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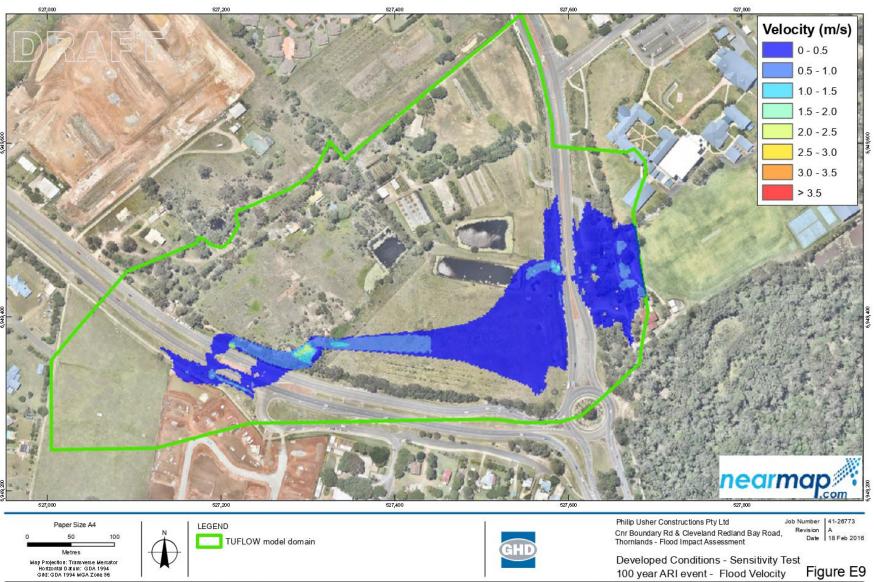
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Document Status

| Rev No. | Author | Reviewer | | Approved for Issue | | |
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| | | Name | Signature | Name | Signature | Date |
| 0 | Federico Groppa | Dan Copelin | Jones | John Postlethwaite | Pon | 18/02/2016 |
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SITE MASTER PLAN

- 3. Pedestrian Access Gates to Pateman Parade
- 4. Semi-transparent Fence with planting buffer
- 5. Acoustic Fence as per acoustic report
- 6. Communal Open Space: Feature
 - planting to define open turf area for informal
 - recreation. To be detailed at Operational Works.
- 7. Proposed informal pathway within communal open
- space. To be detailed at Operational Works.
- 8. Townhouse landscape; refer page 05 for details
- 10. Visitor Carparks / Carwash Bays throughout
- 14. Biobasin with vehicle barrier to maintenance
 - access drive. Planting within basin to WSUD TDG
 - specifications. Small shrubs and trees to perimeter.
- 15. 10m Wide Landscape Buffer to road frontage
 - Vegetated swale to U31-49 (refer to sections P07)

NOTE: STREET TREE PLANTING. Street tree selection & planting to RCC specifications (species to be





INDICATIVE TOWNHOUSE LANDSCAPE PLAN



DWELLING LANDSCAPE NOTES:

This plan is an indicative landscape design that adjusts to each individual townhouse footprint, from single, duplex, and triplex built form. Private courtyards to contain a mix of ground covers, tufted plants, shrubs and small trees. No turf to be included in private spaces - common areas only. Hardstand access is provided around the entry and side of each dwelling. Hard stand patio areas are included in private courtyards to allow for internal - external integration.

Planting themes and landscape finishes are to vary to avoid long drives of repetitive styles.

Screen planting to contain advanced (45L) shrub species capable of screening courtyard fences (1.8m) within 3 to 5 years from planting.

At least one shade / feature tree (45L) to be included in each private courtyard. More where space is available. Side boundaries to contain a tiering of shrubs, ground covers and small trees along boundary fences.

LEGEND

1) SHADE TREE:

Minimum one shade tree per courtyard creates a cooler microclimate. North-west facing gardens to include deciduous trees to permit winter sun and provide summer shade.

2 TIERED LANDSCAPING:

Groundcovers in front, shrubs in the middle and screening trees and large shrubs behind create an enclosed green area for each townhouse

3 PRIVACY FENCING:

1800 high timber fence plus landscape buffer creates privacy between courtyards and reduces overlooking between allotments

(4) PATIO AREA:

Outdoor hardstand area creates opportunity for an outdoor room.

(5) LANDSCAPE SCREENING:

Landscape buffer creates privacy between allotments and reduces overlooking.

6 PEDESTRIAN ACCESS:

1m wide (minimum) hardstand connects from driveway entrance along side of house to hardstand area in backyard. Gate access at side.

(7) BIN STORAGE LOCATION: Waste Bin Enclosure locations to front yard for middle units. Side/rear yard storage for all other units Bins on hardstand with timber screen fence to front with landscape screening to fence







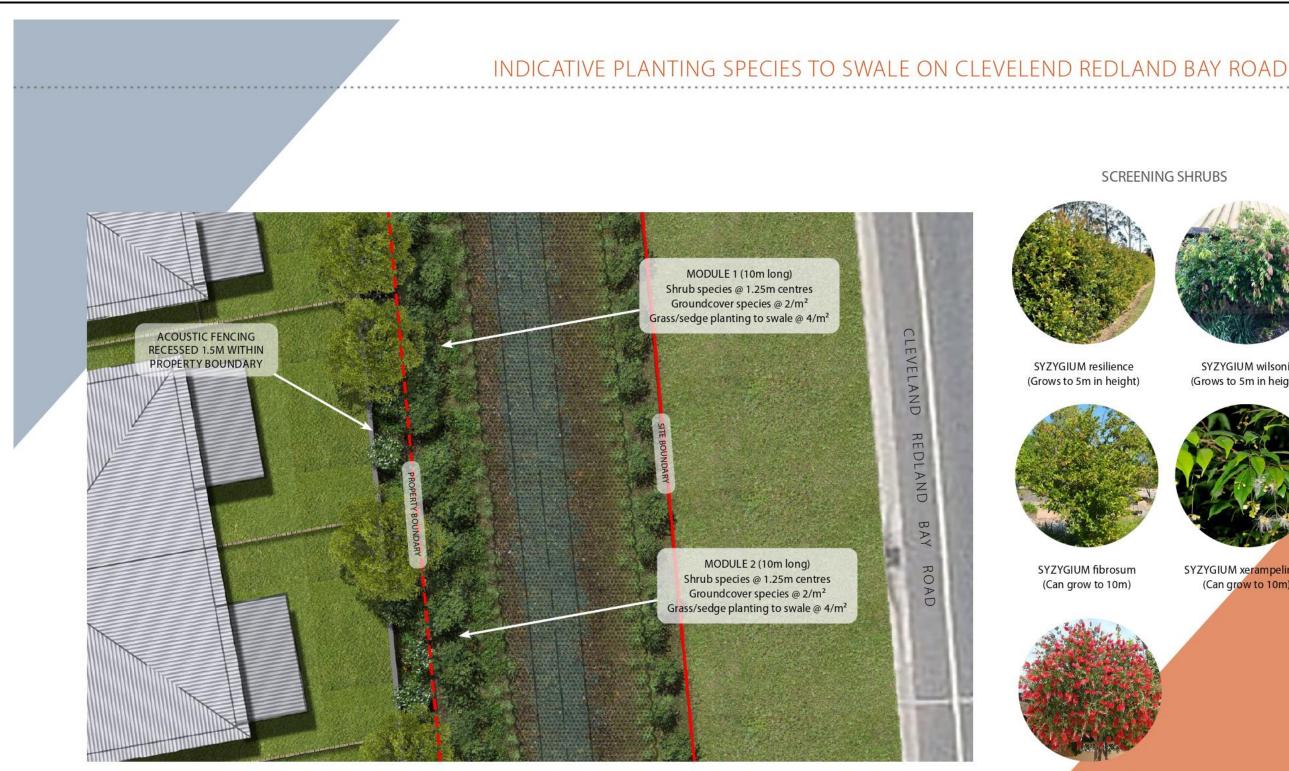
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Indicative use of landscaping to screen front yard area

Indicative landscape buffer and fencing between allotments







CALLISTEMON viminalis (Can grow to 8m in height)



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SCREENING SHRUBS





SYZYGIUM wilsonii (Grows to 5m in height)

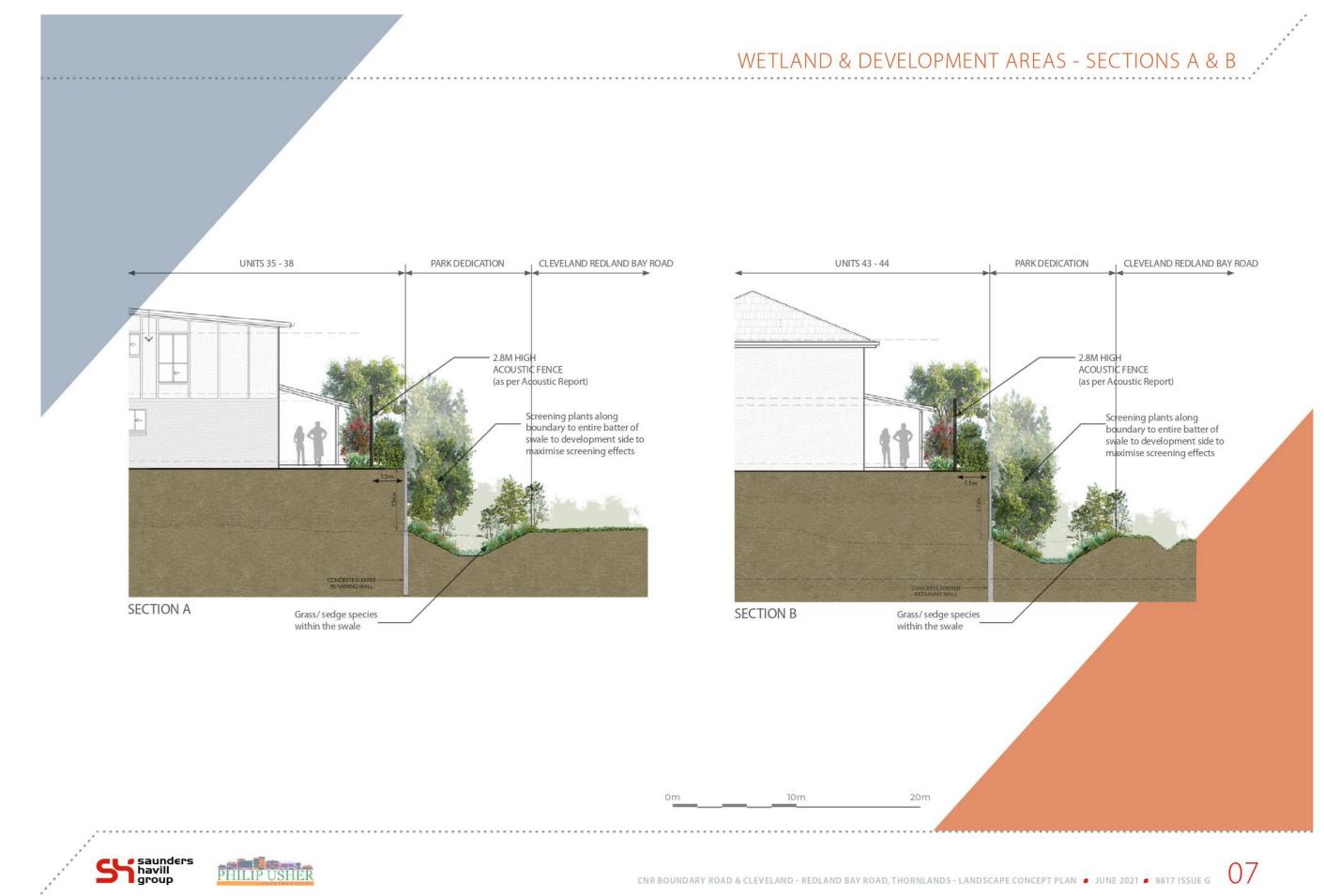


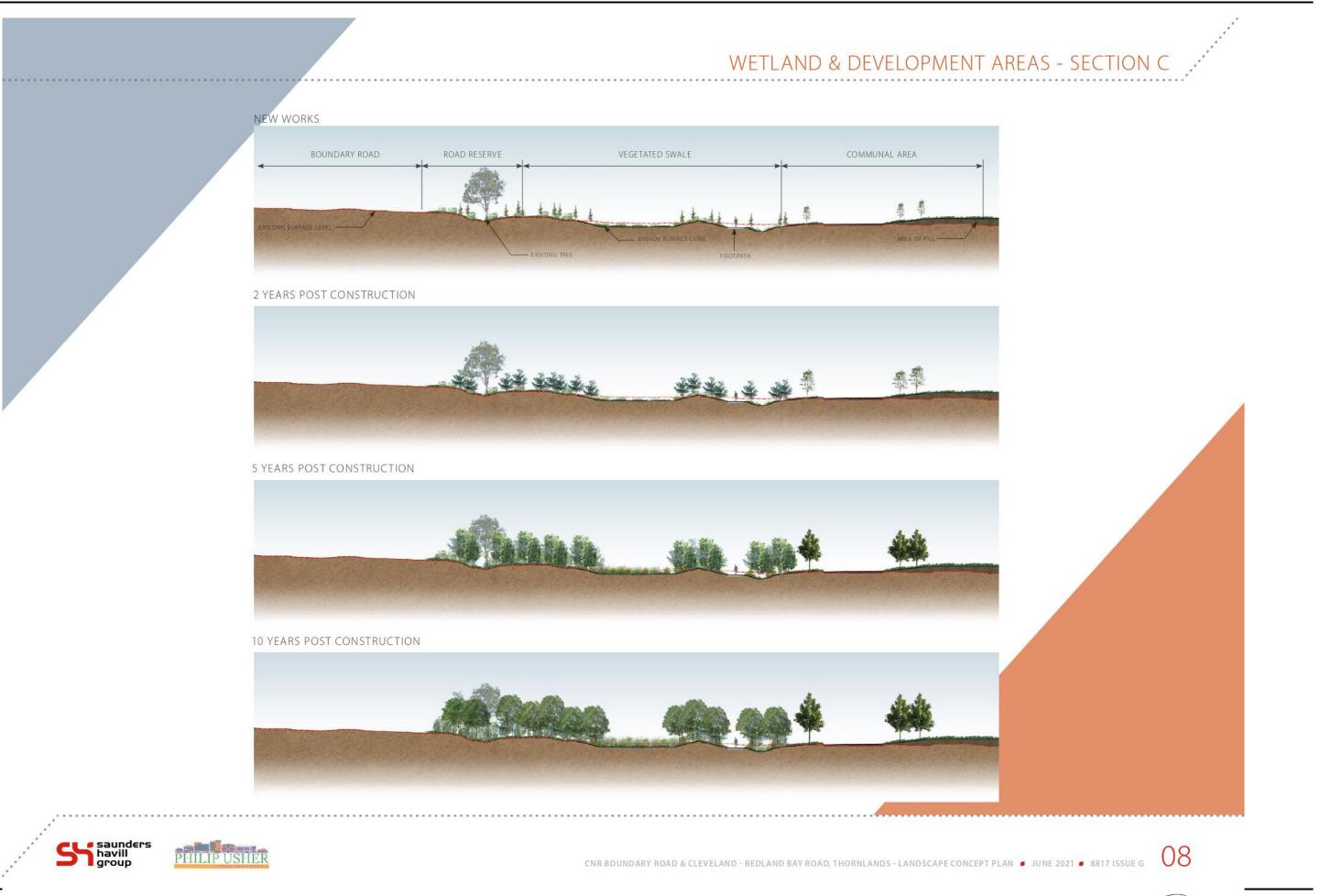
SYZYGIUM xerampelinum (Can grow to 10m)

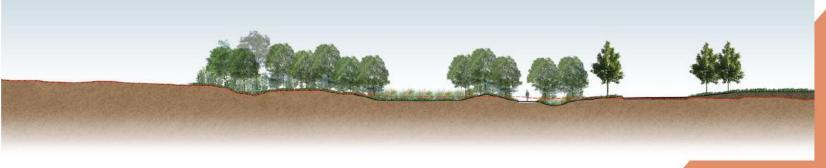


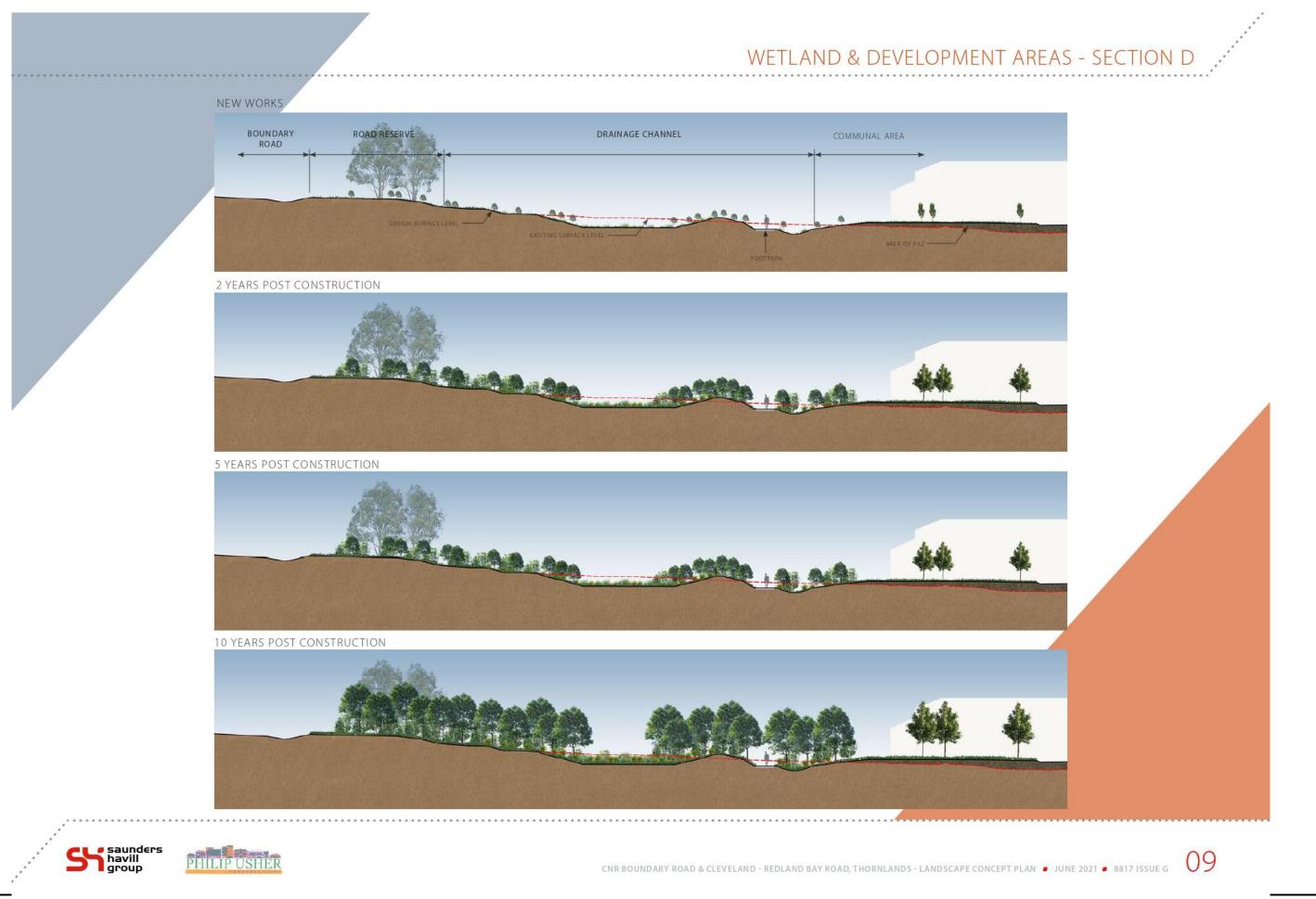


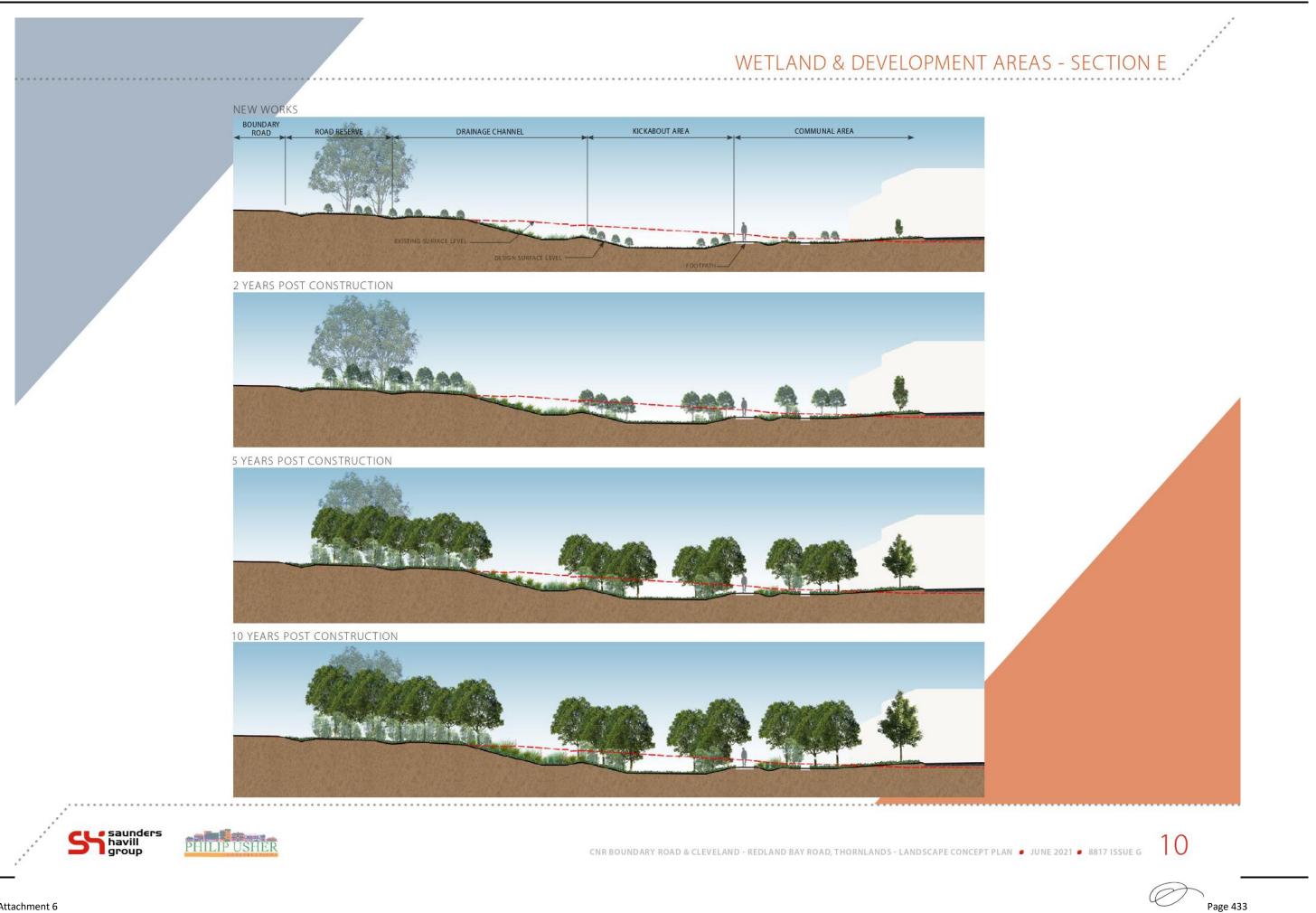














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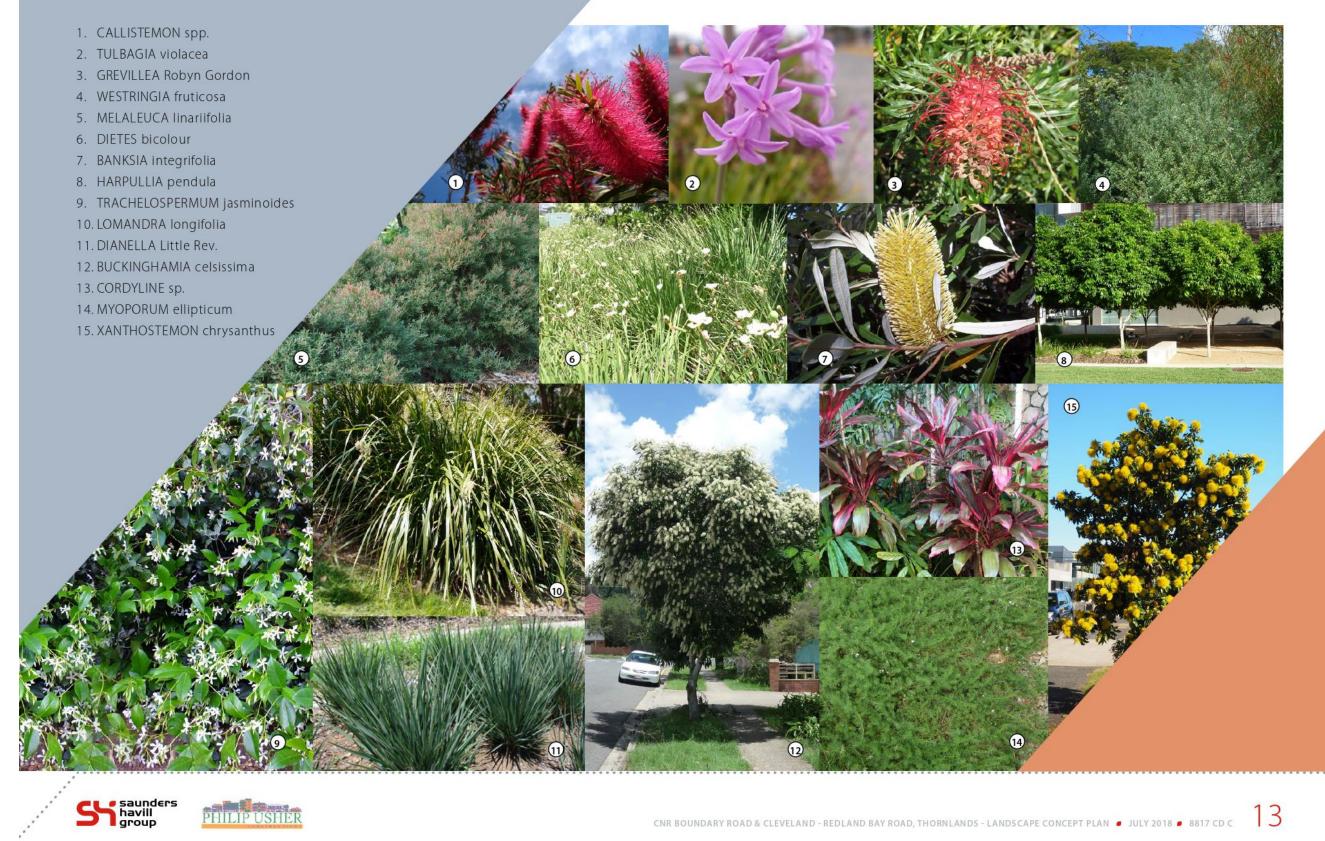




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INDICATIVE PLANT PALETTE



17 NOVEMBER 2021



GENERAL MEETING MINUTES

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ATTACHMENT 7 – DEVELOPMENT CONDITIONS

Application A (MCU17/0057) – Conditions package

| MA | TERIAL CHANGE OF USE | | | | | | |
|-------------------------------|---|-------------------------|-------------------------|-------------------------|-------------------|--|--|
| ASSESSMENT MANAGER CONDITIONS | | | | | TIMING | | |
| 1. | Comply with all conditions of this timing periods specified in the rig indicates that the condition is an o be complied with for the life of the | | | | | | |
| App | roved plans and documents | | | | | | |
| 2. | Undertake the development in acc documents referred to in Table 1, su and any notations by Council on the | ubject to the condition | | Prior comm ongoir | 0 | | |
| | Plan/document title | Reference number | Prepared by | | Plan/doc. date | | |
| | Cover sheet | DA-A3-00/N | Philip Constructions | Usher | 03/08/2021 | | |
| | Site Plan – Overall | DA-A3-01/L | Philip Constructions | Usher | 12/05/2021 | | |
| | Open Space – Overall | DA-A3-02/L | Philip Constructions | Usher | 12/05/2021 | | |
| | Site Plan | DA-A3-03/M | Philip Constructions | Usher | 05/07/2021 | | |
| | Open Space | DA-A3-04/M | Philip Constructions | Usher | 05/07/2021 | | |
| | Earthworks | DA-A3-05/M | Philip Constructions | Usher | 05/07/2021 | | |
| | Waste Management | DA-A3-06/L | Philip Constructions | Usher | 12/05/2021 | | |
| | Streetscapes (as amended in red by Council) | DA-A3-07/N | Philip Constructions | Usher | 03/08/2021 | | |
| | B3 Ground Floor Plan | DA-A3-08/M | Philip Constructions | Usher | 05/07/2021 | | |
| | B3 First Floor Plan | DA-A3-09/M | Philip Constructions | Usher | 05/07/2021 | | |
| | B3 Elevations Sht 1 | DA-A3-10/M | Philip Constructions | Usher | 05/07/2021 | | |

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| B4-R First Floor PlanDA-A3-21/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-R Elevations Sht 1DA-A3-22/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-R Elevations Sht 2DA-A3-23/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-SR Ground Floor PlanDA-A3-24/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-SR First Floor PlanDA-A3-25/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-SR First Floor PlanDA-A3-26/MPhilip ConstructionsUsher05/07/202 Constructions | B4 Elevations Sht 2 | DA-A3-19/M | • | Usher | 05/07/2021 |
| B4-R Elevations Sht 1DA-A3-22/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-R Elevations Sht 2DA-A3-23/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-SR Ground Floor PlanDA-A3-24/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-SR First Floor PlanDA-A3-25/MPhilip ConstructionsUsher05/07/202 ConstructionsB4-SR First Floor PlanDA-A3-26/MPhilip ConstructionsUsher05/07/202 Constructions | B4-R Ground Floor Plan | DA-A3-20/M | | Usher | 05/07/2021 |
| B4-R Elevations Sht 2DA-A3-23/MPhilip ConstructionsUsher05/07/202B4-SR Ground Floor PlanDA-A3-24/MPhilip ConstructionsUsher05/07/202B4-SR First Floor PlanDA-A3-25/MPhilip ConstructionsUsher05/07/202B4-SR Elevations Sht 1DA-A3-26/MPhilipUsher05/07/202 | B4-R First Floor Plan | DA-A3-21/M | • | Usher | 05/07/2021 |
| B4-SR Ground Floor PlanDA-A3-24/MPhilip ConstructionsUsher05/07/202B4-SR First Floor PlanDA-A3-25/MPhilip ConstructionsUsher05/07/202B4-SR Elevations Sht 1DA-A3-26/MPhilipUsher05/07/202 | B4-R Elevations Sht 1 | DA-A3-22/M | | Usher | 05/07/2021 |
| B4-SR First Floor PlanDA-A3-25/MPhilip ConstructionsUsher05/07/202B4-SR Elevations Sht 1DA-A3-26/MPhilipUsher05/07/202 | B4-R Elevations Sht 2 | DA-A3-23/M | - | Usher | 05/07/2021 |
| B4-SR Elevations Sht 1 DA-A3-26/M Philip Usher 05/07/202 | B4-SR Ground Floor Plan | DA-A3-24/M | | Usher | 05/07/2021 |
| | B4-SR First Floor Plan | DA-A3-25/M | | Usher | 05/07/2021 |
| | B4-SR Elevations Sht 1 | DA-A3-26/M | | Usher | 05/07/2021 |
| B4-SR Elevations Sht 2 DA-A3-27/M Philip Usher 05/07/202 Constructions | B4-SR Elevations Sht 2 | DA-A3-27/M | | Usher | 05/07/2021 |
| D6 Ground Floor Plan DA-A3-28/M Philip Usher 05/07/202 Constructions 05/07/202 | D6 Ground Floor Plan | DA-A3-28/M | | Usher | 05/07/2021 |



| DA-A3-29/M | Philip Constructions | Usher | 05/07/2021 |
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| DA-A3-30/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-31/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-32/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-33/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-34/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-35/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-36/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-37/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-38/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-39/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-40/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-41/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-42/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-43/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-44/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-45/M | Philip Constructions | Usher | 05/07/2021 |
| DA-A3-46/M | Philip Constructions | Usher | 05/07/2021 |
| | DA-A3-30/M DA-A3-31/M DA-A3-31/M DA-A3-32/M DA-A3-33/M DA-A3-33/M DA-A3-33/M DA-A3-33/M DA-A3-33/M DA-A3-33/M DA-A3-33/M DA-A3-34/M DA-A3-36/M DA-A3-36/M DA-A3-37/M DA-A3-39/M DA-A3-39/M DA-A3-40/M DA-A3-41/M DA-A3-42/M DA-A3-43/M DA-A3-43/M DA-A3-43/M | Image: ConstructionsDA-A3-30/MPhilip ConstructionsDA-A3-31/MPhilip ConstructionsDA-A3-31/MPhilip ConstructionsDA-A3-32/MPhilip ConstructionsDA-A3-33/MPhilip ConstructionsDA-A3-33/MPhilip ConstructionsDA-A3-34/MPhilip ConstructionsDA-A3-35/MPhilip ConstructionsDA-A3-36/MPhilip ConstructionsDA-A3-36/MPhilip ConstructionsDA-A3-36/MPhilip ConstructionsDA-A3-37/MPhilip ConstructionsDA-A3-39/MPhilip ConstructionsDA-A3-40/MPhilip ConstructionsDA-A3-41/MPhilip ConstructionsDA-A3-41/MPhilip ConstructionsDA-A3-42/MPhilip ConstructionsDA-A3-43/MPhilip ConstructionsDA-A3-44/MPhilip ConstructionsDA-A3-45/MPhilip ConstructionsDA-A3-45/MPhilip Constructions | ConstructionsDA-A3-30/MPhilip ConstructionsUsher ConstructionsDA-A3-31/MPhilip ConstructionsUsherDA-A3-32/MPhilip ConstructionsUsherDA-A3-32/MPhilip ConstructionsUsherDA-A3-33/MPhilip ConstructionsUsherDA-A3-33/MPhilip ConstructionsUsherDA-A3-34/MPhilip ConstructionsUsherDA-A3-35/MPhilip ConstructionsUsherDA-A3-35/MPhilip ConstructionsUsherDA-A3-36/MPhilip ConstructionsUsherDA-A3-37/MPhilip ConstructionsUsherDA-A3-39/MPhilip ConstructionsUsherDA-A3-39/MPhilip ConstructionsUsherDA-A3-40/MPhilip ConstructionsUsherDA-A3-41/MPhilip ConstructionsUsherDA-A3-41/MPhilip ConstructionsUsherDA-A3-42/MPhilip ConstructionsUsherDA-A3-43/MPhilip ConstructionsUsherDA-A3-43/MPhilip ConstructionsUsherDA-A3-44/MPhilip ConstructionsUsherDA-A3-45/MPhilip ConstructionsUsherDA-A3-45/MPhilip ConstructionsUsherDA-A3-45/MPhilip ConstructionsUsherDA-A3-45/MPhilip ConstructionsUsherDA-A3-45/MPhilip ConstructionsUsher |



| DG6 First Floor Plan | DA-A3-47/M | Philip Constructions | Usher | 05/07/2021 |
|--|------------|-------------------------|-------|------------|
| DG6 Elevations | DA-A3-48/M | Philip Constructions | Usher | 05/07/2021 |
| M10 Floor Plans | DA-A3-49/M | Philip Constructions | Usher | 05/07/2021 |
| M10 Elevations | DA-A3-50/M | Philip Constructions | Usher | 05/07/2021 |
| R2 Floor Plans | DA-A3-51/M | Philip Constructions | Usher | 05/07/2021 |
| R2 Elevations | DA-A3-52/M | Philip Constructions | Usher | 05/07/2021 |
| T3 Ground Floor Plan | DA-A3-53/M | Philip Constructions | Usher | 05/07/2021 |
| T3 First Floor Plan | DA-A3-54/M | Philip Constructions | Usher | 05/07/2021 |
| T3 Elevations | DA-A3-55/M | Philip Constructions | Usher | 05/07/2021 |
| T3-P Ground Floor Plan | DA-A3-56/M | Philip Constructions | Usher | 05/07/2021 |
| T3-P First Floor Plan | DA-A3-57/M | Philip Constructions | Usher | 05/07/2021 |
| T3-P Elevations | DA-A3-58/M | Philip Constructions | Usher | 05/07/2021 |
| Gym-Patio Floor Plan & Elevations | DA-A3-59/M | Philip Constructions | Usher | 05/07/2021 |
| Concept Design | SK01 Rev 4 | 2 Develop Urba | n | 29/10/2020 |
| Bulk Earthworks Sketch – Sheet 1 | SKO2 Rev 4 | 2 Develop Urba | n | 29/10/2020 |
| Bulk Earthworks Sketch – Sheet 2 | SKO3 Rev 4 | 2 Develop Urba | n | 29/10/2020 |
| Bulk Earthworks Sketch – Sheet 3 | SKO4 Rev 4 | 2 Develop Urba | n | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 1 | SK05 Rev 3 | 2 Develop Urba | n | 14/05/2020 |
| Bulk Earthworks Cross Sections – SH 2 | SK06 Rev 3 | 2 Develop Urba | n | 29/10/2020 |
| 1 | | 1 | | 1 |



| Bulk Earthworks Cross Sections – SH 3 | SK07 Rev 4 | 2 Develop Urban | 29/10/2020 |
|--|--------------|-----------------------|------------------|
| Bulk Earthworks Cross Sections – SH 4 | SK08 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 5 | SK09 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 6 | SK10 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 7 | SK11 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 8 | SK12 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Proposed Services Connection Plan | SK13 Rev 4 | 2 Develop Urban | 29/10/2020 |
| Detention Basin Discharge Layout Plan | SK14 Rev 1 | 2 Develop Urban | 12/03/2020 |
| Flood Impact Assessment | 41/26773 | GHD | February 2016 |
| Landscape Concept Plan | 8817 Issue G | Saunders Havill Group | June 2021 |
| Acoustic Report | 2017122 R01L | Acoustic Works | 23/11/2020 |

Table 1: Approved plans and documents

| 3. | Comply with the infrastructure agreement relating to the subject site. | Prior to the commencing ongoing. | use and |
|------|--|--|------------|
| Exis | ting structures | | |
| 4. | Demolish or relocate/remove all existing structures on site in accordance with the approved plan(s). Cap all services prior to demolition commencing. | Prior to the commencing. | use |
| Desi | gn | | |
| 5. | Design and construct the development with the following changes as indicated in red on the approved plans: | Prior to the commencing | use and |
| | a) For at least half the length of the front fencing to units 3 and 4, construct the fencing to achieve a minimum of 50% transparency.b) Provide pedestrian access gates to Pateman Parade for units 3 and 4. | ongoing. | |

| 6. | Locate, design and install outdoor lighting, where required, to minimise the potential for light spillage to cause nuisance to neighbouring premises, in accordance with Australian Standard AS4282-2019: Control of the obtrusive effects of outdoor lighting. | Prior to the use commencing and ongoing. |
|------|--|--|
| 7. | Comply with the following requirements where it is proposed that habitable room windows above the ground storey will be within a distance of 6m, and within an angle of 45 degrees, and directly adjacent to habitable rooms of neighbouring dwelling units (internal to the site): | Prior to the use commencing. |
| | a) Provide sill heights a minimum of 1.5m above floor levels; or | |
| | b) Provide fixed translucent, such as frosted or textured glazing, for any part of the window less than 1.5m above floor level; or | |
| | c) Provide fixed external screens that are – i) solid and translucent; or | |
| | ii) perforated panels or trellises that have a maximum of 25% openings, with a maximum opening dimension of 50mm, and that are permanently fixed and durable; and iii) offset a minimum of 300mm from the wall of the building. | |
| 8. | Ensure that air conditioning units are located so that they are not visible from any external streets. | Prior to the use commencing and ongoing. |
| 9. | Ensure that each residential dwelling unit has their own dedicated waste storage area that: | Prior to the use commencing. |
| | a) is located within the curtilage of the dwelling unit, unless noted otherwise on the approved plans, b) allows for containers to be moved from storage area to service point without travelling over steps or through dwelling areas, c) has a hardstand surface and screens bins from view from internal and external streets, and d) is no less than 800mm wide x 1500mm long x 1100 mm high. | |
| | If a bin wash-down bay is to be provided it shall be roofed, have a hose and cock located within the vicinity of the bay, and have a hardstand area suitably drained to a trade waste outlet. | |
| 10. | Provide certification from a Registered Professional Engineer of Queensland (RPEQ) certifying that the internal access roads are of adequate design and construction to allow waste collection vehicles to enter and exit the site in a forward gear. | Prior to the use commencing. |
| Land | dedication | |
| 11. | Transfer the land shown as park on the approved site plan to Council in fee simple (on trust) for the purpose of park. | Prior to Council approval of the survey plan or commencement of the |

| | | sooner. |
|-----|--|--|
| ndi | itioned works assessment | |
| | Submit to Council, and receive appro Assessment for the documents and works | |
| | Document or works | Assessment benchmarks |
| | Landscape plan | Redlands Planning Scheme Part 8 Division 8 – Landscape Code Redlands Planning Scheme Part 9 Schedule 9 – Street Trees Redlands Planning Scheme Part 11 Policy 3 Chapter 3 – Landscaping and Chapter 4 – Security Bonding Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions, Chapter 10 – Parks and Open Space and Chapter 11 – Landscaping Redlands Planning Scheme Part 11 Policy 16 – Safer by Design Redlands Planning Scheme Part 8 Division 9 – |
| | | Stormwater Management Code Redlands Planning Scheme Part 11 Policy 3 Chapter 4 – Security Bonding Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions and Chapter 6 – Stormwater Management Water Sensitive Urban Design Technical Guidelines for South East Queensland State Planning Policy July 2017 Queensland Urban Drainage Manual |
| | Roadworks | Redlands Planning Scheme Part 8 Division 1 – Access and Parking Code Redlands Planning Scheme Part 8 Division 7 – Infrastructure Works Code Redlands Planning Scheme Part 11 Policy 3 Chapter 4 – Security Bonding Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and Genera Conditions and Chapter 5 – Road and Path Design. |
| | Construction Management Plan | Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions Redlands Planning Scheme Part 11 Policy 3 Chapter 4 – Security Bonding. |

Table 2: Conditioned works assessment

| 13. | | mply with all conditions and approved plans in the Conditioned Works proval. | Prior comm | to encing | the g. | use |
|--------------|--|---|---------------|--------------|-----------|-----|
| <u>Parki</u> | ng | | | | | |
| 14. | Pro | wide car parking in accordance with the approved plans. | Prior comm | to | the | use |
| | | cess to car parking spaces, bicycle spaces, bin bays and driveways must nain unobstructed and available for their intended purpose. | ongoir | | 5 | and |
| Ease | nen | <u>ts</u> | | | | |
| 15. | Grant easements for the following and submit the relevant easement documentation to Council for approval. Once approved by Council register the easements on the property title. | | Prior comm | to encinį | the g. | use |
| | a) | A minimum 3m wide easement in gross in favour of Redland City Council, over the stormwater drainage pipes from each Pateman Parade road discharge pipe (825mm diameter pipe and 1200mm diameter pipe), traversing through the development to the treatment area within the development. | | | | |
| | b) | A stormwater drainage easement in gross in favour of Redland City Council, over the Q100 overland path traversing the development from Boundary Road to Cleveland Redland Bay Road. | | | | |
| | c) | Access purposes 1m wide to and around any sewer maintenance holes or structures in favour of Redland City Council and its agents. | | | | |
| | d) | Public access purposes over the 2.5m wide concrete footpath internal to the site connecting from Boundary Road to Cleveland-Redland Bay Road. | | | | |
| Bond | s | | | | | |

16. Lodge with Council the bonds listed in Table 3:

Prior to requesting a pre-start meeting or works commencing, whichever is the sooner.

| Bond item | Amount | Returned |
|----------------------------|---------|--|
| Road cleaning bond | \$2,000 | When works accepted on maintenance by Council. |
| Road opening approval bond | \$5,000 | When works accepted on maintenance by Council. |
| TOTAL | \$7,000 | |

17. Lodge a defects liability bond to the greater value of either: Prior to contributed asset being accepted a) 10% of the contributed asset(s); or on maintenance by b) \$2,500. Council.

This bond will be returned after formal acceptance by Council of the contributed asset(s) off maintenance and the transfer of the applicable works to Council.

Inspections

18. Arrange with Council for the following inspections to be carried out at the
relevant time in accordance with Table 4: Inspections below.At timing indicated in
table 4.

| Inspection | Timing |
|--------------------------------|--|
| Pre-start | Prior to any works commencing. |
| Erosion and sediment control | Immediately after installation of erosion and sediment control measures. |
| Landscape inspection | Must be undertaken prior to any works commencing on the site or external to the site. |
| Driveway crossover/footpath | Box inspection to be undertaken with reinforcing mesh in place and supported on bar chairs prior to the concrete being poured. |
| Stormwater bedding | After the stormwater pipelines are bedded and prior to backfilling. |
| Sewer bedding | After the sewer pipelines are bedded and prior to backfilling. |
| Water main bedding | After the water pipelines are bedded and prior to backfilling. |
| Sewer vacuum testing | During sewer vacuum testing. |
| Water pressure testing | During water main pressure testing. |
| On maintenance | On completion of all civil and landscaping works to be transferred into public ownership as required by this approval and its conditions and prior to the commencement of the 12 months maintenance period. |
| Compliance inspection | On completion of the development in accordance with the approval and its conditions. |
| Off maintenance | At the end of the minimum 12 months maintenance period. |

For the pre-start, on maintenance/compliance and off maintenance inspections, at least **five (5) business days** notice must be given to Council. For all other inspections, a minimum of **24 hours** notice must be given to Council.

The contributed assets must be accepted on maintenance and the development must pass a Compliance Inspection before the use commences.

<u>Note</u>: The Civil Consulting Engineer should inspect the works and satisfy themselves that the works are satisfactory prior to booking the respective inspections. In instances where Council's representative(s) fails an inspection, Council will charge a re-inspection fee prior to re-visiting the site. The cost of this re-inspection is identified in Council's Register of Fees and is reviewed each financial year.

<u>General</u>

19. Provide details to Council of the nominated Principal Contractor, including copies of the Principal Contractor's workcover and public liability currency certificates. The public liability insurance policy must be a minimum of ten million dollars and must indemnify Redland City Council.

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| | | whichever is the sooner. |
|-------------|--|---|
| 20. | Submit to Council a copy of the proposed Program of Civil Works. | Prior to requesting a pre-start meeting or works commencing, whichever is the sooner. |
| 21. | Provide temporary drainage during the building construction phase such that discharge from all constructed roofs and paved areas is disposed of to a lawful point of discharge in accordance with the Queensland Urban Drainage Manual (QUDM). Maintain the temporary system for the duration of the building works. | During construction. |
| 22. | Notify Council within 24hrs and rectify, in consultation with Council, any damage to Council infrastructure as a result of construction activities, at no cost to Council. | As soon as practical following identification of the damage. |
| 23. | Provide written certification from a Registered Professional Engineer Queensland (RPEQ) certifying that all civil works have been completed in accordance with the approved drawings and specifications and to the applicable Australian Standards. | Prior to on maintenance or compliance inspection whichever is the sooner. |
| 24. | Undertake the development works so that there is no risk to public safety at any time on the site, adjacent public land, road reserve or private property. Should the site be unattended or abandoned, public safety must still be maintained. | During construction phase. |
| 25. | Rectify any damage done to the road verge during construction, including topsoiling and re-turfing. | Prior to a compliance inspection. |
| 26. | Maintain all contributed assets for a minimum period of 12 months from the date the works are accepted on maintenance by Council. The works will be accepted off maintenance only where the works have been suitably maintained to any manufacturer's specifications and Council standards and are fit for purpose. | During the on maintenance period. |
| <u>Road</u> | lworks | |
| 27. | Submit to Council for approval, engineering plans and details showing the following works (for Pateman Parade) are in accordance with the assessment criteria listed in Table 2: Conditioned Works Assessment of this approval: | |
| | a) Road construction including concrete kerb and channel and road pavement and concrete shared use footpaths extending from the end of the existing pavement in Pateman Parade to the stage boundary. | |

| | b) | A minimum 6m wide type R-RSC-2 permanent vehicular crossover to the Pateman Parade frontage of the site. | |
|-----|----|--|---|
| | c) | Footpath earthworks, topsoiling and turfing of all disturbed footpath areas. | |
| | d) | Reinstatement of concrete kerb and channel where required. | |
| | e) | Removal of all redundant vehicle crossovers. | |
| | f) | Entry treatment/access to the site along Pateman Parade. | |
| | | ustment and relocations necessary to public utility services resulting m these works. | |
| 28. | | mplete the construction of the road intersection works required under 2017/0058 in accordance with those approved plans and conditions. | Prior to the use commencing for MCU17/0058 (stage 2) or within 5 years of the use commencing for this approval (stage 1), whichever occurs sooner. |
| 29. | wo | omit and have approved by Council a Road Opening Approval for any rks being undertaken within the road reserve. Provide the following to uncil as part of the application: | Prior to requesting a pre-start meeting or works commencing, |
| | a) | A completed application form and associated fee, at the rate applicable at the time of payment. The current rate for the 2020/2021 Financial Year is: | whichever is the sooner. |
| | • | 5,432.00 – this incorporates a refundable bond of $5,000$ and a non-refundable administration fee of $432.00.$ | |
| | b) | A copy of the contractor's Workcover insurance currency certificate. | |
| | c) | A copy of the contractor's Public Liability insurance currency certificate. The public liability insurance policy must be a minimum of ten million dollars and must indemnify Redland City Council. | |
| | d) | Submission of a Traffic Management Plan (TMP) and/or a Traffic Guidance Scheme (TGS) that is prepared and authorised by a person who holds a current DTMR 'Open Level' Traffic Management Design Certification and should include proposed haul routes for construction vehicles associated with the works, as applicable. | |
| 30. | | vide a Construction management Plan which includes the following prmation: | As part of request for conditioned works |
| | a) | Hours of construction. | assessment. |
| | b) | Vehicle access during construction hours, including responsibility for maintenance of the defined cartage route and proposed haulage route for construction traffic (including loading and unloading). | |
| | c) | Traffic management signed by a suitably qualified Workplace Health and Safety professional. | |
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| an Once the works commence and prior to the use commencing. Ongoing condition. |
| a As part of request for nd conditioned works - assessment. |
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| |
| or At the time of works ed occurring. ny in he he |
| nal Prior to the use ns commencing. |
| n , a 2 alue t t |

| 37. | Remove any redundant sewerage connections within the site or servicing the development and provide documentary evidence to Council or its delegate that this has occurred. | Prior to site works commencing. |
|--------------|---|---|
| 38. | Provide water connections and water meters in accordance with Council's Standard Drawings. Provide details to Council of the water meters and their locations. | Prior to on maintenance inspection. |
| Exca | vation and filling | |
| 39. | Undertake any required excavation and fill works in accordance with the following: | During construction. |
| | Design retaining walls/structures to have a minimum design life of 60 years and to be in accordance with Australian Standard 4678:2002 – Earth Retaining Structures (as amended). | |
| | b) Undertake compaction in accordance with Australian Standard 3798:2007 – Guidelines on earthworks for commercial and residential developments (as amended) and Australian Standard 2870:2011 – Residential Slabs and Footings (as amended). | |
| | c) Comply with the relevant requirements of the Building Regulations 2006 (as amended) where involving gradients or embankments. | |
| 40. | Submit to Council test certificates for all filling greater than 400mm in depth, confirming that the necessary levels of compaction have been achieved, including a Level 1 inspection and testing report in accordance with AS3798. | Prior to the use commencing. |
| 41. | Provide certification from a Registered Professional Engineer Queensland (RPEQ) for the design and construction of any retaining walls greater than 1m in height, in relation to stability and resistance to overturning and sliding (overall safety factor of 1.5) in accordance with AS1170.1 and AS3600. | Prior to the use commencing. |
| <u>Erosi</u> | on and sediment control | |
| 42. | Design, implement and maintain measures and practices in accordance with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). | Prior to the works commencing and during construction phase until disturbed areas are stabilised. |
| 43. | Ensure dewatering activities (such as from excavations or sediment basins) do not discharge water offsite or where the water could reasonably be expected to move offsite or into waters or stormwater, unless the following are met: | During construction phase. |
| | The concentration of total suspended solids (TSS) must not exceed 50 mg/L. | |

| | b) Turbidity (nephelometric turbidity units - NTU) values must not be greater than 10% of the turbidity of waters immediately upstream of the sites water entry points at time of the release. | |
|------|--|--|
| | c) The pH must be within the range of 6.5-8.5 except where, and to the extent, the natural receiving waters lie outside this range. | |
| | Water is to be sampled by experienced personnel and tested by a NATA (National Association of Testing Authorities) accredited laboratory. | |
| 44. | Provide a stabilised construction exit at each exit point for the site. Maintain this area so that no loose debris is deposited on to adjoining roadways. Remove any material brought onto the road as soon as possible. | During construction phase. |
| 45. | Implement dust control measures at each phase of site development and operation in accordance with IECA (2008) Best Practice Erosion and Sediment Control. | During construction phase. |
| Surv | ey and as-constructed information | |
| 46. | Submit as constructed drawings and documentation for all civil works and those to be transferred into public ownership, prepared in accordance with the City Plan Planning Scheme Policy 2 – Infrastructure Works. | Prior to the compliance inspection. |
| | Include surveyed as constructed data showing works completed (digital and hard copies) and amended design plan data showing construction deviation from design plans (digital and hard copies). The digital data and the design data must be endorsed by a RPEQ and Landscape Architect holding AILA (Australian Institute of Landscape Architecture) membership and a registered surveyor using the certification clauses contained in Planning Scheme Policy 2 – Infrastructure Works. | |
| Acou | stic requirements | |
| 47. | Incorporate acoustic attenuation into the development as specified in the approved acoustic report. | Prior to the use commencing and ongoing. |
| Land | scape Works | |
| 48. | Submit landscape plans to Council for approval in accordance with the assessment criteria listed in Table 2: Conditioned Works Assessment of this approval. Include the following items: | As part of request for conditioned works assessment. |
| | Designs that are generally in accordance with the approved landscape plans, but which include all planting details including species, densities and planting placements. | |
| | Details of street tree planting in accordance with the Redlands Planning Scheme Landscape Code with species selected from Schedule 9 of the Redlands Planning Scheme. | |
| | | |

| | • A maintenance plan for the entire landscaping component of the development. | |
|-------|--|---|
| | • Details of lighting to communal open space, driveways, public car parks and footpaths within the site. | |
| 49. | Undertake landscaping in accordance with the landscape plans approved as part of conditioned works assessment and the following: | Prior to the use commencing. |
| | • Turf all areas of disturbance within the road verge with turf cut from a weed free source. | |
| | • Remove all weed species from the site, as identified in Redlands Coast Biosecurity Plan 2018-2023. | |
| | • Provide organic mulch to all garden bed areas at a minimum depth of 100mm. | |
| Envir | onment and vegetation management | |
| 50. | Submit a revised Vegetation Management Plan to Council which reflects: | As part of request for conditioned works |
| | Trees identified for removal and retention; | assessment. |
| | All plantings across the site; | |
| | The landscaping and planting works occurring to all of the stormwater treatment areas; and | |
| | Any revegetation works. | |
| 51. | Protect those trees identified on the revised Vegetation Management Plan to be retained by implementing tree protection measures in accordance with Australian Standard AS4970-2009 – Protection of Trees on Development Sites. All tree protection measures must be in place prior to any development works commencing. | Prior to works commencing and during construction phase. |
| 52. | Provide a dam dewatering report which details how the dams on the subject site will be removed. Details are to include but not be limited to: How the dams will be dewatered; | As part of request for conditioned works assessment. |
| | How the silt will be removed and where it will be disposed of; | |
| | Details of the levels that the dams will be removed to; | |
| | Details of any subsoil drainage so that the base of the dam sites do not retain or accumulate ground waters. | |
| 53. | Submit to Council written certification and test results to verify that the earthworks associated with the dam removal have been completed to a Level 1 responsibility in accordance with Australian Standard AS3798-1990. | Prior to a compliance inspection. |
| RECO | NFIGURING A LOT | |
| ASSE | SSMENT MANAGER CONDITIONS | TIMING |
| | | <u></u> |



| 1. | Comply with all conditions of this approval, at no cost to Council, at the | |
|----|--|--|
| | timing periods specified in the right-hand column. Where the column | |
| | indicates that the condition is an ongoing condition, that condition must | |
| | be complied with for the life of the development. | |

Approved plans and documents

2. Undertake the development in accordance with the approved plans and documents referred to in Table 1, subject to the conditions of this approval of the survey and any notations by Council on the plans. Prior to Council approval of the survey plan.

| Plan/document title | Reference number | Prepared by | Plan/doc. date |
|-------------------------|------------------|--------------------------|-------------------|
| Plan of Reconfiguration | SB2866-02/B | Wolter Consulti Group | ng 24/07/2018 |

Table 1: Approved plans and documents

| 3. | Submit to Council a survey plan for approval, in accordance with the approved plans, following compliance with all relevant conditions and requirements of this approval. | Prior to the use commencing. |
|-----|--|---|
| 4. | Pay to Council the split valuation fee prescribed by the State Government. The current value of the fee is \$40.00 (excluding GST) per allotment (2021/2022 financial year). The fee must be paid: | Prior to Council approval of the survey plan. |
| | for each allotment contained on the survey plan, including balance lots, and at the state and is a state time of neuropath | |
| _ | at the rate applicable at the time of payment. | |
| ADD | ITIONAL APPROVALS | |

The following further development permits are necessary to allow the development to be carried out:

Building works approval.

Please be aware that further approvals, other than a development permit, may still be required for your development. This includes, but is not limited to, the following:

- Conditioned works assessment as detailed in Table 2 of the conditions.
- Plumbing and drainage works.
- Capping of sewer for demolition of existing buildings on site.
- Road opening permit for any works proposed within an existing road reserve.

REFERRAL AGENCY CONDITIONS

 Queensland Department of State Development, Manufacturing, Infrastructure and Planning (DSDMIP)

Refer to the attached correspondence from the DSDMIP dated 8 April 2021 (DSDMIP reference 1710-1963-SRA) for MCU17/0057.

ASSESSMENT MANAGER ADVICE

Infrastructure charges

Infrastructure charges apply to the development in accordance with the Adopted Infrastructure Charges Resolution (No. 3.1) 2020 levied by way of an Infrastructure Charges Notice. The infrastructure charges are contained in the attached Redland City Council Infrastructure Charges Notice.

Working within a State-controlled road network

Prior to undertaking any works within a State-controlled road corridor, obtain a Road Corridor Permit from the Queensland Department of Transport and Main Roads.

Live connections

Redland City Council is responsible for all live water and wastewater connections. Contact **must** be made with Council to arrange live works associated with the development.

Further information can be obtained from Council on 07 3829 8999.

Coastal processes and sea level rise

Please be aware that development approvals issued by Redland City Council are based upon current lawful planning provisions which do not necessarily respond immediately to new and developing information on coastal processes and sea level rise. Independent advice about this issue should be sought.

Hours of construction

Please be aware that you are required to comply with the *Environmental Protection Act* in regards to noise standards and hours of construction.

Services installation

It is recommended that where the installation of services and infrastructure will impact on the location of existing vegetation identified for retention, an experienced and qualified arborist that is a member of the Australian Arborist Association or equivalent association, be commissioned to provide impact reports and on site supervision for these works.

Fire ants

Areas within Redland City have been identified as having an infestation of the Red Imported Fire Ant (RIFA). Biosecurity Queensland should be notified on 13 25 23 of proposed development(s) occurring in the Fire Ant Restricted Area before earthworks commence. It should be noted that works involving movements of soil associated with earthworks may be subject to movement controls and failure to obtain necessary approvals from Biosecurity Queensland is an offence. It is a legal obligation to report any sighting or suspicion of fire ants within 24 hours to Biosecurity Queensland on 13 25 23. The Fire Ant Restricted Area as well as general information can be viewed on the Department of Agriculture and Fisheries (DAF) website www.daf.qld.gov.au/fireants



Cultural heritage

The Aboriginal Cultural Heritage Act 2003 requires anyone who carries out a land use activity to exercise a duty of care. Further information on cultural heritage duty of care is available on the Department of Aboriginal and Torres Strait Islander Partnerships (DATSIP) website:

https://www.datsip.qld.gov.au/resources/datsima/people-communities/culturalheritage/cultural-heritage-duty-care.pdf

The DATSIP has established a register and database of recorded cultural heritage matters, which is also available on the Department's website:

https://www.datsip.qld.gov.au/people-communities/aboriginal-torres-strait-islander-culturalheritage/cultural-heritage-search-request

Quandamooka Yoolooburrabee Aboriginal Corporation (QYAC) is the registered cultural heritage body in the Redland City local government area. It is recommended you consult with QYAC in relation to aboriginal and cultural heritage matters prior to the commencement of works on site. QYAC can be contacted on 07 3415 2816 or <u>admin@QYAC.net.au</u>

Should any aboriginal, archaeological or historic sites, items or places be identified, located or exposed during construction or operation of the development, the *Aboriginal and Cultural Heritage Act 2003* requires all activities to cease. Please contact DATSIP for further information.

Fauna protection

It is recommended an accurate inspection of all potential wildlife habitats be undertaken prior to removal of any vegetation on site. Wildlife habitat includes trees (canopies and lower trunk) whether living or dead, other living vegetation, piles of discarded vegetation, boulders, disturbed ground surfaces, etc. It is recommended that you seek advice from the Queensland Parks and Wildlife Service if evidence of wildlife is found.

Environment Protection and Biodiversity Conservation Act

Under the Commonwealth Government's *Environment Protection and Biodiversity Conservation Act* (the EPBC Act), a person must not take an action that is likely to have a significant impact on a matter of national environmental significance without Commonwealth approval. Please be aware that the listing of the Koala as **vulnerable** under this Act may affect your proposal. Penalties for taking such an action without approval are significant. If you think your proposal may have a significant impact on a matter of national environmental significance, or if you are unsure, please contact Environment Australia on 1800 803 772. Further information is available from Environment Australia's website at <u>www.ea.gov.au/epbc</u>

Please note that Commonwealth approval under the EPBC Act is independent of, and will not affect, your application to Council.

Application B (MCU17/0058) – Conditions package

| MAT | MATERIAL CHANGE OF USE | | | | | | | |
|------------------------------|--|--------|------------|--|--|--|--|--|
| ASSE | SSMENT MANAGER CONDITIONS | TIMING | | | | | | |
| 1. | Comply with all conditions of this approval, at no cost to Council, at the timing periods specified in the right-hand column. Where the column indicates that the condition is an ongoing condition, that condition must be complied with for the life of the development. | | | | | | | |
| Approved plans and documents | | | | | | | | |
| 2. | Undertake the development in accordance with the approved plans and documents referred to in Table 1, subject to the conditions of this approval and any notations by Council on the plans. | | use and | | | | | |

| Plan/document title | Reference number | Prepared by | | Plan/doc. date |
|----------------------|------------------|-------------------------|-------|-------------------|
| Cover sheet | DA-A3-00/M | Philip Constructions | Usher | 05/07/2021 |
| Site Plan – Overall | DA-A3-01/M | Philip Constructions | Usher | 05/07/2021 |
| Open Space – Overall | DA-A3-02/M | Philip Constructions | Usher | 05/07/2021 |
| Site Plan | DA-A3-03/M | Philip Constructions | Usher | 05/07/2021 |
| Open Space | DA-A3-04/M | Philip Constructions | Usher | 05/07/2021 |
| Earthworks | DA-A3-05/M | Philip Constructions | Usher | 05/07/2021 |
| Waste Management | DA-A3-06/M | Philip Constructions | Usher | 05/07/2021 |
| Streetscapes | DA-A3-07/M | Philip Constructions | Usher | 05/07/2021 |
| B3 Ground Floor Plan | DA-A3-08/M | Philip Constructions | Usher | 05/07/2021 |
| B3 First Floor Plan | DA-A3-09/M | Philip Constructions | Usher | 05/07/2021 |
| B3 Elevations Sht 1 | DA-A3-10/M | Philip Constructions | Usher | 05/07/2021 |
| B3 Elevations Sht 2 | DA-A3-11/M | Philip Constructions | Usher | 05/07/2021 |
| B4 Ground Floor Plan | DA-A3-12/M | Philip Constructions | Usher | 05/07/2021 |
| B4 First Floor Plan | DA-A3-13/M | Philip Constructions | Usher | 05/07/2021 |



| B4 Elevations Sht 1 | DA-A3-14/M | Philip Constructions | Usher | 05/07/2021 |
|--|------------|-------------------------|-------|------------|
| B4 Elevations Sht 2 | DA-A3-15/M | Philip Constructions | Usher | 05/07/2021 |
| B4-HR Ground Floor Plan | DA-A3-16/M | Philip Constructions | Usher | 05/07/2021 |
| B4-HR First Floor Plan | DA-A3-17/M | Philip Constructions | Usher | 05/07/2021 |
| B4-HR Elevations Sht 1 | DA-A3-18/M | Philip Constructions | Usher | 05/07/2021 |
| B4-HR Elevations Sht 2 | DA-A3-19/M | Philip Constructions | Usher | 05/07/2021 |
| B4-SR Ground Floor Plan | DA-A3-20/M | Philip Constructions | Usher | 05/07/2021 |
| B4-SR First Floor Plan | DA-A3-21/M | Philip Constructions | Usher | 05/07/2021 |
| B4-SR Elevations Sht 1 | DA-A3-22/M | Philip Constructions | Usher | 05/07/2021 |
| B4-SR Elevations Sht 2 | DA-A3-23/M | Philip Constructions | Usher | 05/07/2021 |
| D6 Ground Floor Plan | DA-A3-24/M | Philip Constructions | Usher | 05/07/2021 |
| D6 First Floor Plan | DA-A3-25/M | Philip Constructions | Usher | 05/07/2021 |
| D6 Elevations | DA-A3-26/M | Philip Constructions | Usher | 05/07/2021 |
| D6-P Ground Floor Plan | DA-A3-27/M | Philip Constructions | Usher | 05/07/2021 |
| D6-P First Floor Plan | DA-A3-28/M | Philip Constructions | Usher | 05/07/2021 |
| D6-P Elevations | DA-A3-29/M | Philip Constructions | Usher | 05/07/2021 |
| D6-W Ground Floor Plan | DA-A3-30/M | Philip Constructions | Usher | 05/07/2021 |
| D6-W First Floor Plan | DA-A3-31/M | Philip Constructions | Usher | 05/07/2021 |
| D6-W Elevations (as amended in red by Council) | DA-A3-32/M | Philip Constructions | Usher | 05/07/2021 |
| D7 Ground Floor Plan | DA-A3-33/M | Philip Constructions | Usher | 05/07/2021 |
| D7 First Floor Plan | DA-A3-34/M | Philip Constructions | Usher | 05/07/2021 |
| D7 Elevations | DA-A3-35/M | Philip Constructions | Usher | 05/07/2021 |



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| Bulk Earthworks Cross Sections – SH 4 | SK08 Rev 3 | 2 Develop Urban | 29/10/2020 |
|--|--------------|-----------------------|------------------|
| Bulk Earthworks Cross Sections – SH 5 | SK09 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 6 | SK10 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 7 | SK11 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Bulk Earthworks Cross Sections – SH 8 | SK12 Rev 3 | 2 Develop Urban | 29/10/2020 |
| Proposed Services Connection Plan | SK13 Rev 4 | 2 Develop Urban | 29/10/2020 |
| Detention Basin Discharge Layout Plan | SK14 Rev 1 | 2 Develop Urban | 12/03/2020 |
| Flood Impact Assessment | 41/26773 | GHD | February 2016 |
| Landscape Concept Plan | 8817 Issue G | Saunders Havill Group | June 2021 |
| Acoustic Report | 2017122 R01L | Acoustic Works | 23/11/2020 |

Table 1: Approved plans and documents

| 3. | Comply with the infrastructure agreement relating to the subject site. | Prior to the commencing ongoing. | use and |
|------|--|--|------------|
| 4. | Comply with the conditions of approval, and achieve plan sealing for MCU17/0057. | Prior to the commencing ongoing. | use and |
| Exis | ting structures | | |
| 5. | Demolish or relocate/remove all existing structures on site in accordance with the approved plan(s). Cap all services prior to demolition commencing. | Prior to the commencing. | use |
| Des | ign | | |
| 6. | Locate, design and install outdoor lighting, where required, to minimise the potential for light spillage to cause nuisance to neighbouring premises, in accordance with Australian Standard AS4282-2019: Control of the obtrusive effects of outdoor lighting. | Prior to the commencing ongoing. | use and |
| 7. | Design and construct the development with the following changes as indicated in red on the approved plans: c) Cladding/façade treatment for unit 31. | Prior to the commencing ongoing. | use and |

| | Document or works A | ssessment benchmarks | | |
|------|--|-----------------------------|--|-----------|
| 12. | Submit to Council, and receive approval for, Assessment for the documents and works referred t | | Prior to site of commencing. | work |
| Cond | itioned works assessment | | | |
| 11. | Provide certification from a Registered Profe Queensland (RPEQ) certifying that the internal adequate design and construction to allow waste enter and exit the site in a forward gear. | access roads are of | Prior to the commencing. | us |
| | If a bin wash-down bay is to be provided it shall be and cock located within the vicinity of the bay, and h suitably drained to a trade waste outlet. | | | |
| | external streets, and d) is no less than 800mm wide x 1500mm long x 1 | 100 mm high. | | |
| | without travelling over steps or through dwellir c) has a hardstand surface and screens bins from v | g areas, | | |
| | a) is located within the curtilage of the dwellin otherwise on the approved plans,b) allows for containers to be moved from storage | | | |
| 10. | Ensure that each residential dwelling unit has their storage area that: | | Prior to the commencing. | us |
| 9. | Ensure that air conditioning units are located so that from any external streets. | - | Prior to the commencing ongoing. | us and |
| | c) Provide fixed external screens that are – i) solid and translucent; or ii) perforated panels or trellises that have openings, with a maximum opening dimensi are permanently fixed and durable; and iii) offset a minimum of 300mm from the wall opening | on of 50mm, and that | | |
| | b) Provide fixed translucent, such as frosted or tex part of the window less than 1.5m above floor le | | | |
| | a) Provide sill heights a minimum of 1.5m above flo | oor levels; or | | |
| 8. | Comply with the following requirements where habitable room windows above the ground storey wi of 6m, and within an angle of 45 degrees, and habitable rooms of neighbouring dwelling units (inte | Prior to the commencing. | use | |

| Document or works | Assessment benchmarks |
|-------------------|--|
| Landscape plan | Redlands Planning Scheme Part 8 Division 8 – Landscape Code Redlands Planning Scheme Part 9 Schedule 9 – Street Trees |

| | Redlands Planning Scheme Part 11 Policy 3 Chapter 3 – Landscaping and Chapter 4 – Security Bonding Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions, Chapter 10 – Parks and Open Space and Chapter 11 – Landscaping Redlands Planning Scheme Part 11 Policy 16 – Safer by Design |
|------------------------------|---|
| Stormwater management plan | Redlands Planning Scheme Part 8 Division 9 – Stormwater Management Code Redlands Planning Scheme Part 11 Policy 3 Chapter 4 – Security Bonding Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions and Chapter 6 – Stormwater Management Water Sensitive Urban Design Technical Guidelines for South East Queensland State Planning Policy July 2017 Queensland Urban Drainage Manual |
| Roadworks | Redlands Planning Scheme Part 8 Division 1 – Access and Parking Code Redlands Planning Scheme Part 8 Division 7 – Infrastructure Works Code Redlands Planning Scheme Part 11 Policy 3 Chapter 4 – Security Bonding Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions and Chapter 5 – Road and Path Design. |
| Construction Management Plan | Redlands Planning Scheme Part 11 Policy 9 Chapter 2 – Documentation and General Conditions Redlands Planning Scheme Part 11 Policy 3 Chapter 4 – Security Bonding. |

Table 2: Conditioned works assessment

| 13. | Comply with all conditions and approved plans in the Conditioned Works Approval. | Prior to comment | the ing. | use |
|-------|--|---------------------------------|-------------|------------|
| Parki | ng | | | |
| 14. | Provide car parking in accordance with the approved plans. | Prior to comment ongoing. | the ing | use and |



| | | nain unobstructed and availal | | ays and driveways must led purpose. | |
|------|------|--|---|---|---|
| Ease | ment | <u>s</u> | | | |
| 15. | dod | nt easements for the follow cumentation to Council for ister the easements on the p | Prior to the use commencing. | | |
| | a) | A minimum 3m wide ease Council, over the stormwa Parade road discharge pip diameter pipe), traversin treatment area within the c | ater drainage pipe be (825mm diame ng through the | es from each Pateman ter pipe and 1200mm | |
| | b) | A stormwater drainage eas Council, over the Q100 ov from Boundary Road to Cle | rsing the development | | |
| | c) | Access purposes 1m wide holes or structures in favou | | | |
| | d) | Public access purposes o connecting from the develo within the open space. | | | |
| Bond | ds | | | | |
| 16. | Loc | lge with Council the bonds lis | ted in Table 3: | | Prior to requesting a pre-start meeting o works commencing whichever is the sooner. |
| | Во | ond item | Amount | Returned | |
| | Ro | oad cleaning bond | \$2,000 | When works acc by Council. | cepted on maintenance |
| | Ro | | | When works acc by Council. | cepted on maintenance |
| | т | DTAL | \$7,000 | | |

| | a) 10% of the contributed asset(s); or b) \$2,500. This bond will be returned after formal acceptance by Council of the contributed asset(s) off maintenance and the transfer of the applicable works to Council. | Prior to contributed asset being accepted on maintenance by Council. |
|-------|---|---|
| Inspe | ctions | |
| 18. | Arrange with Council for the following inspections to be carried out at the relevant time in accordance with Table 4: Inspections below. | At timing indicated in table 4. |

| Inspection | Timing | |
|--------------------------------|--|--|
| Pre-start | Prior to any works commencing. | |
| Erosion and sediment control | Immediately after installation of erosion and sediment control measures. | |
| Landscape inspection | Must be undertaken prior to any works commencing on the site or external to the site. | |
| Driveway crossover/footpath | Box inspection to be undertaken with reinforcing mesh in place and supported on bar chairs prior to the concrete being poured. | |
| Stormwater bedding | After the stormwater pipelines are bedded and prior to backfilling. | |
| Sewer bedding | After the sewer pipelines are bedded and prior to backfilling. | |
| Water main bedding | After the water pipelines are bedded and prior to backfilling. | |
| Sewer vacuum testing | During sewer vacuum testing. | |
| Water pressure testing | During water main pressure testing. | |
| On maintenance | On completion of all civil and landscaping works to be transferred into public ownership as required by this approval and its conditions and prior to the commencement of the 12 months maintenance period. | |
| Compliance inspection | On completion of the development in accordance with the approval and its conditions. | |
| Off maintenance | At the end of the minimum 12 months maintenance period. | |

For the pre-start, on maintenance/compliance and off maintenance inspections, at least **five (5) business days** notice must be given to Council. For all other inspections, a minimum of **24 hours** notice must be given to Council.

The contributed assets must be accepted on maintenance and the development must pass a Compliance Inspection before the use commences.

<u>Note</u>: The Civil Consulting Engineer should inspect the works and satisfy themselves that the works are satisfactory prior to booking the respective inspections. In instances where Council's representative(s) fails an inspection, Council will charge a re-inspection fee prior to re-visiting the site. The cost of this re-inspection is identified in Council's Register of Fees and is reviewed each financial year.

| Gene | eral | |
|------|---|---|
| 19. | Provide details to Council of the nominated Principal Contractor, including copies of the Principal Contractor's workcover and public liability currency certificates. The public liability insurance policy must be a minimum of ten million dollars and must indemnify Redland City Council. | Prior to requesting a pre-start meeting or works commencing, whichever is the sooner. |
| 20. | Submit to Council a copy of the proposed Program of Civil Works. | Prior to requesting a pre-start meeting or works commencing, |



| | | | whichever is the sooner. |
|------|---|--|---|
| 21. | Provide temporary drainage during the building construction phase suc that discharge from all constructed roofs and paved areas is disposed on to a lawful point of discharge in accordance with the Queensland Urba Drainage Manual (QUDM). Maintain the temporary system for the duration of the building works. | | During construction. |
| 22. | dar | tify Council within 24hrs and rectify, in consultation with Council, any mage to Council infrastructure as a result of construction activities, at cost to Council. | As soon as practical following identification of the damage. |
| 23. | Qu acc | ovide written certification from a Registered Professional Engineer eensland (RPEQ) certifying that all civil works have been completed in cordance with the approved drawings and specifications and to the olicable Australian Standards. | Prior to on maintenance or compliance inspection whichever is the sooner. |
| 24. | at pro | dertake the development works so that there is no risk to public safety any time on the site, adjacent public land, road reserve or private operty. Should the site be unattended or abandoned, public safety must I be maintained. | During construction phase. |
| 25. | | ctify any damage done to the road verge during construction, including osoiling and re-turfing. | Prior to a compliance inspection. |
| 26. | Maintain all contributed assets for a minimum period of 12 months from the date the works are accepted on maintenance by Council. The works will be accepted off maintenance only where the works have been suitably maintained to any manufacturer's specifications and Council standards and are fit for purpose. | | During the on maintenance period. |
| Road | lwor | <u>ks</u> | |
| 27. | foll ass | omit to Council for approval, engineering plans and details showing the owing works (for Pateman Parade) are in accordance with the essment criteria listed in Table 2: Conditioned Works Assessment of s approval: | As part of request for conditioned works assessment. |
| | c) | Road construction including concrete kerb and channel and road pavement and concrete shared use footpaths extending from the end of the existing pavement in Pateman Parade to the signalised intersection at Cleveland-Redland Bay Road. | |
| | d) | A minimum 6m wide type R-RSC-2 permanent vehicular crossover to the Pateman Parade frontage of the site. | |
| | e) | Footpath earthworks, topsoiling and turfing of all disturbed footpath areas. | |
| | f) | Reinstatement of concrete kerb and channel where required. | |



| | g) Rer | noval of all redundant vehicle crossovers. | |
|-----|-------------------|--|--|
| | h) Ent | ry treatment/access to the site along Pateman Parade. | |
| | - | nent and relocations necessary to public utility services resulting nese works. | |
| 28. | comple Clevela | to Council for approval, engineering plans and details for the tion of the four-way signalised intersection at Pateman Parade, nd Redland Bay Road and Beveridge Road, in accordance with the nent criteria listed in Table 2: Conditioned Works Assessment. | As part of request for conditioned works assessment. |
| | Modify | the existing three-way intersection to include provision of: | |
| | e) | a single westbound traffic lane (minimum 3.5m width) and a separate shared cycle and parking lane (minimum width 4.0m) from the intersection to join with the existing Pateman Parade pavement (2 lane major collector standard); | |
| | f) | a dedicated right turn lane (approximately 130m long), a shared through and right turn lane, a through cycle lane (1.5m minimum width) and a high-angle entry left turn lane on the Pateman Parade approach; | |
| | g) | a dedicated right turn lane on Cleveland Redland Bay Road (a minimum of 85m long) to facilitate right turns into Pateman Parade; | |
| | h) | a dedicated left turn lane (including retention of the existing dedicated cycle lane) on Cleveland Redland Bay Road northbound into Pateman Parade; | |
| | i) | a signalised pedestrian crossing across the Pateman Parade approach; | |
| | j) | modification of the existing staged pedestrian crossing across the northern leg of the intersection, to a full 3.0m wide (unstaged) crossing; | |
| | k) | provision of a 3.0m wide signalised pedestrian crossing across the southern leg of the intersection; | |
| | I) | the extension of the 2.5m wide concrete shared pedestrian and cycle path along the northern verge of Pateman Parade to connect to the existing path on Cleveland Redland Bay Road; and | |
| | m) | the extension of the 2.5m wide concrete shared pedestrian and cycle path along the southern verge of Pateman Parade to connect to the pedestrian crossing noted above and the shared pathway on Cleveland Redland Bay Road to be constructed as part of this approval. | |
| | | ove works must include all necessary changes to line marking, signals and street lighting hardware and reprogramming of the ignals. | |
| | qualifie | d design "for construction" drawings certified by a suitably d and experienced RPEQ Civil or Traffic Engineer and an RPEQ al engineer must be submitted to Council. | |



| 29. | арр | dicate land for the purposes of road where required to facilitate the proved four-way signalised intersection at Pateman Parade, Cleveland Iland Bay Road and Beveridge Road. | Prior to the use commencing. |
|-------|--|--|--|
| 30. | pur | mplete the construction of the road intersection works required suant to condition 27 – 29 in accordance with the approved plans and aditions. | Prior to the use commencing. |
| 31. | Submit and have approved by Council a Road Opening Approval for any works being undertaken within the road reserve. Provide the following to Council as part of the application: | | Prior to requesting a pre-start meeting or works commencing, |
| | a) | A completed application form and associated fee, at the rate applicable at the time of payment. The current rate for the 2020/2021 Financial Year is: | whichever is the sooner. |
| | • | 5,432.00 – this incorporates a refundable bond of $5,000$ and a non-refundable administration fee of \$432.00. | |
| | b) | A copy of the contractor's Workcover insurance currency certificate. | |
| | c) | A copy of the contractor's Public Liability insurance currency certificate. The public liability insurance policy must be a minimum of ten million dollars and must indemnify Redland City Council. | |
| | d) | Submission of a Traffic Management Plan (TMP) and/or a Traffic Guidance Scheme (TGS) that is prepared and authorised by a person who holds a current DTMR 'Open Level' Traffic Management Design Certification and should include proposed haul routes for construction vehicles associated with the works, as applicable. | |
| 32. | Provide a Construction management Plan which includes the following information: | | As part of request for conditioned works |
| | a) | Hours of construction. | assessment. |
| | b) | Vehicle access during construction hours, including responsibility for maintenance of the defined cartage route and proposed haulage route for construction traffic (including loading and unloading). | |
| | c) | Traffic management signed by a suitably qualified Workplace Health and Safety professional. | |
| | d) | Parking of vehicles (trucks, on-site employees and delivery vehicles). | |
| | e) | Maintenance of safe pedestrian movement across the frontage(s) of the site (including disable access). | |
| | f) | On-site dust and noise management so as not to cause unreasonable disturbance to the amenity of the surrounding areas. | |
| | g) | Building waste/refuse disposal. | |
| | h) | Presentation of hoarding to the street. | |
| Storn | nwat | ter management – (non-trunk in accordance with section 145 of PAct) | |



| 33. | Convey roof water and surface water to the proposed detention basin to be constructed at the south-eastern corner of the development. | Prior to the use commencing and ongoing. |
|---------------|--|---|
| 34. | Manage stormwater discharge from the site so as to not cause an actionable nuisance to adjoining properties. | Once the works commence and prior to the use commencing. Ongoing condition. |
| 35. | Submit to Council, and receive Conditioned Works approval for, a stormwater management plan and report addressing both quality and quantity in accordance with the City Plan Planning Scheme Policy 2 – Infrastructure Works, and the following: | As part of request for conditioned works assessment. |
| | Detailed drawings of the proposed stormwater quantity and quality treatment systems and any associated works. The drawings must include longitudinal and cross sections as well as details of treatment media and any associated vegetation/landscaping. | |
| | • An updated electronic copy of the MUSIC model, which reflects the final layout. | |
| 36. | Maintain all internal stormwater management devices for the life of the development in accordance with approved documentation and to manufacturer's specifications. | Ongoing condition |
| <u>Utilit</u> | ties | |
| 37. | Pay the cost of any alterations to existing public utility mains, services or installations due to building and works in relation to the proposed development, or any works required by conditions of this approval. Any cost incurred by Council must be paid at the time the works occur in accordance with the terms of any cost estimate provided to perform the works, or prior to plumbing final or the use commencing, whichever is the sooner. | At the time of works occurring. |
| 38. | Connect the development to external reticulated sewer, external reticulated water and underground electricity supply. | Prior to the use commencing. |
| 39. | Remove any redundant sewerage connections within the site or servicing the development and provide documentary evidence to Council or its delegate that this has occurred. | Prior to site works commencing. |
| | Provide water connections and water meters in accordance with Council's Standard Drawings. Provide details to Council of the water meters and | Prior to or maintenance |

| 41. | Undertake any required excavation and fill works in accordance with the following: | During construction. |
|--------------|---|--|
| | Design retaining walls/structures to have a minimum design life of 60 years and to be in accordance with Australian Standard 4678:2002 – Earth Retaining Structures (as amended). | |
| | b) Undertake compaction in accordance with Australian Standard 3798:2007 – Guidelines on earthworks for commercial and residential developments (as amended) and Australian Standard 2870:2011 – Residential Slabs and Footings (as amended). | |
| | c) Comply with the relevant requirements of the Building Regulations 2006 (as amended) where involving gradients or embankments. | |
| 42. | Submit to Council test certificates for all filling greater than 400mm in depth, confirming that the necessary levels of compaction have been achieved, including a Level 1 inspection and testing report in accordance with AS3798. | Prior to the use commencing. |
| 43. | Provide certification from a Registered Professional Engineer Queensland (RPEQ) for the design and construction of any retaining walls greater than 1m in height, in relation to stability and resistance to overturning and sliding (overall safety factor of 1.5) in accordance with AS1170.1 and AS3600. | |
| <u>Erosi</u> | on and sediment control | |
| | | |
| 44. | Design, implement and maintain measures and practices in accordance with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). | Prior to the works commencing and during construction phase until disturbed areas are stabilised. |
| 44. 45. | with "Best Practice Erosion and Sediment Control" published by the | commencing and during construction phase until disturbed areas are stabilised. |
| | with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). Ensure dewatering activities (such as from excavations or sediment basins) do not discharge water offsite or where the water could reasonably be expected to move offsite or into waters or stormwater, unless the | commencing and during construction phase until disturbed areas are stabilised. During construction |
| | with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). Ensure dewatering activities (such as from excavations or sediment basins) do not discharge water offsite or where the water could reasonably be expected to move offsite or into waters or stormwater, unless the following are met: e) The concentration of total suspended solids (TSS) must not exceed 50 | commencing and during construction phase until disturbed areas are stabilised. During construction phase. |
| | with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). Ensure dewatering activities (such as from excavations or sediment basins) do not discharge water offsite or where the water could reasonably be expected to move offsite or into waters or stormwater, unless the following are met: e) The concentration of total suspended solids (TSS) must not exceed 50 mg/L. f) Turbidity (nephelometric turbidity units - NTU) values must not be greater than 10% of the turbidity of waters immediately upstream of | commencing and during construction phase until disturbed areas are stabilised. During construction phase. |
| | with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). Ensure dewatering activities (such as from excavations or sediment basins) do not discharge water offsite or where the water could reasonably be expected to move offsite or into waters or stormwater, unless the following are met: e) The concentration of total suspended solids (TSS) must not exceed 50 mg/L. f) Turbidity (nephelometric turbidity units - NTU) values must not be greater than 10% of the turbidity of waters immediately upstream of the sites water entry points at time of the release. g) The pH must be within the range of 6.5-8.5 except where, and to the | commencing and during construction phase until disturbed areas are stabilised. During construction phase. |

| | roadways. Remove any material brought onto the road as soon as possible. | |
|-------------|--|--|
| 47. | Implement dust control measures at each phase of site development and operation in accordance with IECA (2008) Best Practice Erosion and Sediment Control. | During construction phase. |
| <u>Surv</u> | ey and as-constructed information | |
| 48. | Submit as constructed drawings and documentation for all civil works and those to be transferred into public ownership, prepared in accordance with the City Plan Planning Scheme Policy 2 – Infrastructure Works. | Prior to the compliance inspection. |
| | Include surveyed as constructed data showing works completed (digital and hard copies) and amended design plan data showing construction deviation from design plans (digital and hard copies). The digital data and the design data must be endorsed by a RPEQ and Landscape Architect holding AILA (Australian Institute of Landscape Architecture) membership and a registered surveyor using the certification clauses contained in Planning Scheme Policy 2 – Infrastructure Works. | |
| Acou | stic requirements | |
| 49. | Incorporate acoustic attenuation into the development as specified in the approved acoustic report. | Prior to the use commencing and ongoing. |
| Land | scape Works | |
| 50. | Submit landscape plans to Council for approval in accordance with the assessment criteria listed in Table 2: Conditioned Works Assessment of this approval. Include the following items: | As part of request for conditioned works assessment. |
| | Designs that are generally in accordance with the approved landscape plans, but which include all planting details including species, densities and planting placements. | |
| | • Details of street tree planting in accordance with the Redlands Planning Scheme Landscape Code with species selected from Schedule 9 of the Redlands Planning Scheme. | |
| | • A maintenance plan for the entire landscaping component of the development. | |
| | • Details of lighting to communal open space, driveways, public car parks and footpaths within the site. | |
| 51. | Undertake landscaping in accordance with the landscape plans approved | Prior to the use |

| Turf all areas of disturbance within the road verge with turf cut from a weed free source. Remove all weed species from the site, as identified in the Redland Coast Biosecurity Plan 2018-2023. | Is |
|---|---|
| Coast Biosecurity Plan 2018-2023. | |
| | of |
| Provide organic mulch to all garden bed areas at a minimum depth of 100mm. | |
| Environment and vegetation management | |
| 52. Submit a revised Vegetation Management Plan to Council which reflects | As part of request for |
| Trees identified for removal and retention; | conditioned works assessment. |
| All plantings across the site; | assessment. |
| The landscaping and planting works occurring to all of th stormwater treatment areas; and | e |
| Any revegetation works. | |
| 53. Protect those trees identified on the revised Vegetation Management Pla to be retained by implementing tree protection measures in accordance with Australian Standard AS4970-2009 – Protection of Trees of Development Sites. All tree protection measures must be in place prior to any development works commencing. | e commencing and n during construction |
| 54. Provide a dam dewatering report which details how the dam on the subject site will be removed. Details are to include but not be limited to: How the dam will be dewatered; | |
| • How the silt will be removed and where it will be disposed of; | |
| • Details of the levels that the dam will be removed to; | |
| Details of any subsoil drainage so that the base of the dam site doe not retain or accumulate ground waters. | s |
| 55. Submit to Council written certification and test results to verify that the earthworks associated with the dam removal have been completed to Level 1 responsibility in accordance with Australian Standard AS3798 1990. | a inspection. |
| ADDITIONAL APPROVALS | |
| | |

The following further development permits are necessary to allow the development to be carried out:

Building works approval.

Please be aware that further approvals, other than a development permit, may still be required for your development. This includes, but is not limited to, the following:

- Conditioned works assessment as detailed in Table 2 of the conditions.
- Plumbing and drainage works.
- Capping of sewer for demolition of existing buildings on site.



• Road opening permit for any works proposed within an existing road reserve.

REFERRAL AGENCY CONDITIONS

Queensland Department of State Development, Manufacturing, Infrastructure and Planning
(DSDMIP)

Refer to the attached correspondence from the DSDMIP dated 8 April 2021 (DSDMIP reference 1710-1966-SRA) for MCU17/0057.

ASSESSMENT MANAGER ADVICE

Infrastructure charges

Infrastructure charges apply to the development in accordance with the Adopted Infrastructure Charges Resolution (No. 3.1) 2020 levied by way of an Infrastructure Charges Notice. The infrastructure charges are contained in the attached Redland City Council Infrastructure Charges Notice.

Working within a State-controlled road network

Prior to undertaking any works within a State-controlled road corridor, obtain a Road Corridor Permit from the Queensland Department of Transport and Main Roads.

Live connections

Redland City Council is responsible for all live water and wastewater connections. Contact **must** be made with Council to arrange live works associated with the development.

Further information can be obtained from Council on 07 3829 8999.

Coastal processes and sea level rise

Please be aware that development approvals issued by Redland City Council are based upon current lawful planning provisions which do not necessarily respond immediately to new and developing information on coastal processes and sea level rise. Independent advice about this issue should be sought.

Hours of construction

Please be aware that you are required to comply with the *Environmental Protection Act* in regards to noise standards and hours of construction.

Services installation

It is recommended that where the installation of services and infrastructure will impact on the location of existing vegetation identified for retention, an experienced and qualified arborist that is a member of the Australian Arborist Association or equivalent association, be commissioned to provide impact reports and on site supervision for these works.

Fire ants

Areas within Redland City have been identified as having an infestation of the Red Imported Fire Ant (RIFA). Biosecurity Queensland should be notified on 13 25 23 of proposed development(s) occurring in the Fire Ant Restricted Area before earthworks commence. It should be noted that works involving movements of soil associated with earthworks may be subject to movement controls and failure to obtain necessary approvals from Biosecurity Queensland is an offence. It is a legal obligation to report any sighting or suspicion of fire ants within 24 hours to Biosecurity Queensland on 13 25 23. The Fire Ant Restricted Area as well as general information can be viewed on the Department of Agriculture and Fisheries (DAF) website www.daf.qld.gov.au/fireants

Cultural heritage

The Aboriginal Cultural Heritage Act 2003 requires anyone who carries out a land use activity to exercise a duty of care. Further information on cultural heritage duty of care is available on the Department of Aboriginal and Torres Strait Islander Partnerships (DATSIP) website:

https://www.datsip.qld.gov.au/resources/datsima/people-communities/culturalheritage/cultural-heritage-duty-care.pdf

The DATSIP has established a register and database of recorded cultural heritage matters, which is also available on the Department's website:

https://www.datsip.qld.gov.au/people-communities/aboriginal-torres-strait-islander-culturalheritage/cultural-heritage-search-request

Quandamooka Yoolooburrabee Aboriginal Corporation (QYAC) is the registered cultural heritage body in the Redland City local government area. It is recommended you consult with QYAC in relation to aboriginal and cultural heritage matters prior to the commencement of works on site. QYAC can be contacted on 07 3415 2816 or <u>admin@QYAC.net.au</u>

Should any aboriginal, archaeological or historic sites, items or places be identified, located or exposed during construction or operation of the development, the *Aboriginal and Cultural Heritage Act 2003* requires all activities to cease. Please contact DATSIP for further information.

Fauna protection

It is recommended an accurate inspection of all potential wildlife habitats be undertaken prior to removal of any vegetation on site. Wildlife habitat includes trees (canopies and lower trunk) whether living or dead, other living vegetation, piles of discarded vegetation, boulders, disturbed ground surfaces, etc. It is recommended that you seek advice from the Queensland Parks and Wildlife Service if evidence of wildlife is found.

Environment Protection and Biodiversity Conservation Act

Under the Commonwealth Government's *Environment Protection and Biodiversity Conservation Act* (the EPBC Act), a person must not take an action that is likely to have a significant impact on a matter of national environmental significance without Commonwealth approval. Please be aware that the listing of the Koala as **vulnerable** under this Act may affect your proposal. Penalties for taking such an action without approval are significant. If you think your proposal may have a significant impact on a matter of national environmental significance, or if you are unsure, please contact Environment

Australia on 1800 803 772. Further information is available from Environment Australia's website at www.ea.gov.au/epbc

Please note that Commonwealth approval under the EPBC Act is independent of, and will not affect, your application to Council.



STATEMENT OF REASONS

Application A (MCU17/0057)

| Assessment | The proposed development was assessed against the following | |
|--------------------|--|--|
| Benchmarks: | assessment benchmarks: | |
| | Redlands Planning Scheme V7.1: | |
| | medium density residential zone code | |
| | urban residential zone code | |
| | open space zone code | |
| | community purposes zone code | |
| | multiple dwelling code | |
| | reconfiguration code | |
| | acid sulfate soils overlay code | |
| | flood prone, storm tide and drainage constrained land overlay code | |
| | habitat protection overlay code | |
| | road and rail noise impacts overlay code | |
| | south-east thornlands structure plan overlay code | |
| | access and parking code | |
| | erosion prevention and sediment control code | |
| | excavation and fill code | |
| | landscape code | |
| | stormwater management code | |
| | State Planning Policy 2017, Part E | |
| | South East Queensland Regional Plan 2017 | |
| | • Planning Regulation 2017, Schedule 11 | |
| | Local Government Infrastructure Plan. | |
| Matters prescribed | Council had regard to the following matters in the assessment of | |
| by Regulation | the application: | |
| | existing development | |
| | current development approvals | |
| | submissions made during public notification | |
| | State Assessment Referral Agency (SARA) response | |
| | City Plan V5. | |
| | | |

The key issues identified in the assessment were:

- consistency of proposal with the Redlands Planning Scheme
- building design/streetscape
- access/road design
- traffic and parking
- environmental values
- stormwater management
- landscaping
- open space design
- earthworks
- sewer and
- road noise.

| Issue | Assessment outcome |
|---|--|
| Consistency of proposal with the Redlands Planning Scheme | While the proposed development deviates from the zoning arrangements in the planning scheme, it achieves the outcomes of these zones and exhibits a more functional layout and a better visual outcomes from the highly prominent corner of Boundary Road and Cleveland Redland Bay Road. |
| Building design/streetscape | The development provides a high level of visual and physical interaction with the Pateman Parade. The buildings are oriented towards Pateman Parade and are provided with a range of aesthetically pleasing design treatments. |
| Access/road design | To provide connectivity in accordance with the South- East Thornlands structure plan, the development is conditioned to construct the remainder of the trunk collector road (Pateman Parade) and provide the fourth leg of the existing intersection at Beveridge/Cleveland Redland Bay Road. |
| Traffic and parking | The development provides suitable access points onto Pateman Parade and sufficient parking for the intended use. |
| Environmental values | The site has limited environmental values and habitat linkages due to surrounding major roads and residential development. Revegetation through the open space portion of the site is conditioned to provide for the establishment of habitat connectivity. Movement |

| | opportunities external to the site are facilitated via existing culverts. |
|-----------------------|---|
| Stormwater management | Stormwater discharge from the development will be treated on-site and managed by a detention basin in the southern portion of the site. |
| Landscaping | Landscaping will be provided along the Boundary and Cleveland Redland Bay Road frontages, which will provide visual relief from the State controlled roads. Landscaping internal to the site will provide suitable amenity for the residents. |
| Open space design | The development is provided with central communal open space areas through the site. The communal open space provides suitable areas for active and passive recreation and facilities, such as a gym and barbecue area for the use of residents. Each dwelling unit is also provided with sufficient private open space for their expected needs. |
| Earthworks | Cut and fill is required across the site to facilitate building areas for the residential component and stormwater management. Generally earthworks will tie into external land with batters, however where retaining is required landscaping is provided to provide visual relief and a suitable amenity outcome. |
| Sewer | Upgrades are required to the sewerage network to service the development and will be provided through an infrastructure agreement. |
| Road noise | Suitable acoustic attenuation is provided through the development design to reduce noise impacts on future residents to an acceptable level. Further requirement for building attenuation will be assessed by a private certifier at the building approval stage. |

| Matters Raised in Submissions | | |
|---|---|--|
| Matter Raised | Description of how matters were dealt with in reaching the decision | |
| Zone/precinct changes inconsistent with | The proposal deviates from the original zoning intent under the Redlands Planning Scheme and the South-East Thornlands structure plan overlay in terms of the location of the uses. This has been | |



| SETSPO. | considered acceptable as an equivalent outcome for the structure plan layout has been achieved through a revised layout. |
|--|---|
| Interurban break not provided. | While the interurban break envisaged by the structure plan is not provided, the development allows for an increase in amenity for future residents of the site and improved visual and physical break between the site and community of Victoria Point. |
| Filling of the flood prone area inconsistent with SETSPO and will impact on waterways. | The earthworks required to facilitate the development is noted to deviate from the existing landform. It is considered that this allows for an improved overall development outcome with residential development set back from the Boundary/Cleveland Redland Bay Road intersection. Stormwater discharge from the development is to be treated on-site and managed by a detention basin the southern portion of the site. Impacts on receiving waters are considered to be minimised. |
| Need for a local park in the area. | A local park has been provided as part of a separate development approval on Harrington Boulevard in lieu of the subject site. |
| Reduction in open space and associated impacts on habitat corridor functionality and greenspace network/recreation space. | The proposal deviates from the original zoning intent under the Redlands Planning Scheme and the South-East Thornlands structure plan overlay in terms of the location of the uses. This has been considered acceptable as an equivalent outcome for the structure plan layout has been achieved through a revised layout. |
| Built design out of character with surrounding area. | The built form of the development is consistent with the character of the surrounding area. |
| Density does not respond to capacity of roads, schools, health care etc. | The proposed development results in a density as envisaged by the zoning of the site. |
| Traffic impacts on surrounding roads network. | To provide connectivity as identified in the South-east Thornlands structure plan overly, the development is conditioned to complete the trunk collector (Pateman Parade) and the four-way intersection at Beveridge/Cleveland Redlands Bay Roads. |
| Insufficient infrastructure. | The development provides suitable infrastructure where not existing through conditions and an infrastructure agreement. |
| Stormwater management. | Stormwater discharge from the development is to be treated on-site and managed by a detention basin the southern portion of the site. |
| | , |



| Pateman Parade not fauna friendly. | Pateman Parade is not intended as a fauna movement corridor. Fauna movement is facilitated through the site in the open space area. |
|---|---|
| Koala offsets have not been considered. | The site is within a koala broad hectare area. Accordingly, offsets were not applicable. Revegetation of the open space area however is required to support habitat values. |
| Dedication of open space. | A local park has been provided as part of a separate development approval on Harrington Boulevard in lieu of the subject site. The open space on the site is to remain in private ownership, however will be traversable by pedestrians. |
| The development conflicts with desired environmental outcome (DEO) 1. | The development provides stormwater management devices suitable for the requirements of the development. The development is conditioned to provide revegetation of the open space area to support habitat values. |
| Stormwater infrastructure should not be located within the greenspace network. | The South-East Thornlands structure plan intends to locate stormwater management infrastructure within the open space network at the subject site. |



Application B (MCU17/0058)

| Assessment Benchmarks: | The proposed development was assessed against the following assessment benchmarks: | |
|-------------------------------------|--|--|
| Benchmarks: | Redlands Planning Scheme V7.1: medium density residential zone code urban residential zone code open space zone code community purposes zone code multiple dwelling code acid sulfate soils overlay code flood prone, storm tide and drainage constrained land overlay code habitat protection overlay code road and rail noise impacts overlay code south-east thornlands structure plan overlay code access and parking code erosion prevention and sediment control code excavation and fill code landscape code stormwater management code South East Queensland Regional Plan 2017 <i>Planning Regulation 2017</i>, Schedule 11 Local Government Infrastructure Plan. | |
| Matters prescribed by Regulation | Local Government Infrastructure Plan. Council had regard to the following matters in the assessment of the application: existing development current development approvals submissions made during public notification State Assessment Referral Agency (SARA) response City Plan V5. | |



The key issues identified in the assessment were:

- consistency of proposal with the Redlands Planning Scheme
- building design/streetscape
- access/road design
- traffic and parking
- environmental values
- stormwater management
- landscaping
- open space design
- earthworks
- sewer
- waste management
- water
- electricity/telecommunications and
- road noise.

| Issue | Assessment outcome |
|---|--|
| Consistency of proposal with the Redlands Planning Scheme | While the proposed development deviates from the zoning arrangements in the planning scheme, it achieves the outcomes of these zones and exhibits a more functional layout and a better visual outcomes from the highly prominent corner of Boundary Road and Cleveland Redland Bay Road. |
| Building design/streetscape | The development provides a high level of visual and physical interaction with the Pateman Parade. The buildings are oriented towards Pateman Parade and are provided with a range of aesthetically pleasing design treatments. |
| Access/road design | To provide connectivity in accordance with the South- East Thornlands structure plan, the development is conditioned to construct the remainder of the trunk collector road (Pateman Parade) and provide the fourth leg of the existing intersection at Beveridge/Cleveland Redland Bay Road. |
| Traffic and parking | The development provides suitable access points onto Pateman Parade and sufficient parking for the intended use. |
| Environmental values | The site has limited environmental values and habitat linkages due to surrounding major roads and residential development. Revegetation through the open space portion of the site is conditioned to provide for the |



| | establishment of habitat connectivity. Movement |
|-----------------------|---|
| | opportunities external to the site are facilitated via existing culverts. |
| Stormwater management | Stormwater discharge from the development will be treated on-site and managed by a detention basin in the southern portion of the site. |
| Landscaping | Landscaping will be provided along the Boundary and Cleveland Redland Bay Road frontages, which will provide visual relief from the State controlled roads. Landscaping internal to the site will provide suitable amenity for the residents. |
| Open space design | The development is provided with central communal open space areas through the site. The communal open space provides suitable areas for active and passive recreation and facilities, such as a gym and barbecue area for the use of residents. Each dwelling unit is also provided with sufficient private open space for their expected needs. |
| Earthworks | Cut and fill is required across the site to facilitate building areas for the residential component and stormwater management. Generally earthworks will tie into external land with batters, however where retaining is required landscaping is provided to provide visual relief and a suitable amenity outcome. |
| Sewer | Upgrades are required to the sewerage network to service the development and will be provided through an infrastructure agreement. |
| Road noise | Suitable acoustic attenuation is provided through the development design to reduce noise impacts on future residents to an acceptable level. Further requirement for building attenuation will be assessed by a private certifier at the building approval stage. |

| Matters Raised in Submissions | | | |
|-------------------------------|---|--|--|
| Matter Raised | Description of how matters were dealt with in reaching the decision | | |
| Zone/precinct changes | The proposal deviates from the original zoning intent under the Redlands Planning Scheme and the South-East Thornlands structure | | |

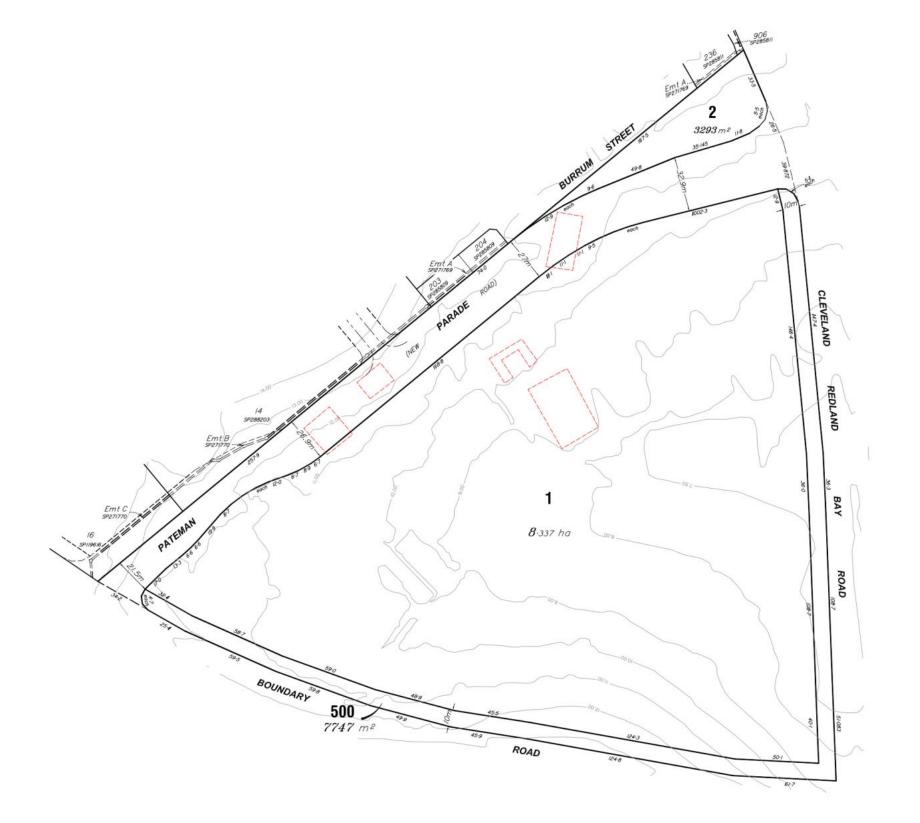


| inconsistent with SETSPO. | plan overlay in terms of the location of the uses. This has been considered acceptable as an equivalent outcome for the structure plan layout has been achieved through a revised layout. |
|--|---|
| Interurban break not provided. | While the interurban break envisaged by the structure plan is not provided, the development allows for an increase in amenity for future residents of the site and improved visual and physical break between the site and community of Victoria Point. |
| Filling of the flood prone area inconsistent with SETSPO and will impact on waterways. | The earthworks required to facilitate the development is noted to deviate from the existing landform. It is considered that this allows for an improved overall development outcome with residential development set back from the Boundary/Cleveland Redland Bay Road intersection. Stormwater discharge from the development is to be treated on-site and managed by a detention basin the southern portion of the site. Impacts on receiving waters are considered to be minimised. |
| Need for a local park in the area. | A local park has been provided as part of a separate development approval on Harrington Boulevard in lieu of the subject site. |
| Reduction in open space and associated impacts on habitat corridor functionality and greenspace network/recreation space. | The proposal deviates from the original zoning intent under the Redlands Planning Scheme and the South-East Thornlands structure plan overlay in terms of the location of the uses. This has been considered acceptable as an equivalent outcome for the structure plan layout has been achieved through a revised layout. |
| Built design out of character with surrounding area. | The built form of the development is consistent with the character of the surrounding area. |
| Density does not respond to capacity of roads, schools, health care etc. | The proposed development results in a density as envisaged by the zoning of the site. |
| Traffic impacts on surrounding roads network. | To provide connectivity as identified in the South-east Thornlands structure plan overly, the development is conditioned to complete the trunk collector (Pateman Parade) and the four-way intersection at Beveridge/Cleveland Redlands Bay Roads. |
| Insufficient infrastructure. | The development provides suitable infrastructure where not existing through conditions and an infrastructure agreement. |



| Stormwater management. | Stormwater discharge from the development is to be treated on-site and managed by a detention basin the southern portion of the site. |
|---|---|
| Pateman Parade not fauna friendly. | Pateman Parade is not intended as a fauna movement corridor. Fauna movement is facilitated through the site in the open space area. |
| Koala offsets have not been considered. | The site is within a koala broad hectare area. Accordingly, offsets were not applicable. Revegetation of the open space area however is required to support habitat values. |
| Dedication of open space. | A local park has been provided as part of a separate development approval on Harrington Boulevard in lieu of the subject site. The open space on the site is to remain in private ownership, however will be traversable by pedestrians. |
| The development conflicts with desired environmental outcome (DEO) 1. | The development provides stormwater management devices suitable for the requirements of the development. The development is conditioned to provide revegetation of the open space area to support habitat values. |
| Stormwater infrastructure should not be located within the greenspace network. | The South-East Thornlands structure plan intends to locate stormwater management infrastructure within the open space network at the subject site. |





| Legend | | | | | |
|--------|----------|--------------------|----|----|---------|
| | Existing | Building/Structure | to | be | removed |

Notes Any er is res cted to the purpose

reement or implied agreement betw ing group and the instructing party.

subject to local authority approval ineering requirements, areas and o mate only and are subject to survey, i is not to be used for engineering de

Contours and buildings/structure locations from others and are approximate only.

Indicative road pavement design, subject to biopods and engineering review.

This note is an integral part of this plan. This plan may not be reproduced without this notation being included.

Table of Development

| Gross area of subject land | |
|------------------------------|----|
| (Including drainage reserve) | |
| Area of new road 1.28 I | ha |
| Length of new road 485 | m |
| Net area of subject land | ha |
| Number of proposed lots | 3 |
| Number of Existing lots | 3 |

Final intended use of new lots: Proposed Lots 1 & 2 are for residential use. Proposed Lot 500 is for landscape purposes.





Item 14.1- Attachment 8 Page 485



Infrastructure Agreement

Philip Usher Constructions Pty Ltd ABN 38 011 008 101 (Applicant)

> Redland City Council ABN 86 058 929 428 (Council)

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Dated:

Parties:

Philip Usher Constructions Pty Ltd ABN 38 011 008 101 of 32 Tradelink Road, Hillcrest QLD 4188 in the State of Queensland

(Applicant)

Redland City Council ABN 86 058 929 428 of Corner Bloomfield & Middle Streets, Cleveland in the State of Queensland

(Council)

Background:

- A. The Applicant lodged a Development Application over the Land.
- B. Council approved the Development Application, and issued the Development Approval.
- C. The Applicant will provide a Trunk Infrastructure Contribution as part of its development of the Land.
- D. The parties have voluntarily agreed to enter into an infrastructure agreement, and have negotiated the terms of this agreement in good faith.
- E. This Document sets out the terms of the agreement reached between the parties.

The parties agree:

- 1. DEFINITIONS AND INTERPRETATION
- 1.1 Definitions

In this agreement, unless inconsistent with the context or subject matter:

- Act means the Planning Act 2016.
- (2) Adopted Resolution means Redland City Council Adopted Infrastructure Charges Resolution (No.3.1) 2020, which took effect on and from 28 February 2020, as amended from time to time.
- (3) Applicant means the person or entity described in Item 4 of Schedule 1 and includes:
 - successors and permitted assigns in the case of a corporation, association or other body whether incorporated or not; and
 - (b) executors, administrators and permitted assigns in the case of a natural person.
- (4) Applicant's Agents includes the Applicant's directors, shareholders, employees, officers, agents, servants, employees, contractors and subcontractors.
- (5) Applicant's Obligations means all obligations of the Applicant pursuant to and identified in this Document, including the Maintenance Obligations.
- (6) Approved Plans means the plans referred to in the Development Approval.
- (7) Authorised Person means an officer of the Council authorised pursuant to section 202 of the Local Government Act 2009 to exercise the powers of the Council under the Local Planning Instrument, Adopted Resolution or this Document.

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- (8) Authorised Representative means, in relation to the Applicant:
 - an employee of the Applicant whose title contains either of the words director or manager; or
 - (c) a person performing the function of any of them; or
 - (d) a solicitor acting on behalf of the Applicant; or
 - (e) a person appointed by the Applicant to act as an Authorised Representative for the purposes of this Document and notified to the others.
- (9) Automatic Increase has the meaning given to it by the Adopted Resolution.
- (10) Charges Notice means an infrastructure charges notice as defined by the Act and includes a notice referred to in section 301(1) of the Act.
- (11) Chief Executive Officer means Council's Chief Executive Officer at a relevant time, or any person the Chief Executive Officer authorises in writing, by way of delegation pursuant to the Local Government Act 2009, to exercise his or her responsibilities for a purpose relating to this Document.
- (12) Clause means a numbered clause, sub-clause or paragraph of this Document, whether referred to individually or collectively.
- (13) **Commencement Date** means the date stated in Item 1 of Schedule 1 and, if item 1 is blank, means the date the last party signs this Document.
- (14) Conditions of Approval means the conditions pursuant to which the Development Application was approved.
- (15) **Council** means the Redland City Council ABN 86 058 929 428 and includes its predecessors, successors, transferees and assigns.
- (16) Council's Agents includes Council's elected members, officers, servants, employees, contractors and subcontractors, and agents and servants of the Council's contractors and subcontractors.
- (17) Current Market Value has the meaning given to it by the Adopted Resolution.
- (18) Default means the failure of the Applicant to comply with any of the Applicant's Obligations.
- (19) Defect Notice means a notice issued pursuant to Clause 9.6
- (20) **Development Application** means the development application described in Item 6 of Schedule 1.
- (21) **Development Approval** means the development approval given by the Council for the Development Application, as described in Item 6 of Schedule 1.
- (22) Document means this agreement, any amendment to this Document (which has been made in compliance with this Document), and any attachments, schedules, plans, maps, tables, drawings, and documentation identified and / or referenced within this Document.
- (23) Endorsement means, when used in reference to a Plan of Subdivision, the notation of the Council's approval on the Plan of Subdivision following the process required by section 69 and Schedule 18 of the Planning Regulation 2017.
- (24) Establishment Cost has the meaning given to it by the Act.

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- (25) Event of Default means an event identified in Clause 13.1 of this Document.
- (26) Force Majeure means an event:
 - being an act of God, act of public enemy, war, international blockade, public riot, lighting, flood, earthquake, fire, storm, or other event whether of a kind herein specified or otherwise; or
 - (b) which is not within the control of the Party claiming Force Majeure; or
 - (c) which could not have been prevented by the exercise by a person of a standard of foresight, care and diligence consistent with that of a prudent and competent person under the circumstances.
- (27) Insolvency Event means the following:
 - (a) if the Applicant or a person comprising the Applicant is a company that company becoming an externally-administered body corporate (as defined by the Corporations Act 2001); or
 - (b) if the Applicant or a person comprising the Applicant is an individual that individual becoming an insolvent under administration.
- (28) Land means the land described in Item 5 of Schedule 1 and includes:
 - (a) any estate or interest in, on, over or under the land; and
 - (b) the airspace above the surface of the land and any estate or interest in the land; and
 - (c) the subsoil of the land and any estate or interest in the subsoil; and
 - (d) any part or parts of the land; and
 - (e) any estate or interest created in respect of any of the above matters.
- (29) Land Dedication means the Land identified in Item 12of Schedule 1, forming part of the Trunk Infrastructure Contribution.
- (30) Levied Charge has the meaning given to it by the Act.
- (31) Local Government Infrastructure Plan has the meaning given to it by the Adopted Resolution.
- (32) Local Law has the meaning given in the Local Government Act 2009, and includes all local law policies made under a Local Law and all approvals granted under a Local Law.
- (33) Maintain includes the repair, maintenance, correction, re-establishment or replacement of the Trunk Infrastructure so it is fit for purpose and free from defects, and includes any Rectification Works as may be required.
- (34) Maintenance Bond means a security provided to Council, in the amount described in Item 14 of Schedule 1, including any part of the Maintenance Bond which has been subsequently reduced in accordance with the terms of this Document, to secure the Applicant's compliance with the Maintenance Obligations.
- (35) Maintenance Obligations are the obligations of the Applicant Identified in Clause 9.4 of this Document.
- (36) Maintenance Period means the timeframe specified in Item 15 of Schedule 1, calculated from the date the Trunk Infrastructure is accepted by Council 'on

Redland City Council | Infrastructure Agreement DJS_141863_2024919 maintenance' pursuant to Clause 9.3, and includes any extension of time pursuant to Clause 10.1. The Maintenance Period concludes on the date Council issues an Offmaintenance Certificate pursuant to Clause 11.2(6).

- (37) Notice means any certificate, demand or notice to be made, given or served by a Party under this Document.
- (38) Offset has the meaning given to it by the Adopted Resolution.
- (39) Off-maintenance means the formal acceptance of the Trunk Infrastructure by Council of the transfer of the maintenance responsibility for the Trunk Infrastructure from the Applicant to Council, following a satisfactory Maintenance Period and Council issuing an Off-maintenance Certificate.
- (40) Off-maintenance Certificate means the certificate to be issued by Council under Clause 11.2(6).
- (41) **On-maintenance Certificate** means the certificate issued by Council under Clause 9.2(1).
- (42) Owner means the registered owner of the Land and includes its predecessors, successors, transferees and assignees.
- (43) Parties / Party means the parties / a party to this Document.
- (44) Plan of Subdivision has the meaning given to it in the Planning Regulation 2017.
- (45) Rectification Works includes the following including physical works, site works, design work and professional services relating to any of the following:
 - any works or action necessary to remedy a failure by the Applicant to comply with the Maintenance Obligations; and
 - (b) any works or action necessary to remedy a failure by the Applicant to ensure that the Maintenance Obligations are complied with; and
 - any alteration or rectification of any works done in or in respect of or in purported compliance with the Maintenance Obligations; and
 - any other works or action that Council considers appropriate to mitigate the effects of any works done in respect of or in purported compliance with the Maintenance Obligations; and
 - (e) any works or action carried out or commissioned by Council in order to fulfil the Applicant's Obligations under this Document, including after an Insolvency Event; and
 - (f) testing of any works described above; and
 - (g) the compilation of 'as constructed' information with respect to any works described above.
- (46) Refund has the meaning given to it by the Adopted Resolution.
- (47) RPEQ means a Registered Professional Engineer of Queensland.
- (48) Security means a security provided to the Council by the Applicant to ensure the performance of the Applicant's Obligations, other than the Maintenance Obligations, identified in Item 13 of Schedule 1.
- (49) Schedule means a schedule to this Document.

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- (50) Schedule of Plans means the plans which identify the Trunk Infrastructure, as referenced in Item 8 of Schedule 1.
- (51) Shortfall means:
 - (a) Any difference between the Security and any cost, expense, loss or damage incurred by Council, or which is likely to be incurred by Council, as a consequence of exercising its rights to rectify a Default or Event of Default; or
 - (b) Any difference between the Maintenance Bond and any cost, expense, loss or damage incurred by Council, or which is likely to be incurred by Council, as a consequence of exercising its rights to rectify a Default or Event of Default during the Maintenance Period.
- (52) Trunk Infrastructure means Trunk Infrastructure to be provided by the Applicant pursuant to this Document, as identified in Item 11 of Schedule 1 and the Schedule of Plans.
- (53) **Trunk Infrastructure Contribution** means the Applicant's contribution of the Trunk Infrastructure identified in Item 11 of Schedule 1, and the Land Dedication identified in Item 12 of Schedule 1.
- (54) Works means all work reasonably necessary or reasonably required to construct or provide the Trunk Infrastructure.

1.2 Interpretation

In this Document, unless inconsistent with the context or subject matter:

- a reference to a person includes any other legal entity; and
- (2) a reference to a legal entity includes a person; and
- (3) words importing the singular number include the plural number; and
- (4) words importing the plural number include the singular number; and
- (5) the masculine gender must be read as also importing the feminine or neuter gender; and
- a reference to a party includes the party's heirs, executors, successors and permitted assigns; and
- (7) headings are for reference purposes only and must not be used in interpretation; and
- (8) where any word or phrase is given a defined meaning any other part of speech or other grammatical form concerning the word or phrase has a corresponding meaning; and
- (9) a reference to a statute includes all regulations and subordinate legislation and consolidations, amendments, re-enactments or replacements of any of them; and
- (10) references to writing include any mode of representing or reproducing words in tangible and permanently visible form, and includes e-mail and fax; and
- (11) a reference to a monetary amount is a reference to an Australian currency amount; and
- (12) an obligation of two or more parties binds them jointly and each of them severally; and
- (13) an obligation incurred in favour of two or more parties is enforceable by them severally; and
- (14) references to time are to local time in Queensland; and

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- (15) where time is to be reckoned from a day or event, the day or the day of the event must be excluded; and
- (16) a reference to a business day means any day on which trading banks are open for business in Queensland; and
- (17) if any time period specified in this agreement expires on a day which is not a business day, the period shall expire at the end of the next business day; and
- (18) a reference to a month means a calendar month; and
- (19) all schedules and attachments to this document form part of this document.

1.3 Other expressions

If a term is not defined in this document it shall, unless the context otherwise requires, have the meaning given to it by the following, in the following sequence:

- (1) the Adopted Resolution; or
- (2) the Act; or
- (3) an applicable Local Planning Instrument; or
- (4) the Macquarie Dictionary; or
- (5) the common understanding of the term or expression in the absence of any other applicable definition.

2. INFRASTRUCTURE AGREEMENT

2.1 Application of the Act

This Document is an infrastructure agreement pursuant to the Act.

2.2 Commencement

This Document commences on the Commencement Date.

- 2.3 Agreement to bind successors in title
 - (1) The Applicant's Obligations under this Document attach to the Land and are binding on the Owner of the Land and any successors in title of the Land in accordance with section 155 of the Act.
 - (2) For the purposes of section 152(1)(a) of the Act, a change in the ownership of the Land or part of the Land will not affect the Parties' obligations under this Document, other than as expressly stated in this Document.

2.4 Owner's consent

The Applicant acknowledges that:

- it is the Owner of the Land; and
- (2) it consents to the Applicant's Obligations being attached to the Land and binding successors-in-title; and
- (3) execution of this Document constitutes endorsement of the Owner's consent to the agreement.

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2.5 Extinguishment or modification of development rights

For the purposes of section 152(1)(b) of the Act, if:

- the fulfilment of obligations under this Document depends on development entitlements that may be affected by a change to a local planning instrument; and
- a change to a local planning instrument generates a substantial adverse impact on those development entitlements;

the Parties must negotiate in good faith to determine the extent to which:

- (3) money paid pursuant to this Document will be repaid; or
- (4) money expended pursuant to this Document will be reimbursed; or
- (5) the extent to which those obligations will be changed or cancelled (where the development entitlements are changed without the consent of the party bound by the obligations).

The parties must prepare and execute promptly, after they reach agreement, a deed varying this Document, to give effect to what they have agreed.

2.6 Agreement prevails to extent of inconsistency

To the extent that this Document is inconsistent with any development approval for the Land, any condition of a development approval, or a Charges Notice, this Document prevails to the extent of inconsistency.

2.7 Delivery as a Deed

Subject to express provisions in this Document to the contrary, each party by signing or executing this Document is deemed to unconditionally sign, seal and deliver this Document as a deed, with the intention of being immediately legally bound by this Document.

2.8 Obligation to negotiate in good faith

For the purposes of section 151 of the Act, the Parties agree and warrant that:

- (1) written agreement was given to entering into negotiation for an infrastructure agreement; and
- (2) in negotiating this Document, the Parties acted in good faith.

2.9 Provision of Security and Maintenance Bond

The Security and Maintenance Bond must be given to Council by the Applicant in the following form:

- (1) money; or
- (2) an unconditional, non-lapsing and irrevocable banker's undertaking in favour of the Council which is:
 - (a) in a form and on terms approved by the Chief Executive Officer in its absolute discretion; and
 - (b) given by a financial institution approved by the Chief Executive Officer in his or her absolute discretion; and
 - (c) in the name of the Applicant or Owner.

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3. PAYMENT OF SECURITY

3.1 Applicant to provide Security to Council

- (1) The Applicant must provide the Security to the Council as security for the due performance and fulfilment of the Applicant's Obligations, other than the Maintenance Obligations.
- (2) The Security must be given to Council by the Applicant no later than 7 business days from the Commencement Date.

3.2 Council's right of recourse to Security

In the Event of Default, other than an Event of Default which occurs during the Maintenance Period, the Council may at its sole discretion and without Notice to the Applicant:

- (1) draw upon and have recourse to the Security; and
- (2) may apply the Security in accordance with this Document.

3.3 Request for Release of Security

The Applicant may apply to Council in writing for the return of the Security:

- upon Council being satisfied that the Applicant has duly fulfilled the Applicant's Obligations, excluding the Maintenance Obligations; and
- (2) upon the commencement of the Maintenance Period for the Trunk Infrastructure, provided that the Maintenance Bond has been paid to Council and cleared by the relevant financial institution; and
- (3) upon an assignment having been undertaken by the Applicant in compliance with Clause 18.1.

3.4 Release of Security

- (1) Subject to Clause 3.4(2), the Council shall release and return to the Applicant the Security, or any remaining balance of the Security as the case may be, within ten (10) Business Days of the Council, in its absolute discretion, being satisfied that the Applicant has completed and fulfilled the Applicant's Obligations, other than the Maintenance Obligations.
- (2) The Security will not be released by the Council until:
 - (a) the Council has accepted the Trunk Infrastructure "on maintenance"; and
 - (b) the Applicant has paid a satisfactory Maintenance Bond to the Council, which has been cleared by any relevant financial institution (if applicable); and
 - (c) all conditions of the Development Approval have been fully complied with; and
 - (d) the Applicant has fulfilled the Applicant's Obligations, other than the Maintenance Obligations, to Council's reasonable satisfaction.

3.5 Security insufficient

Without limiting Council's rights, if the Security is insufficient to rectify an Event of Default, the Council may, at its election and in its sole discretion:

 Rectify the Event of Default, so far as the Security will, in the opinion of the Council, reasonably allow; or

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- (2) Rectify the Event of Default and recover any Shortfall from the Applicant as a liquidated debt; or
- (3) Take whatever steps reasonably required, including but not limited to, legal proceedings in any Court of competent jurisdiction, to compel the Applicant to rectify the Event of Default.

3.6 Interest

If the Security consists of money, then any interest earned on the Security shall belong to the Council.

4. LEVIED CHARGE

- 4.1 Payment of Levied Charge
 - (1) The Applicant must pay the Levied Charge identified in Item 9 of Schedule 1, as adjusted by Council in accordance with Clause 4.1(2).
 - (2) If the Trunk Infrastructure Contribution services or is planned to service premises other than the land the subject of the Development Approval, the Levied Charge payable by the Applicant may, at Council's discretion:
 - (a) be increased by an Automatic Increase pursuant to Clause 4.3; and
 - (b) be reduced by the value of any Offset pursuant to Clause 4.4; and
 - (c) adjusted generally in accordance with the terms of this Document so as to increase or reduce the amount payable by the Applicant.

4.2 Calculation of Levied Charge

Any Levied Charge payable by the Applicant will be calculated by Council in accordance with the Part 3 of the Adopted Resolution.

4.3 Automatic Increase of Levied Charge

Unless expressly stated to the contrary in Schedule 3, the Levied Charge payable by the Applicant will be subject to an Automatic Increase, calculated by Council in accordance with clause 18 of Part 3 of the Adopted Resolution.

4.4 Reduction of Levied Charge by Offset

- (1) In return for providing a Trunk Infrastructure Contribution, the Levied Charge payable by the Applicant may be reduced by an Offset calculated by Council in accordance with Part 4 of the Adopted Resolution.
- (2) The value of the Offset will be determined by Council based on the Establishment Cost of the Trunk Infrastructure Contribution, unless otherwise adjusted in accordance with the Adopted Resolution.

4.5 Adjustment of Levied Charge by Automatic Increase or Offset

Council may adjust the Levied Charge identified in Item 9 of Schedule 1 as at the date of payment by the Applicant to reflect any Automatic Increase or Offset, provided that any Automatic Increase or Offset is calculated by Council in accordance with the Adopted Resolution.

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5. ESTABLISHMENT COST OF TRUNK INFRASTRUCTURE CONTRIBUTION

5.1 Calculation of Offset or Refund

Any Offset or Refund must be determined and calculated by Council in accordance with clauses 27 and 28 of the Adopted Resolution, based upon the Establishment Cost of the Trunk Infrastructure Contribution, or as otherwise determined by Council in accordance with this Document.

5.2 Determination of Establishment Cost

The Establishment Cost for a Trunk Infrastructure Contribution will be determined by Council in accordance with clause 20 of Part 4 of the Adopted Resolution.

5.3 Recalculation of Establishment Cost (other than land) using Market Cost

- (1) Prior to payment of the Levied Charge, and upon receipt of a written request from the Applicant under clause 25(1) of the Adopted Resolution, Council may at its sole discretion, re-calculate the Establishment Cost for a Trunk Infrastructure Contribution for work by using the Market Cost.
- (2) The Market Cost will be calculated and determined by Council in accordance with clause 25 of the Adopted Resolution.
- (3) Any decision made by Council with respect to the calculation of the Market Cost will be communicated to the Applicant by written notice in accordance with clause 25(3)(f) of the Adopted Resolution.
- (4) Any dispute as to the determination of the Market Cost must follow the process set out in clause 25(6) and clause 25(7) of the Adopted Resolution.

5.4 Notice for information for calculation of Market Cost

- (1) Within 15 business days of the date of receipt of a notice from the Applicant under clause 25(3)(b) of the Adopted Resolution, Council may give notice to the Applicant pursuant to clause 25(3)(c) of the Adopted Resolution, seeking further information to enable Council to determine the Market Cost of the Trunk Infrastructure Contribution.
- (2) The Applicant must give Council the information required by the notice issued under Clause 5.4(1) within 30 business days of the date of that notice, or such other timeframe stipulated within that notice.

5.5 Adjustment of Establishment Cost (other than land)

- (1) After completion of the Trunk Infrastructure, and prior to payment of the Levied Charge, the Applicant may lodge a written request to Council to adjust the Establishment Cost in accordance with clause 25(5)(b) of the Adopted Resolution.
- (2) The Establishment Cost may only be adjusted if the circumstances identified in clause 25(5)(a) of the Adopted Resolution apply.
- (3) Any adjustment to the Establishment Cost must be calculated by Council in accordance with clause 25(5) of the Adopted Resolution.
- (4) Within 15 business days of the date of receipt of a notice from the Applicant under clause 25(5)(b) of the Adopted Resolution, Council may give notice to the Applicant pursuant to clause 25(5)(c) of the Adopted Resolution seeking further information to enable Council to determine the value of an adjustment to the Establishment Cost.
- (5) The Applicant must give Council the information required by the notice issued under Clause 5.5(4) within 30 business days of the date of that notice, or such other timeframe stipulated within that notice.

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- (6) Any decision made by Council with respect to any adjustment of the Establishment Cost will be communicated to the Applicant by written notice in accordance with clause 25(5)(f) of the Adopted Resolution.
- (7) Any dispute as to the recalculation of the Establishment Cost must follow the process set out in clause 25(6) and clause 25(7) of the Adopted Resolution.

5.6 Recalculation of Establishment Cost for land using Current Market Value

- (1) Prior to payment of the Levied Charge, the Applicant may lodge a written request to Council, in accordance with clause 26(1) of the Adopted Resolution, to adjust the Establishment Cost for any land forming part of the Trunk Infrastructure Contribution using the Current Market Value of the land.
- (2) Any request for recalculation of the Establishment Cost that is made in compliance with clause 26(2) of the Adopted Resolution:
 - (a) must be considered by Council in accordance with clause 26 of the Adopted Resolution; and
 - (b) may be referred to a valuer under clause 26(12) of the Adopted Resolution at Council's sole discretion and having regard to the matters contained in clause 26(2) and clause 26(11) of the Adopted Resolution.
- (3) If Council:
 - accepts the valuation provided by the Applicant, Council's decision will be communicated to the Applicant by written notice in accordance with clause 26(6) of the Adopted Resolution; or
 - (b) rejects the valuation provided by the Applicant, Council must refer the matter to an independent certified practising valuer in accordance with clause 26(4) of the Adopted Resolution, and give written notice to the Applicant in accordance with clause 26(15) of the Adopted Resolution.
- (4) In addition to the above, the parameters referred to in Chapter 6, Part 1, section 2.2 of the Minister's Guidelines and Rules under the Act apply.

6. TRUNK INFRASTRUCTURE

6.1 Construction of Trunk Infrastructure

The Applicant must provide the Trunk Infrastructure at the Applicant's cost and within the stipulated timeframe, generally in accordance with the Schedule of Plans.

6.2 Submission of designs and specifications

The Applicant must:

- (1) Prepare at its own cost all designs and specifications for the Trunk Infrastructure, which are certified by a RPEQ (where applicable), including but not limited to, the specification of all earthworks, and the final design and specification for the Trunk Infrastructure; and
- (2) Submit the design and specifications to Council and, where necessary to any other statutory authority, for approval.

6.3 Approval for Trunk Infrastructure

(1) Prior to commencement of construction of the Trunk Infrastructure, the Applicant must obtain all necessary development approvals, approvals or permits required by law or a Local Planning Instrument for the Trunk Infrastructure and any Works.

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(2) The Trunk Infrastructure must be constructed in accordance with any development approval, approval or permit, and any condition of any development approval, approval or permit, which has been issued for the Trunk Infrastructure.

6.4 Construction of Trunk Infrastructure

Without limiting the Applicant's obligations under this Document or otherwise, the Applicant warrants that it:

- (1) will provide the Trunk Infrastructure and carry out the Works:
 - (a) in a proper and workmanlike manner in accordance with:
 - (i) applicable legislative requirements; and
 - (ii) applicable codes of practice; and
 - (iii) applicable Australian standards; and
 - the requirements of the applicable Local Planning Instruments, including Planning Scheme Policy 9 – Infrastructure Works – Chapter 5 – Road and Path Design; and
 - (v) the Approved Plans and specifications; and
 - (b) with due diligence and without delay; and
 - (c) using its reasonable endeavours to provide the Trunk Infrastructure and complete the Works within the timeframe stipulated by this Document, or where there is no stipulated timeframe, within a reasonable time from the Commencement Date; and
 - (d) with the standard of skill, care and diligence that would be expected of a provider of work and services of a nature similar to the Works; and
- (2) will ensure that all registrations, permits, licences, qualifications and other requirements of its trade are in full force and effect at all relevant times.

6.5 Final Specification of Trunk Infrastructure and Works

- The Trunk Infrastructure must be provided and the Works done to the satisfaction of the Council, acting reasonably.
- (2) In the interpretation and application of this Clause:
 - (a) it is recognised that some provisions of this Document provide a particular specification for the Works and, in some cases show diagrammatically and in an indicative way the location of the Works on plans or diagrams; and
 - (b) a specification or location has been determined on the basis of present knowledge and expectation as to circumstances which will prevail at the time the Works are to be carried out; and
 - (c) the circumstances actually prevailing at the relevant time may result in it being necessary or appropriate to adopt a different specification or vary the location for the final design or performance of the Works.
- (3) The inclusion of a particular specification in this Document or the depiction of the location of the Trunk Infrastructure or Works on a plan within the Document does not prevent the Applicant from seeking approval for an altered specification or location, and does not prevent the Council from giving approval where an alteration is necessary or appropriate having regard to the circumstances prevailing at the relevant time.

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6.6 Completion of Trunk Infrastructure

- (1) The Applicant must request, in writing, Council to inspect and approve the Trunk Infrastructure when the Applicant considers the Trunk Infrastructure to be complete.
- (2) The Trunk Infrastructure must be complete when the Council accepts, in writing, the Trunk Infrastructure 'On-Maintenance'.
- (3) On the date of the written notice confirming that Council accepts the Infrastructure On Maintenance, the Maintenance Period for the Trunk Infrastructure Contribution will commence.

7. LAND DEDICATION AND EASEMENTS

7.1 Applicant to dedicate land to Council

The Applicant must dedicate the Land Dedication to Council, at the Applicant's cost and within the stipulated timeframe.

7.2 Land forming part of Trunk Infrastructure Contribution

Any land dedicated to Council by the Applicant pursuant to this Document may be subject to an Offset or Refund calculated by Council in accordance with the Adopted Resolution.

7.3 Transfer of land

Where land is required to be dedicated to Council pursuant to this Document, the Applicant must, at the Applicant's expense:

- Obtain all necessary development approvals, approvals or permits required by law or a Local Planning Instrument for the dedication; and
- (2) Arrange for the transfer of the Land Dedication to Council and
- (3) Deliver to Council the stamped transfer instrument for the Land Dedication within the stipulated timeframe.

7.4 Cost of transfer

The Applicant must bear all costs of and incidental to arranging and effecting the transfer, including:

- (1) The price, if any, payable to the transferor in return for the transfer and
- (2) The duty payable on the transaction; and
- (3) The registration fees; and
- (4) All expenses that Council reasonably incurs.

7.5 Registration of transfer

The transfer instrument must be capable of immediate registration.

- 7.6 Requestion
 - If a requisition issues from the Titles Office in relation to the transfer instrument, the Proponent must, following a written request from Council:
 - promptly provide Council with all necessary assistance to answer the requisition; and

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(b) pay any requisition fee payable to the Titles Office.

7.7 Grant of easement over Trunk Infrastructure

Where an easement is required by Council for the Trunk Infrastructure located in private land, the Applicant must grant an easement to Council:

- (1) At no cost to Council; and
- (2) On terms, and in a location, satisfactory to Council; and
- (3) With sufficient dimensions to accommodate the Trunk Infrastructure; and
- (4) In accordance with any condition of the Development Approval or applicable Local Planning Instrument; and
- (5) Prior to Council's Endorsement of a Plan of Subdivision for the Land.

8. OFFSET OR REFUND FOR TRUNK INFRASTRUCTURE CONTRIBUTION

8.1 Applicant to give notice to Council seeking Offset or Refund

If the Applicant is entitled to receive an Offset or Refund for the Trunk Infrastructure Contribution pursuant to this Document, the Applicant must, upon lawful completion and fulfilment of the Trunk Infrastructure Contribution:

- give Council written notice in the prescribed form and in accordance with clause 29(1)(a) and clause 29(1)(b) of the Adopted Resolution; and
- (2) pay the prescribed fee in accordance with clause 29(1)(c) of the Adopted Resolution.

8.2 Calculation of Offset or Refund

- Council will calculate any applicable Offset in accordance with clause 28(1) of the Adopted Resolution.
- (2) Council will calculate any applicable Refund in accordance with clause 28(2) of the Adopted Resolution.

8.3 Council to provide Offset or Refund

Subject to Clause 8.4, upon Council being satisfied that the Trunk Infrastructure Contribution:

- (1) has been lawfully completed by the Applicant; and
- has been provided in accordance with all relevant approvals;

Council will provide the Offset or Refund in accordance with the timeframe identified in Schedule 4, or such extended timeframe as is necessary for Council to be satisfied of the matters identified in clause 29(1)(a) and clause 29(1)(b) of the Adopted Resolution.

8.4 Timing of Offset or Refund

Notwithstanding Clause 8.3, the timing of any Offset or Refund must achieve the Council's policy position identified in clause 29(5) of the Adopted Resolution.

8.5 Conversions

The Applicant is not to take any action under section 139 of the Act to convert any sewer and wastewater infrastructure, beyond that already provided for as part of this Infrastructure Agreement, associated with this development approval.

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9. COMMENCEMENT OF MAINTENANCE PERIOD FOR TRUNK INFRASTRUCTURE

9.1 Warranty by Applicant

In seeking Council's acceptance of the Trunk Infrastructure On-Maintenance, the Applicant warrants to Council that the Trunk Infrastructure is complete, properly functioning, fit for purpose and free from defects.

9.2 Issue of On-Maintenance Certificate

- (1) Upon Council being satisfied that the Trunk Infrastructure has been completed in accordance with the terms of this Document, it will issue an On-maintenance Certificate for the Trunk Infrastructure.
- (2) The On-maintenance Certificate may be issued subject to any conditions Council deems appropriate.

9.3 Commencement

The Maintenance Period for the Trunk Infrastructure commences on the date of issue of the On-maintenance Certificate.

9.4 Applicant's Maintenance Obligations

For the duration of the Maintenance Period, the Applicant must, at its own cost:

- Comply with any conditions identified in the On-maintenance Certificate, within any stipulated timeframe.
- (2) Rectify, remedy or correct any Event of Default.
- (3) Rectify, remedy or correct any defects in the Trunk Infrastructure:
 - (a) identified in a Defect Notice issued during the Maintenance Period, in accordance with the Defect Notice and any timeframes stated within the Defect Notice; or
 - (b) identified in any written report prepared in accordance with Clause 9.4(5); or
 - (c) otherwise brought to the attention of the Applicant.
- (4) Maintain the Trunk Infrastructure (or replace or reconstruct it to the extent that they cannot be repaired) so that the Trunk Infrastructure functions properly, is fit for its intended purpose and free from defects.
- (5) Ensure the Trunk Infrastructure is inspected by a RPEQ for defects at least once every three (3) months, and a written inspection report is submitted to Council and the Applicant no later than 14 days after each inspection has occurred.
- (6) Ensure any assignment complies with Clause 18.1.

9.5 Timeframe for rectification of defects

In the absence of any expressed or stipulated timeframe, the Applicant must rectify any defect in the Trunk Infrastructure no later than thirty (30) Business Days from the date the Applicant becomes aware of, or should reasonably have been aware of, the defect.

9.6 Defect Notice

 If Council identifies or becomes aware of a defect in the Trunk Infrastructure during the Maintenance Period, it may give the Applicant a Defect Notice.

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- (2) The Applicant must comply with the Defect Notice.
- (3) A Defect Notice must allow the Applicant a reasonable period within which to rectify a defect.
- (4) For avoidance of doubt:-
 - Council may give more than one Defect Notice during the Maintenance Period; and
 - (b) A Defect Notice may specify different periods of time for the rectification of different defects.
- (5) The Applicant must request Council to inspect and approve any Rectification Works when the Applicant considers that it has complied with a Defect Notice.
- (6) Council must respond to the request within 14 days from the date of the request.
- (7) Within 14 days after it inspects the Rectification Works, Council must give the Applicant a notice, signed by the Chief Executive Officer:-
 - (a) confirming that it accepts the Rectification Works as complete; or
 - (b) refusing to accept the Rectification Works as complete, and stating with reasonable particularity the reasons for its refusal.
- (8) For the avoidance of doubt:
 - there is no limit to the number of Defect Notices that may be given by Council during the Maintenance Period; and
 - (b) Clause 22.14 does not apply to this Clause.

10. EXTENSION TO MAINTENANCE PERIOD FOR TRUNK INFRASTRUCTURE

- 10.1 Council may extend Maintenance Period for Trunk Infrastructure
 - (1) Council may, at its sole discretion, extend the Maintenance Period for a reasonable period of time if there is an:
 - (a) Event of Default; or
 - (b) event of Force Majeure.
 - (2) If Council determines, acting reasonably and having regard to the time which elapsed before the defect was satisfactorily rectified, that there is to be an extension to the Maintenance Period, it must give written Notice to the Applicant:
 - (a) stating that fact; and
 - (b) stating the duration of the extension to the Maintenance Period; or
 - (c) stating the date upon which the Maintenance Period may conclude, subject to the issuing of an Off-maintenance Certificate.
 - (3) For avoidance of doubt the Applicant's Maintenance Obligations continue to apply during any extension to the Maintenance Period.

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11. ACCEPTANCE OF TRUNK INFRASTRUCTURE OFF-MAINTENANCE

11.1 Applicant to Request Inspection

- (1) Within fourteen (14) days after the date identified in Item 15 of Schedule 1, including any extensions of time pursuant to Clause 10.1, the Applicant must submit a written request to Council for an Off-maintenance inspection.
- (2) Prior to the Off-maintenance inspection, the Applicant must pay any applicable inspection fee to Council, as identified in Council's adopted fees and charges.

11.2 When Trunk Infrastructure accepted "Off-Maintenance"

The Trunk Infrastructure will be accepted by Council "Off-maintenance" when:

- (1) the Trunk Infrastructure is functioning properly and is fit for purpose; and
- (2) any Shortfall has been paid to Council (where applicable); and
- (3) all defects in the Trunk Infrastructure have been rectified; and
- Council is satisfied with the results of an "Off-maintenance" inspection and compliance check against the Conditions of Approval;
- (5) the Applicant's Maintenance Obligations have been satisfied; and
- (6) Council gives the Applicant an Off-maintenance Certificate, signed by the Chief Executive Officer.

11.3 Ownership of Trunk Infrastructure

Council will own the Trunk Infrastructure, and assume responsibility for its ongoing maintenance, from the date Council gives the Applicant an Off-maintenance Certificate for the Trunk Infrastructure.

12. MAINTENANCE BOND

12.1 Purpose of Maintenance Bond

The Applicant must pay the Maintenance Bond to Council as security for the due performance and fulfilment by the Applicant of its Maintenance Obligations during the Maintenance Period.

12.2 Amount of Maintenance Bond

The Maintenance Bond payable by the Applicant is the amount identified in Item 14of Schedule 1.

12.3 Application of Maintenance Bond

In the Event of Default during the Maintenance Period, the Council may, at its sole discretion and without Notice to the Applicant, draw upon the Maintenance Bond and use the Maintenance Bond in accordance with this Document.

12.4 Maintenance Bond insufficient

Without limiting Council's rights, if the Maintenance Bond is insufficient to rectify an Event of Default, the Council may, at its election and in its sole discretion:

(1) Rectify the Event of Default, so far as the Maintenance Bond will, in the opinion of the Council, reasonably allow; or

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- Rectify the Event of Default and recover any Shortfall from the Applicant as a liquidated (2)debt: or
- Take whatever steps reasonably required, including but not limited to, legal (3)proceedings in any Court of competent jurisdiction, to compel the Applicant to rectify the Event of Default.

12.5 Request for Release of Maintenance Bond

The Applicant may apply to Council in writing for the return of the Maintenance Bond:

- (1)At the conclusion of the timeframe identified in Item 15 of Schedule 1; or
- (2)Upon an assignment having been undertaken in compliance with Clause 18.1.

12.6 Release of Maintenance Bond

The Council must release and return to the Applicant the Maintenance Bond, or any remaining balance of the Maintenance Bond, within ten (10) Business Days of the date of the Offmaintenance Certificate provided that Council, in its absolute discretion, is satisfied that the Maintenance Obligations have been fulfilled by the Applicant.

12.7 Interest

If the Maintenance Bond consists of money, then any interest earned on the Maintenance Bond belongs to the Council.

13. DEFAULT BY APPLICANT

Event of Default 13.1

The Applicant Defaults under this Document if:

- The Applicant or Applicant's Agents fail to duly perform or fulfil all or any part of the (1)Applicant's Obligations within any stipulated timeframe, or where no timeframe has been provided, within a reasonable period of time from the Commencement Date; or
- (2)The Applicant or Applicant's Agents fail to duly perform or fulfil all or any part of the Maintenance Obligations; or
- The Applicant or Applicant's Agents fail to perform to a satisfactory standard, or fulfil all (3)or any part of the Rectification Works; or
- The Applicant or Applicant's Agents fail to comply with a Defect Notice, or any part of a (4)Defect Notice, within the timeframe stated in the Defect Notice, or if there is no stated timeframe, within the timeframe stated in Clause 9.5; or
- The Applicant or Applicant's Agents fails to rectify any Default within the applicable (5)timeframe under this Document, or to a satisfactory standard; or
- The Applicant fails to Maintain the Trunk Infrastructure during the Maintenance Period (6) to a satisfactory standard; or
- The Applicant has contravened a warranty given pursuant to this Document; or (7)
- The Applicant assigns its interests, liability, rights or obligations other than in (8)accordance with Clause 18.1; or
- (9)An Insolvency Event occurs

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13.2 Council's rights in the Event of Default

In an Event of Default by the Applicant, Council may, in its absolute discretion, take whatever steps are reasonably required to rectify the Event of Default, including:

- (1) drawing upon and using the Security, including converting into money any part of the Security that does not consist of money, for any Event of Default that does not occur during the Maintenance Period; and
- (2) drawing upon and using the Maintenance Bond, including converting into money any part of the Maintenance Bond that does not consist of money, for any Event of Default which occurs during the Maintenance Period;
- (3) recovering any Shortfall from the Applicant as a liquidated debt; and
- (4) extending the Maintenance Period in accordance with Clause 10.1; and
- (5) taking any reasonable steps required, at Council's discretion, to enforce the Applicant's Obligations or Maintenance Obligations; and
- (6) the commencement of legal proceedings in any Court of competent jurisdiction.

13.3 Council may give Notice at its absolute discretion

Without affecting or derogating from Council's rights under this Document, the Council may, in its absolute discretion, give the Applicant written Notice of any Default and provide the Applicant with the opportunity to remedy the Event of Default within a stipulated timeframe, before exercising its rights under this Document.

13.4 Application of money

In exercising its rights under this Document and at its sole discretion, Council may apply the Security, Maintenance Bond and any Shortfall to or toward any cost or expense incurred by Council to rectify the Event of Default, including all or any of the following:

- (1) carrying out the Maintenance Obligations or Rectification Works; and
- (2) Maintaining any Council assets; and
- (3) rectifying any breach of warranty; and
- (4) fulfilling the Applicant's Obligations; and
- (5) altering, replacing or amending any improperly completed or partly completed or poorly functioning Trunk Infrastructure; and
- (6) carrying out, whether inside or outside or partly inside or partly outside the perimeter of the Land, such other work (including any addition or extension to the Trunk Infrastructure) or activity as the Council reasonably considers necessary to:
 - (a) mitigate the effects of any failure to comply with the Maintenance Obligations; or
 - (b) make such uncompleted, improperly completed, inefficient or poorly functioning Trunk Infrastructure in the opinion of the Authorised Person more effective or useful; and
- (7) paying for, or reimbursing itself for, all costs and expenses directly incurred by Council as a consequence of an Event of Default, including, but not limited to, the following:
 - (a) the acquisition of any land or easements which may be required for the Trunk Infrastructure, including any compensation as may be payable to the landowner,

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and the costs of and incidental to the preparation of registrable instruments, duty and registration fees; and

- (b) any works, material or services reasonably necessary for the provision of the Trunk Infrastructure; and
- (c) the preparation of the detailed engineering design and specifications, and any ancillary documentation directly associated with the preparation of same; and
- (d) obtaining any / all necessary development approvals required in order to provide and construct the Trunk Infrastructure; and
- (e) the purchase and the delivery of materials required in order to facilitate the provision of the Trunk Infrastructure; and
- (f) financing the construction of the Trunk Infrastructure, including interest charges arising from the cost of finance; and
- (g) all work reasonably required to facilitate the construction of the Trunk Infrastructure; and
- indirect job costs incurred by Council, such as supervision and management costs, investigation costs and specialist consultant fees and charges; and
- (i) administration costs and overheads; and
- (j) any third party costs or expenses, including costs of experts or consultants; and
- (k) solicitor and barrister fees and costs, on a solicitor and own client basis; and
- (I) costs of enforcement of the Applicant's obligations; and
- (m) any loss or damage incurred by Council as a consequence of the Event of Default.

13.5 No waiver of effect on other rights and obligations

A failure or delay by Council to exercise any right, remedy, power or privilege under this Document will not operate as a waiver unless and until expressly communicated in writing by Council, under the hand of the Chief Executive Officer, to the Applicant.

13.6 Document does not affect rights

For the avoidance of doubt, this Document does not affect:

- Any other rights, powers or remedies available to Council, including enforcement powers under the Act or the Local Government Act 2009; and
- (2) Any obligations of the Applicant to comply with the Development Approvals, the Act, any Local Planning Instrument, Local Law or other applicable legislation.

14. COUNCIL'S RIGHTS OF ENTRY

14.1 Exercise of Council's rights

The Applicant grants a licence to Council and the Council's Agents to access the Land with any necessary personnel, vehicles, plant and equipment as may be required for the purpose of exercising Council's rights under this Document including (but not limited to) the following:

- (1) undertaking on-maintenance and off-maintenance inspections; and
- (2) inspecting, monitoring or testing the Trunk Infrastructure; and

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- (3) ascertaining whether the Applicant's Obligations, or are being, properly performed and fulfilled; and
- (4) ascertaining or identifying whether an Event of Default has occurred, or may occur; and
- (5) ascertaining whether any Event of Default has been remedied or rectified; and
- (6) determining the scope of work or likely costs required to remedy or rectify any Event of Default; and
- (7) exercising Council's rights to remedy or rectify any Event of Default; and
- (8) exercising the Council's rights under this Document generally.
- 14.2 Right of entry at law

Nothing in this Document in any way limits or restricts any other rights of entry to the Land which Council or Council's Agents may have at law, including but not limited to, the Act, the *Local Government Act 2009*, the *Environmental Protection Act 1994*, a Local Planning Instrument, and a Local Law.

15. INSURANCE TO BE EFFECTED BY APPLICANT

15.1 Insurances to be effected

The Applicant must effect and maintain, or cause to be effected and maintained:

- (1) Public and third party liability insurance:
 - (a) covering claims in respect of:
 - damage to any real or personal property including property owned by Council; and
 - (ii) the injury to, or death of, any person, and
 - (iii) caused by the carrying out of the Works: and
 - (b) for at least \$20 million; and
 - (c) note the interests of Council and also protects all subcontractors and agents engaged in connection with the performance of the Works and the Applicant's obligations under this Document; and
 - (d) for the duration of the carrying out of the Works; and
 - (e) on terms and with an insurer approved by Council, acting reasonably; and
- (2) insurance of the Trunk Infrastructure for its full replacement value, in the joint names of Council and the Applicant; and
- (3) Worker's Compensation insurance:
 - (a) in accordance with all laws; and
 - (b) for the duration of the carrying out of the Works; and
- (4) compulsory third party liability insurance:
 - (a) for registered vehicles owned or leased by the Applicant, in accordance with the requirements of any compulsory motor vehicle third party legislation; and

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- (b) which provides protection to Council arising out of the use of the Applicant's vehicles in addition to the Applicant; and
- (c) for the duration of the carrying out of the Works; and
- (5) property damage liability insurance:
 - (a) covering all motor vehicles owned, leased or hired by the Applicant used in connection with the Works or the Applicant's other obligations under this Agreement including the use of unregistered motor vehicles and plant; and
 - (b) with a limit of not less than \$20 million; and
 - (c) for the duration of the duration of the carrying out of the Works.

15.2 Requirements of Insurance

- (1) The Applicant must pay all deductibles, costs and premiums in relation to all insurance.
- (2) Whenever requested by Council, the Applicant must provide to Council, evidence to Council's satisfaction of its compliance with Clause 15.1. Evidence may, if requested by Council, include a full copy of the insurance policy document.
- (3) If the Applicant fails to provide evidence of insurance in accordance with Clause 15.2(2) Council may:
 - (a) immediately suspend the Applicant's right to claim any payment, Offset or reduction of the Levied Charge for the Trunk Infrastructure Contribution and may direct the Applicant to suspend the Works at its cost; and/or
 - (b) effect the insurance itself, and at its sole discretion, and reimburse itself from the Security.

16. APPLICANT'S OBLIGATIONS TO SUBSIST

The obligations of the Applicant under this Document continue and remain in force and effect until the Applicant's Obligations under this Document have been otherwise fulfilled, or otherwise transferred in accordance with Clause 18.1 of this Document.

17. INDEMNITY AND RELEASE

17.1 Applicant indemnifies Council

The Applicant indemnifies Council and Council's Agents against all liability, claims, loss, penalties, payments, costs, charges and expenses directly or indirectly arising from or incurred in connection with damage to or loss of any property or injury of any person caused or contributed to by Council and Council's Agents in connection with Council doing anything Council is required or permitted to do under this Document, or any other cause including in negligence, trespass, some other tort, contract, under statute or otherwise, except where arising from the negligent act or omission of Council or Council's Agents.

17.2 Applicant releases Council

The Applicant releases Council and Council's Agents from, and agrees that Council and Council's Agents are not liable for, liability, loss, penalties, payments, costs, charges and expenses directly or indirectly arising from or incurred in connection with Council doing anything Council is required or permitted to do under this Document except where arising from the negligent act or omission of Council or Council's Agents.

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17.3 Independence of Applicant's other obligations

The indemnity and release in Clauses 17.1 and 17.2 is independent of Applicant's other obligations under this Document.

18. ASSIGNMENT BY APPLICANT

18.1 Restriction on assignment

The Applicant shall not assign its interests, rights or obligations under this Document prior to the performance and fulfilment of the Applicant's Obligations unless:

- Council gives written consent to the assignment, whether or not the consent is given subject to conditions; and
- (2) the purchaser of the Land enters into a deed of novation of this Document with the Council, on terms reasonably acceptable to the Council, whereby the purchaser becomes contractually bound to the Council to perform and fulfil the provisions of this Document or such of them as remain unperformed or unfulfilled by the Applicant at the time of sale; or
- (3) the assignment is in respect of a subdivided part of the Land, which has been reconfigured in accordance with the Development Approval and is shown on a registrable Plan of Subdivision for the reconfiguring of the Land consented to by the Council.

18.2 Conditions of Council's consent

If the Council grants consent, with may not be unreasonably withheld, the consent is subject to:

- (1) the payment of the Security and Maintenance Bond by the assignee; and
- (2) any conditions imposed by Council for the giving of such consent, which are not inconsistent with this Document.

18.3 Security or Maintenance Bond retained if invalid assignment

The Council may, at its sole discretion, refuse to return the Security or the Maintenance Bond to the Applicant unless and until:

- (1) any assignment complies with Clause 18.1; and
- (2) the Applicant complies with any conditions imposed pursuant to Clause 18.2(2); and
- (3) the assignee has paid Council the full amount of the Security and Maintenance Bond, in substitution for the Maintenance Bond paid by the Applicant; and
- (4) the Applicant has paid any outstanding Shortfall owing to Council; and
- (5) the assignee grants a licence to Council on the same terms as Clause 14.1.

18.4 Applicant to remain liable

In the event of an assignment being made otherwise than in compliance with Clause 18.1 the Applicant must immediately perform and fulfil such of the Applicant's Obligations as have not been performed, notwithstanding the sale of the Land or any part of the Land and notwithstanding that the time for performance or fulfilment of that obligation has not yet arrived.

18.5 Restriction not to apply to subdivided lots

If the Land is subdivided in accordance with a development approval given by Council for reconfiguring a lot, the subdivided portion of the Land is released from the Applicant's Redland City Council | Infrastructure Agreement Page 23 DJS_141863_2024919

Obligations, and the Applicant's Obligations are no longer binding on the owner of the subdivided part of the Land.

19. FORCE MAJEURE

19.1 Excuse from performance of Obligation

If a Party is prevented, hindered or delayed from performing its obligations under this Agreement (other than an obligation to pay money) by an Event of Force Majeure, then as long as that situation continues, that Party shall be excused from performance of the obligation to the extent it is so prevented, hindered or delayed, and that time for performance of the obligation shall be extended accordingly, but if time is of the essence of this Agreement, time shall continue to remain of the essence.

19.2 Notification

A Party affected by an Event of Force Majeure shall immediately notify the other Party of its occurrence and its effect or likely effect, and use all reasonable endeavours to minimise the effect of the Event of Force Majeure and to bring it to an end.

20. GST

20.1 Definitions and interpretation

- (1) Capitalised expressions which are not defined in this Clause but which have a defined meaning in the GST Law have the same meaning in this Document.
- (2) In this Document:
 - (a) GST means the goods and services tax as imposed by the GST Law; and
 - (b) GST Amount means any Payment (or the relevant part of that Payment) multiplied by the appropriate rate of GST (currently 10%); and
 - (c) GST Law has the meaning given to that term in A New Tax System (Goods and Services Tax) Act 1999 (Cth), or, if that Act does not exist for any reason, means any Act imposing or relating to the imposition or administration of a goods and services tax in Australia and any regulation made under that Act; and
 - (d) Payment means any amount payable under or in connection with this Agreement including any amount payable by way of indemnity, reimbursement or otherwise and includes the provision of any non-monetary consideration.

20.2 Payment of GST

The parties agree that:

- (1) all Payments have been calculated without regard to GST; and
- (2) if the whole or any part of any Payment is the consideration for a Taxable Supply for which the payee is liable to GST, the payer must pay to the payee an additional amount equal to the GST Amount, either concurrently with that Payment or as otherwise agreed in writing; and
- (3) any reference to a cost or expense in this Agreement excludes any amount in respect of GST forming part of the relevant cost or expense when incurred by the relevant party for which that party can claim an Input Tax Credit; and
- (4) the payee will provide to the payer a Tax Invoice at the time at which any payment is made under this Clause.

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21. SERVICE

21.1 Service by prepaid post

A Notice is sufficiently made, given or served by a party if left at or forwarded by prepaid post in an envelope addressed to the other Party or any of them (where there are more persons than one comprising the other party) at the address of that Party specified in Schedule 1.

21.2 Deemed service

A Notice if sent by prepaid post is deemed to have been made, given or served at the time when in the due course of the post it would be delivered at the address to which it is directed whether or not it is actually received.

21.3 Proof of service

In proving service of a Notice made, given or served by Council it is only necessary for the Council to certify to that effect under the hand of the Chief Executive Officer.

21.4 Form of Notice

- (1) A Notice given by a Party must be in writing and signed by the Party.
- (2) A Notice given by a Party must be signed by an officer of that Party or the solicitor of that party.
- (3) A Party receiving a Notice is not obliged to enquire as to the authority of the person signing the Notice.

22. GENERAL PROVISIONS

22.1 Payment of costs

The Applicant shall pay all costs, charges and expenses, including those of Council, of and incidental to the negotiation, preparation, and execution of this Document, all counterparts of it, and any other document or instrument required under this Document.

22.2 Binding on successors

This Document shall be for the benefit of and binding upon the Parties and their heirs, executors, successors and permitted assigns.

22.3 Governing law

- (1) This Document is governed by the laws of Queensland and the Commonwealth of Australia which are in force in Queensland.
- (2) The parties submit to the jurisdiction of the Courts of Queensland, relevant Federal Courts and Courts competent to hear appeals from them.

22.4 Further assurances

The parties must execute and deliver all documents and must do all things as are necessary for the complete performance of their respective obligations under this Document.

22.5 Entire understanding

 This Document contains the entire understanding and agreement between the parties as to the subject matter of this Document.

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- (2) All previous negotiations, understandings, representations, warranties, memoranda or commitments about the subject matter of this Document are merged in this Document and are of no further effect.
- (3) No oral explanation or information provided by a party to another affects the meaning or interpretation of this Document or constitutes any collateral agreement, warranty or understanding.
- 22.6 Waiver

No waiver by a party of a provision of this Document is binding unless made in writing.

22.7 Warranty of authority

Each person signing this Document:

- (1) as attorney, by so doing, warrants to the other Parties that, as at the date of signing, the signatory has not received notice or information of the revocation of the power of attorney appointing that person; and
- (2) as an Authorised Representative, agent or trustee of a party, warrants to the other parties that, as at the date of signing, the signatory has full authority to execute this Document on behalf of that party.

22.8 Amendment of Document

- Despite any provision of this Document, the Council and the Applicant may at any time agree to vary the terms of this Document.
- (2) No modification, variation or amendment of this Document is of any force or effect unless:
 - (a) it is in the form of an amendment document and has been signed by the Parties; and
 - (b) where relevant the amendment document complies with the requirements of the Act.

22.9 Severance

If a provision of this Document is void or unenforceable it must be severed from this agreement and the provisions that are not void or unenforceable are unaffected by the severance.

22.10 Cumulative rights

The rights and remedies of a party to this Document are in addition to the rights or remedies conferred on the party at law or in equity.

22.11 Counterparts and fax

This Document may be executed in any number of counterparts and when executed communication of the fact of execution to the other parties may be made by sending evidence of execution by fax or email.

22.12 Attorneys

Where this Document is executed for a party by an attorney, the attorney by executing it declares that the attorney has no notice of revocation of the power of attorney.

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22.13 Contra proferentem

The contra proferentem rule and other rules of construction will not apply to disadvantage a party whether that party put the Clause forward, was responsible for drafting all or part of it or would otherwise benefit from it.

22.14 Dispute resolution

- (1) Except where stated to the contrary in this Document, if a dispute arises in connection with this Document, then a party must deal with the dispute in the manner set out in this Clause.
- (2) A party to a dispute which arises in connection with this Document may give to the other party or parties to the dispute a Notice specifying the dispute and requiring its resolution under this Clause.
- (3) Within 14 days after a Notice is given under Clause 22.14(2) (or such longer period as is agreed in writing by the parties to the dispute) each party to the dispute must use its best efforts to resolve the dispute in good faith.
- (4) If, despite the parties' best efforts, a dispute is not resolved within 14 days after notice a party may by notice to the other party or parties to the dispute refer the dispute for mediation in accordance with the Mediation Rules of The Institute of Arbitrators and Mediators Australia. The mediation will be conducted by a mediator to be appointed by agreement of the parties or in default of agreement to be appointed by the President of the Queensland Law Society or his nominee at the request of a party. The costs of the mediator will be shared jointly by the Parties.
- (5) If the dispute is not resolved within 28 days after the appointment of the mediator any party may take legal proceedings to resolve the dispute.
- (6) The provisions of this Clause do not prevent any party from obtaining any injunctive, declaratory or other interlocutory relief from a Court which may be urgently required.

22.15 Time of the essence

Time is in all cases and in every respect of the essence.

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Executed as a Deed

Execution by Applicant (if an individual):

SIGNED SEALED AND DELIVERED by the Applicant in the presence of:

Signature of witness

Print full name of witness

Execution by Applicant (if a corporation):

EXECUTED by PHILIP USHER CONSTRUCTIONS PTY LTD A.C.N. 011 008 101 in accordance with section 127 of the Corporations Act 2001.

EXECUTED for and on behalf of REDLAND

CITY COUNCIL ABN 86 058 929 428 in accordance with the Local Government Act

2009 (Qld).

Signature of Applicant

Print full name of Applicant

Date signed

Signature of Sole Director/Secretary

PHUS pm BHE Print full name of Sole Director/Secretary

1-10-21 Date signed

Signature of Director/Secretary

Print full name of Director/Secretary

Date signed

Signature of Delegate

CHRISTOPHER RALPH VIZE Print Full name of Delegate

29/10/2021

Date signed

)

Signature of Witness

JUSTIN KEIRAN LEACH Full name of Witness

29/10/2021 Date signed

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Schedule 1 Particulars

| Item 1 | Date | The date the last party signs this Document: |
|---------|--|---|
| Item 2 | Name of document | 410-426 Boundary Road and 359-405 Cleveland Road Infrastructure Agreement |
| Item 3 | Name of local government | Redland City Council ABN 86 058 929 428 |
| | Address | Corner Bloomfield & Middle Streets, Cleveland Qld 4580 |
| ltem 4 | Applicant | Philip Usher Constructions Pty Ltd ABN 38 011 008 101 |
| | Address (registered address if corporation) | 32 Hillcrest Road, Hillcrest QLD 4118 |
| Item 5 | Land | As described below. |
| | Description (lot and registered plan number) | Lot 18 on SP119616, Lot 10 on SP216148 and Lot 20 on SP140739 |
| | Address | 410-426 Boundary Road and 359-405 Cleveland-Redland Bay Road |
| | Site area | 107,220m² (10.722ha) |
| ltem 6 | Development Application | MCU17/0057 & MCU17/0058 |
| ltem 7 | Development Approval | Not issued at time of execution of this Document. |
| Item 8 | Schedule of Plans | Refer to Schedule 2 |
| Item 9 | Levied Charge | Refer to Schedule 3 |
| Item 10 | Offset / Refund | Refer to Schedule 4 |
| Item 11 | Trunk Infrastructure | Refer to Schedule 5 |
| Item 12 | Land Dedication | Refer to Schedule 6 |
| Item 13 | Security | Not required |
| Item 14 | Maintenance Bond | 5% of value of works. |
| Item 15 | Maintenance period | (a) 12 months, plus any extensions of time by Council pursuant to Clause 10.1; and (where applicable) |
| | | (b) 24 months for any Stormwater Quality Improvement Device. |

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Schedule 2 Schedule of Plans

| Plan Title | Plan No. | Revision No. | Drawn by | Date |
|---|----------|-----------------|--------------------|-----------|
| Preliminary Trunk Sewer Reticulation Plan | SK16 | 2 | 2 Develop Urban | 16/7/2021 |

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Schedule 3 Levied Charge

| Applicable development | Adopted charge to be applied (AC) | Additional demand (AD) | Discount to be applied (D) | Automatic increase (yes / no) | Total levied charge payable | Timeframe for payment / Date for payment |
|---------------------------|--------------------------------------|---------------------------|-------------------------------|----------------------------------|--------------------------------|--|
| MCU17/0057 | As stated in the Charg | es Notice issued with th | e Development Approv | al (when issued). | | |
| MCU17/0058 | As stated in the Charg | es Notice issued with th | e Development Approv | val (when issued). | | |

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Schedule 4 Offsets and Refunds

| Applicable infrastructure network | Criteria for Trunk Infrastructure | Establishment Cost | Details of Offset / Refund | Timing of Offset / Refund | |
|---|--|--|--|--|--|
| All | Unless otherwise specified, all works identified in Schedule 5 are Trunk Infrastructure. | To be determined in accordance with section 5 of this Document | As detailed within section 8 of this Document. | As detailed within section 8 of this Document. | |

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Schedule 5 Trunk Infrastructure Contribution

4.1 Transport Infrastructure Network:

| Infrastructure Network | Infrastructure contribution | Specifications of infrastructure contribution | Timing of Infrastructure contribution | Party Responsible for infrastructure contribution | Other requirements |
|------------------------|--------------------------------|---|---|---|--------------------|
| Not applicable | 5 | | | | |

4.2 Public parks and land for community facilities:

| Infrastructure Network | Infrastructure contribution | Specifications of infrastructure contribution | Timing of Infrastructure contribution | Party Responsible for infrastructure contribution | Other requirements |
|------------------------|-----------------------------|---|---|---|--------------------|
| Not applicable | | | | | |

4.3 Stormwater infrastructure Network:

| Infrastructure Network | Infrastructure contribution | Specifications of infrastructure contribution | Timing of Infrastructure contribution | Party Responsible for infrastructure contribution | Other requirements |
|------------------------|--------------------------------|---|---|---|--------------------|
| Not applicable | | | | | |

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4.4 Redland Water - Water Infrastructure Network:

| Infrastructure Network | Infrastructure contribution | Specifications of infrastructure contribution | Timing of Infrastructure contribution | Party Responsible for infrastructure contribution | Other requirements |
|------------------------|--------------------------------|---|---|---|--------------------|
| Not applicable | | | | | |

4.5 Rediand Water - Wastewater Infrastructure Network:

| Infrastructure Network | Infrastructure contribution | Specifications of infrastructure contribution | Timing of Infrastructure contribution | Party Responsible for infrastructure contribution | Other requirements |
|---------------------------|---|---|--|---|---|
| Wastewater | Provision of a sewer gravity main from Point A to Point B as indicatively identified on the Preliminary Trunk Sewer Reticulation Plan referenced in Schedule 2. | The infrastructure contribution is to comprise the provision of Work being: (a) construction of: (i) approximately 50m of 150mm diameter sewer gravity main within the Road Cleveland Redland Bay Road between manhole 223515 and manhole 456377; and (ii) all other Works necessary to operate the sewer gravity main | Prior to the earlier of: (a) the Approval of a Plan of Subdivision or the commencement of a use for Stage 1; (b) a later time specified by the Council in an Approval. | Applicant | The infrastructure contribution is subject to an Offset/Refund. |

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| | | specified in paragraph (i); and (b) designed and constructed to the reasonable satisfaction of the Council generally in accordance with the following: (i) the applicable planning instruments; (ii) the applicable development approvals. | | | |
|------------|--|---|--|-----------|---|
| Wastewater | Decommission existing sewer gravity main from Point C to Point D as indicatively identified on the Preliminary Trunk Sewer Reticulation Plan referenced in Schedule 2. | The infrastructure contribution is to comprise the provision of Work being: (a) Decommission approximately 375 m of DN150 sewer gravity main (and manholes) upstream of the proposed connection point (manhole 223520) located within the Cleveland Redland Bay Road road reserve. | Prior to the earlier of: (a) the Approval of a Plan of Subdivision or the commencement of a use for Stage 1; (b) a later time specified by the Council in an Approval. | Applicant | The infrastructure contribution is subject to an Offset/Refund. |
| | | (b) Final scope of decommissioning to | | | |

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| Wastewater | Provision of a sewer rising main from Point E to Point F as indicatively identified on the Preliminary Trunk Sewer Reticulation Plan referenced in Schedule 2. | be determined by DTMR but will include the following: (i) grout filling of the redundant gravity mains; (ii) demolition and filling of the maintenance structures; (iii) reinstatement of pavements in accordance with DTMR pavement specifications. The infrastructure contribution is to comprise the provision of Work being: (a) construction of approximately 540m of 150mm diameter sewer rising main within the Cleveland Redland Bay Road road reserve from sewer pump station No.68 to the proposed discharge point at manhole | Prior to the earlier of: (a) the Approval of a Plan of Subdivision or the commencement of a use for Stage 1; (b) a later time specified by the Council in an Approval. | Applicant | The infrastructure contribution is subject to an Offset/Refund. |
|------------|--|---|--|-----------|---|
| | | 46375; and (b) all other Works necessary to operate the sewer gravity | | | |

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| WastewaterReplacement of existing sewer pump station No. 68 as indicatively identified on the Preliminary Trunk Sewer Reticulation Plan referenced in Schedule 2.The infrastructure contribution is to comprise the provision of Work being: (a) replacement of existing sewer pump station adjacent to the roundabout of Boundary Road and Cleveland Redland Bay Road, in accordance with SPS068 Needs Specification 2018 v04 FINAL as prepared by Joshua May dated 4 September 202021 April, 2021.Prior to the earlier of: (a) the Approval of a Plan of Subdivision or the commencement of a use for Stage 1;ApplicantThe infrastructure contribution is subjection or the contribution is subjection or the commencement of a use for Stage 1;Prior to the earlier of: (a) the Approval of a Plan of Subdivision or the commencement of a use for Stage 1;ApplicantThe infrastructure contribution is subjection or the commencement of a use for Stage 1;Prior to the earlier of: (a) the Approval.ApplicantThe infrastructure contribution is subjection or the subdivision or the commencement of a use for Stage 1;Prior to the earlier of: (b) a later time specified by the Council in an Approval.ApplicantThe infrastructure contribution is subjection(b)all other Works(b)all other Works(c)(c)(c)(c)(c)(c) |
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| | | necessary to operate the sewer pump station specified in paragraph (a); and (c) designed and constructed to the reasonable satisfaction of the Council generally in accordance with the following: (i) the applicable planning instruments; (ii) the applicable development approvals. | | | |
|------------|---|---|--|-----------|---|
| Wastewater | Provision of emergency storage as indicatively identified on the Preliminary Trunk Sewer Reticulation Plan referenced in Schedule 2. | The infrastructure contribution is to comprise the provision of Work being: (a) Refurbishment of the existing SPS 68 wet well and installation of a 15.4 kL offline emergency storage tank, in accordance with SPS068 Needs Specification 2018 v04 FINAL as prepared by Joshua May dated 21 April, 2021. (b) all other Works necessary to operate | Prior to the earlier of: (d) the Approval of a Plan of Subdivision or the commencement of a use for Stage 1; (e) a later time specified by the Council in an Approval. | Applicant | The infrastructure contribution is subject to an Offset/Refund. |

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| | the sewer pump station specified in paragraph (a); and | | |
|----|--|--|--|
| (0 | c) designed and constructed to the reasonable satisfaction of the Council generally in accordance with the following: | | |
| | the applicable planning instruments; | | |
| | the applicable development approvals. | | |

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Schedule 6 Land Dedication

| | Location of land to be dedicated | Lot and Plan Description | Size of land to be dedicated | Plan Reference | Timeframe for dedication of land | Other requirements |
|---|----------------------------------|--------------------------|------------------------------|----------------|-------------------------------------|--------------------|
| N | lot applicable | | | | | |

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ATTACHMENT 10

REASONS FOR REFUSAL

1. Land use

The proposed development consolidates residential land use to the northern section of the site and provides open space in the south. The Redlands Planning Scheme (V7.1) zoning intends for residential development to be located within the northern and southern sections of the site, separated by an east-west open space corridor. Additionally, the development results in a reduction of the overall provision of open space land which is considered deficient to serve the requirements of the community in the South-East Thornlands structure plan area. The land is also proposed to be maintained in private ownership by a community title scheme, rather than being transferred to public ownership. This will not provide suitable access and use of the open space zoned land for the benefit of the public. The proposal does not comply with the following planning scheme provisions:

2. Redlands Planning Scheme V7.1

- a) South-East Thornlands structure plan overlay code specific outcomes S1.6(1)(a), (b), (c), (g) and (7)(a), (b), (d) and (f); and overall outcome 5.14.7(2)(a)(ii) d.
- b) Open space zone code specific outcomes S1.1, S1.2(1) and S1.3(1)(a) (d); and overall outcome 4.16.7(2)(a)(i) a., (ii) a. and b., and (iii) a. and b.
- c) Desired environmental outcomes 3.1.4(d), (e) and (h), 3.1.6(1)(a) and (f)

3. City Plan V5

- a) Recreation and open space zone code performance outcomes PO1 and PO2(1) and (2); and overall outcomes 6.2.12.2(2)(a), (e) and (g)
- b) Low-medium density residential zone code performance outcomes PO28(5); and overall outcomes 6.2.2.2(3)(a)(iii)
- c) Medium density residential zone code performance outcomes PO31(5)
- d) Reconfiguring a lot code performance outcomes PO1(1), PO5(1), PO9(6), PO14(1) and (2), and PO45(1), (2), (3) and (4); and overall outcomes 9.3.4.2(2)(a)(i) and (ii)
- e) Strategic framework, strategic outcomes 3.3.1.1(5) and (7), 3.3.1.4(5), 3.7.1.1(1), 3.7.1.6(6) and (7)

4. Connectivity

The proposed development does not provide an extension of Pateman Parade and construction of the fourth leg of the existing signalised intersection at Cleveland-Redland Bay and Beveridge Roads. Accordingly, the development does not achieve the intent for the South-East Thornlands structure plan area in terms of provision of vehicular accessibility and connectivity. Adequate connectivity for public pedestrian and cycle movements internal and external to the site is also not provided. The proposal does not comply with the following planning scheme provisions:

ATTACHMENT 10

REASONS FOR REFUSAL

5. Redlands Planning Scheme V7.1

- a) South-East Thornlands structure plan overlay code specific outcome S2.1(1) and (2), S2.8(1) and (2), S2.9(1); and overall outcome 5.14.7(2)(a)(i) e. and i., and (2)(b)(i) a., (ii) and (iii)
- b) Infrastructure works code specific outcome S7(1) and S8(1); and overall outcome 8.7.3(2)(a)(ii) and (vi)
- c) Desired environmental outcomes 3.1.5(1)(a), (b), (c), (h), (n) and (p)

6. City Plan V5

- a) Transport, servicing, access and parking code performance outcomes PO3(1) (7), PO4(1), (2) and (4), PO6, and PO7(1); and overall outcomes 9.3.5.2(2)(1) and (2)
- b) Low-medium density residential zone code performance outcomes PO24 and PO28(1) and (2); and overall outcomes 6.2.2.2(3)(a)(ii) and (iii)
- Medium density residential zone code performance outcomes PO27 and PO31(1) and (2); and overall outcomes 6.2.3.2(3)(f)(ii)
- d) Reconfiguring a lot code performance outcomes PO5(1), PO14(1), (2) and (3), PO17; and overall outcomes 9.3.4.2(2)(a)(i), (ii), (iii) and (vii)
- e) Strategic framework, strategic outcomes 3.3.1.1(2), (6) and (9)(a),(b) and (c), 3.3.1.4(2), (3), 3.7.1.1(1), 3.7.1.3(1), (2), (6), (12), (13) and (14)

7. Environmental connectivity

The proposed development does not provide sufficient area for establishing habitat and movement corridor for koalas and other fauna. The proposal does not comply with the following planning scheme provisions:

8. Redlands Planning Scheme V7.1

- a) South-East Thornlands structure plan overlay code specific outcome S1.6(7)(a); and overall outcome 5.14.7(2)(a)(ii) d.
- b) Desired environmental outcomes 3.1.2(1)(a)(i)(c), (d), (e) and (f), (ii)(a) and (b)

9. City Plan V5

- a) Recreation and open space zone code performance outcomes PO1 and PO13 (3) and (4); and overall outcomes 6.2.12.2(2)(g)
- b) Reconfiguring a lot code performance outcomes PO3(2) and (3), PO5(2), and PO46(1) and (2); and overall outcomes 9.3.4.2(2)(a)(iv)
- c) Strategic framework, strategic outcomes 3.3.1.1(1), (5), 3.3.1.4(4) and 3.5.1.1(2)

ATTACHMENT 10

REASONS FOR REFUSAL

10. Landscape buffer

The proposed development includes landscape buffers of 10m in width to Boundary and Cleveland-Redland Bay Roads. For a section along Cleveland-Redland Bay Road, the buffer area includes a swale for stormwater management purposes, which is not envisioned by the Redlands Planning Scheme and is considered to impact on the function of the landscape buffer. The proposal does not comply with the following planning scheme provisions:

11. Redlands Planning Scheme V7.1

- a) South-East Thornlands structure plan overlay code specific outcome S2.6(1)(e)(iv) and (v) and S2.7(1); and overall outcome 5.14.7(2)(b)(i) d. and (2)(c)(i) b.
- b) Desired environmental outcomes 3.1.5(1)(j)

12. City Plan V5

- a) Reconfiguring a lot code performance outcomes PO18(1) and (2); and overall outcomes 9.3.4.2(2)(a)(a)(i) and (iv)
- b) Low-medium density residential zone code performance outcomes PO26 and PO27(1) and (2); and overall outcomes 6.2.2.2(3)(a)(iii)
- c) Medium density residential zone code performance outcomes PO29 and PO30
- d) Strategic framework, strategic outcomes 3.3.1.1 (9)(h), and 3.5.1.2(1)

14.2 MCU21/0063 - MATERIAL CHANGE OF USE FOR A MULTIPLE DWELLING X 30 UNITS AND BUILDING WORK FOR DEMOLITION OF A LOCAL HERITAGE PLACE (DWELLING HOUSE) AT 217 MIDDLE STREET, CLEVELAND

| Objective Reference: | A5571117 | |
|-----------------------------|--|--|
| Authorising Officer: | Louise Rusan, General Manager Community & Customer Services | |
| Responsible Officer: | David Jeanes, Group Manager City Planning and Assessment | |
| Report Author: | Carol Vito Sula, Planning Officer | |
| Attachments: | Architectural Plans ↓ Aerial and Zone Mapping ↓ Recommended Conditions ↓ | |

4. Australian Heritage Specialist Heritage Advice 🕹

PURPOSE

To recommend Council issue a development permit for the material change of use for a multiple dwelling and building work for demolition of a local heritage place (dwelling house) at 217 and 219 Middle Street, Cleveland.

BACKGROUND

Council has received an application on land at 217 and 219 Middle Street, Cleveland seeking a development permit for material change of use for a multiple dwelling with 30 units and building work for the demolition of a local heritage place (dwelling house).

The owner of the properties is Beached as Bro Holdings Pty Ltd. as trustee. The applicant is York Street Developments Pty Ltd. c/- Urbicus Pty Ltd. (Mark Clayton).

In accordance with the *Planning Act 2016*, the decision date for this application is 17 November 2021. The development application will be a potential deemed refusal should a decision not be made by the due date.

The assessment of the application has occurred in line with the assessment framework outlined in the *Planning Act 2016*. The key issues identified in the assessment are:

- Local heritage value
- Building height
- Building design
- Amenity
- Open space
- Onsite parking
- Stormwater management
- Waste management servicing and manoeuvring areas

ISSUES

Proposal

The development application seeks a development permit for a material change of use for a multiple dwelling with 30 units and building work for the demolition of a local heritage place (dwelling house).





The proposed multiple dwelling comprises the following components:

- A five storey building with maximum height of 17.6m.
- 30 residential units 13 two bedroom units (of which six) units have two bedrooms and a media room); 17 three bedroom units (two penthouse units have three bedrooms and a media room).
- 60 resident parking spaces and four visitor parking spaces including one disabled parking space.
- A 55m² boat parking area, 27 bike parking spaces and 8 visitor bike racks.
- A 225m² communal open space area on the ground floor containing a BBQ/alfresco area, resort pool, spa pool, pool deck and powder room.
- Lobby room on the ground floor with lift access to upper and basement levels.
- Single vehicular access point to Middle Street via the proposed 7m wide crossover.
- Bin room and wash down area on the ground floor.

A copy of the proposal plans can be found in Attachment 1 to this report. The proposal will require the demolition of the heritage place (dwelling house) at 219 Middle Street.

Minor change

As a response to Council's information request, the applicant submitted a change to the application under section 52 of the *Planning Act 2016* on 13 May 2021. The change included assessable building works for the demolition of a local heritage place at 219 Middle Street, Cleveland.

Through the course of the assessment, Council raised concerns with the height, bulk and design of the proposed multiple dwelling. Minor revisions to the building design were received on 9 August 2021, 16 August 2021, 30 August 2021, 15 September 2021, 23 September 2021 and 6 October 2021. Of note, the multiple dwelling was reduced in height from six storeys to five storeys. In addition, design elements were included to significantly reduce building bulk and to contribute to an interesting and attractive building.

The changes are considered to be 'minor' as the changes did not result in a substantially different development, did not trigger referral to any additional referral agencies, did not change the type of development approval sought, and did not change the level of assessment.

Site & Locality

The 2024m² subject site covers two freehold lots legally described as Lot 1 on RP66399 at 217 Middle Street, Cleveland and Lot 2 on RP66399 at 219 Middle Street, Cleveland (refer Attachment 2). The site is currently improved by a dwelling house and associated outbuildings on each lot. The dwelling house on the land at 219 Middle Street is listed as a local heritage place.

The natural ground levels of the site indicate a fall from the western side boundary at 7.5m Australian Height Datum (AHD) to the eastern side boundary at 6.54m AHD, the difference being approximately 0.96m. The site is relatively flat with some mature vegetation located within the site. There are two existing street trees located along the frontage of the site.

The site is located 300m west of the Toondah Harbour priority development area; 250m south of the Raby Bay canal estate; and 900m east of the Cleveland principal centre.

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The immediate surrounding environment consists of a mix of multiple dwellings (of varying height), detached dwellings, neighbourhood centre uses (local shops and Returned and Services League Club) and community facilities such as churches.

Planning History

Approvals on subject site

Development approval MC011621 for a three storey motel on the subject site was granted on 3 November 2009. An application to extend the currency period was received on 19 August 2013. The concurrence period was extended by two years, lapsing on 10 November 2015.

Approvals on adjoining properties

The following approvals have been granted on the adjoining western property at 17-19 and 21 Passage Street:

- A development approval MC012013 for a five storey apartment building (30 units) and two storey multiple dwelling (12 townhouses) was granted on 26 September 2013. Stage 1 has commenced and the two storey townhouses have been constructed.
- A preliminary approval MCU19/0003 for a six storey building, which also included a variation to vary the effects of City Plan to apply MDR4 precinct height provisions.
- A development approval MCU013906 for a six storey apartment building with 29 units was granted on 9 June 2020. The previously approved commercial premises on the ground floor did not form part of this proposal and was replaced by ground floor residential units.

Assessment Framework

The application has been made in accordance with the Planning Act Development Assessment Rules and constitutes an application for a material change of use under the City Plan.

In accordance with section 45 of the *Planning Act 2016*:

(5) An impact assessment is an assessment that—

- (a) must be carried out—
 - (i) against the assessment benchmarks in a categorising instrument for the development; and
 - (ii) having regard to any matters prescribed by regulation for this subparagraph; and
- (b) may be carried out against, or having regard to, any other relevant matter, other than a person's personal circumstances, financial or otherwise.

Examples of another relevant matter—

- a planning need
- the current relevance of the assessment benchmarks in the light of changed circumstances
- whether assessment benchmarks or other prescribed matters were based on material errors
- (6) Subsections (7) and (8) apply if an assessment manager is, under subsection (3) or (5), assessing a development application against or having regard to—



- (a) a statutory instrument; or
- (b) another document applied, adopted or incorporated (with or without changes) in a statutory instrument.
- (7) The assessment manager must assess the development application against or having regard to the statutory instrument, or other document, as in effect when the development application was properly made.
- (8) However, the assessment manager may give the weight the assessment manager considers is appropriate, in the circumstances, to—
 - (a) if the statutory instrument or other document is amended or replaced after the development application is properly made but before it is decided by the assessment manager—the amended or replacement instrument or document; or
 - (b) another statutory instrument—
 - *(i)* that comes into effect after the development application is properly made but before it is decided by the assessment manager; and
 - (ii) that the assessment manager would have been required to assess, or could have assessed, the development application against, or having regard to, if the instrument had been in effect when the application was properly made.'

Section 30 of the *Planning Regulation 2017*, relevantly, identifies that:

- '(1) For section 45(5)(a)(i) of the Act, the impact assessment must be carried out against the assessment benchmarks for the development stated in schedules 9 and 10.
- (2) Also, if the prescribed assessment manager is the local government, the impact assessment must be carried out against the following assessment benchmarks—
 - (a) the assessment benchmarks stated in-
 - (i) the regional plan for a region; and
 - (ii) the State Planning Policy, part E, to the extent part E is not identified in the planning scheme as being appropriately integrated in the planning scheme; and
 - (iii) a temporary State planning policy applying to the premises;
 - (b) if the development is not in a local government area—any local planning instrument for a local government area that may be materially affected by the development;
 - (c) if the local government is an infrastructure provider—the local government's LGIP.
- (3) However, an assessment manager may, in assessing development requiring impact assessment, consider an assessment benchmark only to the extent the assessment benchmark is relevant to the development.'

Section 31 of the *Planning Regulation 2017* identifies that:

'(1) For <u>section 45(5)(a)(ii)</u> of the <u>Act</u>, the impact assessment must be carried out having regard to—



- (a) the matters stated in <u>schedules 9</u> and <u>10</u> for the development; and
- (d) if the prescribed assessment manager is a person other than the chief executive
 - (i) the regional plan for a region; and
 - (ii) the State Planning Policy, to the extent the State Planning Policy is not identified in the planning scheme as being appropriately integrated in the planning scheme; and
 - (iii) for designated premises—the designation for the premises; and
- (e) any temporary State planning policy applying to the premises; and
- (f) development approval for, and any lawful use of, the premises or adjacent premises; and
- (g) common material.
- (2) However-
 - (a) an assessment manager may, in assessing development requiring impact assessment, consider a matter mentioned in subsection (1) only to the extent the assessment manager considers the matter is relevant to the development; and
 - (b) if an assessment manager is required to carry out code assessment against assessment benchmarks in an instrument stated in subsection (1), this section does not require the assessment manager to also have regard to the assessment benchmarks.'

common material, for a development application, means-

- '(a) all the material about the application that the assessment manager receives before the application is decided, including—
 - *(i) any material relating to a proposed development application that is substantially similar to the development application as made; and*
 - (ii) any material attached to, or given with, the development application; and
 - *(iii) any material relating to the application given to the assessment manager after the application is made; and*
 - (iv) any referral agency's response, including any advice or comment given by a referral agency and any response given under section 57 of the Act; and
 - (v) any properly made submissions about the application, other than a submission that is withdrawn; and
 - (vi) any other submission about the application that the assessment manager has accepted; and
 - (vii) any other advice or comment about the application that a person gives to the assessment manager; and
- (b) if a development approval for the development is in effect—the approval; and
- (c) an infrastructure agreement applying to the premises.'



Pursuant to section 45(5) of the *Planning Act 2016*, the application was assessed against the following applicable assessment benchmarks.

- City Plan Version 5 the whole planning scheme with emphasis on the following codes:
 - Medium density residential zone code
 - Healthy waters code
 - Infrastructure works code
 - o Landscape code

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- Transport, servicing, access and parking code
- Heritage overlay code
- Temporary Local Planning Instrument (TLPI) 01/21 Protection of local heritage places
- State Planning Policy 2017, Part E
- South East Queensland Regional Plan 2017
- *Planning Regulation 2017*, Schedule 11
- Local Government Infrastructure Plan

Pursuant to section 45(5) of the *Planning Act 2016*, Council had regard to the following matters in its assessment of the application.

- Existing approvals on adjoining land
- Common material, including written submissions accepted by the assessment manager
- Advice received from external and internal parties

Comments received

External comments received

There were 19 properly made submissions received in relation to the application during the notification period. A further ten submissions were also received during that time, which were not properly made, but were accepted by the assessment manager.

The following planning matters were raised in the written submissions. It is recognised that these submissions were received prior to the revised proposal plans being received.

- The demolition of the heritage place will result in the loss of character and heritage values
- The scale and design of the multiple dwelling is incompatible with surrounding properties and streetscape
- The building design has little architectural merit or interest
- The development will have adverse impacts on amenity and privacy of adjoining properties
- Increased traffic generation from Toondah priority development area (PDA) will restrict access to the site and cause safety issue
- The height of the proposed building will obstruct future water views to the bay for properties across the street that have not yet been developed to their potential under the City Plan

The assessment manager has had regard to the submissions about these planning matters in the assessment of the development against the assessment benchmarks.

Internal comments received

The assessment manager has received assessment advice from the following Council teams/ officers:

- Engineering Assessment
- Environmental Assessment
- Landscaping
- Arborist
- Health and Environment
- Infrastructure Charges Unit

The assessment advice received has been considered by the assessment manager in assessing the development application.

Decision Making Rules

Section 60 of the *Planning Act 2016* states that:

- '(3) To the extent the application involves development that requires impact assessment, and subject to section 62, the assessment manager, after carrying out the assessment, must decide—
 - (a) to approve all or part of the application; or
 - (b) to approve all or part of the application, but impose development conditions on the approval; or
 - (c) to refuse the application.

•••

- (5) The assessment manager may give a preliminary approval for all or part of the development application, even though the development application sought a development permit.
- (6) If an assessment manager approves only part of a development application, the rest is taken to be refused.'

Application Assessment

The application has been assessed against the provisions of the City Plan version 5. The application is subject to impact assessment. In this regard, the application is subject to assessment against the entire planning scheme. The most pertinent parts of the assessment are discussed in this section.

Local heritage values

The existing dwelling at 219 Middle Street was recommended for entry within the amended Local Heritage Place Schedule, as part of the proposed major amendment of City Plan. This amendment process commenced in 2019, with public consultation having been completed. The amendment has not yet commenced in City Plan.

In the interim, the subject site is listed as a local heritage place within the Local Heritage Place Schedule under TLPI 01/21 – Protection of Local Heritage Places. The TLPI was in force prior to the application being lodged and remains in force. The heritage citation as included in the TLPI reads as follows:



| Condition | Very Good | Integrity | Fair | | | |
|---|-----------|-----------|------|--|--|--|
| Statement of Significance | | | | | | |
| 219 Middle Street is a locally significant building which embodies representative and aesthetic heritage values. The building is an intact example of a traditional timber suburban dwelling dating from the second phase of settlement in Cleveland (c.1919) and once included a sawmill alongside. The place makes a positive contribution to the streetscape and locality. | | | | | | |
| Criteria A The place is important in demonstrating the evolution or pattern of the region's history. | | | | | | |
| 219 Middle Street demonstrated the second phase of development at Cleveland west of Cleveland Point in the early twentieth century, whereby a local business person was able to build a residence and sawmill on adjacent land. | | | | | | |
| Criteria E The place is important to the region because of its aesthetic significance. | | | | | | |
| The scale, form and design of the building make a positive contribution to the streetscape as well as the character and appearance of the locality. | | | | | | |

• Figure 1: Heritage citation for the existing dwelling at 219 Middle Street

The proposed demolition of the existing dwelling (local heritage place) triggers a development application under the TLPI 01/21 - Protection of Local Heritage Places, including assessment against the heritage overlay code of the City Plan.

Performance outcome PO1 of the heritage overlay code is relevant to the assessment of local heritage values of the place.

PO1

- 'A building or structure on a heritage place is not demolished or relocated unless:
- 1) it is structurally unsound and is not reasonably capable of being made structurally sound; or
- 2) the change does not result in the loss of the particular heritage values of the place.'

The applicant has submitted a heritage impact statement (HIS) to support the proposed demolition of the heritage place, and thus demonstrate compliance with performance outcome PO1 of the heritage overlay code.

In terms of the structural integrity of the heritage place the HIS notes 'the building appears to be generally in good (but not very good) physical condition but has only fair to poor integrity'.

Demonstration of compliance with PO1 is sought under PO1 (2) as the HIS provides a discussion of the cultural heritage significance (heritage values) of the place. The guideline prepared by the Department of Environment and Heritage Protection titled 'Guideline: Identifying and assessing places of local cultural heritage significance in Queensland' published in 2020 was relied upon in the HIS to assess the cultural heritage significance of the subject dwelling. The HIS provides the following conclusions regarding the heritage values of the place:

- 'With respect to performance outcome PO1 (2) of the Heritage overlay code, the demolition of the building on the heritage place will not result in the loss of any particular heritage values of the place because, on further assessment, the place is not of sufficient cultural heritage value to meet relevant heritage assessment criteria as a local heritage place.
- With respect to overall outcome (2)(a) of the Heritage overlay code, the proposed demolition of the house will not remove or reduce its heritage value to any unacceptable degree because the place has insufficient heritage value to warrant retention.



- It is concluded that the proposed development (including demolition of the existing house) will not result in the loss of any particular heritage values since, on further assessment, the place has insufficient cultural heritage significance to warrant retention.
- As the place has insufficient cultural heritage significance to satisfy any of the Council's heritage assessment criteria, it is recommended that Council not proceed with permanently entering the place in the planning scheme's schedule of local heritage places.'

The applicant also submitted material outlining relevant matters unique to the application:

- Pre-lodgement meeting Council failed to mention the heritage listing under heritage major amendment 03/19 or the TLPI at the pre-lodgement meeting held at Council on 8 February 2021.
- Superseded plan request Applicant has the option to lodge a superseded plan request and compensation may be awarded.
- Other matters central location of the site close to transport, employment and other facilities; development consistent with the SEQ Regional Plan as it support population and economic growth within the local government area; consistent with the Council's housing strategy to ensure that there is sufficient, affordable and diverse housing to meet the needs created by population growth within the local government area.

These additional matters raised are not considered to be relevant to the assessment of this matter.

To assess the heritage value of the premises, Council engaged Australian Heritage Specialists (AHS) on 9 June 2021 to provide specialist heritage advice. The heritage specialist has advised the heritage values of the place are of a lesser score than those that were originally prescribed by the heritage citation and no longer thresholds as a place of local heritage value. This is due to the substantial alterations undertaken to the place internally and to the rear. As such, the subject site does not properly satisfy entry onto the Local Heritage Place Schedule. This has been discovered only after AHS was able to enter the premises, which was not able to be done as part of preparing the citation. A copy of AHS's advice is provided at Attachment 4.

However, the premises does have some historical value, particularly in its connection with the Elfert family and their extended family who were prominent members of the Cleveland community, with a long historical association in the Redlands. AHS note there are archival recordings of the Elfert family and their saw-mill operations currently available for public viewing online and in Council's library. Further to this advice, the heritage specialist has recommended the following measures be conditioned to ensure the heritage values of the place are retained:

- A detailed Archival Recording of the place should be completed prior to demolition, by a qualified heritage consultant. The archival recording should consider options for 3D modelling and be in a format that can also be used in association with the Interpretation Plan recommended below to ensure the local heritage values of the place is captured for future generations.
- Development and implementation of a Heritage Interpretation Plan should be developed by a qualified heritage consultant. The Interpretive Plan should consider appropriate options to interpret the history and use of the place prior to demolition, which could be developed within



the new development (where appropriate), to ensure the local heritage values is presented within the new development.

Given the (physical) heritage values of the place have been diminished through substantial additions and alterations, it is considered that the archival recording and heritage interpretation plan measures will ensure the remaining local heritage values are retained. As such this will ensure the proposed demolition does not result in the loss of the particular heritage values of the place. Subject to this condition, it is considered that the proposal will comply with performance outcome PO1 of the heritage overlay code.

Building height

The subject site is mapped in the medium density residential zone (MDR), therefore the medium density residential zone code is relevant to the assessment of this application. The purpose of the MDR zone code is to provide for medium density living in areas that are close to public transport or centres, and characterised by a mix of dwelling types including dwelling houses on a range of lot sizes, dual occupancies and multiple dwellings. The purpose of the MDR zone code is achieved through eleven overall outcomes. The following overall outcome is relevant in the assessment of the height of the proposed multiple dwelling:

'(f) Development is generally two to three storeys in height, unless otherwise intended in a particular precinct;'

The proposed multiple dwelling will be 17.6 metres and five storeys in height. It is noted the subject site is not mapped within a precinct and therefore the overall outcome intends a building height of two to three storeys. Therefore the proposal does not comply with overall outcome (f) of the MDR zone code. Despite the non-compliance, when assessing impact assessable applications Council may have regard to any other matter they consider relevant to the assessment. The relevant matters applicable to the proposal include existing approvals on adjoining site and the surrounding context of the development site.

Existing approvals:

The following development approvals on the adjoining property (17-19 and 21 Passage Street) to the west of the subject site are relevant to the assessment of this application:

- MC012013 Council approved a five storey apartment building (30 units) and two storey multiple dwelling (12 townhouses).
- MCU19/0003 which granted a preliminary approval for a six storey apartment building, including a variation approval.
- MCU013906 Council approved a six storey apartment building with 29 units.

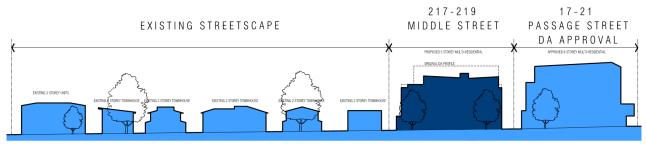
It is noted the adjoining site at 17-19 and 21 Passage Street is also mapped in the MDR zone and does not have a precinct where buildings higher than two to three storeys are intended. In the abovementioned approvals it was determined that the proposed height of the apartment buildings was acceptable for the location as it would take advantage of its strategic position being located on a prominent corner, with areas surrounding the site on Middle and Passage Streets intending to achieve 19 metre high built form. Therefore the development would appear in context when viewed along these road corridors. While the subject site benefits from some of these contextual outcomes, it also differs in some meaningful ways. Firstly, the site is not located on the corner and is therefore separated from higher built form along Passage Street. Secondly,



the subject development site adjoins existing multiple dwellings that are unlikely to be developed in the near future and therefore have an enduring impact on the character of the streetscape along Middle Street.

In having regard to the abovementioned approvals on the adjoining lot, both the approval and reasons for imposed conditions must be considered. Specifically, the preliminary approval MCU19/0003 includes a condition that requires the approved development to step down to the expected three storeys or 13m building height. This was achieved in the approval MCU013906, with the building stepping down to the subject site.

It is recognised that the proposed development is of a lower total height than the existing approval to the west and does, in itself, step down to the existing development to the east. In this regard, the proposed development does provide a transition of building height along the Middle Street streetscape. Figure 2 shows this transition.



• Figure 2: Streetscape section showing the proposed development alongside existing and approved development

In relation to the development's interface to the existing development to the east, performance outcome PO10 seeks the following:

PO10

'Where building height over 13m is intended, buildings step down in height and scale to be of a similar size to intended building height on adjoining residential zoned land.'

The proposed multiple dwelling has been designed so that it steps down to 13m height on those elevations where it adjoins properties mapped in the MDR zone, in this case the eastern and southern elevations. Where the height of the proposed building exceeds 13m, in this case the top floor (L4), a minimum 10m setback is proposed between the walls of the top floor level and common boundaries. As proposed, the top floor level will have a setback of 11.714m from the eastern side boundary and a 10m setback to the rear southern boundary. The top floor maintains a five metre setback from the western site boundary where it adjoins the approved apartment development at 17-21 Passage Street. As such, the proposal achieves the deemed to comply provision AO10.1, which therefore complies with performance outcome PO10.

The surrounding context – streetscape:

The zoning map of the subject site and surrounding area (refer Attachment 2) shows there is a broad range of building heights anticipated in the area:

- MDR 13m height (3 storeys)
- MDR3 22m height
- MDR4 19m height
- Local centre 10.5m height
- Community facilities 12m maximum height applies to CF3/CF5.



Properties directly east and south of the site contain multiple dwellings with heights consistent with the intended height in the MDR zone (refer Figure 2). Across the road on the MDR4 zoned properties, there is a mix of building heights, with some properties yet to be developed to their potential under City Plan (refer Figure 3). The adjoining property to the west at 17-21 Passage Street has been cleared to facilitate the construction of the approved multiple dwelling development. It is considered the height of the proposed development is not inconsistent with the existing and emerging streetscape along this portion of Middle Street.



• Figure 3: Photos showing existing townhouses on the adjoining properties to the east of the site.



• Figure 4: Photos of buildings within the MDR4 zone across the road from the site.

A streetscape section has been submitted to show the proposed development in the context of approved and existing development (see Figure 2).



It is considered the proposed development provides an appropriate transition in building height between the approved apartment block at 17-21 Passage Street to the west and townhouses to the east. The proposal will provide the coherence needed to contribute to the legibility of the street. The proposal is consistent with strategic outcome 3.3.1.2(3), which seeks to ensure development creates an attractive streetscape and a sensitive transition between older and new forms of housing.

Building Design

Performance outcomes PO13, PO14 and PO15 are relevant to the assessment of the proposed design and read as follows:

PO13

'Design elements contribute to an interesting and attractive streetscape and building through:

- 1. the provision of projections and recesses in the facade which reflect changes of internal functions of buildings, including circulation;
- 2. variations in material and building form;
- 3. modulation in the facade, horizontally or vertically;
- 4. articulation of building entrances and openings; and
- 5. corner treatments to address both street frontages.'

PO14

'Design elements promote a subtropical and climate responsive design character through:

- 1. the use of deep verandahs, decks and eaves, and
- 2. integration of buildings within landscape planting.'

PO15

'Roof form assists in reducing the appearance of building bulk by:

- 1. articulating individual buildings;
- 2. incorporating variety in design;
- 3. incorporating a roof pitch, gable or the like in buildings up to 13m; and
- 4. screening plant and equipment, such as vents, lift over-runs or solar energy and storm water collectors.'

The following design elements have been incorporated into the building design:

- Deep recesses on all elevations break up the facade into three distinct vertical sections/blocks. In addition, proposed balconies project forward of the building line to create articulation of the façade and an attractive streetscape.
- A variety of window size and window treatments such as sunhoods and vertical screens create modulation in the facades.
- The corners of the building have been articulated by continuing material and window treatments around the corners. The floor to ceiling elongated windows and brick detail vertical panels at the corners also ensures large blank walls on the side elevations are avoided.
- Planter boxes have been integrated into the design to soften the built form and also to provide additional screening on the terraces of the top floor. Landscaping along the boundaries at ground level include a mix of palms, red head (Acmena smithii), lilly pillies, resilience, emerald lustre and golden penda, which will soften the built form on the lower levels and provide shade to communal areas.
- From the street view, the skillion roof design adds an element of interest and provides contrast to the three vertical block sections and horizontal banding of the balconies on the front façade.





• The lift overrun has been centrally located on the top floor level, which ensures it is not visually dominant when viewed from the street and adjoining properties. A condition is recommended to ensure a recessive colour is used on the lift overrun to further reduce any visual domination.

The combination of design elements not only creates a visually interesting building when viewed from the streetscape but also reduces the bulk of the building when viewed from adjoining properties. A variety of materials are proposed to be used including rendered blocks, face brickwork, vertical boards, aluminium flat bar screens and clear glazed balustrading. However, the finishes schedule submitted with the application (see Figure 5) does not correspond with the colours indicated on the plans. It is considered further investigations are required to ensure the colours and materials complement and highlight the various design elements proposed to reduce building bulk. In turn, the colours and materials will contribute to ensuring the proposed multiple dwelling is a visually interesting and attractive building in the streetscape.

A condition is recommended to require the final finishes schedule be approved by Council at conditioned works assessment stage. Overall, it is considered that the proposed design complies with performance outcomes PO13, PO14 and PO15 and therefore achieves overall outcome (h) which seeks to ensure development incorporates architectural styles and elements that reduce the visual impact of the built form.

| BLK1 BLK2 | BLK1 BLOCKWORK WITH RENDER FINISH: COLOUR DULUX MONUMENT OR EQUAL APPROVED. BLK2 BLOCKWORK WITH RENDER FINISH: COLOUR RESENE SEA FOG OR EQUAL APPROVED. | PA | MES HARDIE STRIA 325mm VERTICAL BOARD. INT FINISH - DULUX GREY PAIL OR EQUAL APPROVED. STR1 |
|-----------|--|--|---|
| BRK | FEATURE FACE BRICKWORK PGH BRICKS: STANDARD RANGE FINISH: <i>GULL GREY</i> OR EQUAL APPROVED. | SCR ALUMINIUM ANGLED FLAT BAR HORIZONTAL SCREENING SET AT 50mm SPACINGS . POWDERCOAT FINISH: BLACK. GLZ CLEAR GLAZED BALUSTRADING | FINISHES LEGEND: |

• Figure 5: proposed finishes schedule

<u>Amenity</u>

Performance outcome PO18 is relevant to the assessment of overlooking impacts:

PO18

'Privacy between dwelling units on the site and adjoining sites is achieved by effective building design and the location of windows and outdoor open spaces to prevent overlooking into habitable rooms or private open space areas or through the use of screening devices. Where screening devices are used, they are integrated with the building design.'

The multiple dwelling will be within a distance of 9m and within an angle of 45 degrees of adjoining dwellings.

To protect the privacy of adjoining dwellings the deemed to comply solutions have been adopted. In particular the windows on the side elevations will have heights above 1.5m or vertical screening; while the balconies to the rear and sides will be provided with floor to ceiling vertical screens. As such, the proposal complies with performance outcome PO18.

The following performance outcome is relevant to the assessment of overshadowing impacts:



PO22

Development minimises the extent of shadows on useable private open space or public spaces and provides adequate sunlight to habitable rooms on the site and adjoining.

The applicant submitted shadow diagrams, which compared the shadows cast by a 13m high building (adopting the acceptable outcome) to the shadows cast by the proposed development at 17.6m height. The shadow diagrams show the eastern adjoining property will receive solar access to habitable rooms and private open space between the hours of 9am and 1pm. At 3pm there is an increase of shadows cast by the proposed 17.6m building, however this is limited to the shadows of the pergolas on the top floor. The western adjoining properties will have at least three (3) hours solar access between the hours of 11am to 3pm. The southern properties will be in shadow between the hours of 9am to 3pm to varying degrees. It is noted that the shadows cast are for a building with the deemed to comply height of 13m and the proposed additional height do not increase these shadows, thus do not further reduce sunlight access to the adjoining sites to the south than already anticipated. Considering the above it is determined the proposal complies with performance outcome PO22 of the MDR zone code.

Open space

Performance outcomes PO4 and PO5 are relevant to the assessment of the proposed communal and private open spaces.

PO4

'Developments involving more than 20 dwellings provide sufficient communal open space to:

- 1. create usable, flexible spaces suitable for a range of activities; and
- 2. provide facilities including seating, landscaping and shade.'

PO5

'Development provides private open space that is:

- 1. useable in size and shape to meet the needs of a diversity of potential residents;
- 2. functional and easily accessible from living or common areas to promotes outdoor living as an extension of the dwelling;
- 3. clearly identified as private open space; and
- 4. provides a high level of privacy for residents and neighbours.'

A 225m² area of the ground floor is allocated for communal open space. It is considered the proposed communal open space is sufficient for the development as it includes a range of facilities available for occupants to use such as a BBQ/alfresco area, resort pool, spa pool, partially covered pool deck and powder room. In addition, the dining and entertainment area is an open and flexible space suitable for a range of passive activities. It is considered that the proposal complies with performance outcome PO4 of the medium density residential zone code.

In terms of private open space, it is noted that all units above ground level are provided with at least $16m^2$ of private open space with a minimum dimension of 3m. The four top floor (L4) units will have private open spaces ranging from $20m^2$ to $50m^2$ with a minimum dimension of 3m.

The private open space of the two ground floor units will range from 46m² to 78m² and have a minimum dimension of 4m. All units will be provided with private open space that achieves the deemed to comply provisions. As such the proposal complies with performance outcome PO5.

Onsite parking

Performance outcome PO8 of the transport, servicing, access and parking code is relevant to the assessment of proposed onsite parking:

P08

'On-site vehicle parking:

- 1. is clearly defined, safe and easily accessible;
- 2. accommodates a sufficient number of vehicles, having regard to:
 - a. the type and size of development;
 - b. expected resident, employee and customer movements;
 - c. the location of the use;
 - *d.* the capacity of the existing road network to accommodate on-street parking; and
 - e. access to public transport;
- 3. includes dedicated parking spaces for people with a disability, motor cycles and bicycles.'

The proposed parking complies with performance outcome PO8 for the following reasons:

- A minimum of two parking spaces are provided onsite for each residential unit.
- Four onsite parking spaces (including one disabled parking space) are provided for visitors. On street parking is also available along the site frontage.
- All parking spaces comply with the minimum dimensions required by the Australian Standard 2890.1 Parking Facilities.
- The proposal promotes active modes of transport with the provision of 27 bike parking spaces for residents and eight bike racks for visitors to use.
- Alternative modes of transport such as public transport are available within close proximity. The site is within 160m walking distance to the bus stop for bus routes 274 and 258. Route 274 has 12 return services during the week starting from 6am to 6.15pm while on Saturday there are 11 return services. Route 258 provides 15 return services between the Cleveland town centre and Toondah Harbour during the week and on Saturdays.

Stormwater management

A site based stormwater management plan was submitted by the applicant to address the stormwater quality requirements of performance outcomes PO9 and PO10 of the Healthy waters code:

PO9

'Development protects and does not adversely impact the environmental values or water quality of receiving waterways.'

PO10

'The entry to and transport of contaminants in stormwater or waste water is avoided.'

The proposed treatment measures include two litter baskets and two storm pits (class 2), which have been verified through MUSIC modelling to achieve the pollutant reduction targets specified in acceptable outcome AO9.1 of the healthy waters code. The conceptual civil drawings submitted do not identify the location and type of all proposed treatment measures. It is necessary that this information be provided on detailed drawings to be submitted as part of conditioned works



assessment. To ensure the proposal complies with PO9 and PO10 of the healthy waters code, a condition is recommended to ensure detailed stormwater management plans are submitted to Council at conditioned works assessment stage.

Servicing and manoeuvring areas

Performance outcomes PO3 and PO20 of the Transport, servicing, access and parking code is relevant to the assessment of the proposed servicing and manoeuvring areas:

PO3

'Development maintains or improves the safe and efficient operation of transport networks having regard to (amongst other things):

- 1. the existing or planned function of the roads affected;
- 2. available sight distances and the location and design of access points;
- 3. accessibility by public transport, pedestrians and cyclists;
- 4. the potential for conflict between vehicles, pedestrians and cyclists;
- 5. the loss or increase of on-street parking;
- 6. the location, construction and maintenance of utility infrastructure; and
- 7. the nature and intensity of traffic and parking generated by the development.'

PO20

'Servicing and manoeuvring areas are located and designed to:

- 1. be clearly defined, safe and easily accessible;
- 2. be separated from areas of pedestrian movement within the premises or on adjoining premises;
- З. provide for the vehicle dimensions and turning paths for the design vehicles expected to access the site;
- 4. maintains clear access to waste containers for collection vehicles;
- 5. ensures that service vehicles entering a site do not queue across footpaths or onto external roads; and
- 6. prevents any manoeuvring occurring within the defined queuing area.'

An updated swept path assessment was provided in support of the proposal to have the refuse collection truck reverse into the site with exit in the forward gear. The applicant's traffic response noted the following:

- 'From the traffic report submitted for the Development Application for the adjacent site (CRG, 24 August 2012), in 2012 the two way peak hour traffic volume on Middle Street in the vicinity of the site was some 300 vehicles per hour. This is not a particularly high traffic volume that would preclude a vehicle reversing onto the site.
- The sight lines along Middle Street at the site access are in excess of design requirements.
- *Reversing on to a site maintains good visibility whilst entering, with exit then in a forward gear.*
- The roundabout to the west of the site requires all traffic travelling along Middle Street to slow. ٠ As such, traffic speeds at the site access are not expected to be particularly high.'



The proposal has demonstrated that the proposed service areas comply with performance outcomes PO3 and PO20 in terms of traffic generation from the proposed development, service areas being clearly defined and separated from pedestrian/vehicle access. However the traffic response has not considered the increase in traffic generation on Middle Street from the future Toondah Harbour priority development area and how this would affect the ability of refuse trucks to safely reverse onto the site.

To ensure compliance with performance outcome PO3, it is recommended that a condition be imposed ensuring refuse collection be conducted outside of peak hours for traffic between 10am and 3pm.

Infrastructure Charges

The proposed development is subject to infrastructure charges in accordance with the Adopted Infrastructure Charges Resolution. The total charge applicable to this development is:

Total charge: \$745,027.90

This charge has been calculated as follows in accordance with Council's Adopted Infrastructure Charges Resolution.

Residential Component

| (17 X Multiple Dwelling - 3 or more bedroom X \$30,677.65) | \$521,520.05 |
|--|--------------|
| (13 X Multiple Dwelling - 1 or 2 bedroom X \$21,912.55) | \$284,863.15 |
| Residential Demand Credit | |
| (2 X Dwelling House - 3 or more bedroom(Area A) X \$30,677.65) | \$61,355.30 |

| Total Council Charge \$745,027.90 |
|-----------------------------------|
|-----------------------------------|

Offsets

There are no offsets that apply under Chapter 4 Part 2 of the Planning Act 2016.

Refunds

There are no refunds that apply under Chapter 4 Part 2 of the Planning Act 2016.

CONCLUSION

The development application conflicts with overall outcome (a) of the heritage overlay code and overall outcome (f) of the medium density residential zone code. On balance, considering the relevant matters the proposal is considered to be acceptable for the subject site. Therefore the application is recommended for approval subject to conditions.

STRATEGIC IMPLICATIONS

Legislative Requirements

The Development Application has been assessed in accordance with the Planning Act 2016.

Risk Management

Standard development application risks apply. In accordance with the *Planning Act 2016* the applicant may appeal a condition of approval or a decision to refuse the application.

Financial

Should an appeal be filed against the decision of Council, subsequent legal costs will apply.

People

There are no implications for staff associated with this report.

Environmental

Environmental impacts are discussed in the 'Issues' section of this report where relevant.

Social

Social impacts are discussed in the 'Issues' section of this report where relevant.

Human Rights

There are no known human rights implications associated with this report.

Alignment with Council's Policy and Plans

The assessment and officer's recommendation align with Council's policies and plans as described within the 'Issues' section of this report.

CONSULTATION

| Consulted | Consultation Date | Comments/Actions |
|-----------------------|----------------------|--|
| Division 2 Councillor | 19 April 2021 | The application was referred to the divisional Councillor as per standard procedure. |

OPTIONS

Option One

That Council resolves to issue a development permit for material change of use for a multiple dwelling and building work for demolition of a local heritage place (dwelling house), on land described as Lot 1 on RP66399 at 217 Middle Street, Cleveland and Lot 2 on RP66399 at 219 Middle Street, Cleveland, subject to the conditions in Attachment 3.

Option Two

That Council resolves to issue a development permit for material change of use for a multiple dwelling and building work for demolition of a local heritage place (dwelling house), on land described as Lot 1 on RP66399 at 217 Middle Street, Cleveland and Lot 2 on RP66399 at 219 Middle Street, Cleveland, without conditions or subject to amended conditions (amended conditions will need to be established).

Option Three

That Council resolves to refuse the application for material change of use for a multiple dwelling and building work for demolition of a local heritage place (dwelling house), on land described as Lot 1 on RP66399 at 217 Middle Street, Cleveland and Lot 2 on RP66399 at 219 Middle Street, Cleveland (grounds of refusal will need to be established).



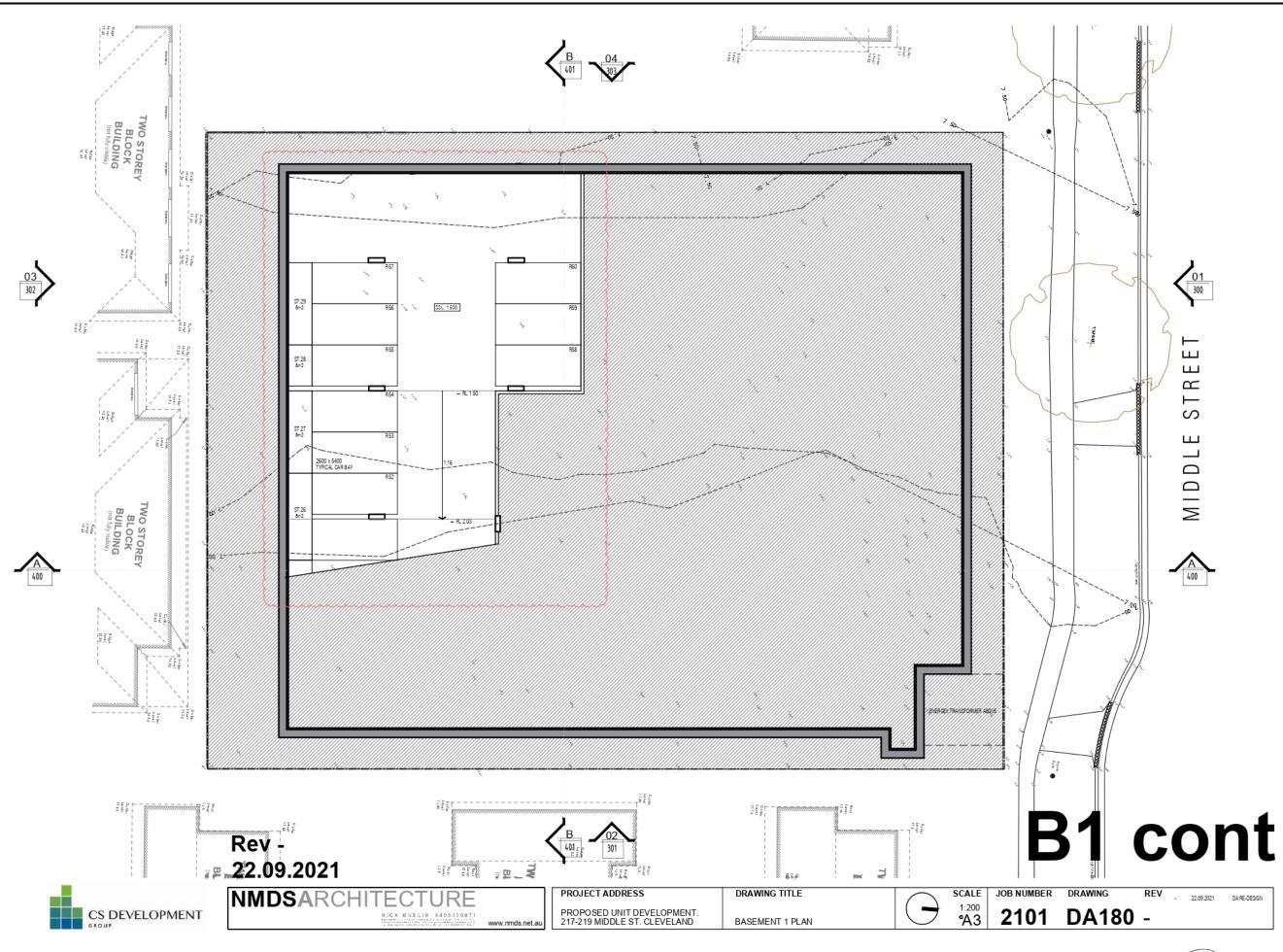
OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/273

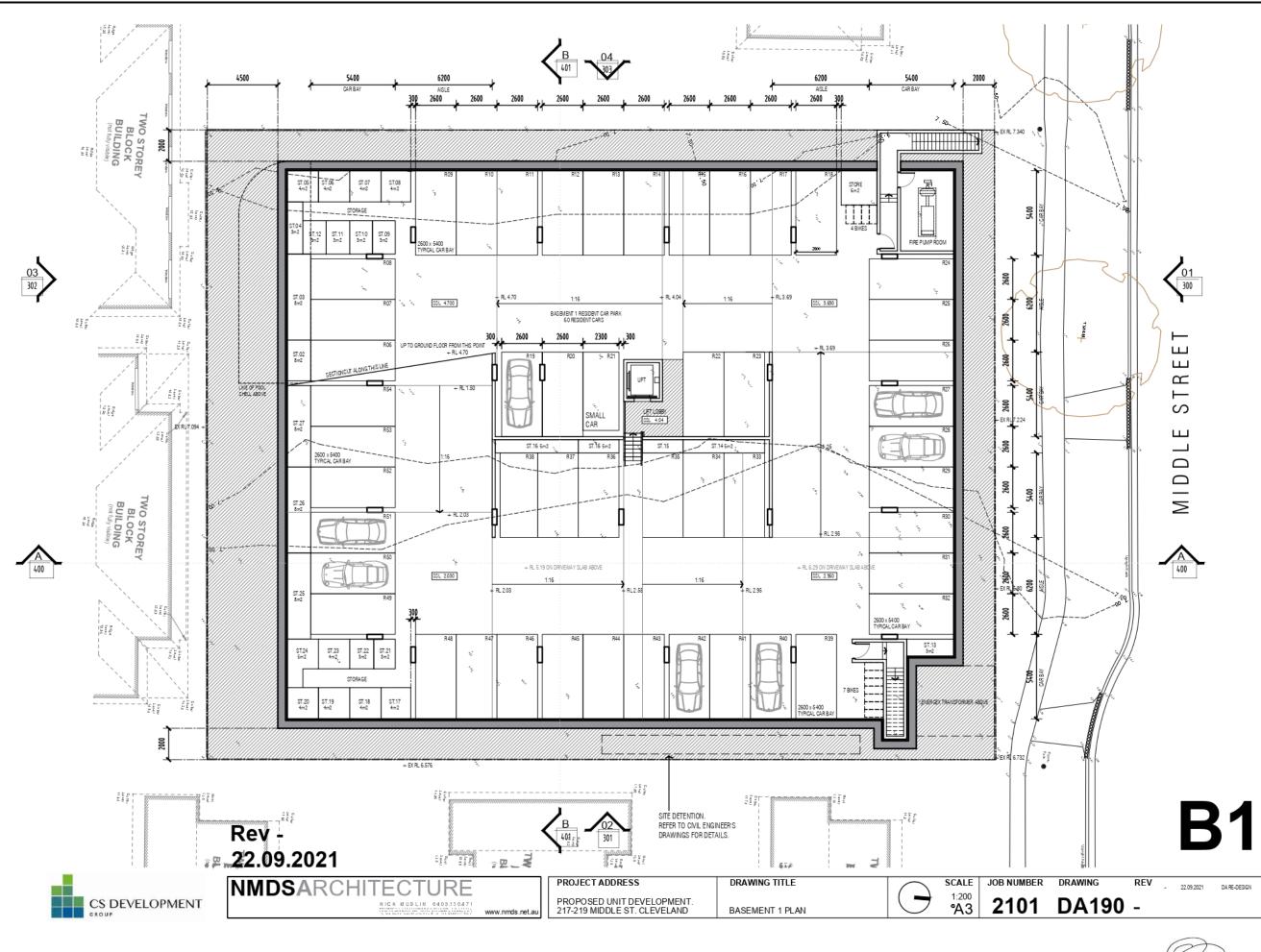
Moved by:Cr Peter MitchellSeconded by:Cr Rowanne McKenzie

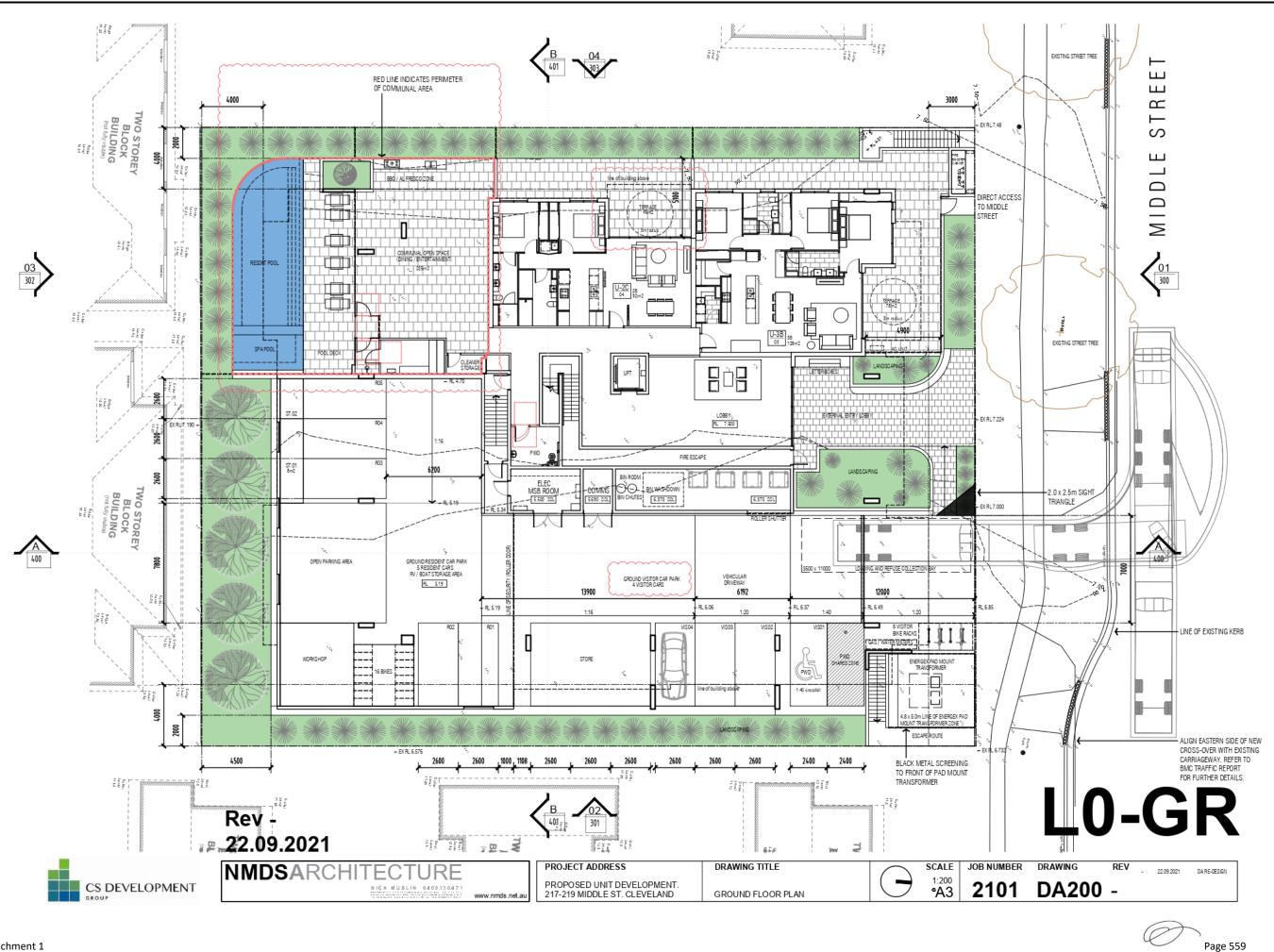
That Council resolves to issue a development permit for material change of use for a multiple dwelling and building work for demolition of a local heritage place (dwelling house), on land described as Lot 1 on RP66399 at 217 Middle Street, Cleveland and Lot 2 on RP66399 at 219 Middle Street, Cleveland, subject to the conditions in Attachment 3.

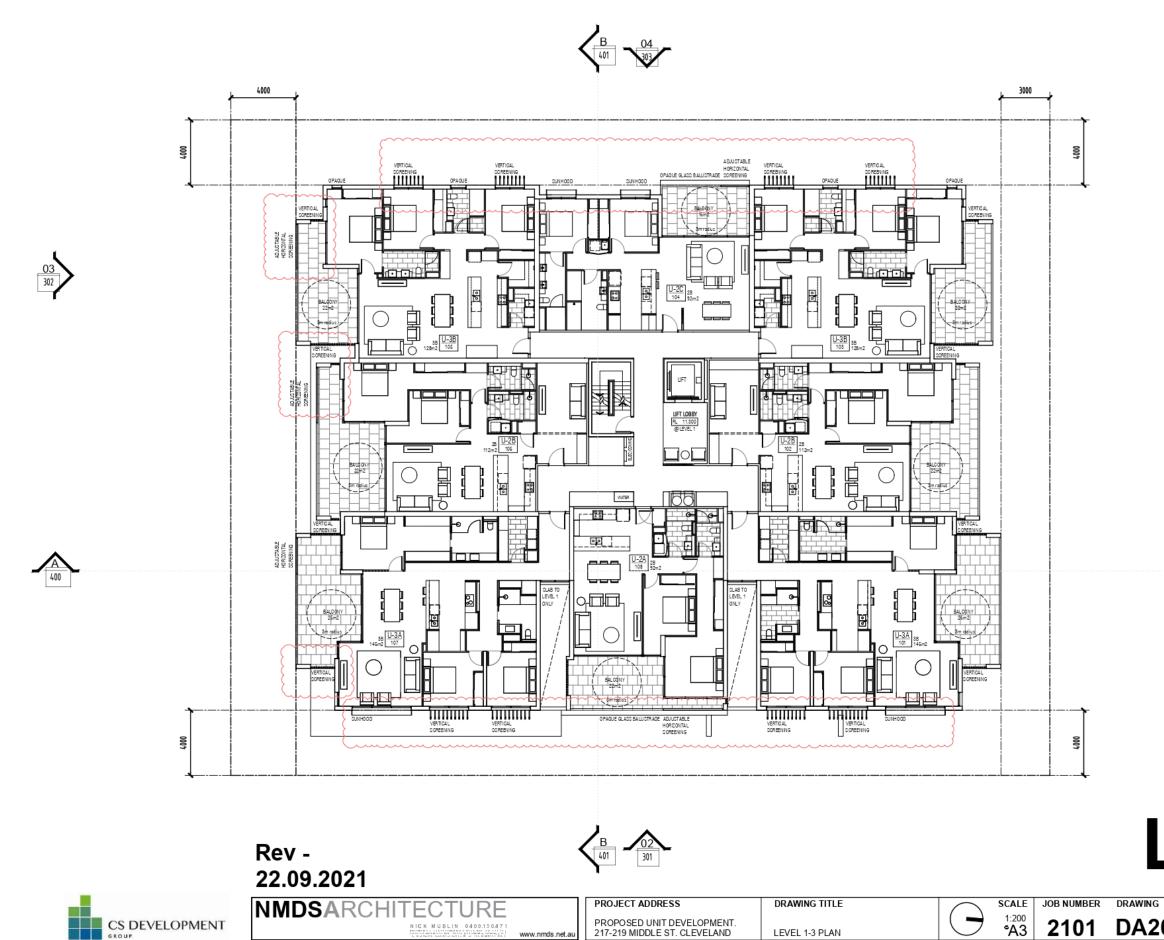
CARRIED 11/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.









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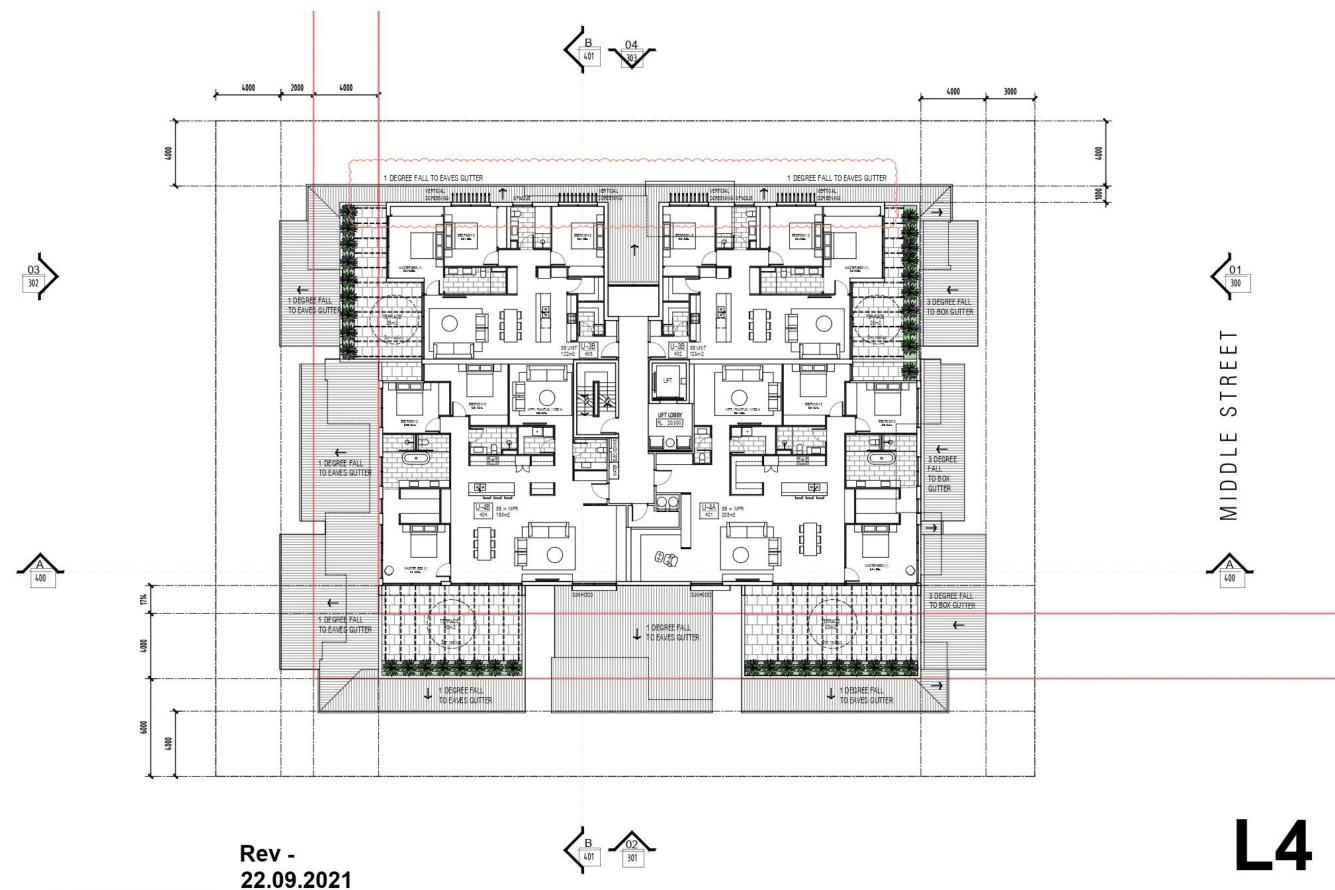
LEVEL 1-3 PLAN











PROJECT ADDRESS

www.nmds.net.

PROPOSED UNIT DEVELOPMENT. 217-219 MIDDLE ST. CLEVELAND DRAWING TITLE

LEVEL 4 PLAN

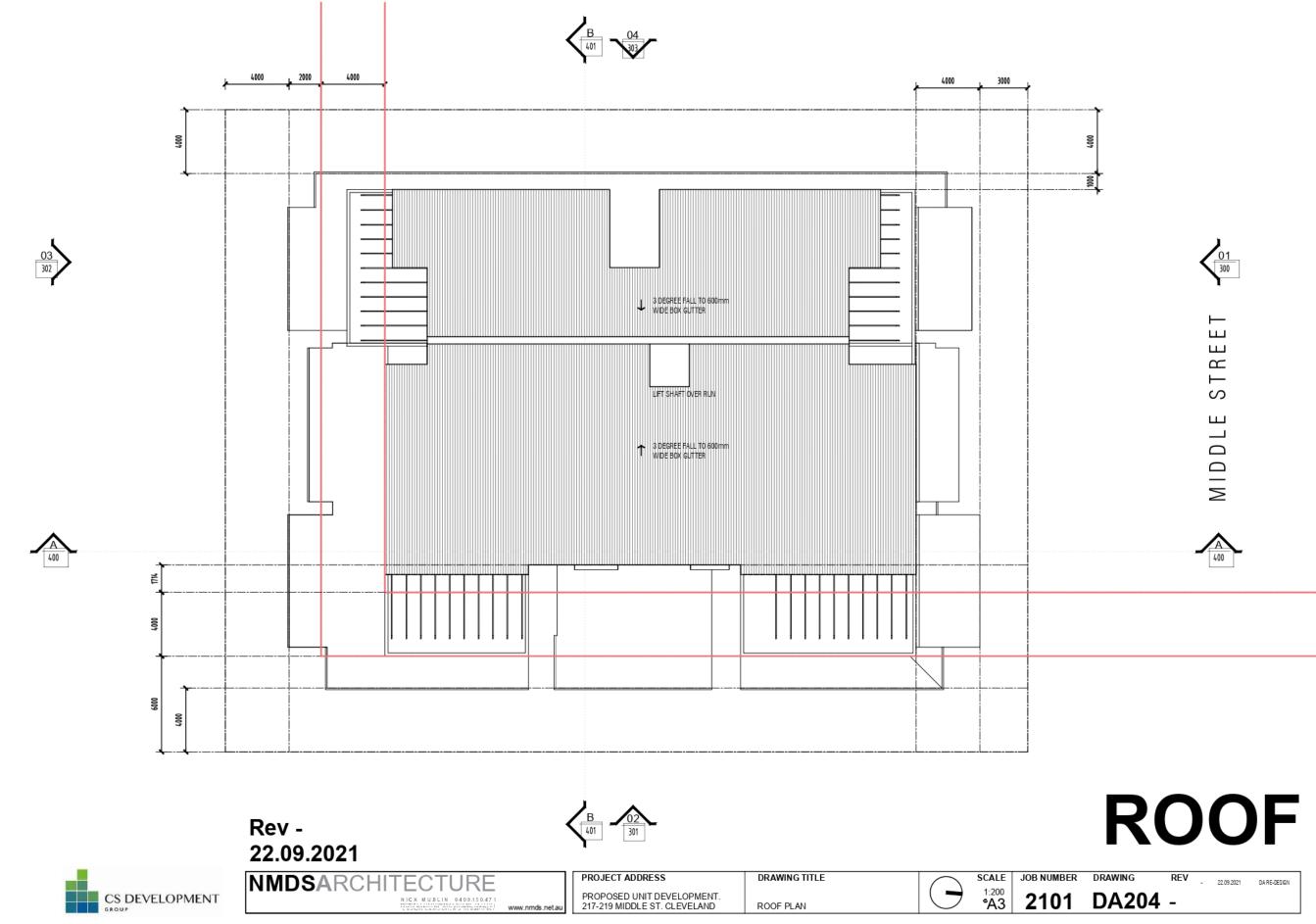
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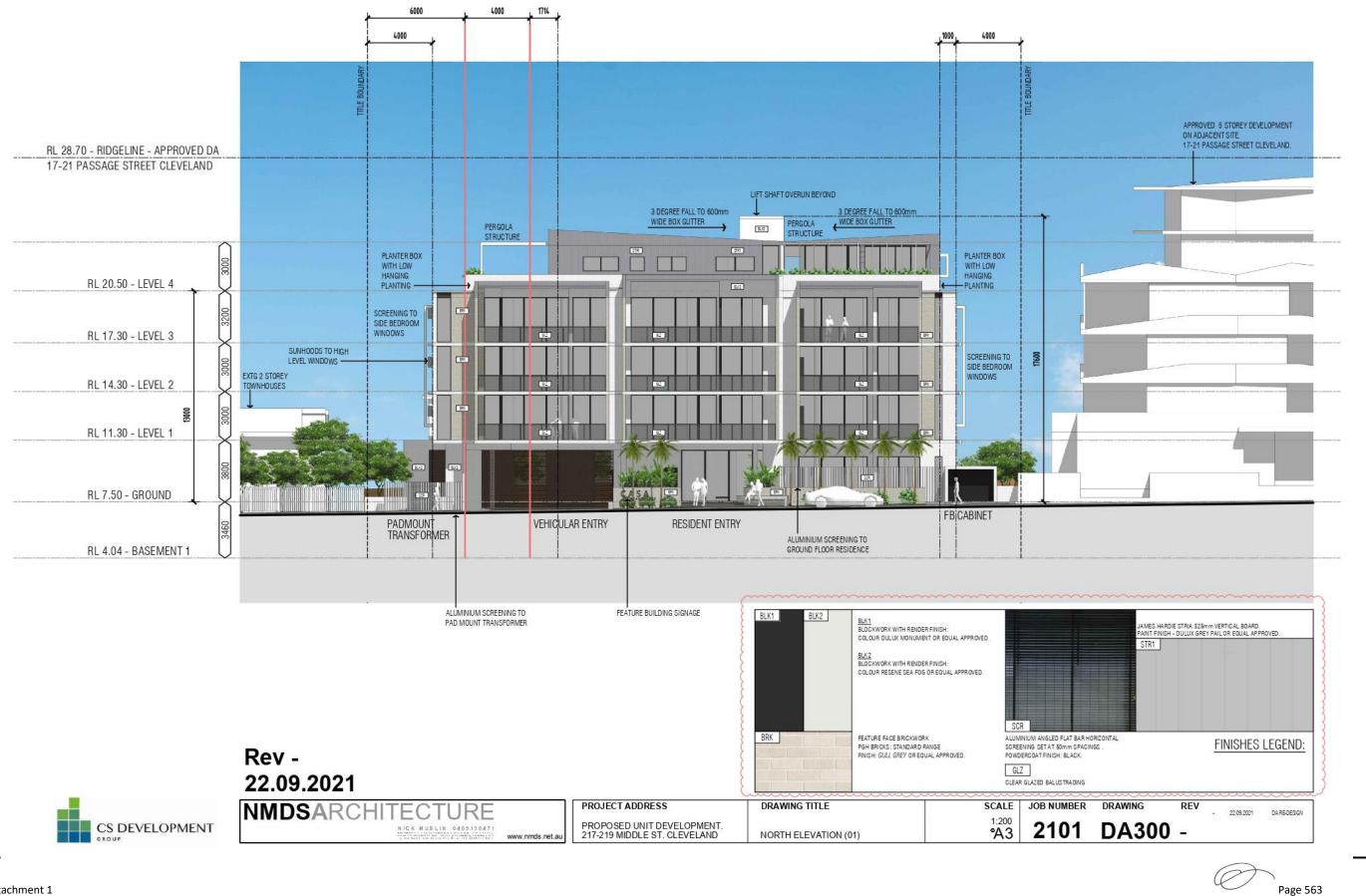
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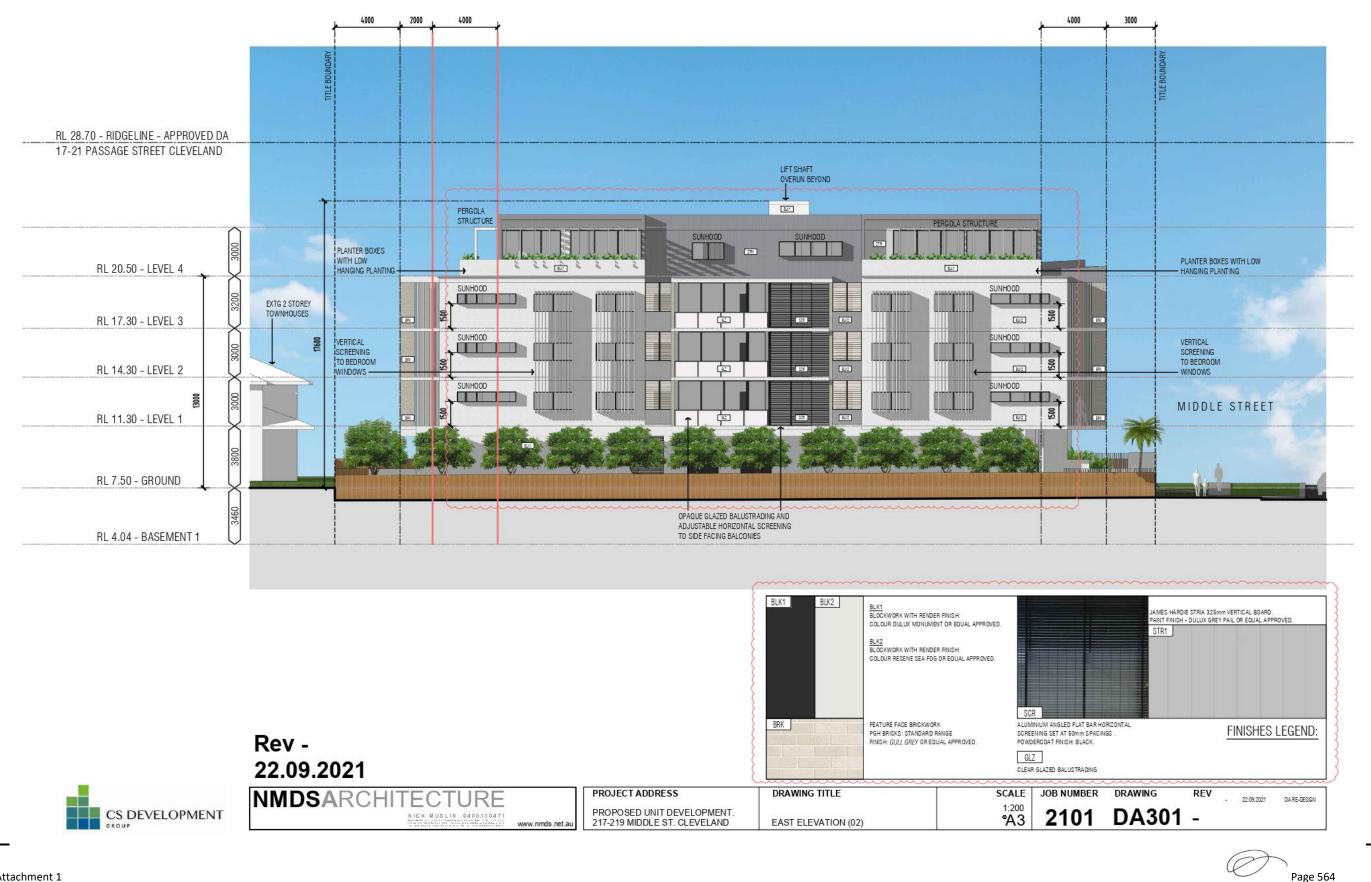


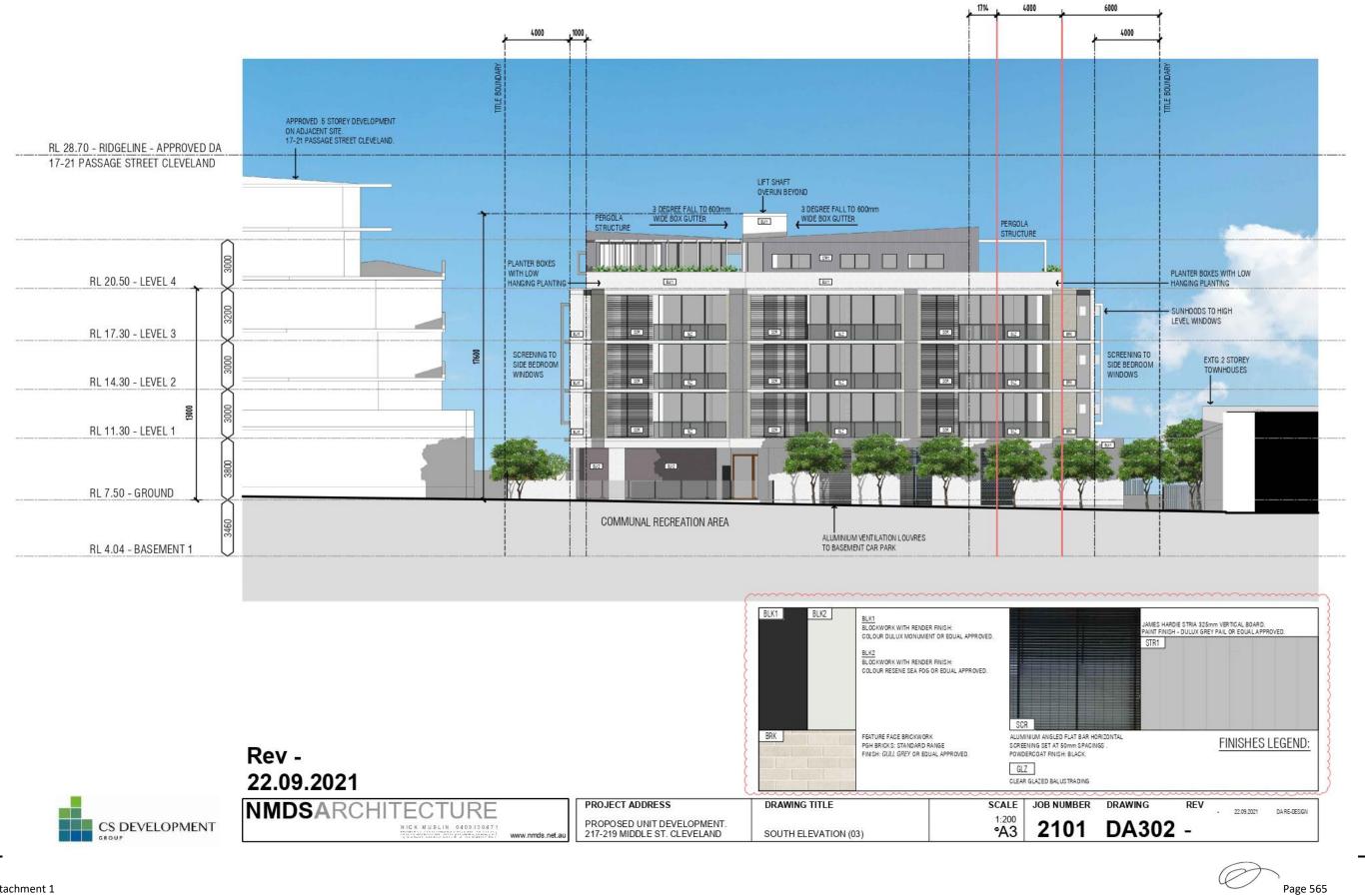
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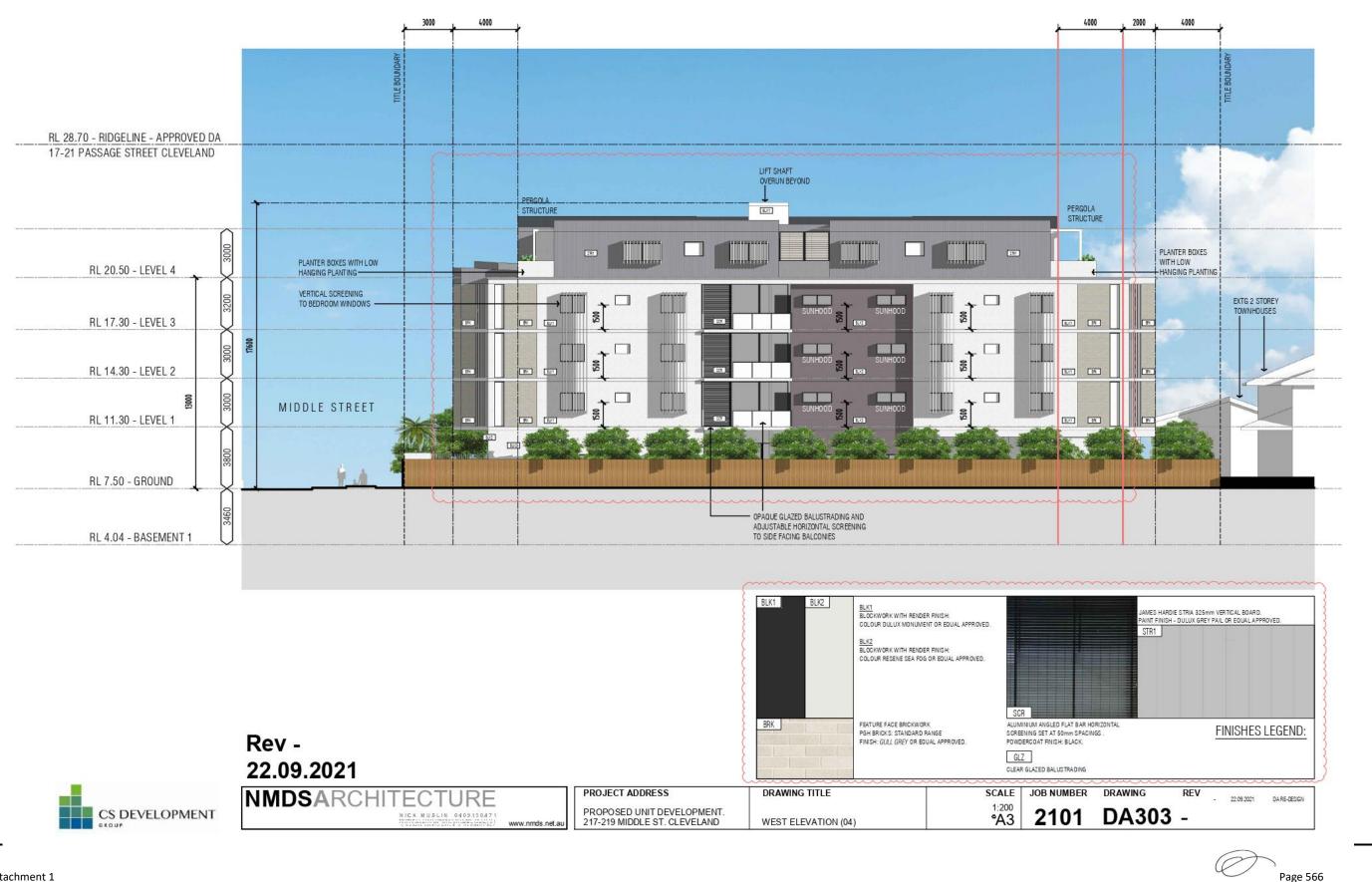


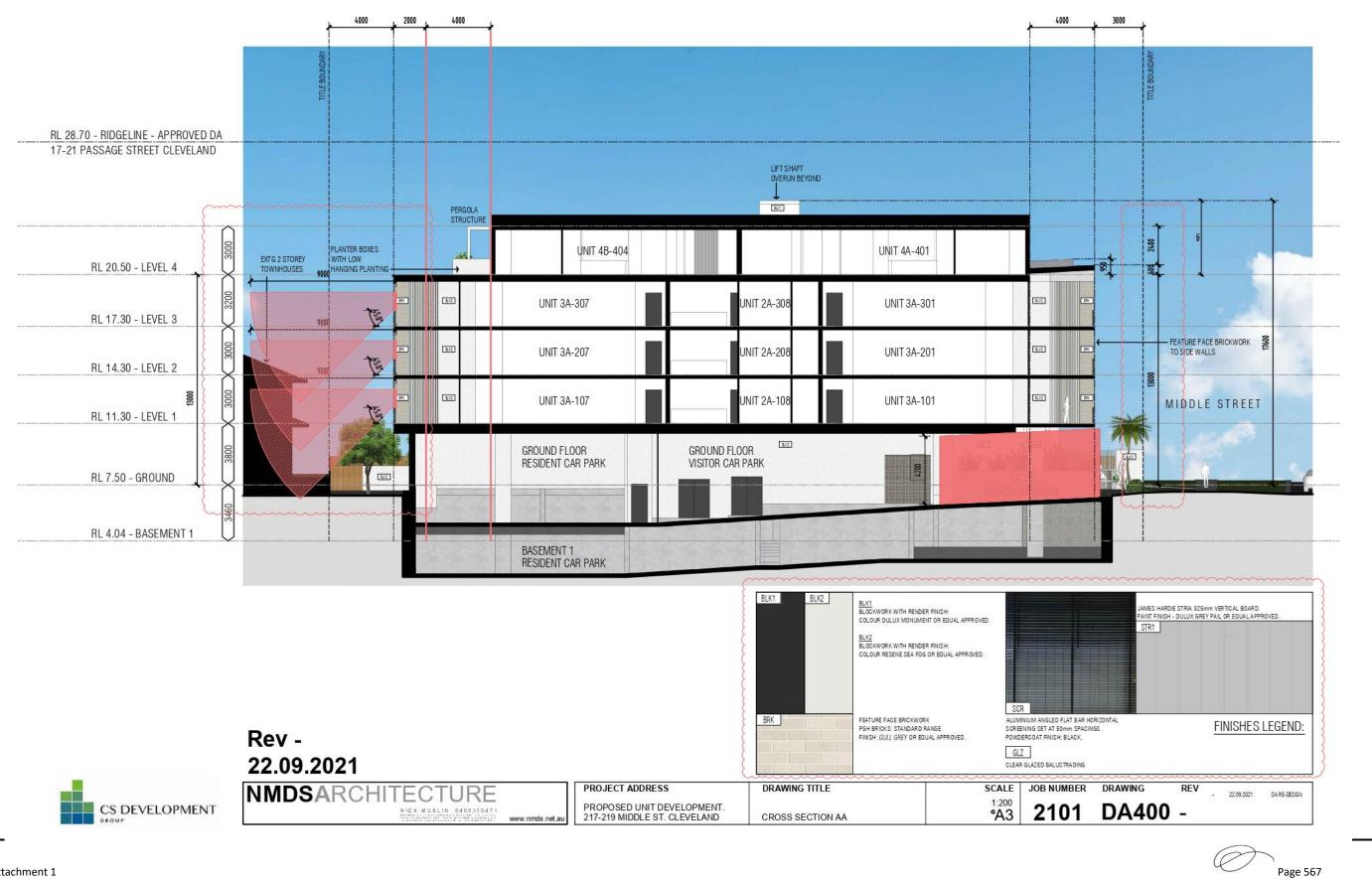


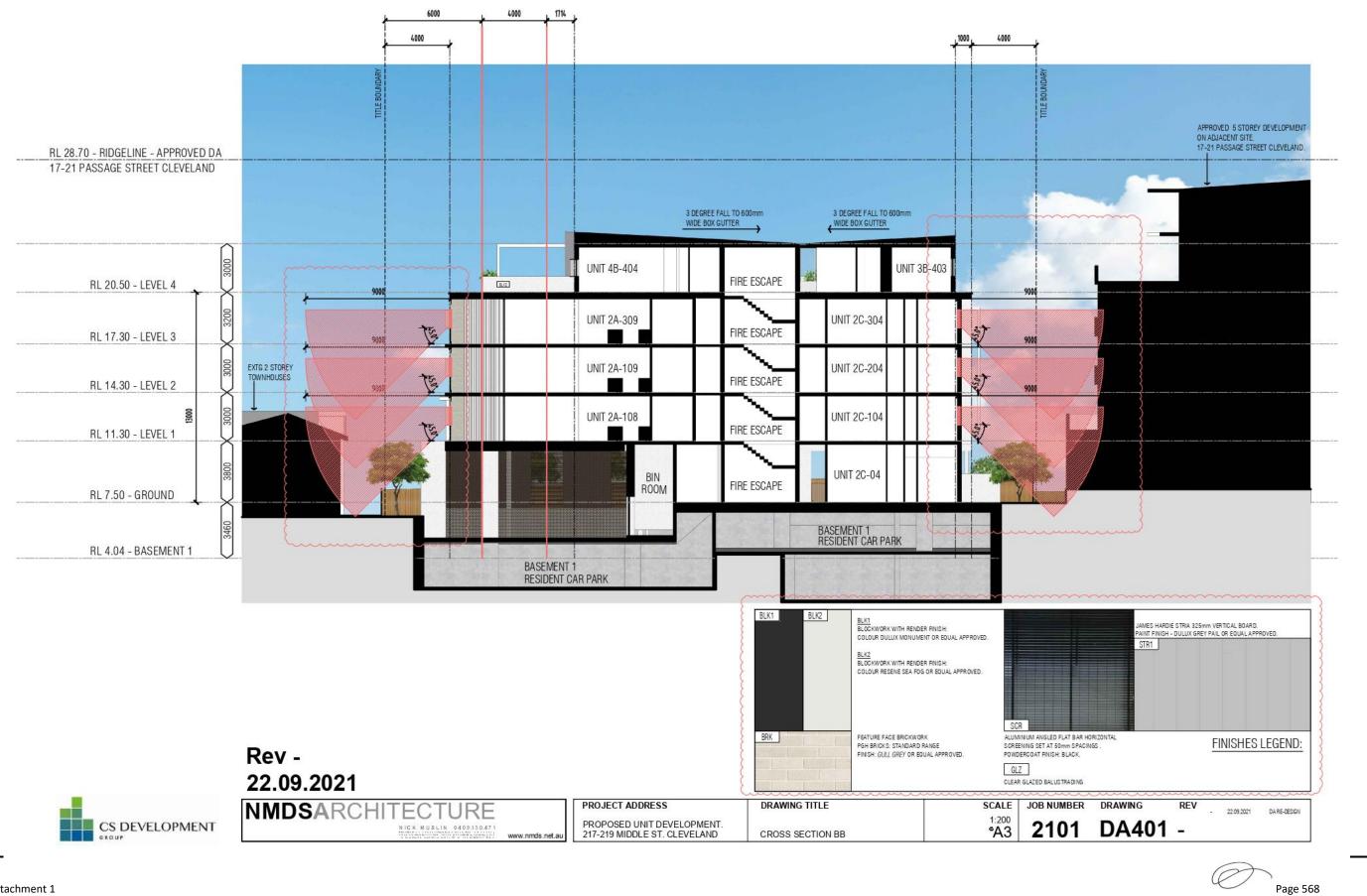




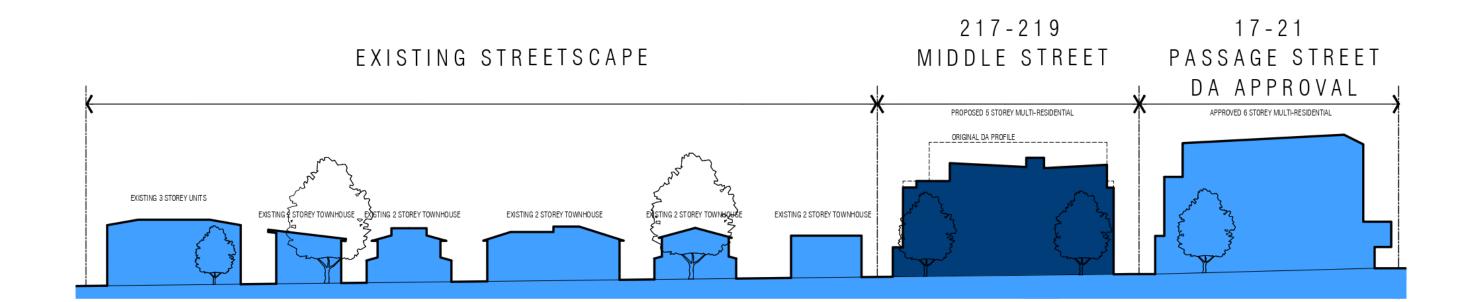








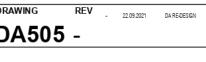




Rev -22.09.2021



| NICK MUBLIN 40 81 30471 PROPOSED UNIT DEVELOPMENT. STREETSCAPE SECTION 1:500 2101 D/ | NMDSARCHITECTURE | PROJECT ADDRESS | DRAWING TITLE | SCALE | JOB NUMBER | DRAW |
|--|------------------------|-----------------|---------------------|-------|------------|------|
| | NICK MUSLIN 0403130471 | | STREETSCAPE SECTION | ¢∆3 | 2101 | DA |













Item 14.2- Attachment 2

| | ASSESSMENT MANAGER CONDITIONS | TIMING | |
|-----|--|--|--|
| 1. | Comply with all conditions of this approval, at no cost to Council, at the timing periods specified in the right-hand column. Where the column indicates that the condition is an ongoing condition, that condition must be complied with for the life of the development. | At the timings specified. | |
| App | proved plans and documents | | |
| 2. | Undertake the development in accordance with the approved plans and documents referred to in Table 1, subject to the conditions of this approval and any notations by Council on the plans. | Prior to the use commencing and ongoing. | |

| Plan/document title | Reference numbe | er | Prepared by | Plan/doc. date |
|---------------------------|--------------------------|-----|----------------------|----------------|
| Basement 1 plan (B1 cont) | DA180, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Basement 1 plan (B1) | DA190, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Ground floor plan (LO-GR) | DA200, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Level 1- 3 plan (L1-3) | DA201, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Level 4 plan (L4) | DA202, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Roof plan | DA204, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| North elevation (01) | DA300, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| East elevation (02) | DA301, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| South elevation (03) | DA302, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| West elevation (04 | DA303, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Cross section AA | DA400, J No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Cross section BB | DA401, J No.2101 | lob | NMDS Architecture | 22/09/2021 |
| Streetscape section | DA505, Ja No.2101 | ob | NMDS Architecture | 22/09/2021 |
| Bulk earthworks plan | Dwg No. 1254568 SK001 | 34- | GHD | 12/04/2021 |



| Landscape Concept Plan – Ground Level | 2023-L-SD01, Rev 02 | AS design | Received by Council 24/09/2021 |
|--|-----------------------------------|---|--------------------------------------|
| Landscape Concept Plan – Level 1-3 | 2023-L-SD02, Rev 02 | AS design | Received by Council 24/09/2021 |
| Landscape Concept Plan – Level 4 | 2023-L-SD03, Rev 02 | AS design | Received by Council 24/09/2021 |
| Landscape Concept Plan – Proposed Plant Palette | 2023-L-SD04, Rev 02 | AS design | Received by Council 24/09/2021 |
| Landscape Concept Plan – Proposed Plant Palette | 2023-L-SD05, Rev 02 | AS design | Received by Council 24/09/2021 |
| 217-219 Middle street, Cleveland - noise assessment | Ref: 13581, Rev 1 | Assured Environmental | 07/04/2021 |
| Geotechnical investigation – unit development (report) | 1-23622, 2021-03- 29, BR VER 1 | Soil Surveys | 30/03/2021 |
| Site based stormwater management plan (report) | 12545684 | GHD | April 2021 |
| Traffic engineering response to information request | BMC21091 | BMC Traffic / Beth Meehan Consulting Pty Ltd | 04/06/2021 |
| 219 Middle Street Cleveland – heritage impact statement | 210507 | Ivan McDonald Architects | August 2021 |

Table 1: Approved plans and documents

<u>Heritage</u>

| 3. | Submit to Council, a detailed Archival Recording of the place by a qualified heritage consultant (Member of International Council on Monuments and Sites – M.ICOMOS). The archival recording must establish a 3D model and be in a format that can also be used in association with the Interpretation Plan to ensure the local heritage values of the place is captured for future generations. | Prior to any demolition works commencing. |
|----|--|---|
| 4. | Submit to Council, a Heritage Interpretation Plan, prepared by a qualified heritage consultant (M.ICOMOS). | Prior to any demolition works commencing. |
| | The Heritage Interpretation Plan must establish appropriate options to interpret the history and use of the place, to ensure the local heritage values are presented within the approved development. | 0 |

| Des | ign | | |
|-----|---|---|--|
| 5. | Demolish all existing structures on the site in accordance with the approved plan(s). Cap all services prior to demolition commencing. | Prior to the use commencing. | |
| 6. | Submit to Council, and receive Conditioned Works approval for, a finishes schedule that ensures materials and colours are utilised on all facades to create visual interest and visually reduce the bulk of the approved building. Ensure the materials and colour contribute and highlight the following: articulation of corners of the building; modulations, projections and the varying heights of the building to be highlighted and emphasised; avoidance of unarticulated blank walls and monotonous building materials and colour; addition of contrasting colours and materials to vertical screening; the use of recessive colours on the lift shaft overrun. | As part of request for conditioned works assessment | |
| 7. | Submit certification to Council from a licensed surveyor, at the stages of building construction listed below, that floor levels and maximum overall height of the building are in accordance with the development approval. All levels must be provided to Australian Height Datum (AHD). a) At completion of the basement level to demonstrate that the building complies with the approved plans at that stage; | At the buildin stages specified i the condition. | |
| | and b) At completion of the slab level to demonstrate that the building complies with the approved plans at that stage; and c) After completion of the construction of the building, but prior to the issue of the Certificate of Classification or Final Building Approval, to demonstrate that the highest point of the building complies with the approval. | | |
| 8. | Comply with the following requirements where it is proposed that habitable room windows above the ground storey will be within a distance of 6m, and within an angle of 45 degrees, and directly adjacent to habitable rooms of neighbouring dwelling units: | Prior to the us commencing. | |

| a |) Provide sill heights or | a minimum of 1.5m above floor levels; | | |
|--|---------------------------------|--|--|--|
| b | | aslucent, such as frosted or textured rt of the window less than 1.5m above | | |
| c) | 25% openings, 50mm, and that | | | |
| minimise the potentia neighbouring premises | | tall outdoor lighting, where required, to I for light spillage to cause nuisance to , in accordance with Australian Standard of the obtrusive effects of outdoor | Prior to the use commencing and ongoing. | |
| Conditi | ioned works assessme | ent | | |
| | | receive approval for, Conditioned Works cuments and works referred to in Table | Prior to site work commencing. | |
| Docum | ent or works | Assessment criteria | | |
| Stormwater management plan | | City Plan Healthy Waters code City Plan Infrastructure Works code City Plan planning scheme policy 2 – Infrast Water Sensitive Urban Design Technical G | | |

| | City Plan planning scheme policy 2 – Infrastructure Works |
|---------------------------|---|
| | Water Sensitive Urban Design Technical Guidelines for South East Queensland |
| | State Planning Policy July 2017 |
| | Queensland Urban Drainage Manual |
| Water supply and sewerage | • South East Queensland (SEQ) Water Supply and Sewerage Design and Construction code |
| | City Plan Infrastructure Works code |
| | City Plan planning scheme policy 2 – Infrastructure Works |
| Access and parking | City Plan Infrastructure Works code |
| | City Plan Transport, Servicing, Access and Parking code |
| | City Plan planning scheme policy 2 – Infrastructure Works |
| | • Australian Standard for parking facilities set (2890:2009) |

| Sediment and erosion control plan/program | City Plan Healthy Waters codeCity Plan Infrastructure Works code |
|---|---|
| | City Plan planning scheme policy 2 – Infrastructure Works |
| | International Erosion Control Association Best Practice Erosion and Sediment Control document |
| Construction management plan | City Plan Healthy Waters code City Plan Infrastructure Works code |
| | City Plan Infrastructure Works code City Plan planning scheme policy 2 – Infrastructure Works |
| Landscape plan | City Plan planning scheme policy 2 – Infrastructure Works City Plan landscape code |
| Architectural plans (finishes schedule) | Performance outcome PO13 of the medium density residentia zone code |

Table 2: Conditioned works assessment

| 11. | Comply with all conditions and approved plans in the Conditioned Works Approval. | Prior to site works commencing. |
|-----|--|--|
| Bon | ds | |
| 12. | Lodge with Council the bonds listed in Table 3: | Prior to requesting a pre-start meeting or works commencing, whichever is the sooner. |

| Bond item | Amount | Returned |
|----------------------------|---------|--|
| Road cleaning bond | \$2,000 | When works accepted on maintenance by Council. |
| Road opening approval bond | \$5,000 | When works accepted on maintenance by Council. |
| TOTAL | \$7,000 | |

Table 3: Bonds

| 13. | Lodge a defects liability bond to the greater value of either: | Prior to contributed asset |
|-----|--|-------------------------------------|
| | a) 10% of the contributed asset(s); orb) \$2,500. | being accepted on maintenance by |
| | This bond will be returned after formal acceptance by Council of the contributed asset(s) off maintenance and the transfer of the applicable works to Council. | Council. |

Inspections

14. Arrange with Council for the following inspections to be carried out at the relevant time in accordance with Table 4: Inspections in table 4. below.

| Inspection | Timing |
|------------------------------|---|
| Pre-start | Prior to any works commencing. |
| Erosion and sediment control | Immediately after installation of erosion and sediment control measures. |
| Driveway crossover/footpath | Box inspection to be undertaken with reinforcing mesh in place and supported on bar chairs prior to the concrete being poured. |
| Stormwater bedding | After the stormwater pipelines are bedded and prior to backfilling. |
| On maintenance | On completion of all civil and landscaping works to be transferred into public ownership as required by this approval and its conditions and prior to the commencement of the 12 months maintenance period. |
| Compliance inspection | On completion of the development in accordance with the approval and its conditions. |
| Off maintenance | At the end of the minimum 12 months maintenance period. |

Table 4: Inspections

For the pre-start, on maintenance/compliance and off maintenance inspections, at least **five (5) business days**' notice must be given to Council. For all other inspections, a minimum of **24 hours**' notice must be given to Council.

The contributed assets must be accepted on maintenance and the development must pass a Compliance Inspection before the issue of a Certificate of Classification.

<u>Note</u>: The Civil Consulting Engineer should inspect the works and satisfy themselves that the works are satisfactory prior to booking the respective inspections. In instances where Council's representative(s) fails an inspection, Council will charge a re-inspection fee prior to re-visiting the site. The cost of this re-inspection is identified in Council's Register of Fees and is reviewed each financial year.

| Gen | eral | |
|-----|--|--|
| 15. | Provide details to Council of the nominated Principal Contractor, including copies of the Principal Contractor's workcover and public liability currency certificates. The public liability insurance policy must be a minimum of ten million dollars and must indemnify Redland City Council. | Prior to requesting a pre-start meeting or works commencing, whichever is the sooner. |
| 16. | Submit to Council a copy of the proposed Program of Civil Works. | Prior to requesting a pre-start meeting |



| | | or works commencing, whichever is the sooner. |
|-----|---|---|
| 17. | Provide temporary drainage during the building construction phase such that discharge from all constructed roofs and paved areas is disposed of to a lawful point of discharge in accordance with the Queensland Urban Drainage Manual (QUDM). Maintain the temporary system for the duration of the building works. | During construction. |
| 18. | Notify Council within 24hrs and rectify, in consultation with Council, any damage to Council infrastructure as a result of construction activities, at no cost to Council. | As soon as practical following identification of the damage. |
| 19. | Provide written certification from a Registered Professional Engineer Queensland (RPEQ) certifying that all civil works have been completed in accordance with the approved drawings and specifications and to the applicable Australian Standards. | Prior to on maintenance or the use commencing, whichever is the sooner. |
| 20. | Undertake the development works so that there is no risk to public safety at any time on the site, adjacent public land, road reserve or private property. Should the site be unattended or abandoned, public safety must still be maintained. | During construction phase. |
| 21. | Rectify any damage done to the road verge during construction, including topsoiling and re-turfing. | Prior to the use commencing. |
| 22. | Maintain all contributed assets for a minimum period of 12 months from the date the works are accepted on maintenance by Council. The works will be accepted off maintenance only where the works have been suitably maintained to any manufacturer's specifications and Council standards and are fit for purpose. | During the on maintenance period. |
| Roa | dworks | |
| 23. | | |



| | Submit to Council for approval, engineering plans and details showing the following frontage works are in accordance with the assessment criteria listed in Table 2: Conditioned Works Assessment of this approval: | As part of request for conditioned works assessment. |
|-------|---|--|
| | a) Footpath earthworks, topsoiling and turfing of all disturbed footpath areas. b) Reinstatement of concrete kerb and channel where required. c) Removal of all redundant vehicle crossovers. d) Entry treatment/access to the site. e) Adjustment and relocations necessary to public utility services resulting from these works. f) A minimum 7m wide type R-RSC-2 permanent vehicular crossover to the Middle Street frontage of the site. | |
| | Submit and have approved by Council a Road Opening Approval for any works being undertaken within the road reserve. Provide the following to Council as part of the application: a) A completed application form and associated fee, at the rate applicable at the time of payment. The current rate for the 2020/2021 Financial Year is: | Prior to requesting a pre-start meeting or works commencing, whichever is the sooner. |
| | \$5,432.00 – this incorporates a refundable bond of \$5,000 and a non-refundable administration fee of \$432.00. | |
| | b) A copy of the contractor's Workcover insurance currency certificate. | |
| | c) A copy of the contractor's Public Liability insurance currency certificate. The public liability insurance policy must be a minimum of ten million dollars and must indemnify Redland City Council. | |
| | d) Submission of a Traffic Management Plan (TMP) and/or a Traffic Guidance Scheme (TGS) that is prepared and authorised by a person who holds a current DTMR 'Open Level' Traffic Management Design Certification and should include proposed haul routes for construction vehicles associated with the works, as applicable. | |
| Storn | nwater management | |
| 25. | | |

| | Submit to Council, and receive Conditioned Works approval for, a stormwater plan and report that is generally in accordance with the approved stormwater management plan, addressing both quality and quantity and the following: Design of allotment drainage. Detailed drawings of the proposed stormwater quality treatment systems and any associated works. The drawings must identify the location and type of all treatment measures proposed. An electronic copy of the MUSIC model. | As part of request for conditioned works assessment. |
|--------------|---|--|
| 26. | Convey roof water and surface water to underground infrastructure in Middle Street in accordance with the City Plan Planning Scheme Policy 2 – Infrastructure Works. | Prior to the use commencing and ongoing. |
| 27. | Manage stormwater discharge from the site in accordance with the City Plan Planning Scheme Policy 2 – Infrastructure Works, so as to not cause an actionable nuisance to adjoining properties. | Prior to the use commencing and ongoing. |
| <u>Utili</u> | ties | |
| 28. | Pay the cost of any alterations to existing public utility mains, services or installations due to building and works in relation to the proposed development, or any works required by conditions of this approval. Any cost incurred by Council must be paid at the time the works occur in accordance with the terms of any cost estimate provided to perform the works, or prior to plumbing final or the use commencing, whichever is the sooner. | At the time of works occurring. |
| 29. | Connect the development to external reticulated sewer, external reticulated water and underground electricity supply. | Prior to the use commencing. |
| <u>Serv</u> | ices | |
| 30. | Remove any redundant sewerage connections within the site or servicing the development and provide documentary evidence to Council or its delegate that this has occurred. | Prior to site works commencing. |
| 31. | | Prior to on maintenance or |

| Provide water connections and water meters in accordance with Council's Standard Drawings. Provide details to Council of the water meters and their locations. | compliance inspection whichever is th sooner. |
|--|--|
| rosion and sediment control | |
| 2. Design, implement and maintain measures and practices in accordance with "Best Practice Erosion and Sediment Control" published by the International Erosion Control Association (Australasian Chapter) (2008). | During th construction phase. |
| 3. Ensure dewatering activities (such as from excavations or sediment basins) do not discharge water offsite or where the water could reasonably be expected to move offsite or into waters or stormwater, unless the following are met: a) The concentration of total suspended solids (TSS) must not exceed 50 mg/L. b) Turbidity (nephelometric turbidity units - NTU) values must not be greater than 10% of the turbidity of waters immediately upstream of the sites water entry points at time of the release. c) The pH must be within the range of 6.5-8.5 except where, and to the extent, the natural receiving waters lie outside this range. d) Water is to be sampled by experienced personnel and tested by a NATA (National Association of Testing Authorities) accredited laboratory. | During th construction phase. |
| 4. Provide a stabilised construction exit at each exit point for the site. Maintain this area so that no loose debris is deposited on to adjoining roadways. Remove any material brought onto the road as soon as possible. | During th construction phase. |
| Implement dust control measures at each phase of site development and operation in accordance with IECA (2008) Best Practice Erosion and Sediment Control. | During th construction phase. |
| Vaste management | |
| 6. Enter into a written agreement with a waste services provider to provide and maintain a bulk bin collection service to the development. | Prior to the u commencing an ongoing. |

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| 37. | Provide bulk waste and recycle bins on site that cater to the waste generation of the use. Waste disposal and servicing areas must be screened so they are not visible from public places, and do not have adverse amenity impacts on adjoining properties. | Prior to the use commencing and ongoing. |
|-----|--|--|
| | Install a screened refuse storage area, for the storage of a minimum of: | |
| | 1,890L recycling / week (2 x 1,100L bins). 2 x 240L green bins, 2,700L waste / week (3 x 1,100L bins). | |
| | The storage area must be impervious, well drained, provided with a hose cock, enclosed and illuminated for night time use. | |
| 38. | Conduct the collection of refuse outside of peak hours for traffic. Refuse collection is to be conducted between the hours of 10am and 3pm. | Ongoing. |
| Lan | dscaping works | |
| 39. | Submit to Council, and obtain approval for, landscape plans in accordance with the approved concept landscape plan and the assessment criteria listed in Table 2: Conditioned Works Assessment. Include the following: | As part of request for conditioned works assessment. |
| | full planting details complete species list, ensuring that no species listed as declared or non-declared weed species in the Redlands Coast Biosecurity Plan 2018-2023 are used a maintenance plan for the entire landscaping component of the development. details of fencing details of lighting types and locations | |
| | other relevant details as outlined in section 6.1.3 of planning scheme policy 2 – infrastructure works. | |

| 41. | Turf all areas of disturbance within the road verge with turf cut from a weed-free source containing no viable weed seed. | Prior to the use commencing. |
|-------------|---|--|
| 42. | Landscape the site in accordance with the landscape plans approved as part of the conditioned works assessment and maintain these landscaped areas. | Prior to the use commencing and ongoing. |
| 43. | Pay to Council a monetary contribution for street tree planting for 3 street trees. The contribution must be paid at the rate current at the time of payment under Council's Register of Fees. The current rate is \$370 per tree (2021/2022 financial year). | Prior to the use commencing. |
| <u>Surv</u> | rey and as-constructed information | |
| 44. | Submit as constructed drawings and documentation for all works that are to be transferred into public ownership, prepared in accordance with the City Plan Planning Scheme Policy 2 – Infrastructure Works. Include surveyed as constructed data showing works completed (digital and hard copies) and amended design plan data showing construction deviation from design plans (digital and hard copies). The digital data and the design data must be endorsed by a RPEQ and Landscape Architect holding AILA (Australian Institute of Landscape Architecture) membership and a registered surveyor using the certification clauses contained in Planning Scheme Policy 2 – Infrastructure Works. | As soon as all works are completed and prior to the request for on maintenance or compliance inspection, whichever is the sooner. |
| Acce | ess and parking | |
| 45. | Provide 64 car parking spaces in accordance with the approved plans. The total number of car parks must include four (4) visitor parking spaces inclusive of one (1) disability parking space. | Prior to the use commencing and ongoing. |
| 46. | Access to car parking spaces, bicycle spaces, bin bays and driveways must remain unobstructed and available for their intended purpose. | Prior to the use commencing and ongoing. |
| Nois | se la | |
| 47. | Incorporate acoustic attenuation into the development as specified in the approved noise report. | |
| | | |

Prior to the use commencing and ongoing.

ADDITIONAL APPROVALS

The following further **development permits** are necessary to allow the development to be carried out:

- Building works approval.
- Building works demolition
 - Provide evidence to Council that a Demolition Permit has been issued for structures that are required to be removed and/or demolished from the site in association with this development.

Further approvals, other than a development permit, are also required for your development. This includes, but is not limited to, the following:

- Conditioned works assessment as detailed in Table 2 of the conditions.
- Plumbing and drainage works.
- Capping of sewer for demolition of existing buildings on site.
- Road opening permit for any works proposed within an existing road reserve.

ASSESSMENT MANAGER ADVICE

Infrastructure charges Infrastructure charges apply to the development in accordance with the Adopted Infrastructure Charges Resolution (No. 2.3) 2016 levied by way of an Infrastructure Charges Notice. The infrastructure charges are contained in the attached Redland City Council Infrastructure Charges Notice.

Live connections

Redland City Council is responsible for all live water and wastewater connections. Contact **must** be made with Council to arrange live works associated with the development.

Further information can be obtained from Council on 07 3829 8999.

Hours of construction

Please be aware that you are required to comply with the *Environmental Protection Act* in regards to noise standards and hours of construction.



Coastal processes and sea level rise

Please be aware that development approvals issued by Redland City Council are based upon current lawful planning provisions which do not necessarily respond immediately to new and developing information on coastal processes and sea level rise. Independent advice about this issue should be sought.

• Services installation

It is recommended that where the installation of services and infrastructure will impact on the location of existing vegetation identified for retention, an experienced and qualified arborist that is a member of the Australian Arborist Association or equivalent association, be commissioned to provide impact reports and on site supervision for these works.

Fire ants

Areas within Redland City have been identified as having an infestation of the Red Imported Fire Ant (RIFA). Biosecurity Queensland should be notified on 13 25 23 of proposed development(s) occurring in the Fire Ant Restricted Area before earthworks commence. It should be noted that works involving movements of soil associated with earthworks may be subject to movement controls and failure to obtain necessary approvals from Biosecurity Queensland is an offence. It is a legal obligation to report any sighting or suspicion of fire ants within 24 hours to Biosecurity Queensland on 13 25 23. The Fire Ant Restricted Area as well as general information can be viewed on the Department of Agriculture and Fisheries (DAF) website www.daf.qld.gov.au/fireants

Cultural heritage

The Aboriginal Cultural Heritage Act 2003 requires anyone who carries out a land use activity to exercise a duty of care. Further information on cultural heritage duty of care is available on the Department of Aboriginal and Torres Strait Islander Partnerships (DATSIP) website:

https://www.datsip.qld.gov.au/resources/datsima/people-communities/culturalheritage/cultural-heritage-duty-care.pdf

The DATSIP has established a register and database of recorded cultural heritage matters, which is also available on the Department's website:

https://www.datsip.qld.gov.au/people-communities/aboriginal-torres-straitislander-cultural-heritage/cultural-heritage-search-request

Quandamooka Yoolooburrabee Aboriginal Corporation (QYAC) is the registered cultural heritage body in the Redland City local government area. It is recommended you consult with QYAC in relation to aboriginal and cultural heritage matters prior to



the commencement of works on site. QYAC can be contacted on 07 3415 2816 or admin@QYAC.net.au

Should any aboriginal, archaeological or historic sites, items or places be identified, located or exposed during construction or operation of the development, the *Aboriginal and Cultural Heritage Act 2003* requires all activities to cease. Please contact DATSIP for further information.

Fauna protection

It is recommended an accurate inspection of all potential wildlife habitats be undertaken prior to removal of any vegetation on site. Wildlife habitat includes trees (canopies and lower trunk) whether living or dead, other living vegetation, piles of discarded vegetation, boulders, disturbed ground surfaces, etc. It is recommended that you seek advice from the Queensland Parks and Wildlife Service if evidence of wildlife is found.

Acid Sulfate soils

It is recommended that where actual acid sulfate soils or potential acid sulfate soil (ASS/PASS soils) are detected further testing be undertaken, at the advice of a geotechnical engineer, to determine the liming rates required to treat the material before disposal from site.

• Transport Noise Corridors

The State Planning Policy Interactive Mapping System identifies that the proposed development site is located within a designated Transport Noise Corridor. Future development located within these areas must comply with the requirements of the Queensland Development Code MP 4.4 – Buildings in a transport Noise Corridor. For further information please visit the Queensland Government website

http://www.hpw.qld.gov.au/construction/buildingplumbing/building/transportnoise corridors/Pages/default.aspx

Asbestos Management and Removal

Please be aware that where asbestos related materials are to be removed on a development site, appropriate measures must be taken to not cause a public health risk under the Public Health Act 2005. A suitably qualified asbestos removalist that holds a current Workplace Health & Safety A or B class asbestos removal licence must be engaged to remove more than 10m² of non-friable asbestos. The removal of friable asbestos must be undertaken by a business that holds a current Class A asbestos removal licence. For further information on asbestos visit the Queensland Government website https://www.asbestos.qld.gov.au/. For licensing enquiries please contact Workplace Health and Safety Queensland on 1300 362 128 or www.worksafe.qld.gov.au/injury-prevention-safety/asbestos.



• Environment Protection and Biodiversity Conservation Act

Under the Commonwealth Government's Environment Protection and Biodiversity Conservation Act (the EPBC Act), a person must not take an action that is likely to have a significant impact on a matter of national environmental significance without Commonwealth approval. Please be aware that the listing of the Koala as vulnerable under this Act may affect your proposal. Penalties for taking such an action without approval are significant. If you think your proposal may have a significant impact on a matter of national environmental significance, or if you are unsure, please contact Environment Australia on 1800 803 772. Further information is available from Environment Australia's website at <u>www.ea.gov.au/epbc</u>

Please note that Commonwealth approval under the EPBC Act is independent of, and will not affect, your application to Council.

STATEMENT OF REASONS

| Assessment Benchmarks: | The proposed development was assessed against the following assessment benchmarks: City Plan Version 5 - the whole planning scheme with emphasis on the following codes: Medium density residential zone code Healthy waters code Infrastructure works code Landscape code Transport, servicing, access and parking code Heritage overlay code TLPI 01/21 – Protection of local heritage places State Planning Policy 2017, Part E South East Queensland Regional Plan 2017 Planning Regulation 2017, Schedule 11 | | |
|-------------------------------------|--|--|--|
| | Planning Regulation 2017, Schedule 11 Local Government Infrastructure Plan. | | |
| Matters prescribed by Regulation | Council had regard to the following matters in the assessment of the application: Existing approvals on adjoining land; Common material, including written submissions accepted by the assessment manager; Advice received from external and internal parties. | | |

The key issues identified in the assessment were:

- Local heritage value;
- Building height;



- Building design;
- Amenity;
- Open space;
- Onsite parking;
- Stormwater management; and
- Waste management servicing and manoeuvring areas.

| Issue | Assessment outcome | | |
|--|--|--|--|
| Local heritage value | The heritage value of the premises has been assessed by an independent heritage consultant on behalf of Council. The consultant considers that the site does not have value that would threshold as a local heritage place. Conditions are recommended to ensure archival records of the premises are captured for future generations. A heritage implementation plan is also to be developed and implemented to ensure the local heritage values are presented within the approved development. | | |
| Building height | The height of the proposed development provides a appropriate transition between the approved six-store development to the west and the existing and expected development to the south and east. | | |
| Building design | The building design is visually interesting when viewed from the streetscape. Collectively, the design elements reduce the bulk of the building when viewed from adjoining properties. | | |
| Amenity | Overlooking impacts are reduced through implementation of privacy measures such as sill heights, privacy screens and screen planting. Overshadowing impacts do not further reduce sunlight access to the adjoining sites than already anticipated for a 13m high building in the MDR zone. | | |
| Open space | The proposed communal open space is sufficient for the development as it includes a range of facilities available for occupants to use such as a BBQ/alfresco area, resort pool, spa pool, partially covered pool deck and powder room. All dwelling units will be provided with private open space that meets the requirements for area and dimensions. | | |
| Onsite parking | The proposed onsite parking will adequately service the residential development. | | |
| Stormwater management | A condition is included for detailed stormwater management plans to be submitted to Council to demonstrate appropriate treatment of stormwater quality before discharge from the site. | | |
| Waste management - servicing and manoeuvring areas | A condition is included to ensure refuse collection is conducted outside of peak hours for traffic between 10am and 3pm. | | |

| Matters Raised in Submissions | | | |
|--|--|--|--|
| The demolition of the heritage place will result in the loss of character and heritage values. | Heritage values of the place have been diminished through substantial additions and alterations. Conditions are recommended for archival recording and heritage interpretation plan for the place prior to demolition. This will ensure the remaining local heritage values are retained. | | |
| The scale and design of the multiple dwelling is incompatible with surrounding properties and streetscape. | The proposed development is of a lower total height than the approved multiple dwelling at 17-21 Passage Street. It has been designed to step down to the existing development to the east and south. | | |
| The building design has little architectural merit or interest. | The design elements incorporated into the building design creates a visually interesting building when viewed from the streetscape and also reduces the bulk of the building when viewed from adjoining properties. | | |
| The development will have adverse impacts on amenity and privacy of adjoining properties. | Privacy measures have been incorporated into the design of the building in the form of 1.5m high sill height, vertical screens and landscape planting. Shadow diagrams show adjoining properties will continue to have at least 3 hours of sunlight access and do not further reduce sunlight access to the adjoining sites to the south than already anticipated. | | |
| Increased traffic generation from Toondah priority development area (PDA) will restrict access to the site and cause safety issues. | It is acknowledged increased traffic generation from Toondah Harbour may restrict access to the site especially for refuse trucks which are proposed to reverse onto the site. To manage any adverse impacts a condition is recommended to ensure refuse collection is conducted outside of peak hours for traffic between 10am and 3pm. | | |
| The height of the proposed building will obstruct future water views to the bay for properties across the street that have not yet been developed to their potential under the City Plan. | The height of the proposed building does not exceed the maximum height of 19m for MDR4 sites located across the street. There remains opportunities to develop these properties to their potential under the City Plan to take advantage of the bay views. | | |





Your heritage partners

Commercial in Confidence

19 September 2021

Carol Vito Sula Planning Officer City Planning and Assessment Redland City Council PO Box 21 Cleveland Qld 4163

E: carol.vitosula@redland.qld.gov.au

Dear Carol,

Re: Heritage Advice - 217-219 Middle Street, Cleveland (MCU21/0063)

Thank you for requesting Australian Heritage Specialists Pty Ltd (AHS) provide heritage advice regarding the proposed development application at 217-219 Middle Street, Cleveland, Redland City. We have pleasure in providing the following information.

Background and Initial Advice

We refer to the abovementioned development application lodged in respect of the property located at 219 Middle Street, Cleveland, Queensland. The development application seeks a development permit for a material change of use to establish multiple dwellings (27 units) and building works for the demolition of a local heritage place.

It is noted that the subject site is a local heritage place which is captured within the recently adopted Local Heritage Place Schedule and under Temporary Local Planning Instrument (TLPI) 01/21 – Protection of Local Heritage Places), within the amended Redland City Plan.

You have requested a review of the Development Assessment with regards to heritage matters to assist Council in making its decision involving the demolition of the local heritage place. To inform this advice, we have considered information provided by you within email correspondence and information accessed via Redland City Council's *i.development* portal, in addition to submissions made against the proposed application.

Following initial advice, our review has included a site visit and a 'without prejudice' meeting with the Applicant, which this advice provides further update upon.

1 | Page



Results of Initial Heritage Review

- 1. The place was identified in previous heritage studies and reviews completed by Redland City Council (Council), with the subject site considered to threshold for local heritage significance and recommended for inclusion in the major amendment during a review in 2017-18.
- An updated Heritage Citation (Annexure A) was subsequently drafted by AHS in 2017, and the subject site was recommended for entry within the amended Local Heritage Place Schedule, as part of the proposed major amendment of Redland City Plan, which commenced in 2019.
- 3. Council completed necessary public notification for the amended Local Heritage Place Schedule, including public consultation on the proposed major amendment as required by the *Queensland Heritage Act 1992* and the process identified in the Minister's Guidelines and Rules under the *Planning Act 2016.*
- 4. The consultation period included formal notice to owners, (including to the owner of the subject site at that time), providing opportunity for formal submissions to be made to Council from 12 October 2020 to 04 December 2020, ensuring the conditions for public notice were met.
- 5. AHS assisted Council in completing the review of public submissions in late 2020 and early 2021, including assistance with the drafting of a public submission review report. The owner of the subject site made a submission objecting to the proposal to list the subject house at that time.
- 6. On 17 March 2021, Council agreed to adopt the public submission review report and progress the proposed major amendment by asking the State Government for approval to enter the places to the amended Local Heritage Schedule, including the subject site. The subject site (and others) was inspected from public viewpoints.
- 7. The subject site is a local heritage place captured within the Local Heritage Place Schedule and under Temporary Local Planning Instrument (TLPI) 01/21 – Protection of Local Heritage Places), within the amended Redland City Plan. The proposed works at the subject site (as a local heritage place) triggers a Development Application under TLPI 01/21 - Protection of Local Heritage Places, including assessment against the Heritage Overlay Code of the *Redland City Plan 2020*.
- Urbicus lodged a Development Application on behalf of the Applicant on 19 April 2021, however, the application did not initially respond to the TLPI requirement for demolition and building work on a local heritage place and did not achieve the relevant Performance Outcomes.
- However, by way of an alternative outcome, a Heritage Impact Statement (HIS) report was prepared by Ivan McDonald Architects (IMA), which sought to demonstrate compliance with PO1.

Performance Outcomes Acceptable Outcomes

| P01 | A01.1 |
|--|--|
| A building or structure on a local heritage place is not demolished or relocated unless: (1) it is structurally unsound and is not reasonably capable of being made structurally sound; or (2) the change does not result in the loss of the particular heritage values of the place. | Development does not result in the partial or total demolition or removal of a building or structure on the site. Editor's note—Where an alternative outcome is proposed, a structural report or heritage impact statement, prepared by suitably qualified persons may be needed to demonstrate compliance with PO1. |
| AUSTRALIAN HERITAGE | e Advice – 217-219 Middle Street, Cleveland |



Review of Heritage Impact Statement (IMA, June 2021)

- 10. A review of the HIS by Ivan McDonald Architects (IMA) was undertaken by AHS and whilst the HIS now forms compliance with PO1 (when considering subsequent amendments by the Applicant), the HIS contains a series of structural errors in the manner it attempts to identify that, (with respect to PO1), the demolition of the place does not result in a loss of heritage values because the place *has no cultural heritage value*.
- 11. In particular, we disagree with this statement as the basis that the demolition of the dwelling is acceptable from a heritage perspective, for the following reasons:
 - The statement made by IMA that the place has no heritage value is factually incorrect.
- 12. As a result of this review, I recommended an inspection of the subject site be undertaken, to establish the further claim made by IMA that the place had been substantially altered internally, as the previous assessment at the time of heritage listing was made from public vantage points (which is normal practice) and the property now required a closer inspection to verify the condition and integrity of the place in order to fully understand the heritage values of the place.

Updated Advice

HIS and Site Visit

- 13. In my earlier advice I found the house to be accurately described by the Heritage Citation (Annexure A). However, having since undertaken a site inspection on 2 August 2021 (from within the property boundary and internally), I generally agree with IMA that the building has been extensively altered to the rear of the dwelling and its integrity is therefore compromised from the previous findings (IMA 2021: 8).
- 14. As abovementioned, I disagree however with IMA's conclusions that the place therefore has no cultural heritage value. In my opinion, even if the integrity of the place is fair-poor, this can still enable a place to threshold at a local level of cultural heritage significance and does not result in an evaluation of 'no heritage value' as per IMA key conclusion within the HIS.
- 15. In particular, the HIS compounds these issues further and is flawed with respect to the empirical evidence to support the conclusions that the place has no cultural heritage value and the methods in which the significance assessment has been made within the HIS. One of the key criticisms made by the HIS relates to the Heritage Citation and Council's assessment of the historical significance of the place (criterion a) based on its use as a sawmill (as noted by Council's submission in Annexure B).

"It is contended in the statement of significance and under criterion (a) that Charles Elfert was a businessman who constructed a sawmill beside the house. No primary historical sources are cited in the citation in support of this contention and no historical evidence has been found to support this contention. There is no surviving sawmill and no apparent physical evidence of there ever having been a sawmill adjoining the house". (IMA 2021: 19).

16. For example, although there is no surviving evidence of a sawmill, a simple search of on-line sources and consultation with Redland City Council's local librarian revealed documentary evidence relating to the presence of a sawmill on the site historically. According to the archival information held in Council's library, the sawmilling was undertaken in the shed located at 217 Middle Street, as shown in the photo below and described by Augusta Elfert's granddaughter Pauline Topfer (who lived in the adjoining house at 217 Middle Street built in 1947).



AHS Heritage Advice - 217-219 Middle Street, Cleveland



Figure 1: Mill was on what is now 217 Middle Street. Half acre allotment originally acquired from GJ Walter c.1919 by Charles Elfert, who built house still on 219 Middle St that same year and sawmill next to it. House now on 217 Middle Street was built c.1947. The men are (left) Herman Holzapfel and Bill Helmhold (Elfert's son in law) and "Jimmie" the horse. Mill mainly chopped firewood (Redland City Council Local History Library, description by Pauline Topfer, Photo Identifier:01332).

- 17. The Elfert's and their extended family were prominent members of the Cleveland community, with a long historical association in the Redlands. The reliance on Kerr's 'Inventory of Queensland Sawmills' (which does not refer to the one on the subject site) also does not refer to the former Beutel's sawmill in the region, so the limitations of Kerr's research within IMA methodology suggests that it is not a comprehensive (nor reliable) inventory of all sawmills in Queensland provided by the HIS.
- 18. Using this poor level of rigour, IMA goes on to state a range of points which are not consistent with heritage assessment guidelines. For example (IMA 2021: p.18 (a)) The HIS states that "To be of historical significance under criterion a), a place should be associated with an event that is significant <u>or famous</u> or important in the history of the Redlands region. <u>Such an event should be, or has been, famous</u>, important or significant to the present generation or past or future generations."
- 19. Such a statement is factually incorrect. The threshold for entry of a place under criterion a) is not tested against whether an event should be or has been 'famous' according to the *Redlands Planning Scheme 2020*, a local heritage place is assessed for local significance (under criteria a) for its importance in demonstrating the pattern or evolution of the region's history.
- 20. As such, I am not satisfied with the HIS's evaluation of 'no heritage value' for the subject site as the place clearly holds heritage significance under criterion a) but whether the threshold for local entry has been achieved was the matter in which the HIS should have more properly developed (which it did not construct).
- 21. Furthermore, the HIS did not consider other options to the proposed demolition (e.g., retention of all or part of the cottage on the site alongside multi-unit development) and as such, my initial advice was that the HIS did not provide a firm base to justify the conclusion that the proposal will not result in the loss of the particular heritage values of the place (PO1).



AHS Heritage Advice – 217-219 Middle Street, Cleveland

- 22. Accordingly, I requested further and better particulars at a 'without prejudice' meeting with Council and the Applicant on 29 July 2021, regarding how the Applicant had considered the option to retain the cottage (or original part thereof), to assist in meeting and alternative option for PO1, whereby the retention of the place be considered alongside the proposal for demolition.
- 23. Following this request, modelling and supporting documentation was provided by the Applicant, including an Alternate Scheme by NMDS Architects (03.09.2021) and the letter from Urbicus (07.09.2021) see Annexure C.
- 24. I acknowledge that the information provided by the Applicant within the Alternate Scheme to demonstrate the commercial and economic issues associated with the retention of the place are suitably prepared and that retention of the place has been shown to be neither prudent nor feasible in this instance.
- 25. I also acknowledge that the heritage values of the place are certainly of a lesser score than those which were originally prescribed by the Heritage Citation (Annexure A), now that we have had the advantage of inspecting the property in more detail, including the internal inspection conducted on 2 August 2021.
- 26. On this basis, my view is that whilst the place has historical and aesthetic values from a heritage perspective under criterion a) and e), (using the up to date information provided by this review) I find that the place does not adequately threshold as a local heritage place, due to the substantial alterations to the place internally and to the rear (IMA 2021: 8).
- 27. As such, it is concluded that the subject site does not properly satisfy entry onto the Local Heritage Place Schedule.

Summary

In summary, following review of the DA material, including the HIS report prepared by IMA and Alternate Scheme (supplementary information), my final review of heritage matters finds that:

- The purpose of the Heritage Overlay Code allows for a closer review of places entered on the Heritage Place Schedule during a development application, which provides for 'natural justice' for development proposals, whereby the Heritage Citation can be reviewed.
- 2. Whilst there were several issues found within the HIS during our initial review, the report assisted a review of the 'heritage status' and our further investigations of the subject site reveal a lower ranking of heritage values than those identified previously by the existing Heritage Citation. This was further verified by a site inspection I conducted on 02 August 2021.
- 3. Of primary concern however, we noted that the HIS did not consider other options (e.g. retention of all or part of the cottage on the site alongside multi-unit development) and as such, my initial advice was that the HIS did not provide a firm base to justify the conclusion that the proposed development will not result in the loss of the particular heritage values of the place (PO1).
- 4. Following a request made at a 'without prejudice' meeting with the Applicant and Council on 29 July 2021, we requested further particulars regarding how the proposal had considered the option to retain the cottage (or original part thereof), to assist in meeting and alternative option for PO1, (whereby the retention of the place be adequately considered).



AHS Heritage Advice – 217-219 Middle Street, Cleveland

- A response was subsequently provided within supporting documentation shown in the Alternate Scheme by NMDS Architects (03.09.2021) and the letter from Urbicus (07.09.2021) see Annexure C).
- In accordance with the Performance Outcomes (PO), whilst the change does result in loss of particular heritage values, the Applicant has therefore demonstrated (when including the Alternate Scheme advice alongside the HIS report) for an alternative Acceptable Outcome for PO1.
- 7. This conclusion is based upon my review of the local heritage status for the subject site, which I believe has been diminished by a range of internal alterations previously that could not have been detected at the time of listing onto the Local Heritage Place Schedule.
- 8. On this basis, my view is that whilst the place has historical and aesthetic values from a heritage perspective under criterion a) and e), I find that the place does not adequately threshold as a local heritage place. As such, it is concluded that the subject site does not properly satisfy entry onto the Local Heritage Place Schedule.
- 9. On this basis, I am content that the application should be approved from a heritage perspective as the place has been found to not adequately threshold as a local heritage place by this assessment (although having heritage values), and
- 10. The Applicant has satisfactorily demonstrated that the option to retain the cottage in-situ have been shown to not be prudent nor feasible in this instance.

Thank you for the opportunity to provide this advice. Your further inquiries would be welcomed.

Yours sincerely,

Benjamin Gall Principal Australian Heritage Specialists Pty Ltd | ABN 51 605 153 419 P: 07-3221 0000 | M: 0476 272 273 E: <u>ben@ahspecialists.com.au</u> Level 8, 231 North Quay BRISBANE QLD 4000 | PO Box 5940 MANLY QLD 4179



AHS Heritage Advice – 217-219 Middle Street, Cleveland

Councillor Peter Mitchell declared a Declarable Conflict of Interest in the following item, stating that he made a separate submission as the Divisional Councillor to the State Department, which was in response to a broad general stakeholders request from the Department.

Councillor Mitchell considered his position and was firmly of the opinion that he could participate in the discussion and vote on the matter in the public interest. (refer Item 6.1 for vote)

Cr Mitchell voted FOR the motion.

| 14.3 SUBMISSION | TO DRAFT MINJERRIBAH PUBLIC TRANSPORT STRATEGY | | |
|-----------------------------|---|--|--|
| Objective Reference: | A6075589 | | |
| Authorising Officer: | David Jeanes, Acting General Manager Community & Customer Services | | |
| Responsible Officer: | Chris Vize, Acting Group Manager City Planning and Assessment | | |
| Report Author: | Ben Clarke, Acting Principal Transport Planner Simon Honywood, Principal Transport Planner | | |
| Attachments: | Draft Minjerribah Public Transport Strategy <u>U</u> Draft Minjerribah Public Transport Strategy - RCC Submission <u>U</u> | | |

PURPOSE

To endorse the submission to the State Government on the Draft Minjerribah (North Stradbroke Island) Public Transport Strategy (Draft Strategy) prepared by the Department of Transport and Main Roads (TMR). Public consultation on the Draft Strategy finishes on 19 November 2021.

BACKGROUND

September 2016 – The State Government finalised and released its North Stradbroke Island Economic Transition Strategy (NSIETS). The NSIETS (now Minjerribah Futures) was developed to assist with the economic transition of North Stradbroke Island (NSI) from sand mining to a tourism based economy. The NSIETS identified the need for a Passenger Transport Study project to assist with the island's economic transition.

May 2019 – TMR provided Council with a preliminary Draft North Stradbroke Island Public Transport Strategy (Draft Strategy) for feedback. The Chief Executive Officer provided a submission on behalf of Council. In relation to the content of the Draft Strategy, the most notable omission was that there were actions that did not have lead agencies. The submission outlined that it was important that all actions were assigned a project lead and project partners to ensure there is an agency accountable for delivering actions with identified program partners. In addition to assigning project leads and partners, the submission detailed the need to elaborate on the costs and funding source of each action.

October 2021 - On 22 October 2021, Council officers received notice from TMR that the Draft Strategy was now open to public consultation with an invitation for Council to provide feedback on the document.

Council officers have since reviewed the Draft Strategy and can advise that there are elements of the Draft Strategy that appear to be promising and provide benefit to residents and visitors. This includes the specific actions for planning and delivering projects in collaboration with Council and other responsible agencies. However, there are elements within the Draft Strategy that need

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addressing in order for Council and other responsible agencies to plan, fund and deliver the actions.

ISSUES

The Draft Strategy is a high-level document with a suite of actions to improve public transport on Minjerribah. The Draft Strategy establishes priorities and actions for developing and supporting the public transport system on and to Minjerribah in a way that supports the community, economy and environment.

The vision of the Draft Strategy is *"Passenger transport enhances Minjerribah as a tourism destination of choice in SEQ for local, national and international visitors, and supports mobility of the resident population"*. To achieve the vision, the Draft Strategy outlines goals, which are to:

- Enhance customer experience
- Provide an efficient network and services
- Support connected communities
- Facilitate technology and innovation

The Draft Strategy details challenges and opportunities and then proposes six priorities, which are:

- 1. Making it easier to pay fares
- 2. Improving passenger information
- 3. Providing quality passenger transport infrastructure
- 4. Providing an efficient network and services
- 5. Encouraging more people to choose passenger transport for their mobility needs, to, from and on Minjerribah
- 6. Supporting appropriate private sector passenger transport services and sustainable tourism opportunities

Each of the priorities have a suite of objectives with short, medium and long term actions for delivery. The responsibility for delivering the actions is detailed in the report as either 'lead' agency or 'partner' agency.

Council is identified as lead for the following actions:

- Improve bus stops to improve bus operations and customer experience
- Investigate locations for bicycle parking at Toondah Harbour
- Interim bicycle parking for One Mile and Gumpi
- Review parking facilities and develop parking policy
- Lighting, safety, security and access enhancements
- Investigate opportunities for improved active transport between Cleveland central business district and Toondah Harbour based on growth and demand

Council is identified as partner for the following actions:

• Signage and wayfinding



- Improved pathway access to ferry terminals (including long-term bicycle storage)
- Bus service timing for improved performance
- Investigate opportunities for enhanced public transport access from key attractor and areas currently un-serviced (including active and flexible transport)
- Promote private sector delivery of transport connections for tourists and visitors

The draft Council submission (Attachment 2) outlines that it is unrealistic for Council to fund and deliver all the actions listed alone and will require guidance and funding from the State Government for many of the actions.

Without a more detailed plan of implementation, there is uncertainty with where responsibilities lie for delivering actions of the Draft Strategy. Until a detailed implementation plan is completed and agreed upon by relevant parties, that identifies lead agency responsibility, indicative costings, funding sources and delivery timeframes, it is recommended that Council would be unable to support the finalisation of the Minjerribah Public Transport Strategy.

STRATEGIC IMPLICATIONS

Legislative Requirements

There are no legislative requirements associated with providing feedback to the Draft Minjerribah Public Transport Strategy.

The State Government's current consultation process on the Draft Strategy is not a formal statutory consultation process.

Risk Management

Resourcing and deliverability - Council currently does not have available staff or other resources to undertake the considerable planning and delivery sought by the Strategy, particularly for supporting infrastructure such as pathways, lighting and supporting bus stop infrastructure.

There is a risk that Council may not be able to fund the actions where Council is nominated as lead or partner, due to there being no financial indication or clear funding responsibility noted for each action.

If Council cannot deliver their nominated actions of the Strategy, it may effect Council's reputation with the community and other stakeholders.

Financial

There are no direct financial implications to Council associated with State Government's public consultation on the Draft Strategy.

They may be financial expectations put on Council to deliver relevant actions. Delivering actions within the Draft Strategy that have Council as lead and partner agency may require significant time and resources of Council to deliver.

The Draft Strategy includes actions that identify a significant number of further investigations to be undertaken. At this stage, no responsibilities have been assigned and indicative cost estimates for the actions have not been defined. Council's submission on the Draft Strategy notes the need for a better understanding of responsibilities and costs to undertake the program of actions as well as details on how actions will be funded.

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People

The actions identified in the Draft Strategy will have implications for staff resources.

Environmental

There are no environmental implications. Improved public transport provision could reduce dependence on mainland and island based private motor vehicle trips and the visual impact of large swathes of parked vehicles in the vicinity of ferry terminals.

Social

There are no social implications.

Human Rights

There are no human rights implications.

Alignment with Council's Policy and Plans

As the Draft Strategy is being led by the State Government, it is difficult to ensure alignment with Council policies and plans. Making a submission on the Draft Strategy is consistent with Council's *Our Future Redland's - A Corporate Plan to 2026 and Beyond -* City Leadership objective to advocate for services and funding across our city to enhance social, cultural, environmental and economic outcomes.

CONSULTATION

| Consulted | Consultation Date | Comments |
|---|-------------------|--|
| Senior Advisor, Strategic Economic Development | 8 November 2021 | Comments received and collated as part of the submission |
| Service Manager, Tourism Events and Engagement | 5 November 2021 | Comments received and collated as part of the submission |
| Service Manager, Civil and Traffic | 3 November 2021 | Comments received and collated as part of the submission |
| Advisor, Marine Strategic Infrastructure | 3 November 2021 | Comments received and collated as part of the submission |

Note: TMR request to provide feedback on the Draft Strategy was received on 22 October 2021.

OPTIONS

Option One

That Council resolves as follows:

- 1. To endorse the draft submission (Attachment 2) to the Draft Minjerribah Public Transport Strategy.
- 2. To issue the submission to the Department of Transport and Main Roads.

Option Two

Council resolves to not make a submission on the Draft Minjerribah Public Transport Strategy.



Option Three

That Council resolves as follows:

- 1. To endorse the submission to the Draft Minjerribah Public Transport Strategy, subject to amendments.
- 2. To issue the submission to the Department of Transport and Main Roads.

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OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/274
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Moved by: Cr Peter Mitchell Seconded by: Cr Wendy Boglary

That Council resolves as follows:

- **1.** To endorse the draft submission (Attachment 2) to the Draft Minjerribah Public Transport Strategy.
- 2. To issue the submission to the Department of Transport and Main Roads.

CARRIED 11/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.



Department of Transport and Main Roads

Minjerribah (North Stradbroke Island) Public Transport Strategy

Draft Strategy for Consultation







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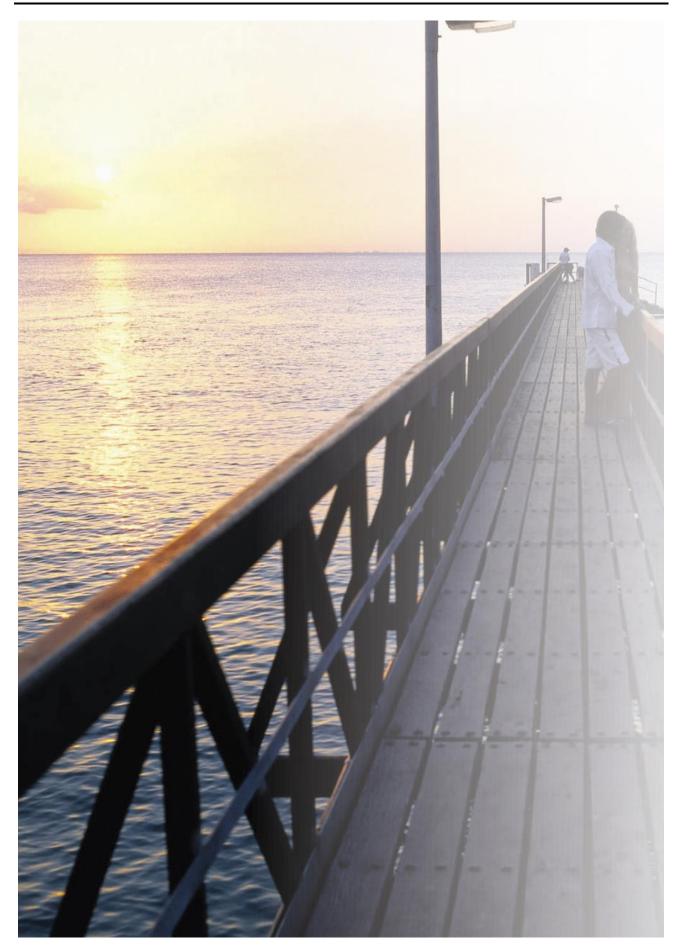
We acknowledge the Traditional Owners and Custodians of the land to which this strategy applies and pay our respects to their Elders both past and present. Aboriginal and Torres Strait Islander readers are warned, images in this document may contain or represent deceased persons which may cause sadness or distress.





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Introduction

Background

Minjerribah (North Stradbroke Island) is the world's second largest sand island and is situated in Moreton Bay, within the Redland City Council local government area. The Island's economy has been supported by sand mining, tourism and education for more than 60 years.

As decades of sand mining concluded on 31 December 2019, a pathway has been collectively mapped to transition Minjerribah and its residents and workers towards a thriving, more sustainable economy, with a pristine natural environment. Minierribah Futures extends the work of the former North Stradbroke Island Economic Transition Strategy and represents the Queensland Government's significant investment in the transition of the Island to a globally recognised cultural and ecotourism destination.

Purpose

The purpose of the draft Strategy is to identify opportunities for improving the public transport network to, from and on Minjerribah. The draft Strategy provides a broad framework for improving the public transport network and identifies potential detailed investigations required to confirm the viability of improvements.

The draft Strategy supports Minjerribah Futures objectives by identifying opportunities for improving public transport access for visitors and for those who commute to the mainland for employment. This approach also benefits residents more broadly through improved access to the mainland and around the Island.

The draft Strategy considers not only the public transport system provided by government but also walk-on and other passenger transport services provided by the private sector for tourists and residents. It identifies elements of the broader passenger

The Minjerribah Futures approach is guided by the values and aspirations of the Ouandamooka people. These values are articulated in Gudjundabu Marumba Gubiyiyanya (Tourism for a Glad Tomorrow): A five-year strategy for sustainable tourism on Quandamooka Country.

The draft Minjerribah (North Stradbroke Island) Public Transport Strategy (the draft Strategy) has been developed in coordination with Minjerribah Futures initiatives.

Once finalised, the draft Strategy will support Minjerribah initiatives including Gudjundabu Marumba Gubiyiyanya Tourism for a Glad Tomorrow and other Quandamooka-led strategies under development such as the Quandamooka Sustainability Strategy

transport system that could be enhanced and provides a guide for public transport investment decisions.

Minjerribah's natural environment and cultural heritage are key attractors for visitors. Improving access to the Island is a vital component of balanced tourism development.

Once visitors have arrived, it is just as important they are able to move around the Island to enjoy the breadth of experiences that Minierribah has to offer

Promoting sustainable transport options to and on the Island (such as public transport, cycling and walking) will contribute to protecting its natural assets and supporting cultural values. Improvements to on-Island travel will assist visitors without cars and provide better solutions for residents who require public transport

Strategy Development

Stage 1

Stage 2

Current network Future assessment service gaps

Stage 3 Options requirements and development and

assessment

Stage 4 Draft Public Transport Strategy -**Public Consultation**

Final Public Transport Strategy

Consultation Draft Minjerribah Public Transport Strategy, Transport and Main Roads, 2021



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Context

Regional

Minjerribah is situated in Moreton Bay, within the Redland City Council local government area. The Island is approximately 40 kilometres south-east of the Brisbane Central Business District (CBD) and the nearest major centre is Cleveland. The location in south-east Queensland provides a significant local tourism market and considerable employment opportunities for Island residents.

Figure 1 shows the regional context of Minjerribah. Toondah Harbour at Cleveland is the mainland gateway to the Island from Brisbane and surrounding areas, with Gumpi (Dunwich) and One Mile ferry terminals the primary entry points on Minjerribah for people and goods. Access from locations such as the Brisbane central business district (CBD), the Brisbane Airport and the Gold Coast are important for Minjerribah as they provide broader tourism market opportunities.

The draft Strategy considers connectivity between Minjerribah and surrounding centres. It identifies potential options for improving integration between elements of the passenger transport network (such as rail, ferry and bus), as well as opportunities to integrate with other transport modes such as cycling, walking and private vehicles.



Quandamooka Country

The Quandamooka People have occupied the islands of Moreton Bay, including Minjerribah and mainland adjacent to Moreton Bay, for more than 20,000 years.

The Quandamooka people include the Nunukul, Gorenpul and Nughi clans. Quandamooka Country includes the waters and lands around Mulgumpin (Moreton Island), Minjerribah, the Southern Moreton Bay Islands and South Stradbroke Island, as well as areas of the mainland.

On 4 July 2011, the Federal Court of Australia made two native title consent determinations recognising the Quandamooka People's native title rights and interests over land and waters on and surrounding Minjerribah. The Quandamooka People also negotiated two Indigenous Land Use Agreements that establish how rights and interests will be exercised on Minjerribah.

The Quandamooka Yoolooburrabee Aboriginal Corporation (QYAC) is the Registered Prescribed Body Corporate created under the *Native Title Act 1993* to manage the Native Title rights and interests of the Quandamooka People. QYAC's vision for Minjerribah is for the Island to be a global eco cultural tourism destination that showcases the Island's natural beauty and Quandamooka cultural heritage.

The Quandamooka people have an important connection to Minjerribah and are a key partner in the delivery of Minjerribah Futures initiatives.



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Minjerribah Context

Minjerribah covers an area of approximately 275 square kilometres, with the Naree Budjong Djara National Park covering approximately half of this area.

The three Island townships of Gumpi, Pulan (Amity Point) and Mulumba (Point Lookout) accommodate the majority of the resident population and are the focus of commercial uses and community facilities. East Coast Road connects Gumpi and Mulumba and supports private vehicles, public transport and bicycle access for tourists and residents.

Demographics

Minjerribah is a unique community with demographic trends that are specific to the Island. Although its resident population is relatively small, the Island experiences significant and variable visitor and tourist numbers, making the delivery of an effective passenger transport network challenging.

Minjerribah had an estimated resident population of 2151 persons as of 30 June 2016. As shown in Table 1, projected population growth on the Island is less than that expected for Queensland overall. The below average population growth trend is expected to continue in the future with the population on Minjerribah projected to increase by 0.7 per cent per annum on average, to a total of 2489 persons by 2036.

Minjerribah as a tourist destination

Minjerribah is a major tourist destination, with estimated tourist visitation of between 345,000 and 405,000 people in 2018^g. The highest levels of visitations occur during peak holiday seasons such as Easter, spring and summer school holiday periods.

Passenger transport has an important role to play in connecting attractors on the Island, such as the townships of Gumpi, Pulan and Mulumba – along with associated residential and commercial areas – and popular beaches and tourist attractions.

The majority of attractions on Minjerribah are focused in and around the townships or along East Coast Road, which is serviced by the Minjerribah bus and shuttle service to Pulan. There are also a number of attractions, including camp grounds, Bummiera (Brown Lake) and Karboora (Blue Lake) for which the dominant mode of access is private vehicle and in some cases four-wheel drive vehicles only.

Minjerribah Futures represents the Queensland Government's investment to support the transition of the Island to a leading cultural and eco-tourism destination, built on a foundation of Quandamooka culture and sustainability.

| Table 1: Key demographic statistics for Minjerribah | | | |
|---|----------------------|-----------------|--|
| | Minjerribah | Queensland | |
| Resident population at 2016 | 2,151 ° | 4,848,877 ° | |
| Predicted growth rate | 0.7% ° | 1.6% ° | |
| Projected population at 2036 | 2,489 ° | 6,686,604 ° | |
| Median Age | 49.2 ^b | 37 | |
| % population 65 and over | 23.6% ^b | 14.7% | |
| Aboriginal and/or Torres Strait Islander peoples | 20.2% ^d | 4% ^d | |
| % population with internet access | 81.4%° | 83.7% | |
| Median weekly total household income | \$1,028 ^f | \$1,402 | |

More than 20 projects across Country, culture and people make up the current Minjerribah Futures program.

This draft Minjerribah Public Transport Strategy aims to support the Island's economic transition by identifying potential improvements to it's public transport system and improving access for residents and visitors.

*Based on Queensland Government Statistician's Office (2018), Queensland Government population projections, 2018 edition, unpublished data. *Based on Australian Bureau of Statistics (2018). Estimated Resident Population,

^bBased on Australian Bureau of Statistics (2018), Estimated Resident Population, Customised Report: Age, by Sex, by all SA1s in Queensland and Tweed Shire, 2011 to 2017, time series spreadsheet, viewed 21 August 2019, http://www.qgso.qld.gov. au/products/tables/erp-age-sex-sa1-qld-consult/index.php.

^cBased on Queensland Government Statistician's Office (2018), Queensland Government population projections, 2018 edition, spreadsheet, viewed 21 August 2019, http://www.qgso.qld.gov.au/subjects/demography/population-projections/ tables/proj-pop-age-group-sex-qld/index.php. ^cBased on Australian Bureau of Statistics (2027). 2016 Census of Population and

"Based on Australian Bureau of Statistics (2017), 2016 Census of Population and Housing, General Community Profile: Table Go7 Indigenous Status by Age by Sex, by Place of Usual Residence.

[°]Based on Australian Bureau of Statistics (2018), 2016 Census of Population and Housing, TableBuilder data: Dwellings, by Dwelling Internet Connection, by Number of Persons Usually Resident in Dwelling.

^rAustralian Bureau of Statistics (2017), 2016 Census of Population and Housing, General Community Profile: Table Go2 Selected Medians and Averages.

⁸ Queensland Governmment (2019). Minjerribah Visitor Research Program: Round 2 Report.

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Key Findings from the Minjerribah Visitor Research Program (part of Minjerribah Futures)





Peak visitor periods occur during the Easter (March – April), spring (late September/early October) and summer (late December/early January) school holiday periods



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Existing Transport Network

Existing Services

The current transport network servicing Minjerribah has multiple transport modes operating across three parts of the journey:

- Mainland transport connections to Toondah Harbour
- Water based transport to and from the Island
- Transport options around the Island.

Figure 2 shows the transport mode options for each of the three parts of the journey. Toondah Harbour, Gumpi and One Mile ferry terminals are the main gateway between the mainland and Minjerribah.

On the Island, passenger transport is generally focussed on connecting the townships and servicing coastal tourist destinations. The passenger transport network servicing both residents and tourists on the Island includes bus and taxi, with passenger ferries connecting the Island and the mainland. Mainland services providing connection to Toondah Harbour include both public and privately operated bus services. These services are supported by a broader network, including rail, that connects to Cleveland and provide for interchange opportunities across the region.

The passenger transport network of rail, bus, ferry and taxi is supported by a range of dedicated tourist services provided by private operators which offer full-service day trips from the Brisbane CBD through to specialised activities on the Island, such as sandboarding and diving. Many of these tourism services offer the convenience of an integrated transport and tourist product often enabling pick up and drop off from Brisbane CBD at a premium price point.



Consultation Draft Minjerribah Public Transport Strategy, Transport and Main Roads, 2021



What you told us

The draft Strategy has been informed by community and stakeholder input captured through a series of community engagement activities and events, including a public transport survey and community information sessions.

Public Transport Survey

A Public Transport Survey was undertaken by the Department of Transport and Main Roads (TMR) to support development of this draft Strategy. The survey identified existing transport patterns to, from and on the Island and the transport needs of visitors and residents.

Carried out in April and May of 2018, the survey sought visitor, resident and business input on what aspects of the passenger transport network are working well and where there are areas for improvement.

Survey respondents

The survey received a high response rate with a total of 726 responses. A majority of survey responses were from residents (51 per cent) followed by visitors to the Island (35 per cent) as shown in Figure 3.

The majority of residents who responded to the survey (50 per cent) live in Mulumba, followed by 24 per cent living in Pulan and 23 per cent living in Gumpi.

Survey responses indicate the preferred transport mode for residents is private vehicle, with 85 per cent undertaking the mainland portion of their trip in a private vehicle and 92 per cent using a private vehicle on Minjerribah. This suggests use of the bus services on the Island and the mainland are not highly used by residents. It also suggests a primary function of bus services is catering for tourist demand, which is consistent with the seasonal variations observed in the Minjerribah bus service data.

A majority of survey responses were from residents (51 per cent) followed by visitors to the Island (35 per cent)

Of the 35 per cent of survey responses provided by visitors to Minjerribah, 91 per cent were domestic visitors travelling for leisure, recreation or tourism purposes and 9 per cent were domestic visitors travelling to the Island for work or business. Only one respondent identified as an international visitor. A majority of visitors came from the Redlands and Brisbane regions, as shown in Figure 4.

Similar to residents, almost all visitors travelled by private vehicle to Toondah Harbour (91 per cent). However this figure drops on Minjerribah to 70 per cent, with more people using the bus service to get around the Island.





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Views on the passenger transport system

Of those that responded to the survey, 86 per cent provided their views on the current passenger transport network servicing the Island by rating their level of agreement or disagreement with statements about key network features.

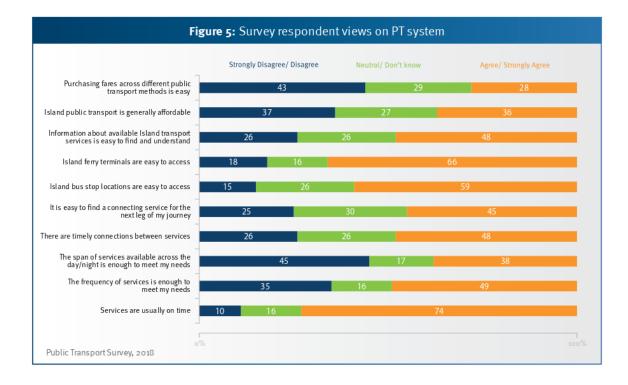
As shown in Figure 5, respondents tended to agree the Island's public transport services are usually on time and that ferry terminals are easy to access.

Respondents showed less agreement that purchasing fares across different transport modes is easy and that services are generally affordable. Survey respondents were also asked to identify their top three priorities for improving the passenger transport network servicing Minjerribah, with 89 per cent of respondents providing a response.

The most frequently identified priority areas were:

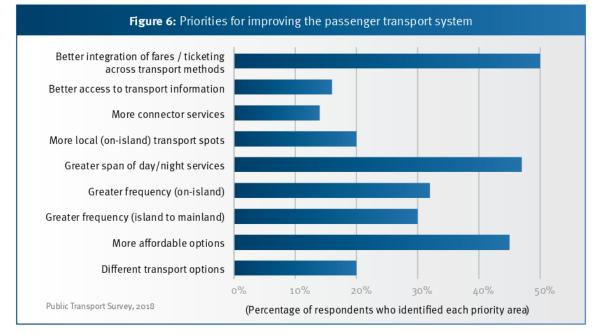
- Better integration of fares and ticketing across different passenger transport modes
- Greater span of passenger transport services across the day and night
- More affordable passenger transport services.

Priority areas for improvement and the percentage of respondents who identified those areas are provided in Figure 6.



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Community Information Sessions

Three community information sessions were held on Minjerribah, at Pulan, Gumpi and Mulumba on 12 and 13 July 2018.

The sessions provided a forum for the project team to share information about the scope of the study and results from the Public Transport Survey while discussing community input on the challenges identified and potential solutions.

Key points raised

The sessions provided further information on challenges for the passenger transport system and identified a number of potential areas for improvement such as:

- Better integration of fares and ticketing across different public transport modes
- A ticketing system to make ticketing more efficient, speed up the boarding process and improve reliability of the services
- Better integration of scheduling across different transport modes
- More information and flexibility around journey management for residents and visitors alike
- Greater span of public transport services across the day and night, primarily for residents, but with a view to increasing access to the Cleveland CBD for residents, and to Minjerribah for those wishing to attend events
- Improved connectivity between local towns, venues and attractions
- Affordable group transport options to and from the Island

· Parking at ferry terminals on the mainland and Minjerribah.

Improved connections to Pulan was also suggested at the Pulan session, where a number of participants expressed a desire for improved service to provide an alternative to private vehicles and attract tourists to the township.

A particular focus at the Mulumba and Gumpi sessions was concern about the affordability of the vehicle barge and a perceived lack of competition. While this draft Strategy focusses on walk-on passenger transport services (rather than vehicular services), concerns about affordable access to the Island have been considered in its preparation.

Another concern is the impact from potential significant growth in people accessing the Island by private vehicles and maritime vessels. This concern will need to be addressed through investigations outside this draft Strategy.

A future bridge connecting the Island to the mainland was raised, with community members expressing divided views. Delivery of a bridge connection to Minjerribah is not currently being considered and is not in keeping with protecting and promoting the Island's unique and sensitive ecological and cultural features.

Community and visitor feedback has been used to inform the Vision, Goals and Priorities for passenger transport discussed in the next section, along with potential actions identified to improve the Minjerribah passenger transport system.



Looking to the future for passenger transport

Vision

Passenger transport enhances Minjerribah as a tourism destination of choice in SEQ for local, national and international visitors, and supports mobility of the resident population.

Goals

Enhance customer experience

Passenger transport to, from and on Minjerribah will provide a travel experience that is safe, comfortable and userfriendly. The passenger transport system will be tailored to respond to the needs of both residents and visitors with respect to passenger information, payment options and customer service.

Provide an efficient network and services

The network supporting passenger transport services to, from and on Minjerribah will be an efficient multi-modal network, which is legible, well integrated, and services key destinations for both residents and visitors. Services are reliable and provide for integration at key terminals on Minjerribah and the mainland.

Support connected communities

Passenger transport to, from and on Minjerribah provides access to key tourist destinations while addressing the need for residents to access employment and services. The network and services are responsive to the land use pattern on the Island, efficiently and reliably connecting the Island's communities, key tourist destinations and the mainland.

Technology and innovation

Fit-for-purpose responses to passenger transport challenges and opportunities will be encouraged. Innovative approaches to the provision of passenger transport will be facilitated to ensure they contribute to the Vision for passenger transport on Minjerribah and respond to community need.



Consultation Draft Minjerribah Public Transport Strategy, Transport and Main Roads, 2021





Challenges

The geographical location of Minjerribah, mix of resident and tourist demands, number of transport modes and service providers, and need to interchange between services, are key challenges for the Island's passenger transport network. However, this also presents opportunities for improvements to certain aspects of the passenger transport system.

Fare payment and ticketing

The fare payment and ticketing systems vary across the passenger transport modes, potentially resulting in confusion for visitors who are not familiar with the passenger transport system.

The Public Transport Survey undertaken by TMR identified that passengers expect integrated payment options when transferring from one service to another. The separate fare payment and ticketing systems for each leg of the transport journey can impact on the efficiency of services and customer experience, particularly for the bus services during peak periods.

Integration between modes

The passenger transport network to, from and on Minjerribah consists of a range of modes and a three-part journey (mainland, water crossing and Island transport) requiring interchange between modes and services provided by both public and private operators. Coordinating services to enable seamless interchanging is a challenge for this multicomponent network of services.

Cash-only payments for Minjerribah bus services can result in long boarding times during busy periods, and potentially result in late-running services. In turn, this can result in delayed ferry departure times, due to the need to wait for passengers traveling to terminals by bus, or else result in passengers missing ferry services all together.

Passenger information

Due to the mix of public and private services that make up the passenger transport network, there is currently no central location where information on passenger transport services to, from and on Minjerribah can be found. This presents a challenge when planning trips to the Island, particularly for visitors who are not familiar with the passenger transport network and do not know where to find relevant, up-to-date information. In addition, bus service departure times on the Island are only provided for stops at Mulumba and Gumpi. For all other stops, passengers are required to estimate departure times. This creates a challenge for trip planning and can potentially result in long waiting times for those arriving early at stops or missed services for those arriving late.

Reliable services

Reliable services are essential in encouraging people to use passenger transport. However, challenges relating to peak period demand, payment of fares and integration between modes can impact on service reliability and coordination.

Fluctuating demand

The number of residents, workers and visitors using passenger transport services to, from and on Minjerribah is variable and influenced by a range of seasonal factors, including holidays, events and good weather. Passenger boarding data indicates that use of the passenger ferries is less susceptible to seasonal demands than bus patronage on Minjerribah. However, variation in demand presents a challenge for all modes in providing a timetable that provides enough certainty to passengers while allowing flexibility to accommodate fluctuations in demand and patronage.

Fit for purpose facilities

Safe, comfortable and fit-for-purpose facilities can enhance user experience and encourage travel by passenger transport. For example, well-lit terminals and stops with appropriate seating and shelter can enhance the feeling of safety and comfort, while bicycle storage facilities and raised platforms can enhance access to services for those who can not drive and those with mobility impairments respectively.

Given the range of passenger transport modes and facilities, providing fit-for-purpose facilities which support tourism goals for the Island would require significant investment. Variations in passenger demand (depending on time of year and weather conditions) and the need to coordinate with potentially changing future land uses present challenges to the provision of fit-for-purpose facilities which provide value for money and justify significant investment.

Consultation Draft Minjerribah Public Transport Strategy, Transport and Main Roads, 2021



Opportunities

The nature of the current passenger transport network servicing Minjerribah presents opportunities for value-for-money enhancements, which have the potential to make significant positive impacts. Other Minjerribah Futures initiatives, such as the Gumpi Master Plan, also provide opportunities for the future of passenger transport on the Island.

Increased visitor numbers outside of peak periods

The focus on increasing visitor numbers outside of peak periods presents an opportunity to encourage more visitors to use passenger transport and promote sustainable year-round patronage of passenger transport services.

Increased passenger volumes outside peak periods would support further investment in planning and delivery of improvements to the passenger transport network, which benefits both visitors and residents.

Growth in the education and training sector presents an opportunity to increase passenger transport demand

Investment in eco-tourism

The focus and planned investment in eco-tourism is consistent with increasing the use of the passenger transport network. Passenger transport services provide a sustainable alternative to the use of private vehicles by visitors and residents and have the capacity to move visitors more sustainably than private vehicles.

Private tourism operator investment in zero or low-emission servies (such as through electric buses and ferries) could help protect Minjerribah's natural environment and would be consistent with a focus on eco-tourism.



Reinforcing gateways on mainland and Minjerribah

Toondah Harbour and Gumpi represent the gateways to Minjerribah from a transport perspective. There is an opportunity to support these locations as gateways, gain network efficiencies and enhance customer experience, through the provision of quality passenger transport facilities and services at these locations.

Growth in the education and training sector

Minjerribah Futures identifies growth in the education and training sector on Minjerribah as a way of diversifying the Island's economy.

While still seasonal due to academic term times, growth in the education and training sector presents an opportunity to increase passenger transport demand during off-peak periods, and is less variable and susceptible to peak seasonal demands of tourism.

Led by QYAC, the old secondary school campus in Gumpi is being transformed into the Minjerribah education, employment and training centre known as Minjerribah Ganaba (Place to Hear, Think and Understand). Passenger transport will play an important role in supporting this and other education and training opportunities.

Zero and low-emission ferries

The first hybrid (diesel-electric) passenger/vehicular ferry service commenced in Scotland in 2013. Since then, Scandinavia has led the world in the implementation of electric ferry services, with the first all-electric service commencing in Norway in 2015.

Electric ferries offer a range of benefits over existing diesel-powered vessels, including significantly less CO₂ emission, less noise and lower operating costs.

Priorities

Six priorities have been identified to support the Vision and Goals for the future of passenger transport to, from and on Minjerribah. These priorities have been informed by the challenges and opportunities identified and in response to feedback from the community and key stakeholders.

Each priority identifies objectives, along with potential further investigations required to achieve the objectives.

| Priority 1 | Make it easier to pay fares | |
|------------|---|---|
| Priority 2 | Improve passenger information | |
| Priority 3 | Provide quality passenger transport infrastructure | |
| Priority 4 | Efficient network and services | |
| Priority 5 | Encourage more people to choose passenger transport for their mobility needs, to, from and on Minjerribah | |
| Priority 6 | Support appropriate private sector passenger transport services and sustainable tourism opportunities | a second a s |





Priority 1: Make it easier to pay fares

Improvements to ticketing and the integration of fares across the mainland, water-crossing and Island services was the most requested and discussed item throughout community engagement activities. Integration of fares and ticketing across transport modes was identified by 50 per cent of survey respondents as one their top three priorities.

PRIORITY 1 SUPPORTS:

- Goal 1 Customer experience
- Goal 2 Efficient network and services
- Goal 4 Technology and innovation

Queensland Tourism and Transport Strategy

• Theme 3: Ticketing and products

Transport Coordination Plan 2017–2027

- Customer experience and affordability key area
- Efficiency and productivity key area

Minjerribah Futures Tourism initiative – Enhance the visitor experience

Smart Ticketing

The Queensland Government allocated \$371.1 million in June 2018 to design, develop, build and implement a new ticketing solution for all of Queensland's urban public transport networks.

Minjerribah's public transport network will be included in the system rollout, expected by 2022.

The solution will allow all public transport customers to use, in addition to existing go cards and paper tickets, their contactless debit or credit cards, smart phones and smart watches to pay for trips.

Customers will also benefit from an improved digital experience, including an integrated payment, ticketing and journey planning app and website, with precise real time information.

The project is finalising detailed planning, with design and development commencing in 2018. Trialing of elements of Smart Ticketing commenced on Minjerribah in 2019. Making it easier for passengers to pay fares will significantly improve customer experience. Easier payment of fares will also have positive impacts on the efficiency and reliability of the bus service on Minjerribah and provide a consistent experience for passengers.

Objective 1.1: Customers are provided with an increased range of payment options on passenger transport services

Additional payment options was the element most frequently raised by the community in engagement activities. The cash-only arrangement on the Minjerribah bus service is a particular issue and does not align with passenger expectations with regard to payment options. It may also result in negative experiences of the Island as a tourist destination for day visitors, should they not be able to board their desired connecting bus due to not having cash. The cash only payment for bus fares also impacts the reliability of the bus timetable due to the impacts on boarding times.

"An integrated ticketing system would be great. The cash for public bus holds up the bus services and the bus drivers."

Introducing a variety of payment methods on the bus service in particular is anticipated to significantly decrease boarding times and improve customer experience.

"Electronic ticketing/passes will speed up bus services as biggest delay at peak times is boarding and paying fares."

Smart ticketing presents a significant opportunity to increase the range of payment options available to passengers using the bus and train services to travel to, from and on Minjerribah. The project provides a medium term solution to the challenges of a cash only system on the bus service.

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Objective 1.2: Ticketing is integrated across passenger transport modes through an easy to use platform

Currently, separate fares and payment methods exist across the three components of the passenger transport journey (mainland connections, water crossing and Island services). The separation of fares is likely to be more accepted by passengers should there be integration of ticketing and payment options with greater choice in how passengers can pay for a ticket.

As private services, the fare and payment system for the passenger ferries is not regulated by the state government and is not part of the TransLink network. The smart ticketing project provides an opportunity in the medium to long terms to enable third party services run by private operators, such as the passenger ferries, to be paid for using a customer's choice of payment method.

Working with ferry operators to investigate opportunities for customers to pay for the passenger ferry using their choice of payment method presents an opportunity for a seamless transfer, without the need to pay for fares separately.

Objective 1.3: Ticketing products are tourist friendly

The *Queensland Tourism and Transport Strategy* is focused on improving ticketing products and enabling tailored tourist tickets to be developed, which make it easier for visitors to travel on passenger transport in Queensland.

There is an opportunity in the medium to long term to develop a tourist ticket that makes it easier for visitors to travel on passenger transport to, from and on Minjerribah. This ticket could be tailored to Minjerribah visitors or part of a broader South-East Queensland tourist ticket.

"Removing barriers such as the need for visitor to find, purchase, understand and use different ticketing systems can encourage visitors to see more of Queensland's destination."

Challenges

- Inconsistent mobile coverage on the Island limits implementation and use of smart devices and apps to assist journey planning and wayfinding
- Integrating payment options across public and privately operated passenger transport services.

Opportunities

- Capitalising on improvements made to the system as part of the smart ticketing project
- Addressing on-time running challenges by replacing the cash only service on the Island.

Opal Pay

Sydney has an extensive ferry and water taxi network provided by both subsidised public transport and private operators. Ferry routes and water taxis operated privately are subject to an independent fare structure separate to the Opal ticketing system (the equivalent of a TransLink *go* card).

In January 2018 'OpalPay' was rolled out on three of the private ferry routes in Sydney operated by SeaLink and Captain Cook Cruises to Manly, Watsons Bay and Lane Cove (including the Manly Fast Ferry). 'OpalPay' is the first extension of the NSW public transport ticketing system to the private sector. Passengers now have the option to purchase the private ferry operators fare using the available balance on their Opal card by tapping on at the point-of-sale device on board.

This offers a single form of payment and no cash or other payment type is required which offers an increased level of convenience for passengers. However, the agreement does not provide customers with integrated ticketing or incorporate 'best fare' arrangements into transfers between modes and relevant incentive schemes and discounts do not apply.

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Draft actions:

| Actio | | Proposed Lead | Proposed Partners | Indicative Timeframe |
|-------|--|----------------------|-------------------------|-------------------------|
| 1.1 | Implement a variety of payment methods on current bus and train services through the smart ticketing project. | TMR | Bus and train operators | Short to medium |
| 1.2 | Investigate opportunities to enhance passenger ferry payment options which may arise through implementation of the smart ticketing project. | TMR | Ferry operators | Medium |
| 1.3 | Tourism operators to work with TMR to develop a tourist ticket or product that includes access to passenger transport services (e.g. bus and ferry) to, from and on Minjerribah. | Tourism operators | TMR DTIS | Medium |





Priority 2: Improve passenger information

PRIORITY 2 SUPPORTS:

Goal 1 – Customer experience

Goal 4 - Technology and innovation

Queensland Tourism and Transport Strategy
Theme 1: Visitor information

Transport Coordination Plan 2017–2027

Customer experience and affordability key area

Minjerribah Futures Tourism initiative – Enhance the visitor experience

A key element of the passenger experience is the quality of information provided to assist passengers in:

- making the choice to travel by passenger transport especially when for certain components of a trip there are cost and time competitive alternatives
- undertaking their journey
- · finding their destination easily.

Improved passenger information can enhance customer experience by making it easier for passengers to plan their journey and tailor their trip on passenger transport services. Use of online platforms and applications can enable alignment with existing journey planner tools and maximise the opportunity for innovation.

Objective 2.1: Journey planning and passenger information is readily available, easy to use, consistent and reliable.

Information about passenger transport to, from and on Minjerribah is currently available across a number of different websites and platforms. This has been highlighted by passengers as a challenge when planning their journey.

"Information, communicated clearly and well in advance, is necessary at all points."

There is an opportunity to integrate journey planning information into a centralised location, such as the TransLink Journey Planner, for a better customer outcome.

"For tourists it's very difficult to go to one site for information on how to travel to North Stradbroke Island [Minjerribah]..." Providing a centralised, consistent and reliable source of information about all passenger transport services to, from and on Minjerribah will help to improve awareness of available services, and share information about timetables, fares, payment options and service updates.

"Easier to access timetable information online and at stops (particularly for bus)".

Journey planning tools such as stop timetables, online journey planning and real-time updates on services contribute to making it easier for passengers to figure out how they can get to where they need to go.

Objective 2.2: Wayfinding tools, such as signage, clearly show passengers where and how they can access passenger transport services

Improved wayfinding on Minjerribah is identified in Minjerribah Futures as key to enhancing the visitor experience.

The passenger transport information signage on Minjerribah lacks consistency, with limited information provided at ferry terminals and bus stops. Information is not readily accessible to passengers, which can impact on visitors in particular who are not familiar with the Island.

The provision of wayfinding signage at ferry terminals was raised during community consultation as a way to assist visitors who are unfamiliar with the Island. It was suggested improved wayfinding signage at ferry terminals and signage directing visitors to the Gumpi centre would help visitors to access goods and services and spend more time in Gumpi.

Providing wayfinding tools, such as signage, will support passengers in accessing passenger transport services on the Island, and knowing where they can access goods and services and how to get there.

Queensland Tourism and Transport Strategy

"Understanding the local public transport network is the most significant challenge for visitors. The fare structure and ticketing system can be confusing, information difficult to access and often not in their native language." Tourism and Transport Forum



Challenges

- Inconsistent mobile coverage on the Island limits implementation and use of smart devices and apps to assist journey
 planning and wayfinding
- · Sharing and coordination of service data across different systems, such as TransLink, Qconnect and privately-run services.

Opportunities

- · Link to movement, connectivity and wayfinding strategies and transport projects in the Gumpi Master Plan.
- Continue to share data with private sector web developers to encourage development of new journey planning platforms and applications, providing the opportunity to provide information on public and private sector transport services in a combined journey planning platform
- The smart ticketing project provides the platform for the future potential provision of digital wayfinding and real-time passenger information systems
- · Reinforce Gumpi as the gateway to Minjerribah through wayfinding tools.



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Draft actions:

| Action | | Proposed Lead | Proposed Partners | Indicative Timeframe |
|--------|--|------------------|------------------------------|-------------------------|
| 2.1 | Provide mainland transport, passenger ferry and Minjerribah bus information on one journey planning platform, as part of the smart ticketing solution, to enable end-to-end trip planning. | TMR | Service operators | Short to medium |
| 2.2 | Work with partner agencies to develop a signage strategy which includes wayfinding and passenger information signage at: Cleveland Train Station and Cleveland CBD bus stops Bus stops and ferry terminals at Toondah Harbour Ferry terminals on Minjerribah Bus stops on Minjerribah Mulumba, Pulan and Gumpi. | TMR | RCC QR DSDILGP QYAC | Medium |
| 2.3 | Investigate the feasibility of rolling out stop-specific timetables for all bus stops on Minjerribah. | TMR | Bus operator | Short |
| 2.4 | Continue to share public transport timetable information with third-parties, to facilitate development of integrated journey planners that may also include private tourist services. | TMR | Service operators | Medium |
| 2.5 | Implement real-time information on Minjerribah bus services to support journey planning. | TMR | Service operators | Short |
| 2.6 | Investigate the feasibility of including the Minjerribah bus service in the South East Queensland TransLink zones system. | TMR | | Medium |

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Priority 3 Provide quality passenger transport infrastructure

The passenger experience is influenced by the quality of passenger transport facilities. Investigations aimed at improving the passenger transport network should consider the quality of existing facilities (particularly in terms of safety) as well as the effectiveness of their current location.

While they accommodate the current service arrangement, bus stops on the Island could be improved through better passenger information, facilities such as seating, and in some cases, disability access requirements.

The quality of current bus stops was also raised during community consultation as an area for improvement. It was noted that many bus stops had no shade or protection from the elements, which made them unpleasant to wait at.

The ferry terminals are currently fit-for-purpose. However, they lack wayfinding and other passenger information that would make journeys easier, particularly for visitors. The terminals at Toondah Harbour, Gumpi and One Mile do not present a strong entrance statement, which is considered a missed opportunity for enhanced tourism. The Gumpi Master Plan represents an opportunity to explore ferry terminal upgrades on Minjerribah.

PRIORITY 3 SUPPORTS:

Goal 1 - Customer experience

- Goal 2 Efficient network and services
- Goal 3 Connected communities

Queensland Tourism and Transport Strategy

· Theme 2: Transport services

Transport Coordination Plan 2017–2027

- Customer experience and affordability key area
- Efficiency and productivity key area

Minjerribah Futures Tourism initiative – Enhance the visitor experience

Objective 3.1: Passenger transport facilities are provided as part of a coordinated network that meets passenger needs

Establishing a clear network of passenger transport facilities, with a consistent approach to features such as seating, shade and passenger information, can improve the legibility of the passenger transport network and enhance customer experience. A clear network and hierarchy of facilities will also guide investment to ensure that available funding for upgrades delivers value for money.



Objective 3.2: Bus access to key locations and ferry terminals is prioritised, seamless and safe

Current access to ferry terminals and some bus stops on Minjerribah and at Toondah Harbour is difficult for buses and other passenger transport options such as taxis and ride share.

The configuration of certain bus stops, particularly the bus stop in the SeaLink terminal at Toondah Harbour, presents safety and service efficiency challenges. This can make it difficult for bus operators to safety exit and enter the flow of traffic and impact upon the safe and efficient boarding of passengers.

The ferry terminals at Toondah Harbour, One Mile (Yabby Street) and Gumpi (Junner Street) should have clear access routes for buses to access the bus stops at these locations. Currently, access for bus services conflicts with parking and drop off zones for private vehicles. This makes it difficult for buses to enter and exit the ferry terminals safely, to pick up and drop off passengers.

Objective 3.3: Access to passenger transport facilities by walking or cycling is improved

Enabling people to use active transport (walking and cycling) to get to and from bus stops and ferry terminals is an important element of building an effective passenger transport system, and ensuring that passenger transport services are accessible for all.

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Providing clear pathways and connections to ferry terminals and bus stops can improve access by walking and cycling, encouraging a shift away from private vehicles. Improvements to the pedestrian and cyclist environment in proximity to ferry terminals and key bus stops can make passenger transport services accessible to more people (including those unable to drive) and reduce the need for people to use a private vehicle.

" Encourage walking. Plant more shade trees. Pedestrianise key areas / create shared zones. Prioritise pedestrians and wildlife"

"Bike lanes would be useful."

Improved lighting at ferry terminals and key bus stops can significantly improve the real and perceived safety and security of these areas, improving the customer experience. There is lighting available at the ferry terminals and some bus stops on the network. However, a number of bus stops do not have lighting and the pedestrian routes to the ferry terminals and bus stops are not well lit, resulting in potential tripping hazards and safety concerns.

Gumpi Master Plan

The Gumpi Master Plan will be identifying key movement pathways through and around the township. This presents an opportunity for access to the ferry terminals to be considered and reviewed as well as stronger connections to the Gumpi centre to be established, providing for better integration of land use and transport.

Objective 3.4: Parking at ferry terminals is fit for purpose and sustainable

The focus of this draft Strategy is to promote and improve passenger transport to, from and on Minjerribah. When considered as part of a suite of access opportunities, the provision of car parking at passenger transport facilities is an important consideration. Car parking at Toondah Harbour and the ferry terminals at One Mile and Gumpi provide access to the passenger ferries and also interface with the movement of buses, cyclists and pedestrians in these locations. Parking at ferry terminals was consistently highlighted as an issue through the Public Transport Survey and community information sessions. Feedback from some in the community highlights that parking at the ferry terminals at Toondah Harbour, Gumpi and One Mile is not meeting the needs of those that drive. This is reflected in some of the feedback provided in the Public Transport Survey quoted below.

"More access to public parking at ferry and barge terminals, in particular One Mile ferry".

"Car parking at One Mile ferry terminal requires lined car park areas and more lighting."

"It would be great to see provision for long term and short term parking."

Addressing the challenges with car parking at Toondah Harbour, One Mile and Gumpi ferry terminals needs to be considered to ensure the needs of residents and visitors (both short and long stay) are balanced against the provision of strong active and public transport links.

"Widen parking area alongside road to allow larger vehicles, for example bus, more room to turn corner and drive along road."

A parking solution that provides for sustainable long term management of private vehicles, responds to various parking needs (long term, short term) and does not impose upon or preclude access by buses, cyclists or pedestrians is required.

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Challenges

- Balancing needs of private vehicle users against passenger transport and active transport users
- Achieving compliance with the Disability Discrimination Act and Disability Standards for Accessible Public Transport
- · Significant works required to address some accessibility issues.

Opportunities

- Promote sustainable transport options that are compatible with the Island's ecological and cultural values, such as buses, taxis, cycling and walking
- · Identify key passenger transport and active transport movement corridors as part of the Gumpi Master Plan
- · Potential to provide passenger transport facilities tailored to Minjerribah with specific branding
- Upgrades to bus and ferry fleet to focus on customer experience, such as luggage racks and on-board information.

Draft actions:

| Action | Action | | Proposed Partners | Indicative Timeframe |
|--------|--|---------|-----------------------------------|-------------------------|
| 3.1 | As part of the Gumpi Master Plan, identify opportunities for improved access to ferry terminals by walking, cycling, buses and taxis, including appropriate longer-term bicycle storage locations at Gumpi and One Mile ferry terminals. | DSDILGP | TMR RCC | Medium to long |
| 3.2 | Review Minjerribah bus stops to improve operations of the bus service and customer experience. | TMR | RCC | Short |
| 3.3 | Investigate appropriate locations for bicycle storage (such as bike racks) at Toondah Harbour. | RCC | TMR DSDILGP Ferry operators | Short |
| 3.4 | Investigate appropriate locations for interim bicycle storage (such as bike racks) at Gumpi and One Mile ferry terminals, until a more permanent bike storage solution is provided. | RCC | Ferry operators | Short |
| 3.5 | Review parking facilities including safety, access and availability and develop in partnership with key stakeholders a parking policy position for transport facilities such as at Toondah Harbour. | RCC | TMR DSDILGP | Medium to long |
| 3.6 | Investigate opportunities to enhance safety and security through improved lighting along key pedestrian routes to ferry terminals and bus stops. | RCC | TMR | Medium |
| 3.7 | Consider opportunities to enhance safe and efficient access to Toondah Harbour ferry terminals for buses, taxis, pedestrians and cyclists. | RCC | TMR Ferry operators | Medium to long |
| 3.8 | Review the fleet type provided by the Minjerribah bus system to ensure that any replacement vehicles meet the unique requirements of tourists and residents. | TMR | Bus operator | Medium |

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Priority 4 Efficient network and services

An efficient passenger transport network makes the best use of the timetabled services and available facilities. Efficient services are reliable and have clear routes and stopping patterns, providing a level of certainty to passengers.

Minjerribah is serviced by a multi-modal network with multiple journey components. As a result, reliable services are essential in ensuring integration between modes and journey components.

PRIORITY 4 SUPPORTS:

Goal 1 – Customer experience Goal 2 – Efficient network and services

• Theme 2: Transport services

Queensland Tourism and Transport Strategy

Transport Coordination Plan 2017–2027

- Customer experience and affordability key area
- Efficiency and productivity key area

Objective 4.1: A passenger transport network with clear and consistent services and routes

A network of services that are easy to use and have clear, consistent routes is important for both visitors and residents to have confidence in the reliability of passenger transport services.

Providing certainty to passengers by having defined routes and clear stopping patterns (such as set stops) promotes confidence in the network and may encourage more people to use passenger transport.

Visitors to Minjerribah can potentially benefit from the simple and direct nature of the passenger ferries and bus services on the Island, as it makes an unfamiliar location easier to navigate. This can be further improved by having clear and consistent stops that are well located and connected to key attractions.

Objective 4.2: Sustainable response to variable demand

A challenge in providing efficient services to, from and on Minjerribah is the highly variable nature of passenger numbers, which is influenced by school holidays, events and good weather. While the capacity and frequency of services provided cater for demand most of the time, significant increases in demand for passenger transport (particularly for bus services) during peak periods can be difficult to manage and service efficiently. One of the long-term goals for tourism on Minjerribah is to spread visitor trips more evenly throughout the year so that peaks are less pronounced. This will provide more consistent demand for passenger transport services and make it easier to provide a sustainable transport network. However, recognition of the different levels of demand within and outside of peak times will still be needed.

Objective 4.3: Services are well integrated across passenger transport modes

Connections between the different modes (bus, passenger ferry and rail) are crucial in the Minjerribah passenger transport network. Passengers expect to be able to make the journey from the mainland to their Island destination, and vice versa, seamlessly and without missing connections.

Responses to the Public Transport Survey highlighted the importance of integrated services, with many regularly undertaking journeys with three or more interchanges.

Gumpi as Gateway to Minjerribah

Minjerribah Futures recognises potential opportunities for Gumpi as the gateway to Minjerribah. These could include opportunities to develop the local economy, enhance tourism, culture, education, research and services business opportunities.

Reinforcing Gumpi as the primary entry point to Minjerribah is an important element in providing a clear entrance statement, especially for visitors. The ferry terminals at Gumpi (Junner Street) and One Mile (Yabby Street) do not present a clear entrance statement to Minjerribah, which is considered a missed opportunity.

The geographic separation of the ferry terminals at Toondah Harbour and more significantly at Gumpi and One Mile can result in confusion for passengers, impacting on the efficiency of the Minjerribah bus service.

Consolidating passenger ferry services at Gumpi is a potential option for addressing these issues and may be explored as part of the Gumpi Master Plan.



"Once a fortnight I go to Physio in Thornlands: I walk 10 mins to bus; take bus; take One Mile ferry; then courtesy bus to Cleveland shops; a 274 bus to Thornlands; then private car to my destination."

Currently these connections, when running to timetable, work well. However, delay in a timetabled service can disrupt the connections and result in a lack of integration. This is particularly prevalent in peak times where the time required to board a higher number of passengers can impact on the on-time running of services, particularly the bus service on Minjerribah. The hourly frequency across services to, from and on Minjerribah also mean there is a long wait time if a connection is missed, as evidenced by feedback provided in the Public Transport Survey.

"Simple clockface timetabling and fare integration using a *go* card both to and on the island would make it much easier to use...I don't believe any infrastructure change is needed, just ticketing, fares, timetables, and information."

The physical integration of services, such as the bus and ferry on both mainland terminals, is also important. Clear pathways and wayfinding to assist passengers in locating where they need to go is important to meet connections and ensure an integrated journey.

The operation of two ferry terminals in close proximity (at Gumpi and One Mile) has an impact on the efficiency of the Island's bus service, with two stops required to load and drop off passengers connecting with the ferry services. The challenges associated with access to the bus stops, particularly at One Mile, exacerbate these efficiency impacts. A combined terminal for the passenger ferry services would address efficiency impacts and provide a clear gateway to Minjerribah as well as reducing confusion for passengers who are not aware of which service goes to which ferry terminal.

Objective 4.4: Services that are reliable

Reliability of service is vital to a quality passenger transport system. Passengers expect that services are punctual and that journey times are consistent. This is particularly important in the Minjerribah passenger transport network due to the multiple connections required to undertake a full journey using passenger transport, as discussed above. Late-running of services can have major impacts on the ability of passengers to meet their connecting services.

There are a number of factors slowing the boarding process, which make it difficult for the bus timetable to be achieved in peak times. These factors include:

- cash payments for tickets
- loading of luggage
- questions from visitors about stops and attractions.

To offset this and maximise reliability, there are a number of potential solutions across the folowing areas:

- increased payment options
- buses with luggage storage areas that are easy to access and use
- improved passenger information.

Challenges

- Responding to variation in demand for passenger transport services across peak and non-peak times
- Achieving seamless integration across a number of interchanges.

Opportunities

- A clear and consistent network will benefit journey planning activities and focus investment in infrastructure
- Potential to increase awareness of bus stop locations
- Better use of existing services and infrastructure
- Improvements to fares and ticketing will result in improvements to service efficiency.



Seasonal timetable

There are examples of the variability of demand impacting the reliability of services in peak times. A seasonal timetable can address some of the on-time running challenges by providing a more realistic timetable in peak times.

Development of a seasonal timetable would need to reflect the increased travel time on the Minjerribah bus route during peak times. This would reduce pressure on the bus operator with regard to on-time running and provide greater certainty to passengers regarding the travel time at peak times.



Draft actions:

| Action | | Proposed Lead | Proposed Partners | Indicative Timeframe |
|--------|--|------------------|----------------------|-------------------------|
| 4.1 | As part of any future network review of the Minjerribah bus service, identify key timing points on bus routes to support management of bus service performance. | TMR | RCC | Short to medium |
| 4.2 | Investigate the feasibility of seasonal timetables that account for increases in patronage and changes in travel time during periods of high demand. | TMR | Bus operator | Short |
| 43 | As part of the Gumpi Master Plan, consider community feedback for: The Gumpi (Junner Street) ferry terminal to function as a single integrated facility for all regular passenger ferry The One Mile (Yabby Street) terminal being retained for use by recreational users, Volunteer Marine Rescue Stradbroke Island and emergency services. | DSDILGP | | Medium to long |



Priority 5 Encourage more people to choose passenger transport for their mobility needs to, from and on Minjerribah

Encouraging more people to use the passenger transport system to get to, from and on Minjerribah will contribute to more sustainable use of transport infrastructure. This aligns with the shared goal for the Island's future identified in Minjerribah Futures: to make Minjerribah Australia's most accessible and sustainable island community.

Encouraging a mode shift to passenger transport modes such as buses, passenger ferries and taxis provides an opportunity to move more people more sustainably and reduce the use of private vehicles.

Important elements in encouraging more people to use passenger transport more often are reliable services that offer flexibility and get people where they need to go.

With a low base demand for passenger transport on Minjerribah, there is an opportunity for alternative delivery models to be explored which provide flexible services that reach outside of the current route along East Coast Road.

Objective 5.1: Increase flexibility in services

The passenger transport task to, from and on Minjerribah is highly variable and influenced by school holidays, events and weather. While the capacity and frequency of services provided cater for demand most of the time, there is limited flexibility in the current fixed timetable to provide for the significant variation in patronage and the high demand for access to the Island in peak times.

An extended span of hours (i.e. earlier and later bus and ferry services) was raised consistently throughout the stakeholder engagement activities as a desired outcome for the future passenger transport network.

"A late night ferry say 10pm off and on the island would help holiday makers and those who work on the mainland and live in the island. Amity [Pulan] needs more bus services. An earlier ferry would allow more who work on mainland to live on island."

PRIORITY 5 SUPPORTS:

Goal 2 – Efficient network and services

Goal 3 – Connected communities Goal 4 – Technology and Innovation

Queensland Tourism and Transport Strategy

Theme 2: Transport services

Transport Coordination Plan 2017–2027

- Customer experience and affordability key area
- Efficiency and productivity key area







Extension of the bus network to meet all passenger ferry services would provide additional morning and evening services. Further investigation would be required to confirm demand for new services and would need to be informed by detailed patronage data and understanding of driver and vehicle utilisation.

The current passenger transport system on Minjerribah is consistent with a traditional service delivery model. However, the seasonal fluctuations in demand, as well as the requirement to address the needs of residents and tourists, suggest an opportunity for alternative delivery models that could support the passenger vision and goals.

Connections to Pulan

Increased frequency of bus services to Pulan was raised consistently in the community information sessions and Public Transport Survey. The Pulan service currently experiences low patronage as could be expected by the low residential population. However, feedback from the community has been that the low frequency of the bus service is seen as a barrier to people using the service regularly, and for tourists wanting to access Pulan.

"More frequent buses on the island. Lots of day trippers have no other way of getting around on the island...better service from Amity Point [Pulan]." Increased frequency of bus services to Pulan would require additional investment on a route that is currently recording very low patronage. Despite this, the opportunities for increased patronage, improved transport offerings for visitors to access attractions at Pulan and the potential for reduced parking at the passenger ferry terminals at Gumpi and One Mile are considered significant enough to explore improved services further in the medium term.

Further investigation would be required to confirm demand for improved services and will need to be informed by detailed patronage data and understanding of driver and vehicle utilisation.

"More frequent access to Amity [Pulan] via bus or boat."



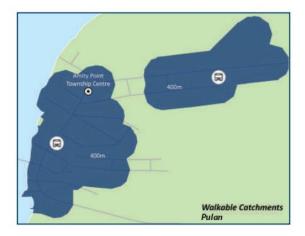


Objective 5.2: Extend the coverage of passenger transport services

Respondents to the *Public Transport Survey* listed improved access to Pulan and other residential areas as gaps in the bus network on the Island. Connections to the South Moreton Bay Islands and reliable access to mainland attractors – particularly health and education – were also highlighted.

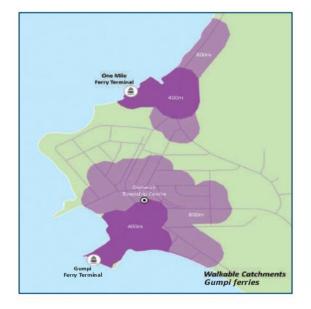
"On-island transport needs: Bus connections from Tramican Street, Cumming Parade and George Nothing Drive at Point Lookout [Mulumba]... Possibly also from Rainbow Crescent, Illawong Drive and other streets in Dunwich [Gumpi]. More connections to Amity Point [Pulan] from Dunwich [Gumpi] and Point Lookout [Mulumba]"

"...a shuttle bus service..to link up with the bus service that runs between Point Lookout [Mulumba] and Dunwich [Gumpi]." The current bus network on the Island provides consistent access from Gumpi to Mulumba, with bus services focussed on connecting visitors with attractions at Mulumba. Visitor attractions and accommodation located along the East Coast Road corridor are well serviced by the current bus route. However, there are a number of attractions that are not accessible by public transport, including Bummiera and Karboora.









For residents, the coverage of the current bus route does not extend to all residential areas of Gumpi and Mulumba. As a result of the topography in these areas, the walk to the nearest bus stop can be seen as unattractive and a barrier to using the bus by some residents. It is considered that more flexible passenger transport services, such as Demand Responsive Transport, may provide future opportunities.

"In the holidays, there could be a little shuttle bus that circulates around Point Lookout [Mulumba] on an hourly basis maybe."



Providing 'last mile' transport connections to enable people to access the Minjerribah bus and/or passenger ferry terminals without a private vehicle is key to encouraging improved passenger transport mode share and reducing the parking challenges at the ferry terminals.

While community feedback indicates there is a desire to facilitate new service delivery models in the short term, a medium to long term need for the Minjerribah passenger transport system is to consider and leverage future mobility solutions. This could include the concept of mobility management and accessing a range of mobility services through a single platform such as Mobility as a Service (MaaS).





Mobility as a Service (MaaS)

TMR is exploring the concept of MaaS, which reduces reliance on personal cars by offering more integrated mobility solutions. MaaS is the combination of public and private transportation services accessed via an application which provides personalised journey planning, booking and payment.

The european island communities of Arran and Orkney in Scotland, Madeira in Portugal, Crete in Greece, Gran Canaria in the Canary Islands and Elba in Italy are among the first locations for trials of MaaS. While the population and number of tourists attracted to these areas varies compared to Minjerribah, they share a number of characteristics including:

- High seasonal demand
- Disparate origins and few concentrated destinations
- Low overall use of public transport
- Correspondingly large use of private vehicles
- Fragmentation of information, marketing, accessibility and cooperation.

MaaS in a tourist island setting could help to encourage sustainable transport modes (such as public transport and ride-sharing) and add to the attractiveness, sustainability and accessibility of the island for visitors.



Challenges

- Low patronage and low base demand for services
- Different needs across passenger groups such as visitors and residents.

Opportunities

- Increased span of hours and improved passenger transport catchments can encourage mode shift to passenger transport and improve mobility outcomes
- Explore additional options for transporting visitors to Minjerribah including new service offerings and nontraditional passenger transport services.

Draft actions:

| Action | Action | | Proposed Partners | Indicative Timeframe |
|--------|--|--|-----------------------|-------------------------|
| 5.1 | Explore the opportunity for more flexible passenger transport options to increase sustainable mobility options for residents and visitors to Pulan. | TMR | Bus operator | Medium |
| 5.2 | Investigate the feasibility of additional bus services that connect with earlier ferries, and later services from Gumpi/One Mile. | TMR | | Medium |
| 5-3 | Investigate opportunities (including active and flexible transport options) for enhancing access to public transport services at key attractors and areas that are not currently serviced. | TMR | RCC Private sector | Medium |
| 5.4 | Undertake a human-centred design workshop with Minjerribah stakeholders to understand travel needs from a customer perspective and look to inform a possible MaaS proof-of-concept that could be tested. | TMR | | Short to medium |
| 5.5 | Investigate opportunities to improve active transport and passenger transport connections between Toondah Harbour and the Cleveland CBD, based on growth and demand. | RCC (active transport) TMR (passenger transport) | | Medium |



Priority 6 Support appropriate private sector passenger transport services and sustainable tourism opportunities

The *Queensland Tourism and Transport* Strategy recognises that public and private partnerships with a commercial focus can be key to achieving desirable transport outcomes.

Minjerribah is already supported by private sector investment in passenger transport, with both passenger ferries and the vehicle barge being privately owned and operated. There is currently no Government regulation over the passenger ferry crossing.

There is also a wide range of integrated transport and tourism products available that make use of the passenger ferry or vehicle barge marine infrastructure on Minjerribah, such as privately-operated Island tours which include transport to, from and around the Island as part of their package.

Efficiently servicing both the Island's low base-demand for passenger transport services and peak-period visitor demand is a key challenge for the passenger transport network. However, there is potential for additional private-sector passenger transport services to cater for demands that cannot be efficiently met by the existing network.

There is significant opportunity for improvements to passenger transport to, from and on Minjerribah to be led by the private sector with guidance and support from the state government where required.

For example, in July 2019, the Queensland Government announced it would partner with Australian Jet Boats Pty Ltd in a \$2.1 million project to build 4 new jet boats, to be used to enhance visitor access to Moreton Bay.

The new jet boats have been used to provide a privately run tour between central Brisbane and tourist destinations in Moreton Bay, including Minjerribah. The first of four locally-built boats was launched in February 2020.

There is also a wide range of integrated transport and tourism products available that make use of the passenger ferry or vehicle barge marine infrastructure on Minjerribah.





Objective 6.1: Support appropriate private sector investment in tourism-focused passenger transport services

The state government is supportive of leveraging private sector investment and innovation to develop a sustainable passenger transport network and achieve good economic and social outcomes.

A number of factors and opportunities suggest the private sector is well placed to invest in tourism-focussed passenger transport services.

With no government regulation over the passenger ferry crossing, there are opportunities for private operators to establish new service offerings from a range of mainland destinations, such as Brisbane or the Gold Coast.

"Allow Quandamooka Coast and other island tours to grow, particularly the international market on week days (low season) supporting year round stability in economy."

This could involve, for example, private entities developing tourist packages which include marine transport between popular tourist locations on the mainland (such as Raby Bay and the Gold Coast) and Minjerribah. This could enhance the appeal of Minjerribah for visitors by making the marine journey a focus of the visitor experience.

PRIORITY 6 SUPPORTS:

- Goal 1 Enhance customer experience
- Goal 2 -Efficient network and services
- Goal 3 Connected communities
- Goal 4 Technology and innovation

Queensland Tourism and Transport Strategy

- Theme 2: Transport services
- Theme 4: Planning and investment

Transport Coordination Plan 2017–2027

Challenges

- Responding to variation in demand for passenger transport services across peak and non-peak times
- Low patronage and low base demand for services
- Different needs across passenger groups (resident and visitor).

Opportunities

In developing the draft Strategy a number of potential connections for visitors between Minjerribah and the mainland were identified. These connections are not currently appropriate for the State to provide but may be feasible for delivery by the private sector. Suggested connections are shown in Figure 7.





Draft action:

| Action | | Proposed Lead | Proposed Partners | Indicative Timeframe |
|--------|---|---------------------|----------------------|-------------------------|
| 6.1 | Promote the opportunity for private sector delivery of new transport connections to, from and on Minjerribah for tourists and visitors. | DTIS | RCC QYAC TMR | Short to medium |
| 6.2 | Explore use of zero and low-emission vehicle fleets such as electric bus and ferries. | Private operator | TMR DTIS | Medium to long |



Where to from here

This draft Strategy identifies a range of priority improvements to Minjerribah's passenger transport network, along with objectives and potential further investigations aimed at achieving these priority improvements.

The priority improvements aim to support Minjerribah Futures by supporting better access to, from and on Minjerribah for visitors and residents. In addition, these priority improvements support a more sustainable transport system, which is compatible with the Island's ecological and cultural values.

The Queensland Government will continue to work with key partners and collaborate with industry to determine further short term investigations and explore innovative ways to deliver on the medium and long term opportunities.

Have your say

To help us plan for the future of Minjerribah's passenger transport network, we invite you to provide feedback on this draft Strategy by emailing:

NStradbrokePTStrategy@tmr.qld.gov.au

Alternatively, you can write to:

Draft Minjerribah Public Transport Strategy Department of Transport and Main Roads GPO Box 213 BRISBANE QLD 4001





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| Contents Page | Deadmans Beach, Point Lookout, Tourism and Events Queensland. |
| Page 2 | Couple on Amity Jetty, Tourism and Events Queensland. |
| Page 4 | Frenchmans Beach, Tourism and Events Queensland. |
| Page 6 | Headland at Point Lookout, Tourism and Events Queensland. |
| Page 8 | Beach, Tourism and Events Queensland. |
| Page 11 | Surf Life Saving, Cylinder Beach, Tourism and Events Queensland. |
| Page 13 | Whale Watch Tower, Point Lookout, Tourism and Events Queensland. |
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| Page 19 | Adder Rock Campground, Mulumba, David Kelly. |
| Page 21 | Gorge walk, Point Lookout, Tourism and Events Queensland. |
| Page 26 | One Mile Boat Harbour, Dunwich, Tourism and Events Queensland. |
| Page 27 | Mirriginpah place marker at Pulan by Belinda Close, Minjerribah Futures. |
| Page 28 | Deadmans Beach Point Lookout, Tourism and Events Queensland. |
| Page 28 | Fishing, Amity jetty, Tourism and Events Queensland. |
| Page 30 | Frenchmans Beach, Tourism and Events Queensland. |
| Page 32 | Blue Lake, Tourism and Events Queensland. |
| Page 34 | Walking Access to Deadmans Beach, Tourism and Events Queensland. |
| Page 35 | Deadmans Beach, Point Lookout Tourism and Events Queensland. |
| | |

| Acronyms | |
|----------|--|
| CBD | Central Business District |
| DTIS | Department of Tourism, Innovation and Sport |
| MaaS | Mobility as a Service |
| QGSO | Queensland Government Statistician's Office |
| DSDILGP | Department of State Development, Infrastructure, Local Government and Planning |
| QYAC | Quandamooka Yoolooburrabee Aboriginal Corporation |
| RCC | Redland City Council |
| SCoC | Straddie Chamber of Commerce |
| TMR | Department of Transport and Main Roads |







Draft Minjerribah (North Stradbroke Island) Public Transport Strategy

Redland City Council Submission - November 2021

Background

On Friday 22 October 2021, the Department of Transport and Main Roads (TMR) notified Redland City Council (Council) that the draft Minjerribah Public Transport Strategy (Draft Strategy) was released for public consultation. TMR invited Council to provide feedback on the Draft Strategy by Friday 19 November 2021. This report outlines Council's response to the Draft Strategy.

Responsibility for delivery of actions

Council supports the delivery of the Draft Strategy, as there is a clear need for improved public transport service and access. It is acknowledged that there are multiple stakeholders involved in delivering the plan. However, Council does have some concerns with where responsibilities lie for delivering actions of the Draft Strategy. It is unrealistic for Council to fund and deliver all the actions listed without guidance and resourcing from the other levels of Government for many of these actions.

Council's capacity to resource and fund the planning and infrastructure-related actions is limited. Therefore, it is imperative that TMR indicates how it will support Council with delivering actions where the Draft Strategy nominates Council as lead or partner agency. It is important to note that Council's capacity to fund trunk infrastructure on Minjerribah is limited due to Council's current list of priorities around the city. Also, some of the actions assigned to Council within the Toondah Harbour Priority Development Area and Draft Gumpi Dunwich Master Plan Area may be the responsibility of other government agencies.

There is an urgent need for improved public transport services and facilities to support tourism and growth arising from Minjerribah's (North Stradbroke Island) economic and tourism transition, agreements with Quandamooka Yoolooburrabee Aboriginal Council (QYAC) as well as the upcoming Olympics, which has been confirmed since the preliminary draft was provided for comment in 2019.

Level of Detail in the Strategy

In 2018, Council had been involved as a stakeholder and provided comment on the North Stradbroke Island Passenger Transport Study – Draft Summary Report (Draft Summary Report). The Draft Summary Report was detailed and explained the analysis and rationale supporting a long list of actions. These have been removed from the Draft Strategy, which now lacks the detail and range of actions that were outlined in the Draft Summary Report (such as the provision of additional public transport services). The Draft Summary Report outlined that, depending on the nominated lead and supporting agencies (now written as partner agencies in the Draft Strategy), there may be several funding opportunities for a given action – this important information is now missing in the Draft Strategy.



One of the major concerns Council has with the Draft Strategy is that the actions do not provide adequate detail as to how they will be implemented and does not provide indicative cost and funding responsibilities.

The Draft Strategy should include an implementation plan that identifies lead agency responsibility, indicative costings, funding sources and delivery timeframes and reflects Council's capacity to deliver initiatives assigned to it as lead agency.

Need to Update the Strategy in light of other Initiatives

Since the Draft Summary Report was released, there have been a number of other strategies prepared covering both the mainland and Minjerribah. These include the Minjerribah Futures Project and the Draft Gumpi Dunwich Master Plan. The Draft Strategy should integrate the findings and recommendations of these projects into the report.

The assumptions underpinning the projected population growth in the Draft Strategy also need to be adjusted upwards to reflect the impact of these initiatives on transport and public transport demand and identify actions that address this increase.

Need to improve the level of public transport and infrastructure supporting it

Substantially enhancing public transport services and improving terminal facilities are needed to boost the economic and social benefits arising from the Minjerribah Futures Project and the 2032 Brisbane Olympics for island and mainland communities. The detailed comments below outline some actions that could boost these benefits, while supporting an integrated transport system.

Consideration of initiatives that reduce parking demand at ferry terminals on the mainland and island.

There needs to be consideration of initiatives that reduce parking and vehicular demand on Minjerribah and on the mainland. These include increased frequency, extent and duration of public transport and ferry services, consideration of car share schemes, improved connectivity and facilities for active transport and e- mobility modes, as well as improved and integrated ferry and bus interchange facilities.

Detailed considerations

Detailed feedback from across Council on the Draft Strategy is provided in the table below.



Table 1: Redland City Council consolidated feedback on the Draft Minjerribah Public Transport Strategy (as of 8 November 2021)

| Page reference | Theme | Comment |
|----------------|--------------------------|--|
| General | Coordination | There is scope for better integration of the Draft Strategy and the draft Gumpi Dunwich Master Plan. It is noted in the Draft Strategy that it covers a broader area than Gumpi, however the Draft Strategy could include further references and details on relevant implementation actions from the draft Gumpi Dunwich Master Plan. In addition, some case matters referred to in the Draft Strategy have already been addressed in more detail in the draft Gumpi Master Plan – e.g. wayfinding, movement and connectivity. |
| General | Marine infrastructure | Council is the manager of some of the marine assets for example barge and ferry landings such as the One Mile Pontoon. However the facilities used by SeaLink at Gumpi are SeaLink's infrastructure. |
| General | Background | Council considers that any planning or actions in relation to Toondah Harbour should be subject to the Priority Development Area (PDA) over the space which is the responsibility of Economic Development Queensland (EDQ). |
| General | Background | Suggest adding more feedback that was provided by the community during the engagement sessions in 2018. Much of the feedback and suggestions still resonate today and in fact some, more relevant than ever. <u>https://www.tmr.qld.gov.au/projects/minjerribah-north-stradbroke-island-public-transport-</u> <u>strategy/community-information-sessions-engagement-summary</u> |
| General | Background | Transport is an integral element in facilitating the movement of goods and people and provides value to local economies. In Minjerribah, transport is critical in the island's transition to tourism as one of its major industries. Tourism's development is significantly influenced by the development of transportation. Poor accessibility can hinder visitor numbers, and also exacerbate the cost of living for island residents who are reliant on public transport. |
| General | Background | While the strategy is seen as primarily supporting the tourism industry, it is also important to note that facilitating an efficient and integrated transport network can positively impact business growth and development. Hence, facilitating an opportunity for a more diversified local economy and job creation. It also ensures the mobility of resident workers as well as workers who work on the island but live on the mainland. |
| General | Background | The Minjerribah Visitor Research Program which was conducted by the University of Queensland (2018-19) as part of the NSI Economic Transition Strategy (ETS) identified three major improvements required: transport connectivity, customer service and enhancing food and beverage offerings. All of these areas of improvement are interlinked and are essential in creating a well-rounded tourism product/experience. |
| General | Parking | Car-share is identified in the Redlands Coast Transport Strategy as a way in which Council could reduce car dependence and parking demand within Redland City. Recently, Council successfully trialled a car-share trial at |

| | | the Redland Bay Marina ferry terminal from Aug 2020 to April 2021. Since then, Council have nominated a suitable car-share operator to run the car-share operation over an extended period. Results from the trial showed that many people were signing up and using the car-share scheme. This is a positive sign and may relieve park-n-ride demand into the future. We would recommend that TMR consider building this into the report as an opportunity, even if it is noted as a case study. The Summary Report from the trial recommends that such car-share schemes be implemented in similar types of environments (such as Minjerribah and Toondah Harbour). |
|---------------------------------------|--------------|--|
| p. 4 Quandamooka Country | Background | Council is aware the State Government has made planning, land use and development commitments with the Quandamooka People through a confidential Indigenous Land Use Agreement (ILUA). It is understood the State Government commencement of the Temporary Local Planning Instrument (TLPI) 2/2020 Quandamooka Land Aspirations Area in 2020 is directly related to these commitments. The Draft Strategy should include appropriate background and information on the State Government and Quandamooka ILUA and the likely outcomes of them as they will potentially have major implications and add significant demand to all of the transport networks servicing Minjerribah. |
| p. 5 Demographics | Background | The report currently notes estimated resident population of approximately 2,150 persons increasing to just under 2,500 by 2036. In 2020, the State Government commenced TLPI 2/2020 Quandamooka Land Aspirations Area which amended the zoning of 25 properties on the island from generally a conservation or open space zone to residential, industrial, and commercial or an emerging community zoning. In July 2021, the Planning Minister directed Council to amend its City Plan to incorporate the zonings applied through the TLPI. Council's understanding is the rezoning is directly related to commitments made by the State Government in its ILUA with the Quandamooka People. In addition, it is understood that potentially the State Government and Quandamooka ILUA provides further commitments in terms of future land use and development. Recognising the extent of area that has been rezoned and high likelihood that the State has made further land use and development commitments, it is questionable whether the projected population growth of 350 persons over the next 15 years is accurate or whether the projection should be somewhat higher. The potential growth as a result of State ILUA commitments will have implications and add demand to all of the transport networks and should be considered in detail as part of the Draft Strategy. |
| p. 8 Public Transport Survey | Consultation | The report should better detail the involvement of key stakeholders and perhaps detail their critical interests and how it has contributed to the Draft Strategy (particularly QYAC's involvement). QYAC are the largest single entity and major land holder on NSI without their input and support, infrastructure cannot be built. The needs of Minjerribah are very different from the mainland and solutions will need to accommodate this. For example, these solutions should be responsive to seasonal fluctuations in visitor numbers and the diverse |

| | | needs of the local indigenous community. The three townships also have different transport requirements and very little existing infrastructure to support their needs. |
|------------------------|--------------------------|--|
| p. 12 Challenges | Service duration | The lack of late-night passenger ferry services beyond 7.55pm, coupled with the lack of on-island longer-term rental accommodation is contributing to a significant skills shortage of workers on Minjerribah, which impacts on tourism and hospitality service delivery, consistency of experience offering and viability. |
| | Frequency | Bus service frequency and available vehicles and drivers is resulting in overcrowding on the bus service network – contributing to a poor visitor experience. |
| | Access to attractions | Limited route alignment on-island and insufficient active transport infrastructure to disperse visitors safely across the island including to popular tourism attractions – e.g. there is currently limited bus services to Amity Point township, no public transport or safe pathways to Brown Lake, Blue Lake and other attractors in Dunwich. |
| p. 13 Opportunities | Water transport | Opportunities for better and more competitive water transport options and more specifically to investigate the feasibility to connect our islands by having regular inter-island ferry services to and from each of the various ports including Toondah Harbour, Victoria Point, Redland Bay and Brisbane CBD. This would reduce the parking constraints currently experienced at Toondah Harbour during peak seasons. It would also provide a more consistent, flexible and adaptable use of the public transport connections across Moreton Bay. |
| | Increased Services | Opportunities for better visitor dispersal on Minjerribah – through increased bus services and routes to all townships not just from Dunwich to Point Lookout. |
| | Frequency | Opportunities for peak season could be a frequent hop-on/hop-off model specifically designed for visitors/tourists that runs every 5-10 minutes to assist with visitor dispersal across the island in a stress-free, relaxed and consistent manner. This would also minimise the need for visitors to bring their vehicles on the island and would support lowering the carbon-footprint and align with the islands vision as an eco-cultural destination. High frequency public transport is an Advocacy Priority for Council, as detailed in the Redlands Coast Transport Strategy 2020. |
| | Coordination | This section generally notes opportunity to reinforce gateways on the mainland and Minjerribah (Gumpi). This section should however include further references to the draft Gumpi Dunwich Master Plan and how it intends to support Gumpi as the gateway to the island while also encouraging people to stay longer in Gumpi. For example, this section could reference some of the specific implementation actions from the Gumpi Dunwich Master Plan like the redevelopment of the Junner Street Barge and Passenger Ferry Terminal. |
| p. 14 Priorities | Active Transport | Active transport infrastructure should have a stronger reference in the six priority areas. |



| p. 12-16 Challenges, Opportunities and Priorities | Analysis (identifying Challenges and Opportunities) | The document does not adequately elaborate on past studies and investigations that have been undertaken. The document appears to arrive at priorities and actions with no obvious story or connection between current circumstances and future needs – i.e. It is not clear where the six priorities come from. |
|--|--|---|
| p. 18 Priority 2.2 Wayfinding | Wayfinding | This objective should reference and link with similar wayfinding and movement initiatives identified in the draft Gumpi Dunwich Master Plan. Wayfinding should also include interpretative signage for all transport modes (including walking, cycling, ferry, bus). |
| p. 20 Priority 2 | Passenger information | Suggest an action around bus drivers to be upskilled to act as an island ambassador/tour guide to provide visitor information/education to improve the travel experience for residents and visitors. |
| p. 21 Provide quality passenger transport infrastructure | Ferries | This section notes that the ferry passenger terminals are currently fit for purpose, however, it is understood that work undertaken by State Planning the as part of the Gumpi Dunwich Master Plan found that the Junner Street passenger terminal due for replacement and requiring upgrade to meet current standards. The draft Gumpi Dunwich Master Plan also notes that the Junner Street barge and passenger arrival point is currently run down and suitable for redevelopment and identifies a potential layout for a redeveloped terminal. It is unclear what level of technical investigations have been undertaken to support the proposed layout. |
| | Parking | It is unclear on how future car parking demand is anticipated to be managed. For example, based on projected growth of permanent residents and visitors as well as shift to public and active transport usage, how many vehicle parking spaces are needing to be provided in close proximity to the passenger terminals and can parking be provided away from the passenger terminals. Council expects that between this Draft Strategy and the draft Gumpi Master Plan, a more detailed analysis of this needs to be undertaken and passed on to Council. TMR has previously expressed their intent to undertake a parking survey for Minjerribah park'n'ride i (including One Mile, Junner Street and Toondah Harbour). |



| p. 21 Objective 3.2 | Ferries | This section talks about continuing public transport services to Junner Street and One Mile passenger ferry terminals. It is considered that a decision on whether Gumpi will continue to be accessed by two passenger ferry terminals is likely to have the biggest impact on integration of speed and ease of bus and ferry travel time to/on the island. A clear decision on this is also linked to Priority 4.3 Efficient network and services and 4.3 services are well integrated across passenger transport modes. |
|----------------------------------|------------------|--|
| p.21 Objective 3.2 and 5.2 | Service Area | Action needs refining and strengthening. How do you ensure connectivity to other destinations to ensure a well-rounded tourism product e.g. Brown lake. |
| p. 21 Objective 3.3 | Active Transport | From an active transport perspective, it may be wise to refocus the active transport related actions to prioritise delivery of key connections – e.g. direct access to ferry terminal and bus stops, as well as active transport connections between the townships, which will reduce the need for use of private vehicles to access public transport. Bicycle parking and storage can be integrated as part of this delivery and/or follow the delivery of these key connections. |
| p. 22 Objective 3.4 | Parking | The Draft Strategy notes generally the importance of car parking in close proximity to passenger ferry terminals. Recognising many island residents currently seek to park in close proximity to the island ferry terminal it's expected that between the Draft Strategy and Gumpi Dunwich Master Plan, further work would be undertaken on this. For example, based on projected growth in residents and tourists (as well as likely trends in public transport usage on the island) how many car parking spaces are needing to be provided in close proximity to the ferry terminals and how these spaces should be managed. |
| | Active Transport | Suggest including in the first paragraph 'providing clear pathways and connections across townships and to ferry terminals and bus stops'. Note – there is a great opportunity to connect Amity Point and Point Lookout townships by formalising the use of the fire trail along Claytons Road and provide some further wayfinding – it takes walkers and cyclists off the road and makes connections a bit safer and more environmentally appealing. |
| p. 22 Priority 3 | Safety | Suggest removing 'car parking at One Mile Ferry terminal requires lined car park areas' –this should already be in place. This section should highlight the need for safe and connected cycling and walking pathways on the island that connect the townships. |
| p. 23 Priority 3 | Active Transport | First dot point under opportunities mentions promote sustainable transport options – however it does not reflect the need for improved active transport infrastructure to support this opportunity in the actions section. |
| p. 23 Action 3.5 | Parking | Following on from the above comment for Objective 3.4, action 3.5 would be delivered and led through the Public Transport Strategy and Gumpi Dunwich Master Plan. This can be conducted in partnership with Council. |
| p. 23 | Active Transport | Council is currently identified as the lead on these actions which generally requires Council to investigate bike storage, parking facilities (including safe and efficient access and availability), active transport access to public |

| Actions 3.2, 3.3, 3.4, 3.6, 3.7 | | transport and improved lighting within or in close proximity to bus stops and ferry terminals. The NSI Passenger Transport Study – Draft Summary Report outlined that the State Government agencies would be the lead agency for accessibility, particularly within the Gumpi Master Plan area. As noted earlier, the draft Gumpi Master Plan identifies complete redevelopment of the arrival and includes a potential altered layout for the Junner Street Ferry Terminal. Recognising the draft Gumpi Master Plan identifies need for this major redevelopment, it's highly unlikely Council would be looking to investigate interim upgrades to bike storage, parking requirements and policy or lighting provision within Gumpi. |
|--|--|--|
| p. 24 Gumpi as Gateway to Minjerribah | Ferries | The Draft Gumpi Master Plan notes some of the benefits of consolidating the two ferry passenger terminals and recognises this as a potential longer term opportunity. Both the Draft Strategy and Gumpi Dunwich Master Plan note some of the benefits of this without making a clear recommendation. Clear direction on this would be important for the redevelopment of Junner Street barge and ferry terminal as well as current and future use of One Mile terminal. Background reports and material prepared for State Planning on the Gumpi Dunwich Master Plan noted that the future configuration of passenger ferry terminal/s was a key organising element of the Gumpi Dunwich Master Plan and important to the overall success of the master plan and achievement of the NSI ETS/Minjerribah Futures. |
| p. 24 Efficient network and services | Visitor bus service Service Duration | This sections needs to highlight more specifically and heavily the need for a dedicated visitor/tourist service running during peak periods frequently – such as a hop on/hop off bus service that goes to each township including Amity Point to help facilitate visitor dispersal, minimise overcrowding on general resident bus services and promote sustainable visitation numbers across the island instead of taking every visitor to Point Lookout. This section needs to address the need for early morning and particularly late-night ferry and bus services (such as what Coochiemudlo Island and the Southern Moreton Bay Islands currently have). The lack of late-night passenger ferry services beyond 7.55pm, coupled with the lack of on-island longer-term rental accommodation is contributing to a significant skills shortage of workers on Minjerribah, which impacts on tourism and hospitality service delivery, consistency of experience offering and viability. |
| | Water transport | Suggest that Amity Point jetty be highlighted as a potential to attract additional ferry services from Brisbane and the other islands. |
| p. 26 Seasonal Timetable | Seasonal travel demand | The Draft Strategy has not indicated a commitment for increased bus and ferry services to cater for visitor and commuter peak-periods. This was previously listed as an action in the draft North Stradbroke Island Passenger Transport Study – Draft Summary Report (Draft Summary Report) from 2019, which identified a need to deliver higher frequency of services, particularly connecting the bus routes from Dunwich-Point Lookout to the Amity Point township. Also, the survey was conducted in winter and therefore not inclusive of the peak summer period. |



| p. 28 Objective 4.1 | Service Duration | By providing a higher frequency of public transport services (both ferry and bus) on island, this will create a more attractive transport option for both residents and visitors, which has many benefits including reduced parking demand and a convenient, more enjoyable customer experience for tourists. Were the needs of workers considered, particularly from the hospitality industry? |
|--|----------------------------|---|
| p. 29 Objective 5.2 | Background | Consider replacing 'south' with 'southern' in the first paragraph. Please add 'Brown lake' after Bummiera and 'Blue Lake' after Karboora. The diagram at the bottom of the page doesn't have any captions – It should at least reference points of interest/beaches etc. |
| p. 31 Objective 5.2 | Service Duration | It should include an action that is to investigate extended night time ferry and bus services beyond 7.55pm – this is beyond what is mentioned in 5.2. |
| draft actions | Background | Action 5.3 should reflect Bummiera (Brown Lake), Karboora (Blue Lake), Keyholes, Amity Point. |
| p. 31 Mobility as a Service (MaaS) | Customer experience | The concept of Mobility as a Service (MaaS) will rely on mobile coverage, which can be patchy and unreliable on the island. However, customers can be encouraged to plan their journey. |
| p. 32 Priority 6 | Background | It is understood that Australia Jet Boats Pty Ltd is not the correct company that partnered with the State Government – it was perhaps <u>River to Bay Pty Ltd</u> . |
| , | Ferries | This section should reference the opportunity to investigate feasibility to connect our islands by having regular inter-island ferry services to and from each of the various ports including Toondah Harbour, Victoria Point, Redland Bay and Brisbane CBD. This would reduce the parking constraints currently experienced at Toondah Harbour during peak seasons, and it would provide a more consistent, flexible and adaptable use of the public transport connections across Moreton Bay. |
| p. 33 Action 6.1, 6.2 | Low-carbon Alternatives | There is a missed opportunity to integrate sustainable travel (electric buses/ferries and emerging technologies such as e-mobility). This could go a long way to decrease parking demand at ferry terminals and in providing modes that can integrate well with public transport while also providing a tourism benefit. The Draft Strategy should consider E-mobility and ride sharing options to improve efficient access to public transport and reduce car dependency. |
| p. 33 Figure 7 | Background | Suggest adding Coochiemudlo Island and Southern Moreton Bay Islands into the diagram. |





14.4 ANIMAL MANAGEMENT SYSTEMATIC INSPECTION PROGRAM

| Objective Reference: | A5888153 |
|-----------------------------|--|
| Authorising Officer: | David Jeanes, Acting General Manager Community & Customer Services |
| Responsible Officer: | Graham Simpson, Group Manager Environment and Regulation |
| Report Author: | Donna Wilson, Service Manager Compliance Services |
| Attachments: | Nil |

PURPOSE

To seek approval to conduct systematic inspection programs for unregistered cats throughout Redland City, under the *Local Government Act 2009* and unregistered dogs throughout Redland City under the *Animal Management (Cats and Dogs) Act 2008.*

BACKGROUND

The Animal Management (Cats and Dogs) Act 2008 places a mandatory requirement throughout Queensland for all dogs over the age of 12 weeks to be registered with the local authority in which the dog/s reside.

Redland City Council resolved to retain cat registration at its meeting of 9 October 2013. Registration of cats is a local law requirement under *Part 6 – Registration of Cats of Local Law No.2* (Animal Management) 2015.

On 18 November 2020, Council resolved to commence the systematic inspection program for the period 6 January 2021 to 31 December 2021 inclusive. For the period 6 January 2021 to 30 September 2021, 14,031 properties have been visited and 2,278 unregistered animals have been detected.

In order to determine the accuracy of records, obtain new registrations for unregistered animals and follow up on overdue registrations, it is necessary for Council's Animal Management Team to carry out a Systematic Inspection Program (SIP) which covers all premises within Redland City for the period 3 January 2022 to 31 December 2022 inclusive.

ISSUES

1. Systematic Inspection Program for unregistered cats

The provisions for cats contained within the *Local Government Act 2009* provide for a maximum approved inspection period of three (3) months per inspection program. The Systematic Inspection Program for unregistered cats will be undertaken in four (4) programs to enable the program to be active throughout Redland City during 2022.

Program 1 is proposed to commence on 3 January 2022 and will operate for a period of three (3) months until 1 April 2022.

Program 2 is proposed to commence on 4 April 2022 and will operate for a period of three (3) months until 1 July 2022.

Program 3 is proposed to commence on 4 July 2022 and will operate for a period of three (3) months until 30 September 2022.



Program 4 is proposed to commence on 3 October 2022 and will operate for a period of three (3) months until 31 December 2022.

2. Systematic Inspection Program for unregistered dogs

The provisions for dogs contained within the *Animal Management (Cats and Dogs) Act 2008* provide for a maximum approved inspection period of six (6) months. The Systematic Inspection Program for unregistered dogs will be undertaken in two (2) programs to enable the program to be active throughout Redland City during 2022.

Program 1 is proposed to commence on 3 January 2022 and will operate for a period of six (6) months until 1 July 2022.

Program 2 is proposed to commence on 4 July 2022 and will operate for a period of six (6) months until 31 December 2022.

The purpose of the SIP is to verify the accuracy of Council's animal registration records including obtaining new registrations of animals not yet registered and obtain re-registration for out-of-date animal registrations. Penalty infringement notices may be issued in instances where pet owners have failed to either register or renew their animal's registration.

The program will allow authorised officers to gain access to the front door of the dwelling, and to all business premises by way of normal public access. Officers will be directed not to access other parts of a property unless with the consent of the property owner.

The program will be conducted between 8am and 5pm, Monday to Friday (except public holidays) within the approved period(s).

Copies of the program providing details of the SIP will be made available at Council's Customer Contact Centres and can be viewed on Council's website. Members of the public can obtain a copy of the program at no charge.

<u>Education</u>

The SIP for both unregistered cats and unregistered dogs will be advertised in the local paper at least 14 days, but not more than 28 days prior to commencement of the approved inspection programs.

A communication plan leading up to, and during the program will be implemented to assist in educating animal owners on the requirements of animal registration, the benefits and provide an overview of how animal registration fees are used across the city. The SIP also provides an opportunity to provide a range of domestic animal educational material to residents that may have city-wide or local importance, including the importance of controls such as enclosures and dog leads in public to reduce incidents of attacks on people, wildlife and other pets.

STRATEGIC IMPLICATIONS

Legislative Requirements

Undertaking the SIP for unregistered cats is in accordance with the provision of sections 133 and 134 of the *Local Government Act 2009.*

Undertaking the SIP for unregistered dogs is in accordance with the provision of sections 113, 114 and 115 of the *Animal Management (Cats and Dogs) Act 2008.*

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Risk Management

The SIP assists with regulating and managing the keeping of animals by minimising the risk to community health, safety and amenity.

Financial

The purpose of the SIP is to verify Council's animal registration records, obtain new registrations, obtain payment for out of date animal registrations and, where necessary, to issue penalty infringement notices to dog and cat owners who fail to renew their animal's registration. It is expected that this will assist Council collecting revenue to offset costs of operating its animal management responsibilities. The SIP activity will be undertaken within existing operational budget.

People

There are no identified implications for Council staff as existing staff from the Environment and Regulation Group will be utilised to undertake these programs.

Environmental

The SIP is in accordance with the *Animal Management (Cats and Dogs) Act 2008* and Council's *Local Law No. 2 (Animal Management) 2015* to regulate and manage the keeping of animals to reduce environmental nuisance.

Social

The SIP is in accordance with the *Animal Management (Cats and Dogs) Act 2008* and Council's *Local Law No. 2 (Animal Management) 2015* to regulate and manage the keeping of animals by supporting animal owners to keep their animals in a manner that is consistent with the expectations of the community, including public safety.

Human Rights

There are no known relevant human rights matters associated with this report.

Alignment with Council's Policy and Plans

Along with legislative compliance, the SIP aligns with Goal 2.1 Strong Communities of the Corporate Plan *Our Future Redlands – A Corporate Plan to 2026 and Beyond* being:

Enhance the health, safety and wellbeing of our community through the delivery of inclusive and responsive services focused on preserving and improving our naturally wonderful lifestyle by leveraging partnerships, networks, facilities and infrastructure.

To achieve this objective, Council is committed to encouraging responsible dog and cat ownership through the enforcement of the provisions of *Local Law No. 2 (Animal Management) 2015,* and the *Animal Management (Cats and Dogs) Act 2008*.

CONSULTATION

| Consulted | Consultation Date | Comments/Actions |
|--|----------------------|--|
| Financial Business Partnering - Management Accountant | 12 October 2021 | Informed of report content and reviewed report |

OPTIONS

Option One

That Council resolves as follows:

- 1. That pursuant to sections 133 and 134 of the *Local Government Act 2009*, Council resolves to approve four (4) systematic inspection programs for unregistered cats to inspect all premises, including residential and business premises, within the Local Government area for the periods 3 January 2022 to 1 April 2022 (Program 1), 4 April 2022 to 1 July 2022 (Program 2), 4 July 2022 to 30 September 2022 (Program 3) and 3 October 2022 to 31 December 2022 (Program 4), inclusive.
- 2. That pursuant to sections 113, 114 and 115 of the Animal Management (Cats and Dogs) Act 2008, Council resolves to approve the systematic inspection program for unregistered dogs to inspect all premises, including residential and business premises, within the Local Government area for the periods 3 January 2022 to 1 July 2022 (Program 1) and 4 July 2022 to 31 December 2022 (Program 2), inclusive.

Option Two

That Council resolves to act only on complaints received.

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/275

Moved by: Cr Lance Hewlett Seconded by: Cr Rowanne McKenzie

That Council resolves as follows:

- That pursuant to sections 133 and 134 of the Local Government Act 2009, Council resolves to approve four (4) systematic inspection programs for unregistered cats to inspect all premises, including residential and business premises, within the Local Government area for the periods 3 January 2022 to 1 April 2022 (Program 1), 4 April 2022 to 1 July 2022 (Program 2), 4 July 2022 to 30 September 2022 (Program 3) and 3 October 2022 to 31 December 2022 (Program 4), inclusive.
- 2. That pursuant to sections 113, 114 and 115 of the Animal Management (Cats and Dogs) Act 2008, Council resolves to approve the systematic inspection program for unregistered dogs to inspect all premises, including residential and business premises, within the Local Government area for the periods 3 January 2022 to 1 July 2022 (Program 1) and 4 July 2022 to 31 December 2022 (Program 2), inclusive.

CARRIED 10/1

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Julie Talty voted AGAINST the motion.



15 REPORTS FROM INFRASTRUCTURE & OPERATIONS

| 15.1 AMENDMENT TO APPROVED REGISTER OF FEES REQUESTED BY CITY WATER | | | |
|---|---|--|--|
| Objective Reference: | A5580900 | | |
| Authorising Officer: | Dr Nicole Davis, General Manager Infrastructure & Operations | | |
| Responsible Officer: | Richard Mann, Group Manager City Water | | |
| Report Author: | Ann Marshall, Contracts Officer | | |
| Attachments: | 1. City Water - Proposed New Fees (Amendments to 2021-2022 Register of Fees) 🖞 | | |

PURPOSE

To approve an amendment to the 2021-2022 Register of Fees to include new cost recovery fees.

BACKGROUND

Damage to the City's residential water services/water meters, costs approximately \$150k per annum. These costs should be fully recovered using the cost recovery principles of Council. For example, in June 2021, City Water replaced five stolen meters and repaired/replaced 17 damaged residential services/meters at a total approximate cost of \$12k for the month.

Section 40 of the *Water Supply (Safety and Reliability) Act 2008* enables the reasonable cost of repairing damage to infrastructure to be recovered, whilst section 21.4.3 of the *South East Queensland Customer Water and Wastewater Code* states that if a customer damages the water meter, Council is able to recover the reasonable cost of repairing the damage from the customer.

Therefore, City Water carried out a review of the proposed fees in April 2021, and undertook a benchmark comparison of surrounding Council water businesses and Water Entities. This comparison included Logan City Council, Gold Coast City Council, Urban Utilities and Unitywater. It was identified that these entities already apply equivalent cost recovery fees to those proposed in this report.

In addition, the adoption of nine new fees will enable full cost recovery, support a user pays principle, and will assist Council in delivering a more equitable service.

ISSUES

At present, costs of quoted private works and damage to Council's water infrastructure are absorbed by City Water. This is not consistent with the principles of transparency, user pays and full cost recovery.

Both the Water Supply (Safety and Reliability) Act 2008 and the South East Queensland Customer Water and Wastewater Code have provision and processes for cost recovery, and enable Council to charge for damage to the water infrastructure. The recently adopted Redland City Council Water and Wastewater Customer Commitment Charter specifies that Council may recover the amount of the loss or the reasonable cost of repairing damage to Council's infrastructure from the owner of the property. This includes water meters. The recovery costs associated with damage to residential water meters/services is estimated to impact less than 0.5% of residential customers



annually. This approach is aligned and benchmarked against other Council owned commercial water businesses.

The proposed three fee amendments and nine new fees for City Water Network Operations are detailed in attachment 1. This attachment shows the format that fees will be displayed in the updated 2021-2022 Register of Fees.

New Fees

Currently City Water undertakes all works on the 'live' water supply and sewerage systems to ensure customers and infrastructure are protected, and to meet the legislative requirements. In these instances, a fee is paid on the initial application for the works, to cover the administrative costs for processing a standard application. However, often, an applicant requests changes to scope, and City Water is obligated to undertake this rework, over and above the requirements of a standard application process.

Nine new fees are proposed for a range of activities and applications as detailed in attachment 1.

In Council's 2021-2022 Register of Fees, fees for City Water are split into three categories: Water Supply, Alterations Water and Wastewater. We are requesting to add two additional fees for each of these categories (six fees in total) to recover costs incurred by Council where the applicant amends or cancels their application:

- 1. Requests for Amended Quotes
- 2. Cancellation of Applications (where a refund is required)

Further detail on the new proposed fees is detailed below:

Requests for Amended Quotes:

An additional or amended quote request is required due to the applicant not correctly specifying the required works on their application form or changing the scope of works. Examples of typical quote amendments include:

- When asking Council to undertake works on Council's infrastructure, an applicant requests a quote for a scope of work. Council charges for this initial quotation based on a cost recovery basis, including charging for installation of pipework and time for undertaking the live connection. However, once the quotation is received, if an applicant changes their requirements, there is no ability to recover the administrative costs of any changes to the scope and the preparation of a new quote.
- Another example is when an applicant requested multiple quotes from Council for a live connection/pipe installation. After three quotes, Council declined to provide additional quotations for the pipe installation, and advised the customer to engage a private contractor to undertake the pipe installation (Council is only required to undertake the live connection). The private contractor's quotes was much higher than Councils and consequently the applicant engaged Council to undertake both the live connection <u>and</u> the installation of necessary pipework to the site. The administrative costs of the multiple re-quotes are not currently recoverable.

Cancellation of Applications - refund required:

Situations where an applicant requires a refund of an application typically arises from the following two scenarios:



- An applicant may have changed circumstances impacting the design or project requirements
- Funding for a project may become unavailable and the project cannot progress as initially planned.

The above scenarios are not within Council's control and often City Water has organised schedules, permits and materials on payment of quote fee. There are impacts both in time and financially for the purchase of materials when a cancellation occurs. The proposed fee will recover the cost for work already completed and for administrative costs of cancelling the application and processing a refund.

The remaining three new fees are related to costs incurred by City Water in a new fee category.

Repair of City Water Infrastructure

Section 40 of the *Water Supply (Safety and Reliability) Act 2008* enables the reasonable cost of repairing damage to infrastructure to be recovered, whilst section 21.4.3 of the *South East Queensland Customer Water and Wastewater Code* states that if a customer damages the water meter that Council can recover the reasonable cost of repairing the damage from the customer.

Benchmarking of surrounding Council owned commercialised water businesses has demonstrated that it is usual and reasonable to expect the customer/land owner to be responsible for damage to the water meter. It is reasonable that damage to a water meter is the responsibility of the owner of the land as the water meters are located on the owner's land. This responsibility is now clear under the updated Redland City Council Water and Waste Water Customer Commitment Statement. Under full cost recovery principles, and transparency, it is also reasonable that if the water meter is damaged then the cost of repair is recovered.

The proposed new fees relate to third party damage/vandalism to City Water Assets:

- Replacement of stolen water meter (residential)
- Repairs to standard 20mm water service/meter caused by third party Business Hours
- Repairs to standard 20mm water service/meter caused by third party After Hours

Fee amendments

Three fee amendments are to round current fees to whole dollar amounts (Attachment 1). The minor rounding of fee amounts aims to improve the customer experience while streamlining the administration and processing. The details of the amendments are:

- New Wastewater connection quotation fee was \$94.05 and is now \$94.00
- New Non Standard Water Service quotation fee was \$94.05 and is now \$94.00
- Relocation residential water service Raise/Lower fee was \$299.15 and is now \$300.00.

The 2021-2022 Register of Fees will need to be amended to provision for the new fees and, therefore, it is a prudent time to concurrently make minor adjustments these existing fees in order to simplify processing the charges.

STRATEGIC IMPLICATIONS

Legislative Requirements

Section 98 of the *Local Government Act 2009* requires a local government to keep a register of cost recovery fees.



Section 40 of the *Water Supply (Safety and Reliability) Act 2008* enables the reasonable cost of repairing damage to infrastructure to be recovered, whilst section 21.4.3 of the *South East Queensland Customer Water and Wastewater Code* states that if a customer damages the water meter that Council is able to recover the reasonable cost of repairing the damage from the customer.

Risk Management

As part of the fee review, City Water benchmarked fees of the surrounding Council water businesses and Water Entities to ensure Council's fees are comparable for the relevant services.

Financial

It is estimated that City Water's fee revenue could increase by approximately \$100k to \$150k per annum with the introduction of the new fees underpinned by the principles in Council's 2021-2022 Revenue Policy. Revenue will be dependent on the amount of rectification works and quotations undertaken.

The costs of these proposed fees are already being incurred within City Water. Introduction of these new fees will allow the commercialised business to undertake transparent and full cost recovery principles.

A cost recovery process will need to be created to allow for invoices to be issued in instances where third party damage to City Water infrastructure has been identified, resulting in repairs/replacements being made. Unpaid invoices will be added to quarterly rates notices, similar to other unpaid Council fees.

People

It is believed that the additional fees for amended quotes will reduce the number of such requests received and reduce rework.

The costs for third party damage will result initially in an increase in administrative work due to the need to raise and issue invoices for each incident and the follow-up for unpaid invoices and debt recovery. It is anticipated that over the long term, as awareness of the consequences develops, the amount of damaged infrastructure will decrease.

Environmental

There are no specific environmental implications associated with this report.

Social

It is acknowledged that there may be some minor social impact with the introduction of third party damages recovery. However recouping these costs aligns to transparent and cost recovery principles and brings us in line with surrounding Councils and Water Entities who currently recover these costs, and aligns to the principles in Council's 2021-2022 Revenue Policy.

Human Rights

No anticipated conflict human rights legislation is anticipated with this report.

Alignment with Council's Policy and Plans

The proposed fees align with FIN-017-P Revenue Policy and FIN-017-002-G Register of Fees.



CONSULTATION

| Consulted | Consultation Date | Comments/Actions | | |
|--|------------------------------|---|--|--|
| Application Management Team leader | 10 September 2021 | Provided feedback on development of a Cost of Recovery Process | | |
| Service Manager, Financial Operations | 01 September 2021 | Provided feedback on adding these fees onto rates notices | | |
| Councillor workshop | 11 July 2021 | Presentation of initiatives | | |
| Business Transformation | 14 July 2020 27 July 2020 | Identification of potential savings or income streams for Council | | |

OPTIONS

Option One

That Council resolves to adopt the proposed fee amendments and new fees as detailed in Attachment 1 and to approve the amendment to the 2021-2022 Register of Fees.

Option Two

That Council resolves to not adopt the proposed fee amendments or any of the proposed new fees.

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/276

Moved by: Cr Peter Mitchell

Seconded by: Cr Wendy Boglary

That Council resolves to adopt the proposed fee amendments and new fees as detailed in Attachment 1 and to approve the amendment to the 2021-2022 Register of Fees.

CARRIED 11/0



| Fee Code | Description | Unit | Base Charge \$ | GST \$ | Final Charge \$ | Туре | |
|---|--|-----------------|----------------|--------|-----------------|------|-----------|
| Alterations Water | | | | | | | |
| | Request for amended quote | As required | 94.00 | | 94.00 | С | New |
| | Cancellation of application (where refund required) | As required | 250.00 | | 250.00 | С | New |
| Wastewater | | | | | | | |
| WRSP1.8 | New Wastewater connection quotation | per connection | 94.00 | | 94.00 | С | Amendment |
| | Request for amended quote | As required | 94.00 | | 94.00 | С | New |
| | Cancellation of application (where refund required) | As required | 250.00 | | 250.00 | С | New |
| Water Supply | | | | | | | |
| WRSP1.7 | New Non Standard Water Service quotation | per application | 94.00 | | 94.00 | С | Amendment |
| | Request for amended quote | As required | 94.00 | | 94.00 | С | New |
| | Cancellation of application (where refund required) | As required | 250.00 | | 250.00 | С | New |
| WRSP1.9 | Relocation residential water service - Raise/Lower | per connection | 300.00 | | 300.00 | С | Amendment |
| | Replacement of stolen water meter (residential) | As required | 350.00 | | 350.00 | С | New |
| Repairs to standard 20mm waterservice/meter caused by 3rd party – Work Hours | | As required | 595.00 | | 595.00 | с | New |
| | Repairs to standard 20mm waterservice/meter caused by 3rd party – After Hours | As required | 835.00 | | 835.00 | с | New |

15.2 NEW TRUSTEE LEASE - STRADBROKE EARLY LEARNING ASSOCIATION INCORPORATED

| Objective Reference: | A5448099 | | |
|-----------------------------|--|--|--|
| Authorising Officer: | Dr Nicole Davis, General Manager Infrastructure & Operations | | |
| Responsible Officer: | Sherry Clarke, Group Manager City Operations | | |
| Report Author: | Kate Mullens, Principal Adviser City Sports and Venues Katie Woodrow, Acting Senior Leasing Officer | | |
| Attachments: | 1. Concept Plan 🗓 | | |
| | 2. Draft Land Management Plan 😃 | | |
| | 3. Site Plan 🗓 | | |

PURPOSE

To approve a trustee lease to Stradbroke Early Learning Association Incorporated over part of Junner Street South Park, Dunwich, North Stradbroke Island.

BACKGROUND

Stradbroke Early Learning Association Incorporated (SELAI) is the only centre-run child care facility on North Stradbroke Island, providing quality care and education for pre-school aged children. Since establishment in 2000 the centre has grown considerably, adding additional classrooms and capacity.

SELAI has requested a lease from Council to occupy 450sqm of Lot 3 CP865498, described as Junner Street South Park Dunwich, for the purpose of establishing a nature play area and cultural community garden. A concept plan for the proposed lease area is outlined in Attachment 1.

The land is a state reserve, held by Council as trustee. Use of the reserve is primarily open space for environmental purposes and protection of local heritage values.

The group currently lease the adjacent land (Lot 6 CP865498) from the State of Queensland, on which the child care centre is located. The location of the proposed nature play area and cultural community garden will adjoin the existing child care centre and enable easy access by the children and carers. Broader community participation will also be invited by planned programs outside of child care centre hours.

Community need for the nature play area and cultural community garden was recognised by SELAI through surveys to parents. This identified significant community support for the proposal as a means to increase access to nature for children and for broader learning activities.

Council Officers support issuing a trustee lease to SELAI for a 10 year term. This aligns with the Community Leasing Policy and Council's vision for enriching community lifestyle by preserving community assets and achieving maximum benefit from its community property portfolio.

ISSUES

The proposed use has been assessed against relevant provisions and consultation with both the community and council stakeholders. Concerns raised will be conditioned within the trustee lease.

Community consultation



Community consultation generated 88 submissions. Overall the proposal is highly supported with 90.9% in favour.

The supportive submitters indicated that the proposal would create space for nature play learning and provide educational benefits for children as well as broader community involvement.

The eight objections involved neighbour concerns regarding loss of rear pedestrian (only) access to the park. It is not Council policy or responsibility to accommodate rear access from private property to parks or open space areas due to public safety and infrastructure management considerations. Pedestrian access to the park remains available to these neighbours along the street, and provides relatively close access to the park.

Lease conditions will require a vegetated buffer between the land and adjoining neighbours to minimise impacts. The purpose of the vegetated buffer is to provide visual and physical separation between the nature play area and adjoining neighbouring properties. This will contribute to managing considerations and perceptions of the nature play area, such as time of use, activities and noise. These issues will also be managed via the communications with the neighbours prior to the execution of the lease.

Public access

The proposed use includes inviting community access and participation via planned programs in collaboration with SELAI. The association is required to display public accessible hours on fence signage, a requirement which is supported by the Department of Resources (DoR).

Compliance with the Land Act 1994 and Queensland Government's Operational Policy

The DoR confirmed its preference for Council to enter into a trustee lease over the required area, noting the use is consistent with the primary purpose of the reserve.

The public access components of DoR's Operational Policy have been satisfied and the proposed fencing is permitted, with the provision that signage advertising public opening hours is installed and conditioned within the trustee lease.

Whilst it is acknowledged the proposed use is consistent with the purpose of the reserve, a basic Land Management Plan (LMP) has been requested by DoR in order to meet the requirements of the *Land Act 1994* and DoR's Operational Policy. The purpose of the LMP is to provide information required to support use of the trust land.

The Draft LMP for Lot 3 CP865498 is attached (Attachment 2). It details the proposed use, proposed trustee lease and stakeholder consultation. Council's endorsement is requested through resolution, prior to submitting to DoR for approval.

State heritage

The Department of Environment and Science requested Council notify SELAI of provisions within the *Queensland Heritage Act 1992* that protect discoveries of important non-aboriginal archaeological artefacts from disturbance. Assessment of the proposed lease area found a reasonably high chance that ground disturbance may reveal artefacts of this nature. The group has been advised of the provisions.

Native Title/Aboriginal Cultural Heritage

Quandamooka Yoolooburrabee Aboriginal Corporation (QYAC) approved the proposed use and noted no known impact to Aboriginal Cultural Heritage, subject to the site attendance of a Cultural



Heritage Services Monitor during all activities likely to cause ground disturbance. The lessee will bear the costs incurred for an Aboriginal Cultural Heritage Officer, and other requirements and provisions of the *Aboriginal Cultural Heritage Act 2003*.

QYAC confirmed there is no current issue in regards to Native Title however, should the tenure/use change in the future, further consultation is required in accordance with the Redland City Council Indigenous Land Use Agreement (ILUA).

North Stradbroke Island Museum on Minjerribah representatives completed a site inspection of the proposed lease area and confirmed the proposed use is consistent with the heritage values and would preserve them. The Museum is supportive of the project.

Local Heritage

The reserve is local heritage registered for building foundations, trees and other structures. Advice from Council's Planning Assessment Team confirmed the proposed use does not trigger Heritage Overlays and consequently, no planning approval is required.

Gumpi (Dunwich) Draft Master Plan

The Department of State Development, Infrastructure, Local Government and Planning have prepared the Gumpi (Dunwich) draft master plan.

Council's Strategic Planning Unit advised the draft master plan is yet to be finalised however, material made available to Council does not propose any changes to the subject site. It was noted there is no requirement to delay a Council decision on this proposal pending completion of the draft master plan.

STRATEGIC IMPLICATIONS

Legislative Requirements

The *Local Government Regulation 2012* (the Regulation) s.236(1)(b)(ii) requires that Council agree by resolution that it is appropriate to dispose of an interest in land to a community organisation, other than by tender or auction.

SELAI meets the Regulation's definition of a community organisation, as it is an entity that carries on activities for a public purpose and whose primary object is not directed at making a profit.

Council is permitted to resolve to issue SELAI a lease under s.236(1)(b)(ii).

Risk Management

The lease requires SELAI to maintain public liability insurance for an amount of not less than \$20 million. Responsibility for building insurance of lessee owned fixtures also resides with the group.

Prior to developing the proposed area, SELAI is required to present a detailed project plan to Council including a management plan and staged program of works. The information will be assessed by Council Officers before consent to proceed is granted.

Financial

Consent to proceed with the proposed building works is required from Council, as the land owner, prior to commencement. Before granting consent, Council officers will require robust costings, quotations and evidence of SELAI successful acquisition of grant funding to ensure project costs can be sufficiently covered by the lessee.

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The lessee bears all costs associated with the preparation and registration of the lease. The lessee will also bear the cost of water usage, and both building and public liability insurance. Maintenance of the lease/licence area is a shared responsibility between Council and the Lessee in accordance with the relevant terms in the tenure.

The lessee will bear the costs incurred for cultural heritage monitoring and management, including an Aboriginal Cultural Heritage Officer, if required. A sustainability check conducted in October 2020 confirmed SELAI is financially sound.

People

This recommendation does not have Council staff implications.

Environmental

In assessing the proposal, Council's Environmental Education and Parks and Conservation teams were consulted. Whilst both teams are supportive of the use, the below environmental concerns were raised which will be conditioned within the trustee lease.

- Biosecurity Restricted/prohibited matters, weeds of national significance and environmental weeds must not be planted. Planting of native vegetation, vegetables and herbs is permitted.
- Fauna Nesting Curlews were observed on the subject site, works must not occur until the Curlews have vacated the direct area.
- Existing trees Impact to existing trees is not permitted. This includes removal of limbs, unless a potential safety risk is identified.
- Fencing Proposed fencing must be designed to allow for koala movement.
- Soil Soil or mulch introduced to the area must be free of weeds and fire ants.

Social

SELAI has provided care and education for pre-school aged children of North Stradbroke Island since 2000. Providing tenure to facilitate the proposed nature play and cultural community garden supports both physical and social benefits, by increasing access to nature for children and enabling broader learning activities.

Human Rights

The recommended land disposal advances the purpose of s.36(1) of the *Human Rights Act 2019* by supporting and enhancing the right to education for children.

Alignment with Council's Policy and Plans

CDV-001-P Community Leasing Policy supports leases to not-for-profit community organisations.

Our Future Redlands – A Corporate Plan to 2026 and Beyond is supported by this proposal, particularly:

GOAL 2. Strong Communities

2.1 Enhance the health, safety and wellbeing of our community through the delivery of inclusive and responsive services focused on preserving and improving our naturally wonderful lifestyle by leveraging partnerships, networks, facilities and infrastructure.



2.4 Enhance community inclusion where people of all locations, ages, abilities and cultures can participate and have access to the necessary services and facilities.

GOAL 5. Liveable Neighbourhoods

5.1 Enhance the unique character and liveability of our city for its communities through coordinated planning, place making, and management of community assets.

CONSULTATION

| Consulted | Consultation Date | Comments/Actions |
|--------------------------------|--------------------------|---|
| Service Manager, City Sport | 26 October 2021 | Report reviewed and trustee lease supported. |
| and Venues | | |
| Strategic Adviser of Social | 8 July 2021 | Report reviewed and supported. |
| Planning, Community and | | |
| Economic Development | | |
| Senior Adviser Civic and Open | 24 March 2021 | Reviewed and provided support of Draft Land Management |
| Space Planning and Policy | | Plan. |
| Councillor, Division 2 | 26 October 2021 | Advised of proposal and expressed support for the project. |
| | 15 October 2020 | Updated prior to community consultation. Advised of |
| | 14 May 2020 | consultation outcomes. Advised of report |
| | 6 March 2020 | recommendations. |
| Community Development | Ongoing | Coordinated assessment of proposed use, stakeholder and |
| Officer, Community and | | community consultation. Client liaison for SELAI. Completed |
| Customer Services | | site inspection. Prepared Draft Land Management Plan. |
| Department of Resources | 12 November 2020 | Consulted by email. Comments raised are noted in the |
| (DoR) | 30 October 2019 | issues section of this report. |
| | | |
| Service Manager, Business | 29 October 2020 | Assessed SELAI's financial sustainability. |
| Partnering Unit | | |
| Service Manager, Legal | 28 October 2020 | No objection to proposed trustee lease. Requested |
| Services | | inclusion of signage advertising public open hours. |
| Minjerribah Moorgumpin | 11 June 2020 | Consulted by email. No feedback or concerns raised. |
| Elders in Council Aboriginal | | |
| Corporation | | |
| Department of Environment | 13 May 2020 | Consulted by email. Comments raised are noted in the |
| and Science | | issues section of this report. |
| Quandamooka | 6 May 2020 | Consulted by email. Comments raised are noted in the |
| Yoolooburrabee Aboriginal | | issues section of this report. |
| Corporation (QYAC) | | |
| North Stradbroke Island | 5 May 2020 | Consulted by email. Comments raised are noted in the |
| Museum on Minjerribah | - | issues section of this report. |
| Planning Liaison Officer, City | 24 April 2020 | Confirmed proposed use does not trigger planning |
| Planning and Assessment | | approval. |
| | | |
| Senior Adviser Open Space | 19 March 2020 | Completed site inspection and provided support of |
| Strategy, Civic & Open Space | | proposed use. |
| Asset Management | | |
| Senior Adviser, Environment, | 16 March 2020 | Comments raised are noted in the environmental section of |
| Community and Customer | | this report. |
| Services | | |
| Design Lead, Landscape | 12 March 2020 | Consulted by email. Comments raised are noted in the |
| Architecture, Design and | | issues section of this report. |
| Technical Services | | |
| Indigenous Partnerships and | 11 March 2020 | Advised of proposal and consultation with QYAC. |

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GENERAL MEETING MINUTES

| Consulted | Consultation Date | Comments/Actions |
|------------------------------|-------------------|---|
| Programs Coordinator | | |
| Senior Island Coordinator, | 10 March 2020 | Parks and Conservation comments raised are noted in the |
| North Stradbroke Island. | | environmental section of this report. |
| Principal Strategic Planner, | 9 March 2020 | Consulted by email. Comments raised are noted in the |
| Strategic Planning Unit | | issues section of this report. |

OPTIONS

Option One

That Council resolves as follows:

- 1. To approve and discharge, as Trustee, a new lease to Stradbroke Early Learning Association Incorporated over part of Lot 3 CP865498 situated at Junner Street South Park, Dunwich, North Stradbroke Island, as shown on Attachment 3, with a lease term of 10 years.
- 2. To endorse the Draft Land Management Plan for Lot 3 CP865498 at Attachment 2.
- 3. To agree in accordance with s.236(2) of the *Local Government Regulation 2012* that s.236(1)(b)(ii) of the *Local Government Regulation 2012* applies allowing the proposed lease to a community organisation, other than by tender or auction.
- 4. To authorise the Chief Executive Officer to execute all documents in regard to this matter.

Option Two

That Council does not approve a new trustee lease to Stradbroke Early Learning Association Incorporated, and investigates alternative arrangements.

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/277

Moved by:Cr Peter MitchellSeconded by:Cr Wendy Boglary

That Council resolves as follows:

- 1. To approve and discharge, as Trustee, a new lease to Stradbroke Early Learning Association Incorporated over part of Lot 3 CP865498 situated at Junner Street South Park, Dunwich, North Stradbroke Island, as shown on Attachment 3, with a lease term of 10 years.
- 2. To endorse the Draft Land Management Plan for Lot 3 CP865498 at Attachment 2.
- 3. To agree in accordance with s.236(2) of the Local Government Regulation 2012 that s.236(1)(b)(ii) of the Local Government Regulation 2012 applies allowing the proposed lease to a community organisation, other than by tender or auction.
- 4. To authorise the Chief Executive Officer to execute all documents in regard to this matter.

CARRIED 11/0

Attachment 1

Stradbroke Early Learning Association Incorporated – Part of Lot 3 CP865498 Concept plan of proposed nature play area and cultural community garden.





Community engagement proudly supported by Redland City Council



DRAFT LAND MANAGEMENT PLAN

1. INTRODUCTION

This Draft Land Management Plan (LMP) applies to the trust land known as Junner Street South Park,

Dunwich, North Stradbroke Island.

Subject Land: Lot 3 on CP865498

Land Management Plan Duration:

Duration: Ongoing

Review: Review of the land management plan will occur:

- at the expiry or surrender of current trustee leases;
- when a request is received for secondary use;
- in consultation with relevant users/lessees at least every five (5) years or as required with three (3) months' notice of the requirement for such review being given; or
- earlier as required by the Minister, subject to three (3) months' notice being given to the trustee of a requirement for review from the Minister in accordance with the Land Act 1994.

Minor amendment may be made by the trustee to improve operational efficiency of the plan with the amended land management plan being submitted for approval under the *Land Act 1994* within one (1) month of such amendment being made.

2. TRUSTEE DETAILS

| Trustee's Name | Redland City Council |
|-------------------|-------------------------------|
| Trustee's Address | PO Box 21, Cleveland QLD 4163 |

3. EXISTING TENURE OF THE SUBJECT LAND

Trust Land Description: Reserve for Park purposes Title Ref 49006067.

| Lot / Plan: | Lot 3 on CP865498 |
|-----------------------|--|
| Parish / County: | Parish of Stradbroke / County of Stanley |
| Locality: | Dunwich / Goompi |
| Local Government: | Redland City Council |
| Area of Land: | 0.5422 ha |
| Conditions of Tenure: | Nil |

LAND MANAGEMENT PLAN – RESERVE FOR PARK PURPOSES REF 49006067

4. EXISTING 'USES'

4.1 Existing Uses – Primary and Secondary

The trust land is primarily used for open space/environmental purposes and protection of local heritage values.

4.2 Existing Interests

There are no leases or licences in respect of the land. The trust land is listed as a Local Heritage Place. Some relics remain of the historic Dunwich Benevolent Asylum which was located at Junner Street South Park such as building foundations, trees, and other minor site disturbance which occurred during the asylum years. There are no known relics on that part of the site subject to the proposed community use.

4.3 Native Title Status

Native Title has not been determined over the area of the community garden, however if the tenure is to change in the future, QYAC will need to be further consulted in accordance with the Redland City Council Indigenous Land Use Agreement.

5. PROPOSED USE/S OF THE SUBJECT LAND

5.1 Proposed Use/s of the Subject Land

The proposal is community-led by Stradbroke Early Learning Centre, the only on-island childcare centre and a not-for-profit incorporated association with a lease over adjoining Lot 6 on CP865498. The association is financially viable and holds public liability insurance.

The proposed use is a nature play and cultural community garden (native plants and bush tucker garden) adjoining Stradbroke Early Learning Centre for children of the centre to easily access. Broader community access and participation is also proposed via planned programs in collaboration with the early learning centre. No buildings are proposed. Water for the garden will be accessed from the existing water to Stradbroke Early Learning Centre using hoses / irrigation.

The use will enhance the primary use of the trust land by the community. The proposal matches the natural attributes of the subject land, being an already segmented section of land adjoining the existing childcare centre. A Trustee Lease is proposed and the use which is consistent with no other future plans for the site.

From a local area perspective, the proposal is centrally located with other community uses including the community hall (85 metres away) and Ron Stark Oval including public amenities (300 metres away).

The proposed use involves a part of the lot that doesn't appear to contain heritage structures, however a special lease condition is proposed to ensure any discoveries of artefacts are reported.

The alternative use of the subject land is for it to remain as open space.

5.2 Is the Proposed or Existing Secondary Use consistent with the Primary Use of the Trust Land?

Yes. The proposed secondary use being a nature play and cultural community garden is consistent with the 'Park' purpose of the reserve. It is also noted that the proposed secondary use occupies a small section of the reserve and the majority of the park remains unaffected.

LAND MANAGEMENT PLAN – RESERVE FOR PARK PURPOSES REF 49006067

6. COMMUNITY CONSULTATION

6.1 General Community Consultation

Community consultation generated 88 submissions, providing good insight into community views. Overall the proposal is highly supported with 90.9% in favour.

The supportive submitters indicated that the proposal would create space for nature play learning and provide educational benefits for children as well as broader community involvement.

The 8 objections involved neighbour concerns mainly around losing rear access to the park. It is noted that Council should not be actively accommodating rear access from private property to park land. Access to the park for these neighbours will remain available a short distance (100m) down the street. The design of the nature play / cultural garden intends to include a hedge buffer between the land and adjoining neighbours.

6.2 Quandamooka Yoolooburrabee Aboriginal Cooporation

QYAC have been consulted on the project with a response received 6 May 2020 advising:

- Approval to proceed due to no known impact to Aboriginal Cultural Heritage.
- A QYAC Cultural Heritage Monitor will be required on site during all excavation or other activities that are likely to cause ground disturbance.
- From a Native Title Perspective, there is no current issue with the community garden, however if the tenure is to change in the future, QYAC will need to be consulted in accordance with the Redland City Council Indigenous Land Use Agreement.

6.3 Minjerribah Moorgumpin Elders in Council Aboriginal Corporation

The Elders were consulted on the project on 11 June 2020. No feedback or concerns were raised.

6.4 North Stradbroke Island Museum on Minjerribah

The museum has visited the site and the two adjoining State Heritage Registered Sites, the Dunwich Public Hall and the Public Reserve. The museum advised that the proposed use is consistent with the heritage values and would preserve them. The museum support the SELC securing tenure and the proposed nature play and cultural garden.

7. SUMMARY AND RECOMMENDATION

The proposal is a consistent trustee leasing of the trust land that does not diminish use of the reserve for its dedicated purpose (Park).

The nature play and cultural community garden use will enhance the primary use of the trust land by the community. The proposal matches the natural attributes of the subject land, being an already segmented section of land adjoining the existing childcare centre.

From a local area perspective, the proposal is centrally located with other community uses and the proposed community use is consistent with no future plans for the subject site.

LAND MANAGEMENT PLAN - RESERVE FOR PARK PURPOSES REF 49006067

A Trustee Lease is recommended.

Lease conditions will require a hedge buffer between the land and adjoining neighbours to minimise impacts.

There is no current Native Title issue with the community garden, however if the tenure is to change in the future, QYAC will need to be further consulted in accordance with the Redland City Council Indigenous Land Use Agreement.



LAND MANAGEMENT PLAN – RESERVE FOR PARK PURPOSES REF 49006067

8. APPENDICES

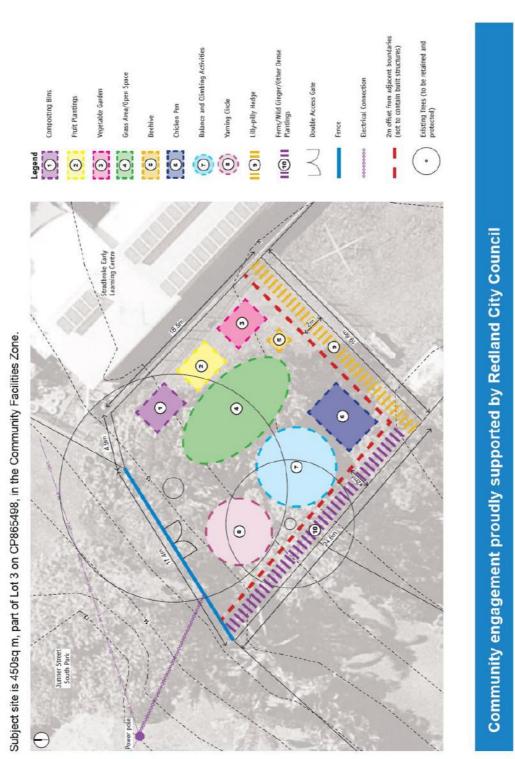
Appendix A - Locality Map



LAND MANAGEMENT PLAN – RESERVE FOR PARK PURPOSES REF 49006067

Appendix B - Aerial photo showing location of Trust Land and surrounding parcels





LAND MANAGEMENT PLAN - RESERVE FOR PARK PURPOSES REF 49006067

Appendix C – Concept Plan of Proposal

GENERAL MEETING MINUTES

Attachment 3

Stradbroke Early Learning Association Incorporated - Part of Lot 3 CP865498

Proposed trustee lease area outlined in red



15.3 LEASE RENEWAL - NORTH STRADBROKE ISLAND RUGBY LEAGUE AND ALL SPORTS CLUB INCORPORATED

| Objective Reference: | A5436538 | | |
|-----------------------------|---|--|--|
| Authorising Officer: | Dr Nicole Davis, General Manager Infrastructure & Operations | | |
| Responsible Officer: | Sherry Clarke, Group Manager City Operations | | |
| Report Author: | Kate Mullens, Principal Adviser City Sports and Venues Katie Woodrow, Acting Senior Leasing Officer | | |
| Attachments: | 1. North Stradbroke Island Rugby League and All Sports Club - Registered Survey Plan $\underline{\mathbb{J}}$ | | |

PURPOSE

To approve a lease renewal for North Stradbroke Island Rugby League and All Sports Club Incorporated (Straddie Sharks) on Ballow Road, Dunwich, North Stradbroke Island.

BACKGROUND

Straddie Sharks was granted an original trustee lease in 1991 as North Stradbroke Island All Sports and Social Association Inc. before amalgamating in 1994 with the North Stradbroke Island Rugby League Football Club Inc. The amalgamated club became North Stradbroke Island Rugby League and All Sports Club Incorporated.

Straddie Sharks lease area covers the club house building, oval and tennis courts on Lot 1 CP D90414 and part Lot 12 CP 865858, described as 2 Ballow Road, Dunwich, North Stradbroke Island. The land tenure is State Reserve with Council as Trustee.

A review of Council's overall community leasing process, has identified 10 year renewal periods ensure a standardised and consistent approach across all community leased groups. The review of community leasing, currently in progress, will include a benchmarking process with other agencies. The 10 year lease period will permit more oversight by Council of the strategic use of its assets. This aligns with the Community Leasing Policy and Council's vision for enriching community lifestyle by preserving community assets and achieving maximum benefit from its community property portfolio.

ISSUES

Straddie Sharks has a gaming licence with 14 gaming machines, and all revenue is reinvested into the club. In recent years, approximately \$300k has been reinvested into the premises for extensions and renovations. Council has also invested approximately \$30k during financial year 2019/2020 for the installation of a fire hydrant to assist the Straddie Sharks improve building compliance considerations.

In January 2019, Redlands RSL and Stradbroke RSL expressed an interest in entering into discussions regarding a merger agreement. However, a new Straddie Sharks committee rejected the RSL proposal, and a lease renewal was requested from Council.

The lease area is located within the extent of the Gumpi (Dunwich) Draft Master Plan. Council's Strategic Planning Unit advised the draft plan is yet to be finalised, however, materials made

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available to Council do not propose any changes to the subject site. It was noted there is no requirement to delay a Council decision on this proposal pending completion of the Master Plan. **STRATEGIC IMPLICATIONS**

Legislative Requirements

The *Local Government Regulation 2012* (the Regulation) s.236(1)(b)(ii) requires that Council agree by resolution that it is appropriate to dispose of an interest in land to a community organisation, other than by tender or auction.

Straddie Sharks meets the Regulation's definition of a community organisation, as it is an entity that carries on activities for a public purpose and whose primary object is not directed at making a profit.

Council is permitted to resolve to issue Straddie Sharks a lease under s.236(1)(b)(ii).

Risk Management

All new leases require the lessee to maintain full building and public liability insurance.

Council's Facility Services Unit conducts regular inspections to ensure legislative compliance regarding occupant safety and building condition.

A building inspection was completed in February 2018 and a Final Certificate was issued in December 2020 confirming the building is now compliant.

Financial

The lessee holds a Community Club Licence and bears the costs associated with Rates and Utility. In addition, the lessee bears all costs associated with the preparation and registration of this lease as well as maintain full building and public liability insurance. Maintenance of the premises is a shared responsibility between Council and the Lessee in accordance with the relevant terms in the tenure. A sustainability check conducted in May 2021 confirmed Straddie Sharks is financially sound.

People

The only staffing implications for Council are related to the periodic building inspections and liaison with Straddie Sharks, if required.

Environmental

This recommendation does not have any environmental implications.

Social

Straddie Sharks has catered for the youth and general community of North Stradbroke Island for the past 35 years. Continuity of tenure provides a physical and social benefit, endorses the strategic priority and gives the residents the foundation for a strong future.

Human Rights

There are no impacts to Human Rights as a result of this report.

Alignment with Council's Policy and Plans

CDV-001-P Community Leasing Policy supports leases to not-for-profit community organisations.



Our Future Redlands – A Corporate Plan to 2026 and Beyond is supported by this proposal, particularly:

GOAL 2. Strong Communities

2.1 Enhance the health, safety and wellbeing of our community through the delivery of inclusive and responsive services focused on preserving and improving our naturally wonderful lifestyle by leveraging partnerships, networks, facilities and infrastructure.

2.4 Enhance community inclusion where people of all locations, ages, abilities and cultures can participate and have access to the necessary services and facilities.

And

GOAL 5. Liveable Neighbourhoods

5.1 Enhance the unique character and liveability of our city for its communities through coordinated planning, place making, and management of community assets.

CONSULTATION

| Consulted | Consultation Date | Comments/Actions |
|---|-------------------|---|
| Straddie Sharks | 1 July 2021 | Meeting with club to discuss renewal. |
| | 8 December 2020 | Advised club of support of renewal for 10 year term. |
| Division 2 Councillor | 2 June 2021 | Notified of impending report for lease renewal. |
| Acting Service Manager, Facility Services Unit | 18 May 2021 | Supportive of new 10 year lease. |
| Service Manager, Civic and | 18 May 2021 | Supportive of new 10 year lease. |
| Open Spaces Asset | | |
| Management Unit | | |
| Service Manager, | 18 May 2021 | Supportive of new 10 year lease. |
| Strengthening Communities | | |
| Service Manager, Parks and | 18 May 2021 | Supportive of new 10 year lease. |
| Conservation | | |
| Principal Advisor, Strategic | 18 May 2021 | Consulted regarding current status of Goompi Master Plan. |
| Planning Unit | | |
| Service Manager, City Sport | 8 December 2020 | Meeting with club to discuss renewal. |
| and Venues | | |

OPTIONS

Option One

That Council resolves as follows:

- 1. To approve and discharge, as Trustee, a new lease to North Stradbroke Island Rugby League and All Sports Club Inc. over part Lot 12 CP 865858 and Lot 1 CP D90414 situated at 2 Ballow Road, Dunwich, as shown on the attached site plan, with a lease term to 2 June 2031.
- 2. To agree in accordance with s.236(2) of the *Local Government Regulation 2012* that s.236(1)(b)(ii) of the *Local Government Regulation 2012* applies allowing the proposed lease to a community organisation, other than by tender or auction.
- 3. To authorise the Chief Executive Officer to execute all documents in regard to this matter.



Option Two

That Council does not approve a new lease to North Stradbroke Island Rugby League and All Sports Club Inc. and investigates alternative arrangements.

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/278

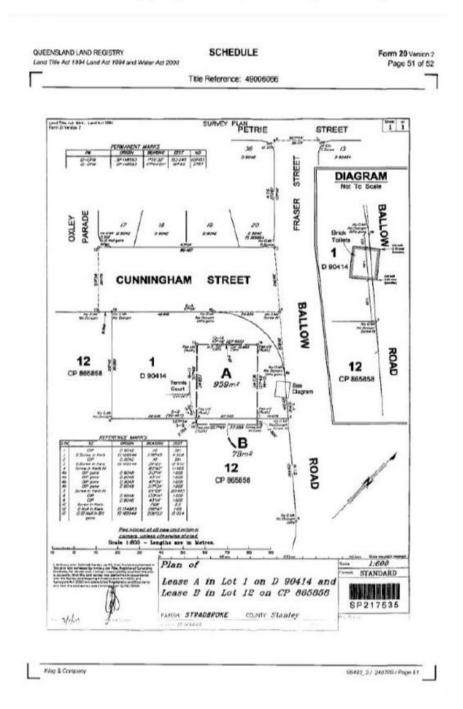
| Moved by: | Cr Peter Mitchell |
|--------------|-------------------|
| Seconded by: | Cr Wendy Boglary |

That Council resolves as follows:

- 1. To approve and discharge, as Trustee, a new lease to North Stradbroke Island Rugby League and All Sports Club Inc. over part Lot 12 CP 865858 and Lot 1 CP D90414 situated at 2 Ballow Road, Dunwich, as shown on the attached site plan, with a lease term to 2 June 2031.
- 2. To agree in accordance with s.236(2) of the Local Government Regulation 2012 that s.236(1)(b)(ii) of the Local Government Regulation 2012 applies allowing the proposed lease to a community organisation, other than by tender or auction.
- 3. To authorise the Chief Executive Officer to execute all documents in regard to this matter.

CARRIED 11/0





North Stradbroke Island Rugby League and All Sports Club - Registered Survey Plan



15.4 LEASE RENEWAL - REDLANDS MODERN COUNTRY MUSIC INC.

| Objective Reference: | A5531791 | |
|-----------------------------|--|--|
| Authorising Officer: | Dr Nicole Davis, General Manager Infrastructure & Operations | |
| Responsible Officer: | Sherry Clarke, Group Manager City Operations | |
| Report Author: | Kate Mullens, Principal Adviser City Sports and Venues Katie Woodrow, Acting Senior Leasing Officer | |
| Attachments: | 1. Redlands Modern Country Music Inc - Site Plan J | |

PURPOSE

To approve a lease renewal for Redlands Modern Country Music Inc. on Redland Bay Road, Thornlands.

BACKGROUND

Redlands Modern Country Music Inc. (RMCMI) has operated in Redland City since the mid-1970s. Located within a building in the Pinklands Sporting Complex. RMCMI is located on part of Lot 2 SL 5716, described as 190-262 Redland Bay Road Thornlands. It has previously held a 30 year lease over this site between 1 October 1991 and 30 September 2021. An interim Licence to Occupy has been initiated until the lease renewal outcome is determined.

RMCMI engages in regular activities involving both members and non-members. Its annual Country Music Festival attracts contestants and visitors from all over South-East Queensland for a three-day weekend event. This aligns with the Community Leasing Policy and Council's vision for enriching community lifestyle by preserving community assets and achieving maximum benefit from its community property portfolio.

ISSUES

A review of Council's overall community leasing process, has identified 10 year renewal periods ensure a standardised and consistent approach across all community leased groups. The review of community leasing, currently in progress, will include a benchmarking process with other agencies. The 10 year lease period will permit more oversight by Council of the strategic use of its assets. This aligns with the Community Leasing Policy and Council's vision for enriching community lifestyle by preserving community assets and achieving maximum benefit from its community property portfolio.

RMCMI had requested a renewal term of 30 years, however have accepted a 10 year lease in line with Councils approach. RMCMI has requested an increase to their current lease footprint (current footprint outlined in yellow on Attachment 1) to accommodate extensions that have occurred outside the lease arrangement and proposed concepts for future development (outlined in green). The organisation is aware they are required to submit a project proposal for any future plans for Council approval in accordance with the land owners consent process. Council officers support the expansion of the lease area.

STRATEGIC IMPLICATIONS

Legislative Requirements

The *Local Government Regulation 2012* (the Regulation) s.236(1)(b)(ii) requires that Council agree by resolution that it is appropriate to dispose of an interest in land to a community organisation other than by tender or auction.

RMCMI meets the Regulation definition of a community organisation, as it is an entity that carries on activities for a public purpose and whose primary object is not directed at making a profit.

Council is permitted to resolve to issue RMCMI a lease under s.236(1)(b)(ii).

Risk Management

RMCMI is required to maintain full building and public liability insurance.

Council's Facilities Services Unit conducts regular inspections to ensure legislative compliance regarding occupant safety and building condition.

A building inspection was completed on 19 March 2021 confirming RMCMI is compliant.

Financial

The lessee bears all costs associated with the preparation and registration of the lease. Maintenance of the premises is a shared responsibility between Council and the Lessee in accordance with the relevant terms in the tenure. The lessee also bears utility costs associated to water, sewage and electricity. A sustainability check conducted in March 2021 confirmed RMCMI is financially sound.

People

Nil

Environmental

There are no environmental implications related to the lease renewal or increase in lease footprint.

Social

RCCMI is approaching its 50th anniversary and is well established within the Redlands community. The current membership is 228, with the majority being local Redlands Coast residents, though a significant number of members are also from outside the Redlands.

Members voluntarily hold performances in Aged Care and other community sectors and regularly hold fundraising activities with proceeds going to Charity.

Human Rights

There are no impacts to Human Rights as a result of this report.

Alignment with Council's Policy and Plans

CDV-001-P Community Leasing Policy supports leases to not-for-profit community organisations.

Our Future Redlands – A Corporate Plan to 2026 and Beyond is supported by this proposal, particularly:



GOAL 2. Strong Communities

2.1 Enhance the health, safety and wellbeing of our community through the delivery of inclusive and responsive services focused on preserving and improving our naturally wonderful lifestyle by leveraging partnerships, networks, facilities and infrastructure.

2.4 Enhance community inclusion where people of all locations, ages, abilities and cultures can participate and have access to the necessary services and facilities.

GOAL 5. Liveable Neighbourhoods

5.1 Enhance the unique character and liveability of our city for its communities through coordinated planning, place making, and management of community assets.

CONSULTATION

| Consulted | Consultation Date | Comments/Actions |
|--|--------------------------------|---|
| Redlands Modern Country Music Inc | 12 January 2021 9 July 2021 | Request to organisation for confirmation of lease renewal Email to advise of impending report to Council for 10 year term |
| Sport & Recreation Officer | 11 February 2021 | Met with organisation to discuss lease renewal and expansion |
| Senior Advisor Asset Management | 18 February 2021 | Support for maximum 3 year lease renewal |
| Senior Advisor, Environmental Planning & Policy | 18 February 2021 | Supportive of increase in lease footprint |
| Facilities Coordinator | 19 March 2021 | Compliance inspection |
| Division 3 Councillor | 9 July 2021 | Email to advise of report to Council for 10 year lease renewal |

OPTIONS

Option One

That Council resolves as follows:

- 1. To approve and discharge a new lease to Redlands Modern Country Music Inc., with an expansion of the footprint, on part Lot 2 SL 5716, at 190-262 Redland Bay Road Thornlands, as shown on the attached site plan outlined in green, with a lease term of 10 years.
- 2. To agree in accordance with s.236(2) of the *Local Government Regulation 2012* that s.236(1)(b)(ii) of the *Local Government Regulation 2012* applies allowing the proposed lease to a community organisation, other than by tender or auction.
- 3. To authorise the Chief Executive Officer to execute all documents in regard to this matter.

Option Two

That Council does not approve a new lease to Redland Modern Country Music Inc. and investigates alternative arrangements.



OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/279

Moved by: Cr Paul Gollè Seconded by: Cr Tracey Huges

That Council resolves as follows:

- 1. To approve and discharge a new lease to Redlands Modern Country Music Inc., with an expansion of the footprint, on part Lot 2 SL 5716, at 190-262 Redland Bay Road Thornlands, as shown on the attached site plan outlined in green, with a lease term of 10 years.
- 2. To agree in accordance with s.236(2) of the Local Government Regulation 2012 that s.236(1)(b)(ii) of the Local Government Regulation 2012 applies allowing the proposed lease to a community organisation, other than by tender or auction.
- 3. To authorise the Chief Executive Officer to execute all documents in regard to this matter.

CARRIED 11/0



Redlands Modern Country Music Inc – Site Plan

Yellow area - current lease area.

Green area - existing extensions and proposed future development to lease area.





16 NOTICES OF INTENTION TO REPEAL OR AMEND A RESOLUTION

Nil

17 NOTICES OF MOTION

Nil



18 URGENT BUSINESS WITHOUT NOTICE

19 CONFIDENTIAL ITEMS

MOTION TO MOVE INTO CLOSED SESSION AT 12:43PM

COUNCIL RESOLUTION 2021/280

Moved by: Cr Julie Talty Seconded by: Cr Tracey Huges

That Council considers the confidential report(s) listed below in a meeting closed to the public in accordance with Section 254J of the *Local Government Regulation 2012*.

CARRIED 10/1

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Paul Gollè voted AGAINST the motion.

19.1 Voluntary Transfer of Land Concession

This matter is considered to be confidential under Section 254J(3)(d) of the Local Government Regulation 2012, and the Council is satisfied that discussion of this matter in an open meeting would, on balance, be contrary to the public interest as it deals with rating concessions.

Overview

To consider the voluntary transfer of unencumbered land in full or part payment of rates and charges identified in the attached schedule, VOL November 2021, for overdue rates and charges.

19.2 Tenure of Wastewater Irrigation Area Dunwich

This matter is considered to be confidential under Section 254J(3)(g) of the Local Government Regulation 2012, and the Council is satisfied that discussion of this matter in an open meeting would, on balance, be contrary to the public interest as it deals with negotiations relating to a commercial matter involving the local government for which a public discussion would be likely to prejudice the interests of the local government.

Overview

To seek endorsement to resolve the tenure of land used by Council for the purpose of waste water irrigation, including acquiring trusteeship of land currently in the trusteeship of Seqwater.

19.3 Encroachment Issue - Birkdale

This matter is considered to be confidential under Section 254J(3)(e) of the Local Government Regulation 2012, and the Council is satisfied that discussion of this matter in an open meeting would, on balance, be contrary to the public interest as it deals with legal advice obtained by the local government or legal proceedings involving the local government including, for example, legal proceedings that may be taken by or against the local government.

Overview

Council will rectify an historical boundary encroachment through the transfer of the affected land to the adjoining owner; thereby ensuring operational security of Council assets and protection of private owner's tenure and property.



19.4 2021/22 Sponsorship Round 1 Report - Applications over \$15,000

This matter is considered to be confidential under Section 254J(3)(g) of the Local Government Regulation 2012, and the Council is satisfied that discussion of this matter in an open meeting would, on balance, be contrary to the public interest as it deals with negotiations relating to a commercial matter involving the local government for which a public discussion would be likely to prejudice the interests of the local government.

Overview

To seek endorsement by Council to approve Sponsorship Panels funding recommendation for Sponsorship Applications requesting over \$15,000.

MOTION TO MOVE INTO OPEN SESSION AT 12:51PM

COUNCIL RESOLUTION 2021/281

Moved by: Cr Peter Mitchell Seconded by: Cr Julie Talty

That Council moves out of Closed Council into Open Council.

CARRIED 10/0

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Lance Hewlett, Mark Edwards, Julie Talty, Rowanne McKenzie, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Tracey Huges was not present when the motion was put.



19.1 VOLUNTARY TRANSFER OF LAND CONCESSION

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/282

Moved by:Cr Peter MitchellSeconded by:Cr Julie Talty

That Council resolves as follows:

- 1. To grant a concession to the stated ratepayers detailed in the attached schedule, VOL November 2021, to accept the transfer of unencumbered land in full payment of the rates and charges, as pursuant to Section 121(c) of the Local Government Regulation 2012.
- 2. To note the due date for payment of the rates and charges is detailed in the attached schedule, VOL November 2021.
- 3. That this report and attachment remains confidential until such time as required by any legal or statutory obligation, subject to maintaining the confidentiality of legal privileged, private and commercial in confidence information.

CARRIED 11/0



19.2 TENURE OF WASTEWATER IRRIGATION AREA DUNWICH

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/283

Moved by: Cr Peter Mitchell Seconded by: Cr Adelia Berridge

That Council resolves as follows:

- 1. To seek tenure of the land area, as identified and generally in accordance with Attachment 3, with Council as Trustee.
- 2. To enter into, if necessary, an interim Trustee Lease for an appropriate land area, as identified and generally in accordance with Attachment 3, to be negotiated with Seqwater.
- 3. To enter into, if necessary, an interim sub-lease arrangement of part of the land area, to be negotiated with the Quandamooka Yoolooburrabee Aboriginal Corporation, in accordance with the application of the exception to dispose of land or an interest in land, other than by tender or auction, under s.236(1)(c)(iii) of the Local Government Regulation 2012, for renewing leases.
- 4. To maintain the report and attachments as confidential in accordance with any legal and statutory obligation, subject to maintaining confidentiality of legally privileged, private and commercial in confidence information until such time as all land actions have been finalised.

CARRIED 11/0



19.3 ENCROACHMENT ISSUE - BIRKDALE

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/284

Moved by: Cr Julie Talty Seconded by: Cr Paul Bishop

That Council resolves as follows:

- 1. That the exception to dispose of land under subparagraph 236(1)(c)(iv) of the Local Government Regulation 2012, without tender or auction to the owners of the adjoining land applies.
- 2. To dispose of the land to the adjoining owner at fair market value and to hold any revenue from sale of the land for future strategic land acquisitions.
- 3. To maintain the contents and attachments of this report as confidential until the approval and registration of the survey plan, subject to maintaining the confidentiality of legally privileged, private and commercial in confidence information.

CARRIED 11/0



Councillor Lance Hewlett declared a Declarable Conflict of Interest in the following Item (Item 6.14 refers).

Cr Lance Hewlett left the meeting at 12:47pm and returned at 12:49pm (after the discussion of the Item in closed session).

Councillor Hewlett left the meeting at 12:55pm (before the Item was voted on), and returned at 12:55pm (after the Item was voted on).

19.4 2021/22 SPONSORSHIP ROUND 1 REPORT - APPLICATIONS OVER \$15,000

OFFICER'S RECOMMENDATION/COUNCIL RESOLUTION 2021/285

Moved by: Cr Adelia Berridge Seconded by: Cr Wendy Boglary

That Council resolves as follows:

- 1. To accept the Sponsorship Assessment Panel's recommendation on applications S22-R1-013, S22-R1-014 and S22-R1-016.
- 2. That this report remains confidential until all applicants have been advised of the outcome, subject to maintaining the confidentiality of legally privileged, private and commercial in confidence information.

CARRIED 9/1

Crs Karen Williams, Wendy Boglary, Peter Mitchell, Paul Gollè, Mark Edwards, Rowanne McKenzie, Tracey Huges, Adelia Berridge and Paul Bishop voted FOR the motion.

Cr Julie Talty voted AGAINST the motion.

Cr Lance Hewlett was absent from the meeting.



20 MEETING CLOSURE

The Meeting closed at 12:56pm.

The minutes of this meeting were confirmed at the General Meeting held on 15 December 2021.

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CHAIRPERSON

